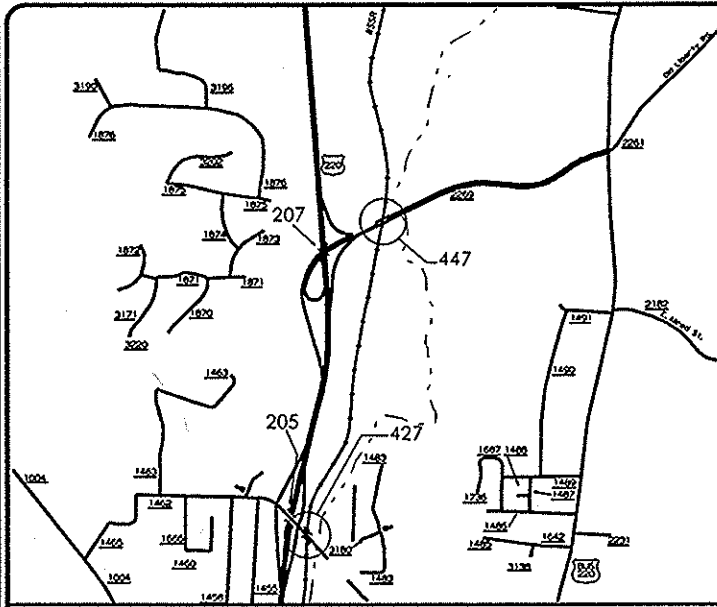
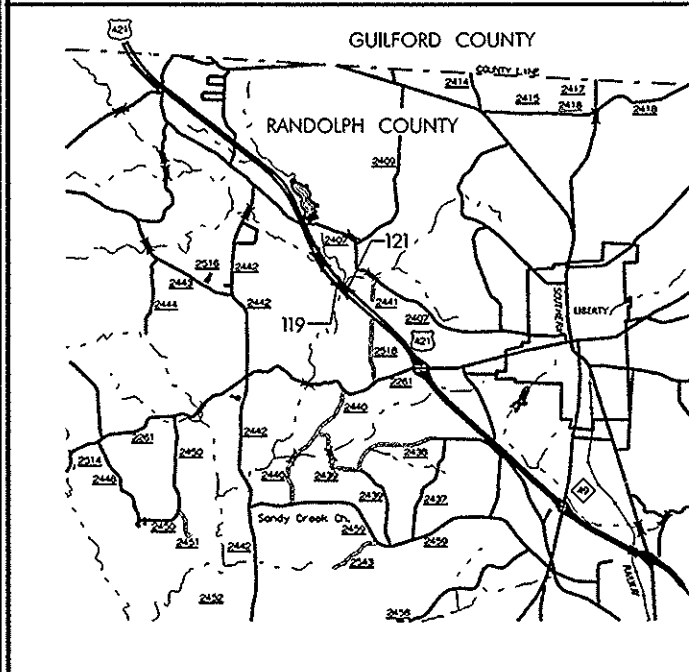


PROJECT: BP-5300G

DO00102



VICINITY MAP



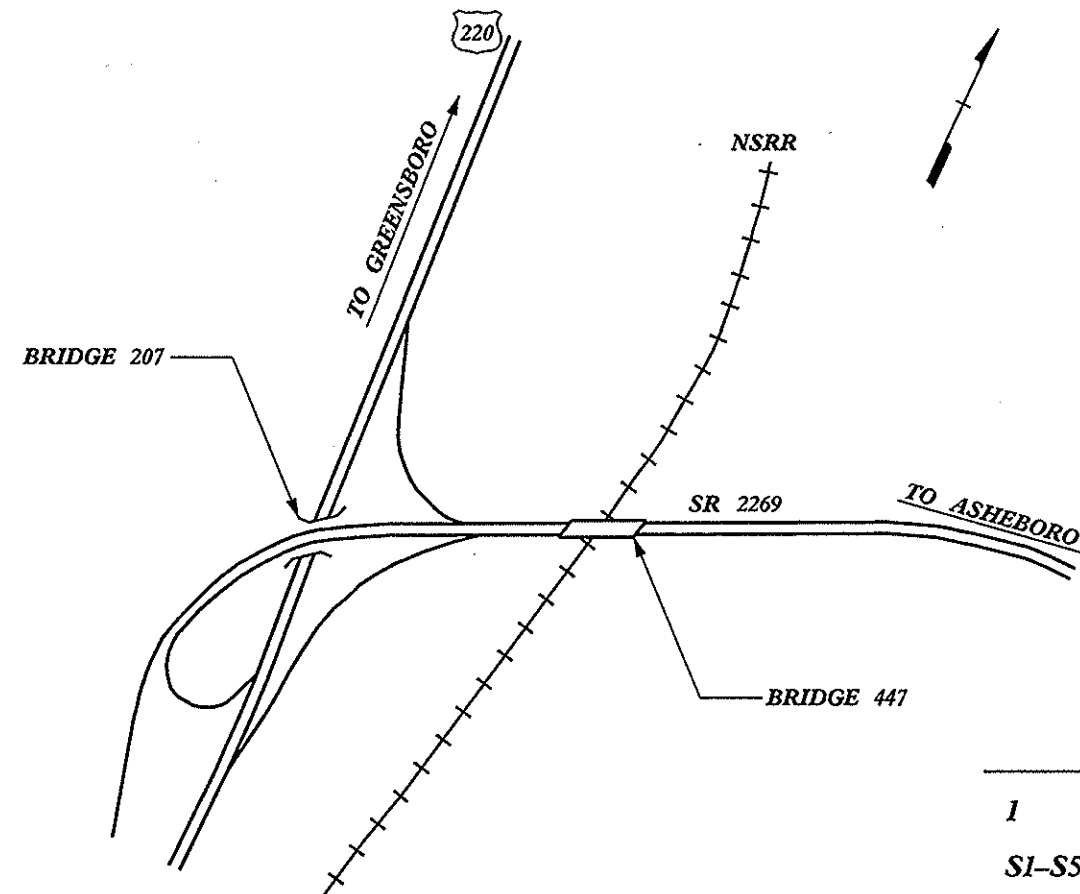
VICINITY MAP

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

RANDOLPH COUNTY

LOCATION: BRIDGE NO. 447 ON SR 2269 OVER NORFOLK SOUTHERN RAILWAY, NO. 427 ON SR 1462 OVER NORFOLK SOUTHERN RAILWAY, NO. 119 ON US 421 EBL OVER SANDY CREEK AND NO. 121 ON US 421 WBL OVER SANDY CREEK

TYPE OF WORK: BRIDGE PRESERVATION: STRUCTURAL STEEL REPAIRS AND CLEANING PAINTING OF STRUCTURAL STEEL



INDEX OF SHEETS

1	TITLE SHEET
SI-S5	STRUCTURES
TMP-1 - TMP-4A	TRAFFIC MANAGEMENT PLANS

STATE	STATE PROJECT REFERENCE NO.	SHEET NO.	TOTAL SHEETS
N.C.	BP-5300G		
STATE PROJ. NO.	F.A. PROJ. NO.	DESCRIPTION	
45438.1.1	BRNHS-0421(69)	P.E.	
45438.3.7	BRNHS-0421(69)	CONST.	



2012 STANDARD SPECIFICATIONS

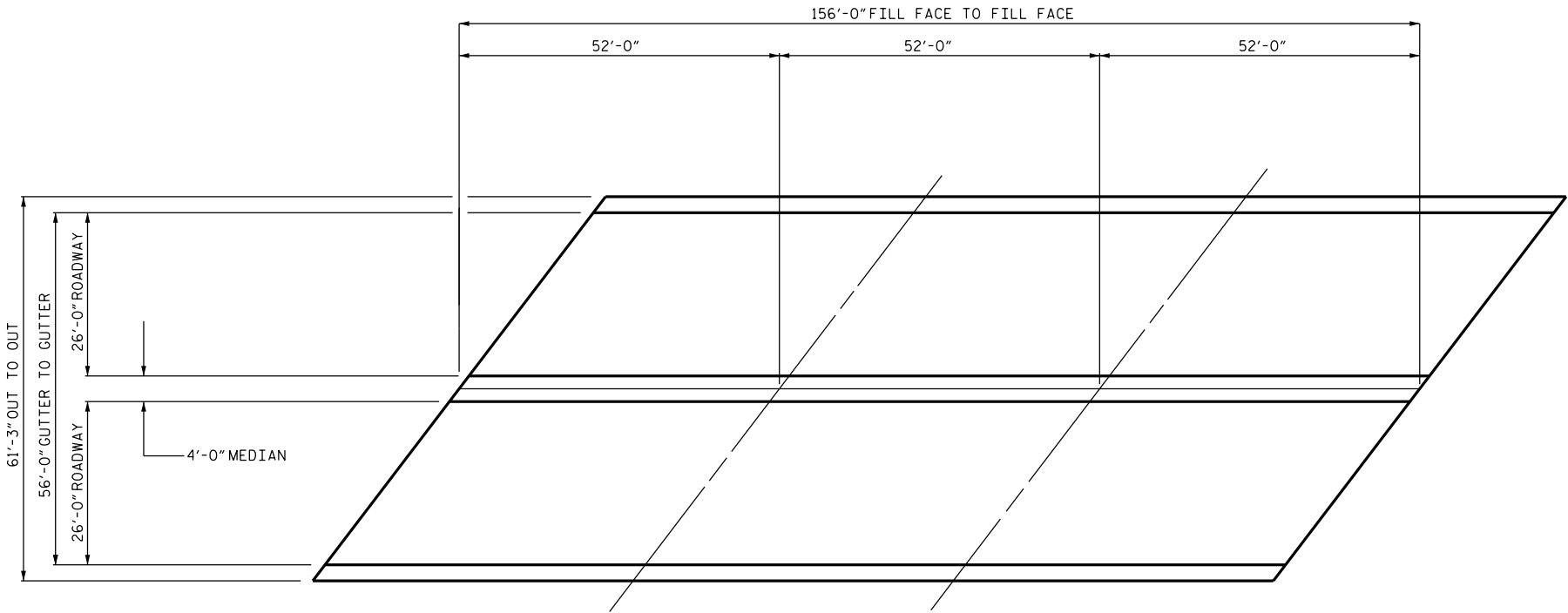
LETTING DATE:
FEBRUARY 29, 2012

Prepared In the Office of:
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
STRUCTURES MANAGEMENT UNIT - PRESERVATION & REPAIR GROUP
1000 BIRCH RIDGE DR. RALEIGH, N.C. 27610

RICK NELSON, P.E.
PROJECT ENGINEER

ENGINEER

T.J. BEACH, P.E.
PROJECT DESIGN ENGINEER



GENERAL NOTES

INASMUCH AS THE PAINT SYSTEM ON THE EXISTING STRUCTURAL STEEL CONTAINS LEAD, THE CONTRACTOR'S ATTENTION IS DIRECTED TO ARTICLE 107-1 OF THE STANDARD SPECIFICATIONS. ANY COST RESULTING FROM COMPLIANCE WITH APPLICABLE STATE OR FEDERAL REGULATIONS PERTAINING TO HANDLING OF MATERIALS CONTAINING LEAD BASED PAINT SHALL BE INCLUDED IN THE LUMP SUM BID PRICE FOR THIS PROJECT.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO FOLLOW ALL OSHA AND STATE SAFETY REQUIREMENTS.

DIMENSIONS IN THESE PLANS ARE BASED ON ORIGINAL PLANS, THE CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS PRIOR TO STARTING WORK.

EXISTING BRIDGE AND REPAIR DETAILS INDICATED ON THE PLANS ARE FROM THE BEST INFORMATION AVAILABLE. SINCE THIS INFORMATION IS SHOWN FOR THE CONVENIENCE OF THE CONTRACTOR, THE CONTRACTOR SHALL HAVE NO CLAIM AGAINST THE DEPARTMENT OF TRANSPORTATION FOR ANY DELAYS OR ADDITIONAL COST INCURRED BASED ON DIFFERENCES BETWEEN THE EXISTING BRIDGE AND REPAIR DETAILS SHOWN ON THE PLANS AND THE ACTUAL CONDITION ON THE SITE.

WORK ON THIS BRIDGE SHALL BE PREFORMED SO AS NOT TO ALLOW DEBRIS TO FALL BELOW. THE CONTRACTOR SHALL SUBMIT PLANS FOR CONSTRUCTION ACCESS IN ACCORDANCE WITH ARTICLE 402-2 OF THE STANDARD SPECIFICATIONS AND THE PROJECT SPECIAL PROVISIONS.

ALL VISUAL AND NON-DESTRUCTIVE TESTING WILL BE PERFORMED BY NCDOT.

FOR RAILROAD COORDINATION, SEE "SPECIAL PROVISIONS FOR PROTECTION OF RAILWAY INTEREST-NOLFOK SOUTHERN RAILROAD".

FOR CRANE SAFETY, SEE SPECIAL PROVISIONS.

TOTAL BILL OF MATERIAL								
	CLEANING & REPAINTING OF BRIDGE #119	CLEANING & REPAINTING OF BRIDGE #121	CLEANING & REPAINTING OF BRIDGE #427	CLEANING & REPAINTING OF BRIDGE #447	POLLUTION CONTROL	SPAN JACKING BRIDGE #447	STRUCTURAL STEEL FOR GIRDER REPAIR BRIDGE #447	BEARING REPLACEMENT
	LUMP SUM	LUMP SUM	LUMP SUM	LUMP SUM	LUMP SUM	LUMP SUM	LBS.	EA.
TOTAL	LUMP SUM	LUMP SUM	LUMP SUM	LUMP SUM	LUMP SUM	LUMP SUM	4,150	6

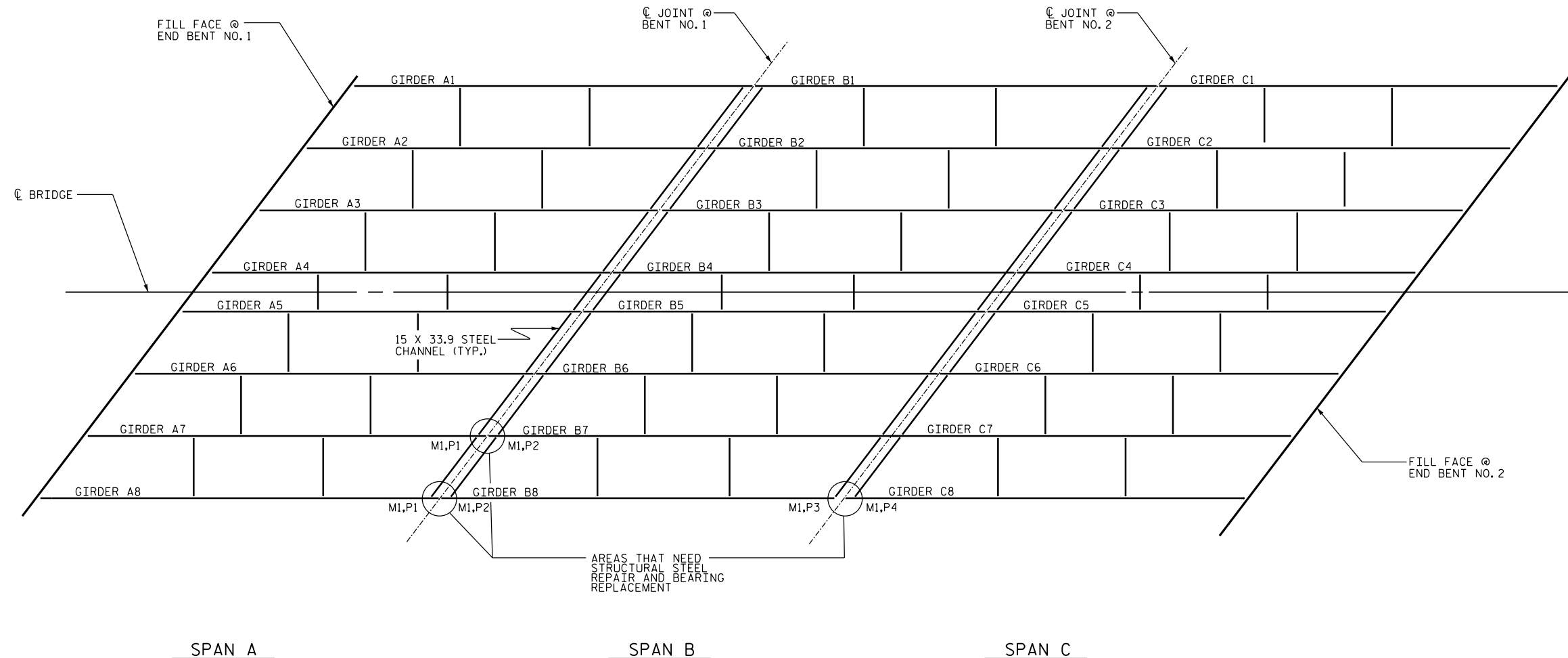
PROJECT NO. BP-5300G
RANDOLPH COUNTY
STATION: 447

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH
GIRDER REPAIR
TOTAL BILL OF MATERIAL AND GENERAL NOTES

DRAWN BY : T.J. BEACH DATE : 8-2011
CHECKED BY : D. MULLER DATE : 9-2011

09-FEB-2012 09:47
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REVISIONS						SHEET NO. S-1
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2			4			

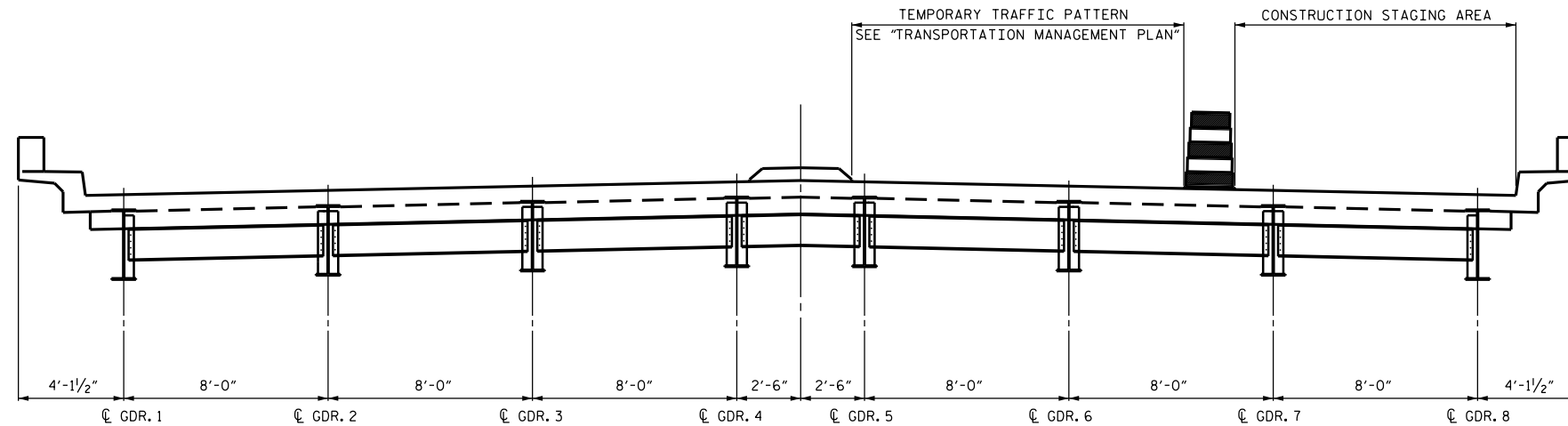


FRAMING PLAN
 (MASONRY P AND SOLE P DESIGNATIONS SHOWN
 ABOVE, SEE SHEET S-5 FOR DETAILS)

PROJECT NO. BP-5300G
RANDOLPH COUNTY
 BRIDGE NO. 447

STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION RALEIGH					
GIRDER REPAIR					
FRAMING PLAN					
REVISIONS					
NO.	BY:	DATE:	NO.	BY:	DATE:
1			3		
2			4		
SHEET NO. S-2					TOTAL SHEETS 5

DRAWN BY : T.J. BEACH DATE : 8-2011
 CHECKED BY : D. MULLER DATE : 9-2011



NOTE: JACKS SHALL HAVE A MINIMUM SAFE LOAD OF 125% OF THE SERVICE LOADS SPECIFIED.

SERVICE REACTIONS PER BEARING	
GIRDER NO. 7: DL + LL + IMPACT	= 89.3 KIPS
GIRDER NO. 8: DL + CONSTRUCTION	= 47.0 KIPS

JACKING NOTES:

THE CONTRACTOR SHALL SUBMIT JACKING PLANS AND CALCULATIONS FOR REVIEW AND APPROVAL PRIOR TO MATERIAL PURCHASE OR FABRICATION OF THE JACKING SYSTEM.

BEFORE THE JACKING OPERATION BEGINS, THE ONE RAIL METAL RAIL SHALL BE DISCONNECTED IN ORDER TO AVOID DAMAGE TO THE RAILING SYSTEM.

THE CONTRACTOR SHALL JACK ALL GIRDERS IN A SPAN ON AN INDIVIDUAL BENT SIMULTANEOUSLY BY MEANS OF A DUAL-FLOW PRESSURIZED PUMP CONTROLLING THE JACKS.

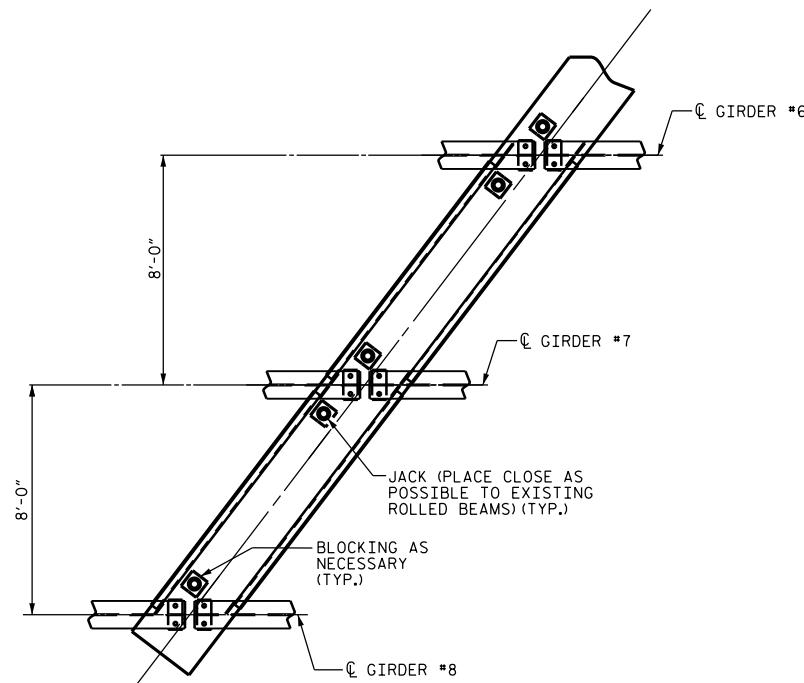
THE SPAN SHALL BE LIFTED ENOUGH THAT THE BEAMS CLEAR THE BEARINGS AND ALL LOAD IS SUPPORTED BY THE JACKS. AFTER JACKING IS COMPLETE THE CONTRACTOR SHALL PROVIDE A METHOD TO SUPPORT THE SPAN FOR DEAD AND LIVE LOADS AND REMOVE THE JACKS DURING BEAM REPAIR OR IF JACKS REMAIN IN PLACE DURING THE ENTIRE JACKING AND REPAIR OPERATION IT SHALL HAVE MECHANICAL LOCK OFF CAPABILITIES.

IF DURING THE JACKING PROCESS OR WHILE THE SPAN IS BEING SUPPORTED THE BEAMS SHIFT FROM THEIR ORIGINAL POSITION, ALL WORK WILL CEASE AND THE ENGINEER SHALL BE NOTIFIED IMMEDIATELY.

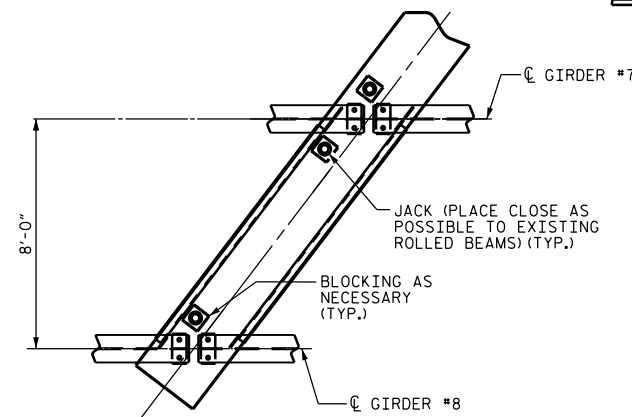
PRIOR TO JACKING, THE CONTRACTOR SHALL ENSURE THERE ARE NO OBSTACLES PREVENTING THE SPAN FROM BEING LIFTED.

ALL ADJACENT BEARINGS OF BEAMS NOT BEING JACKED MAY BE LOOSEN TO DECREASE THE RESISTANCE OF THE DECK SLAB DURING JACKING. ALL BEARINGS LOOSEN SHALL BE TIGHTEN BACK AFTER THE BEAMS ARE REPAIRED AND THE JACKS AND BLOCKING HAVE BEEN REMOVED.

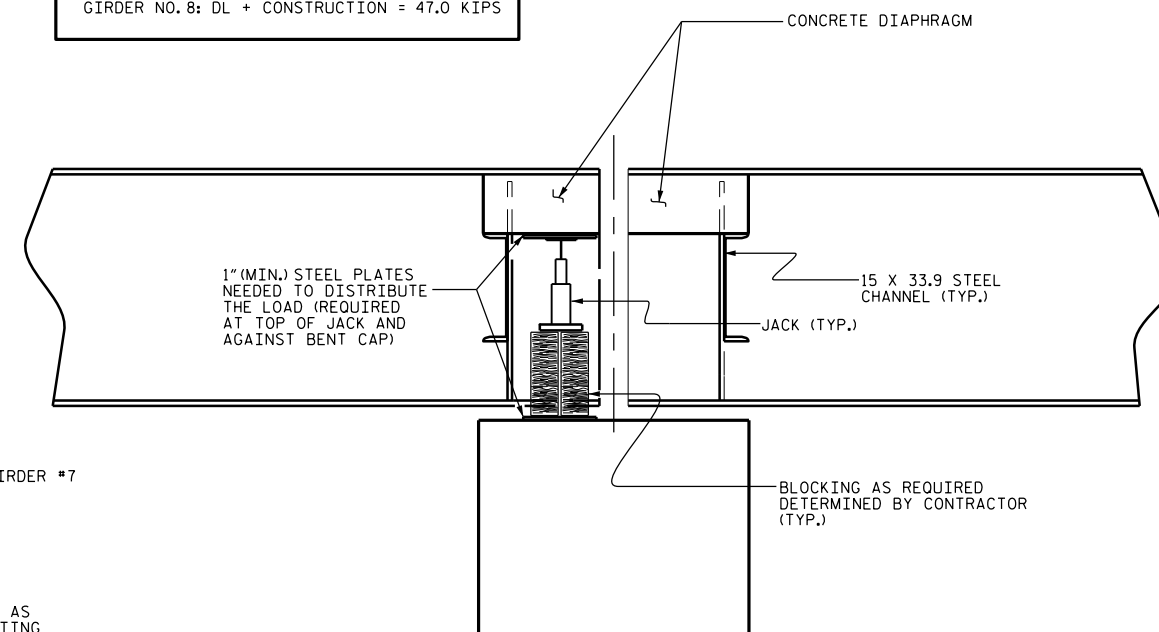
PAYMENT OF JACKING WILL BE MADE AT THE LUMP SUM PRICE BID FOR BRIDGE JACKING. SUCH LUMP SUM PRICE WILL BE FULL COMPENSATION FOR ALL MATERIALS, EQUIPMENT, TOOLS, LABOR, AND INCIDENTALS NECESSARY TO COMPLETE THE WORK.



PLAN VIEW - BENT NO. 1



PLAN VIEW - BENT NO. 2



SECTION THRU DIAPHRAGM

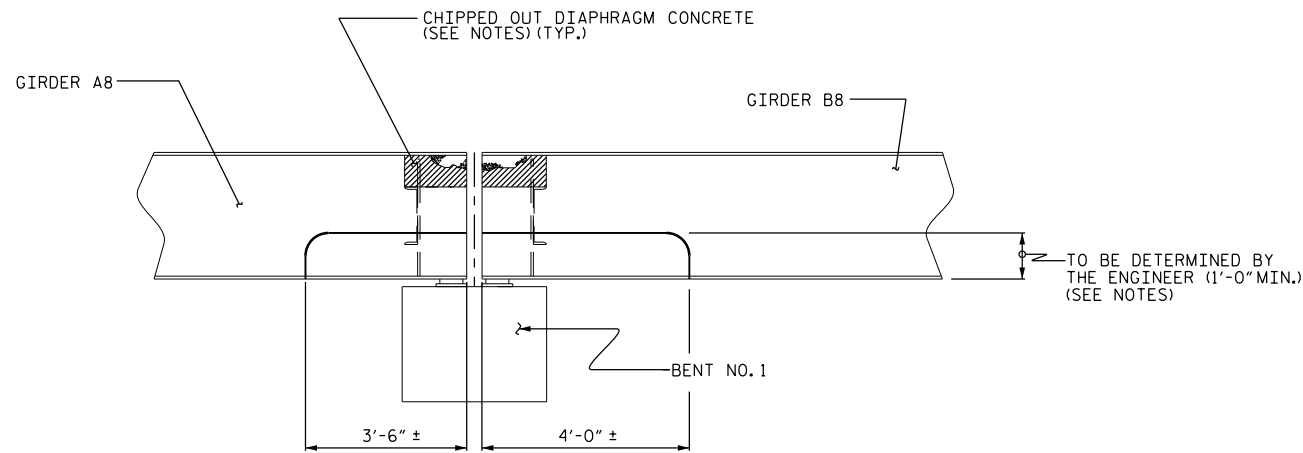
PROJECT NO. BP-5300G
RANDOLPH COUNTY
 BRIDGE NO. 447

DRAWN BY : T.J. BEACH DATE : 8-2011
 CHECKED BY : D. MULLER DATE : 9-2011

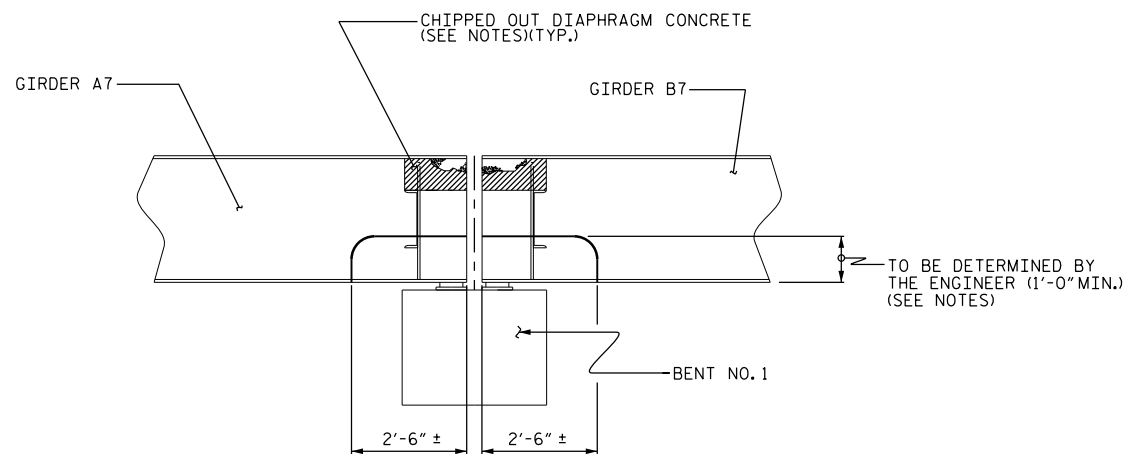
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REVISIONS						SHEET NO.
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2			4			TOTAL SHEETS
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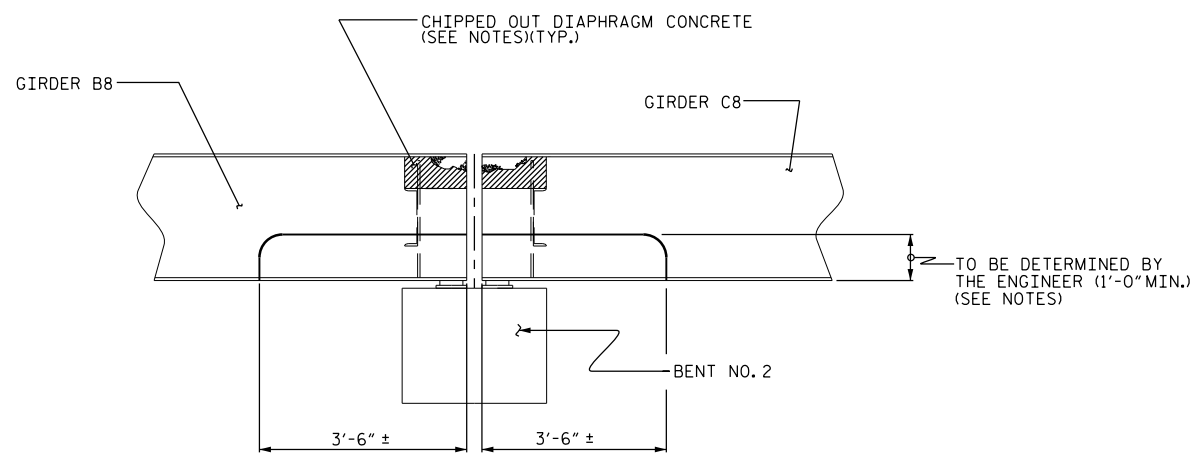
STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH
 GIRDER REPAIR
 JACKING DETAILS



EXTERIOR GIRDER A8 & B8 AT BENT NO. 1

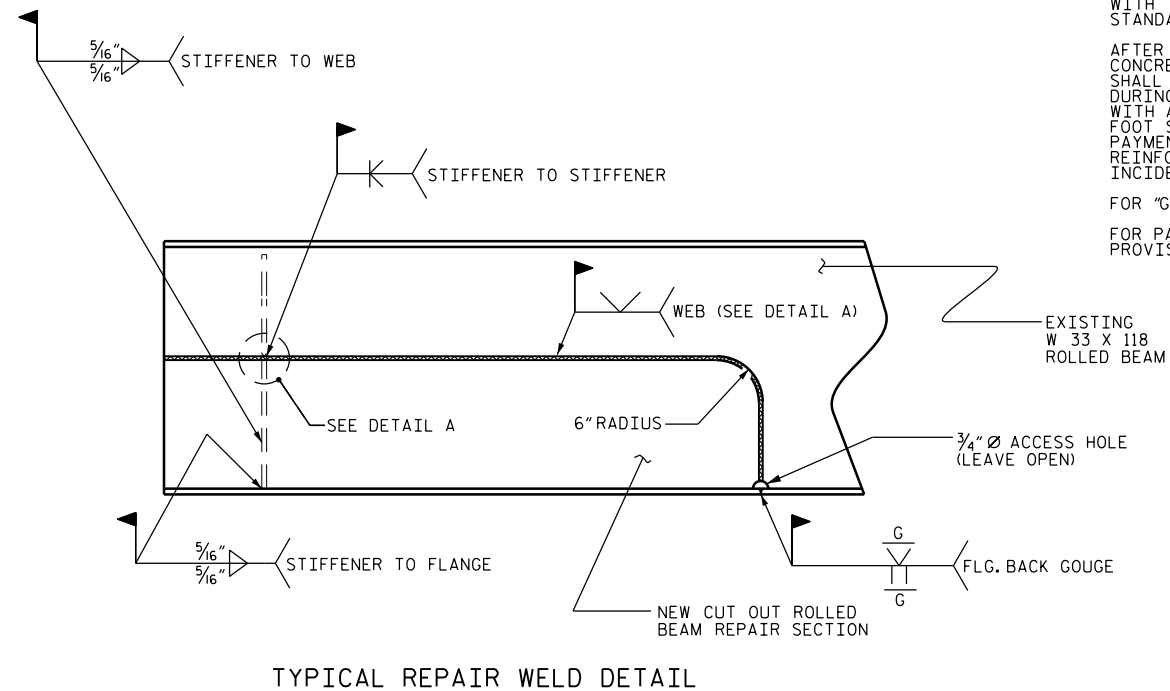


INTERIOR GIRDER A7 & B7 AT BENT NO. 1

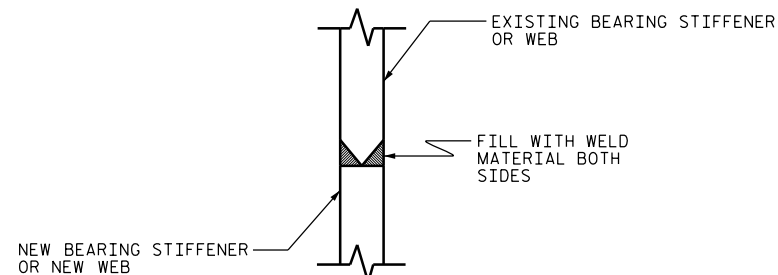


EXTERIOR GIRDER B8 & C8 AT BENT NO. 2

LIMITS OF BEAM REPAIRS



TYPICAL REPAIR WELD DETAIL



DETAIL A

REPAIR SEQUENCE:

GIRDER REPAIR DETAILS AND DIMENSIONS PROVIDED IN PLANS MAY BE MODIFIED BASED ON FIELD CONDITIONS BY THE ENGINEER.

CHIP AWAY CONCRETE FROM BENT DIAPHRAGMS AS NEEDED TO DETERMINE LIMITS OF REPAIR.

MECHANICALLY CLEAN RUST & SCALE & EXISTING PAINT TO AT LEAST 4" BEYOND REPAIR AREA LIMITS.

DETERMINE CUT-TO-FIT STEEL SECTION NEEDED TO REPAIR SECTION LOSS.

REPLACEMENT BEAM SECTION SHALL BE CUT FROM A W 33 X 118 ROLLED BEAM AND SHALL BE AASHTO M270 GRADE 50, OR APPROVED EQUIVALENT.

INSTALL CUT-TO-FIT SECTION, FULLY WELD ALONG TOP AND SIDES OF PLATE.

IN ACCORDANCE WITH THE SPECIAL PROVISIONS, AFTER REPAIR GRIND ALL WELDS FLUSH, THOROUGHLY CLEAN AREA TO REMOVE DEBRIS AND OILS FROM REPAIR PROCESS, CLEAN AND PAINT STRUCTURAL STEEL.

ALL WELDS WILL BE INSPECTED AND TESTED BY THE NCDOT MATERIALS AND TEST UNIT IN ACCORDANCE WITH THE CURRENT AWS BRIDGE WELDING CODE AND STANDARD SPECIFICATIONS.

AFTER GIRDERS ARE REPAIRED AND PAINTED THE CONCRETE REMOVED FROM THE BENT DIAPHRAGMS SHALL BE CAST BACK. ANY REINFORCING STEEL CUT DURING THE REMOVAL PROCESS SHALL BE SPLCED WITH A SIMILAR SIZE BAR WITH AT LEAST A ONE FOOT SPLICE TO THE EXISTING STEEL. NO SEPARATE PAYMENT SHALL BE MADE FOR CONCRETE AND REINFORCING STEEL AS THIS IS CONSIDERED INCIDENTAL TO THE PAY ITEM "GIRDER REPAIR".

FOR "GIRDER REPAIR", SEE SPECIAL PROVISIONS.

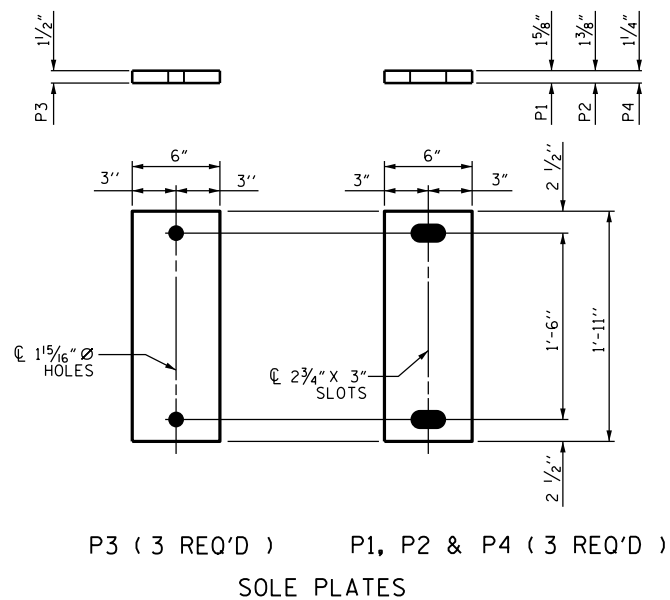
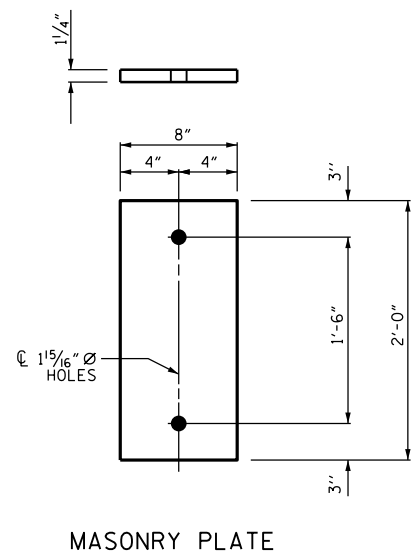
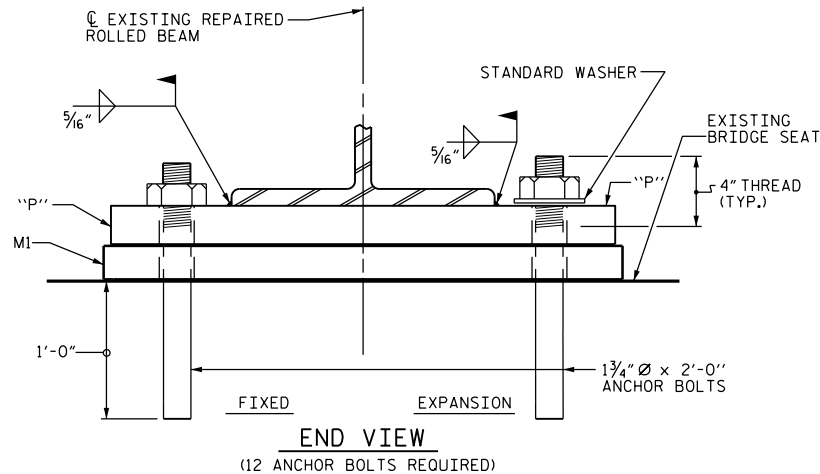
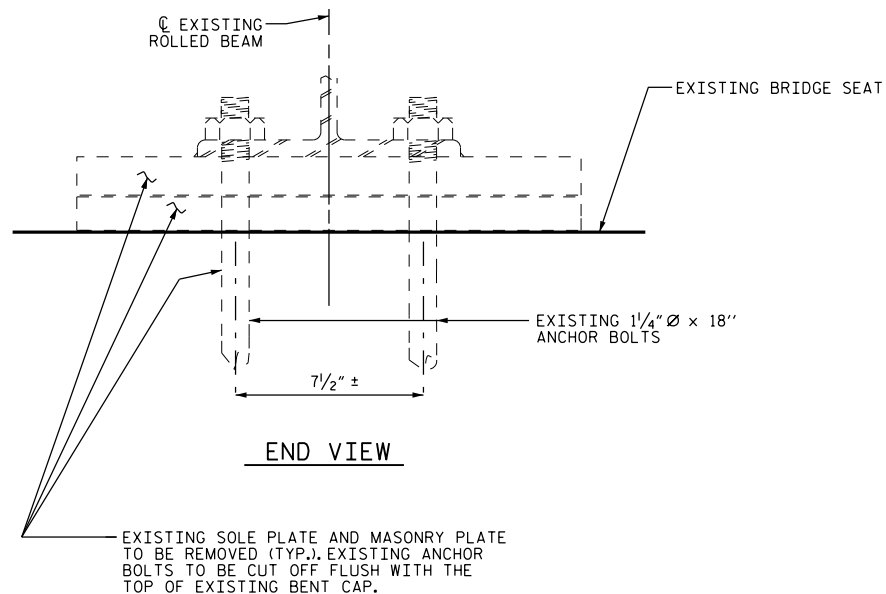
FOR PAINTING EXISTING STRUCTURES, SEE SPECIAL PROVISIONS.

PROJECT NO. BP-5300G
RANDOLPH COUNTY
 BRIDGE NO. 447

STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION RALEIGH						
GIRDER REPAIR						
BEAM REPAIR DETAILS						
REVISIONS						SHEET NO. S-4
NO.	BY:	DATE:	NO.	BY:	DATE:	TOTAL SHEETS 5
1			3			
2			4			

DRAWN BY : T.J. BEACH DATE : 8-2011
 CHECKED BY : D. MULLER DATE : 9-2011

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 tbeach



NOTES

AT ALL FIXED POINTS OF SUPPORT, NUTS FOR ANCHOR BOLTS ARE TO BE TIGHTENED FINGER TIGHT AND THEN BACKED OFF 1/2 TURN. THE THREAD OF THE NUT AND BOLT SHALL THEN BE BURRED WITH A SHARP POINTED TOOL.

FOR PAINTED STRUCTURAL STEEL (EXCLUDING AASHTO M270 GRADE 50W), SOLE PLATES, ANCHOR BOLTS, NUTS AND WASHERS SHALL BE GALVANIZED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.

ANCHOR BOLTS SHALL MEET THE REQUIREMENTS OF ASTM A449. NUTS SHALL MEET THE REQUIREMENTS OF AASHTO M291-DH OR AASHTO M292-2H. WASHERS SHALL MEET THE REQUIREMENTS OF AASHTO M293. SHOP DRAWINGS ARE NOT REQUIRED FOR ANCHOR BOLTS, NUTS AND WASHERS. SHOP INSPECTION IS REQUIRED.

WHEN FIELD WELDING THE SOLE PLATE TO THE GIRDER FLANGE, USE TEMPERATURE INDICATING WAX PENS, OR OTHER SUITABLE MEANS, TO ENSURE THAT THE TEMPERATURE OF THE SOLE PLATE DOES NOT EXCEED 300°F. TEMPERATURES ABOVE THIS MAY DAMAGE THE ELASTOMER.

ALL SURFACES OF BEARING PLATES SHALL BE SMOOTH AND STRAIGHT.

THE CONTRACTOR SHALL CORE INTO EXISTING BENT CAP TO INSTALL 1 3/4" ANCHOR BOLTS. BOLTS SHALL BE ADHESIVELY ANCHORED, SEE SPECIAL PROVISIONS. NO FIELD TESTING WILL BE REQUIRED FOR INSTALLATION OF ADHESIVELY ANCHORED BOLTS.

FOR MASONRY P AND SOLE P LOCATIONS, SEE "FRAMING PLAN" SHEET S-2.

PROJECT NO. BP-5300G
RANDOLPH COUNTY
 BRIDGE NO. 447

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

GIRDER REPAIR

BEARING SHEET

ASSEMBLED BY : T.J. BEACH	DATE : 8-2011
CHECKED BY : D. MULLER	DATE : 9-2011
DRAWN BY : JMB 11/87	REV. 8/16/99 MAB/LES
CHECKED BY : ARB 11/87	REV. 10/17/00 RWW/LES
	REV. 5/1/06 TLA/GM

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2			4			TOTAL SHEETS 5

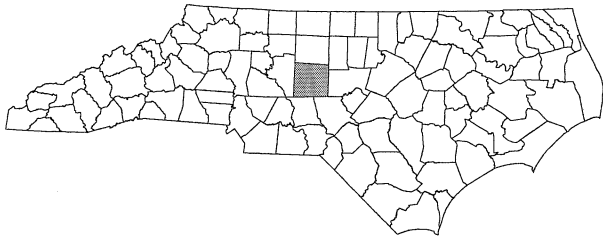
STD. NO. EB1 (SHT 2)

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

TRANSPORTATION MANAGEMENT PLAN

RANDOLPH COUNTY

DIVISION 8



BRIDGE PAINTING

BRIDGE #119 AND #121 – US 421 OVER SANDY CREEK
BRIDGE #427 – SR 1462 (W PRESNELL ST) OVER RAILROAD
BRIDGE #447 – SR 2269 (VISION DR) OVER RAILROAD

BRIDGE PRESERVATION

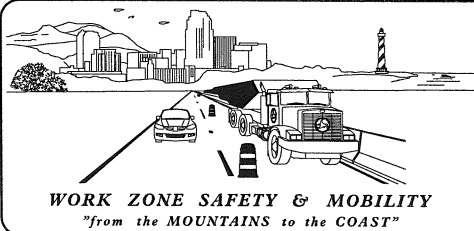
BRIDGE #447 – SR 2269 (VISION DR) OVER RAILROAD

INDEX OF SHEETS

SHEET NO.	TITLE
TMP-1	TITLE SHEET, AND INDEX OF SHEETS
TMP-1A	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, AND LEGEND
TMP-2	GENERAL NOTES
TMP-3	PHASING
TMP 4-4A	BRIDGE #447 OUTSIDE LANE CLOSURES

TRAFFIC MANAGEMENT STRATEGY

PROPOSED BRIDGE WORK WILL BE PERFORMED USING TIME RESTRICTED
LANE CLOSURES. REFER TO SHEET TMP-3 FOR PHASING.



PLAN PREPARED FOR NCDOT BRIDGE MANAGEMENT UNIT
RALEIGH, NC



PLAN PREPARED BY:
Stantec Consulting Services Inc.
801 Jones Franklin Road-Suite 300
Raleigh, NC 27606
Tel. 919.851.6866
Fax. 919.851.7024
www.stantec.com

BETSY L. WATSON, P.E.
TRAFFIC ENGINEER
GEORGE KARAGEORGE
SR. TRANSPORTATION DESIGNER

APPROVED: *Betsy Watson*
DATE: 2/17/12

SEAL



SHEET NO.

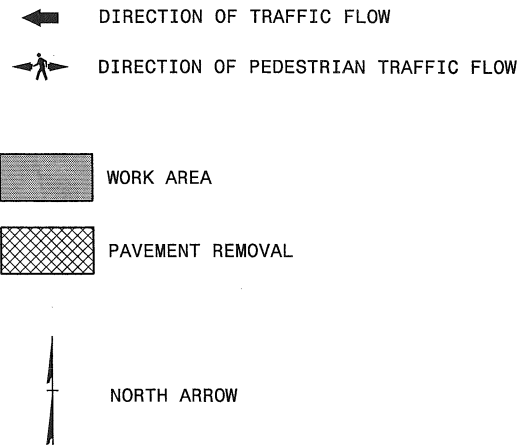
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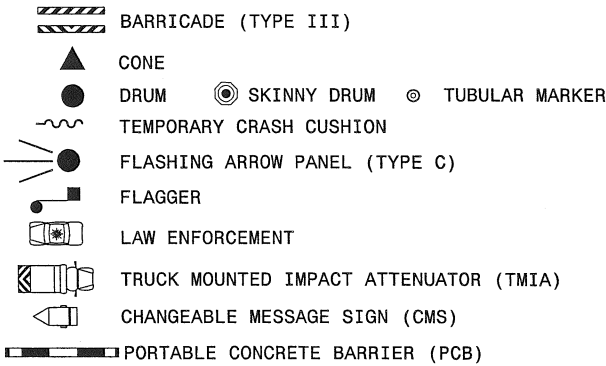
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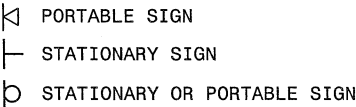
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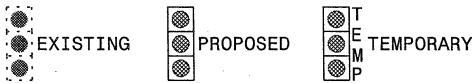
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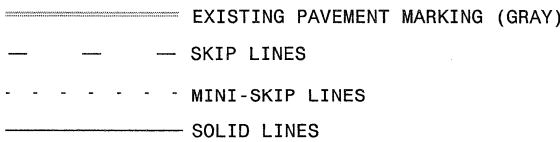
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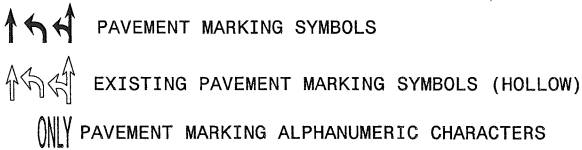
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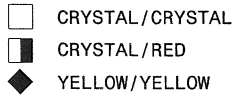
PAVEMENT MARKINGS



PAVEMENT MARKING SYMBOLS



PAVEMENT MARKERS



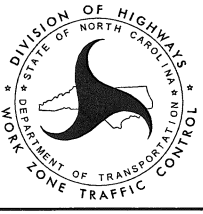
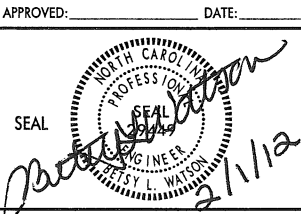
ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS" - PROJECT SERVICES UNIT - N.C. DEPARTMENT OF TRANSPORTATION - RALEIGH, N.C., DATED JANUARY 2012 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.01	WORK ZONE ADVANCE WARNING SIGNS
1101.02	TEMPORARY LANE CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW BOARDS
1130.01	DRUMS
1135.01	CONES
1150.01	FLAGGING DEVICES
1165.01	WORK VEHICLE LIGHTING SYSTEMS AND TMA DELINEATION
1180.01	SKINNY - DRUM



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Fax. (919) 851-7024
www.stantec.com
License No. F-0672



ROADWAY STANDARD DRAWINGS
&
LEGEND

GENERAL NOTES

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE PROJECT EXCEPT WHEN OTHERWISE NOTED IN THE PLAN OR DIRECTED BY THE ENGINEER.

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS, AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS OR RESULT IN DUPLICATE OR UNDESIRED OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING, OR REMOVAL OF DEVICES AS DIRECTED BY THE ENGINEER.

LANE CLOSURE TIME RESTRICTIONS

A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
ALL ROADS	6:00 A.M.- 9:00 A.M. MONDAY THRU FRIDAY 4:00 P.M.- 7:00 P.M. MONDAY THRU FRIDAY

HOLIDAY & HOLIDAY WEEKEND LANE CLOSURE TIME RESTRICTIONS

B) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND HOLIDAY WEEKENDS AS FOLLOWS:

ROAD NAME
ALL ROADS

- 1) FOR ANY UNEXPECTED OCCURRENCE THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
- 2) FOR NEW YEAR'S, BETWEEN THE HOURS OF 6:00 A.M. DECEMBER 31st TO 7:00 P.M. JANUARY 2nd. IF NEW YEAR'S DAY IS ON A FRIDAY, SATURDAY, SUNDAY, OR MONDAY THEN UNTIL 7:00 P.M. THE FOLLOWING TUESDAY.
- 3) FOR EASTER, BETWEEN THE HOURS OF 6:00 A.M. THURSDAY AND 7:00 P.M. MONDAY.
- 4) FOR MEMORIAL DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY TO 7:00 P.M. TUESDAY.
- 5) FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 6:00 A.M. THE DAY BEFORE INDEPENDENCE DAY AND 7:00 P.M. THE DAY AFTER INDEPENDENCE DAY. IF INDEPENDENCE DAY IS ON A FRIDAY, SATURDAY, SUNDAY OR MONDAY; THEN BETWEEN THE HOURS OF 6:00 A.M. THE THURSDAY BEFORE INDEPENDENCE DAY AND 7:00 P.M. THE TUESDAY AFTER INDEPENDENCE DAY.
- 6) FOR LABOR DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY AND 7:00 P.M. TUESDAY.
- 7) FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 6:00 A.M. TUESDAY TO 7:00 P.M. MONDAY.
- 8) FOR CHRISTMAS, BETWEEN THE HOURS OF 6:00 A.M. THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 7:00 P.M. THE FOLLOWING TUESDAY AFTER THE WEEK OF CHRISTMAS.

LANE AND SHOULDER CLOSURE REQUIREMENTS

- C) LANE CLOSURES ARE REQUIRED WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN ANY PORTION OF A TRAVEL LANE. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.
- D) INSTALL ALL LANE CLOSURES ACCORDING TO THE PLANS, ROADWAY STANDARD DRAWINGS (1101.02), OR AS DIRECTED BY THE ENGINEER.
- E) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 15 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR A LANE CLOSURE IS INSTALLED.
- F) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- G) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

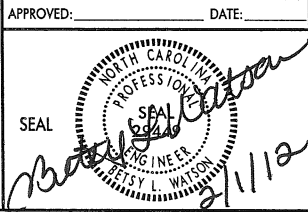
MISCELLANEOUS

- H) LAW ENFORCEMENT MAY BE USED TO MAINTAIN TRAFFIC THROUGH THE WORK AREA AND/OR INTERSECTIONS AS DIRECTED BY THE ENGINEER. LOCATIONS SHOWN IN THE PLANS ARE APPROXIMATE AND MAY BE REVISED AS THE OFFICER OR THE ENGINEER DEEM NECESSARY.
- I) ALL DIMENSIONS AND STATIONS IN THE TRAFFIC MANAGEMENT PLAN AND PHASING ARE APPROXIMATE (+/-); FIELD ADJUST AS NECESSARY OR AS DIRECTED BY THE ENGINEER.
- J) ENSURE THE OVERSIZE/OVERWEIGHT PERMIT UNIT (919) 733-4740 HAS BEEN ADVISED OF THE ONGOING TRAFFIC OPERATIONS THROUGH THE DIVISION OFFICE.

2/1/2012
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GENERAL NOTES

PHASING

BRIDGE #119-US 421 EASTBOUND OVER SANDY CREEK

WHEN PAINTING BRIDGE #119 USE LANE CLOSURES PER RDWY. STD. DWG. 1101.02, SHEET 3 ON US 421, AS NEEDED.

BRIDGE #121-US 421 WESTBOUND OVER SANDY CREEK

WHEN PAINTING BRIDGE #119 USE LANE CLOSURES PER RDWY. STD. DWG. 1101.02, SHEET 3 ON US 421, AS NEEDED.

BRIDGE #427-SR 1462 (W PRESNELL ST) OVER RAILROAD

WHEN PAINTING BRIDGE #427 USE FLAGGING OPERATION ON SR 1462 (W PRESNELL ST) PER RDWY. STD. 1101.02, SHEET 1, AS NEEDED.

COORDINATE ALL OPERATIONS WITH RAILROAD.

BRIDGE #447-SR 2269 (VISION DR) OVER RAILROAD

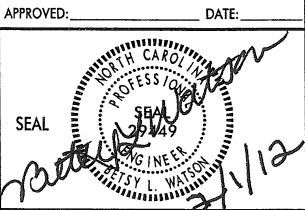
WHEN WORKING ON BRIDGE #447 USE RIGHT LANE CLOSURES ON SR 2269 FOR STAGING AREA AS SHOWN ON SHEETS TMP-4 AND TMP-4A.

COORDINATE ALL OPERATIONS WITH RAILROAD.

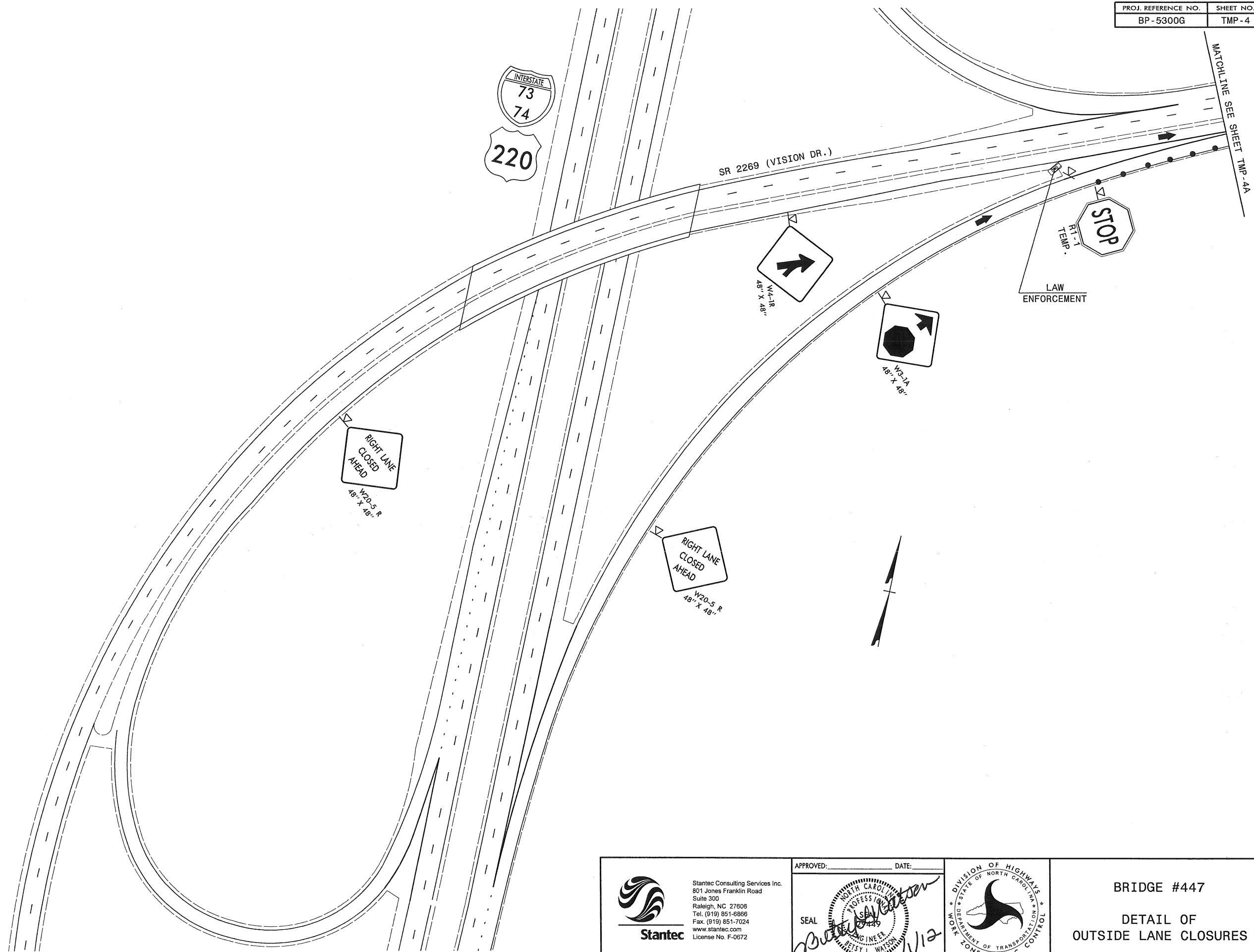
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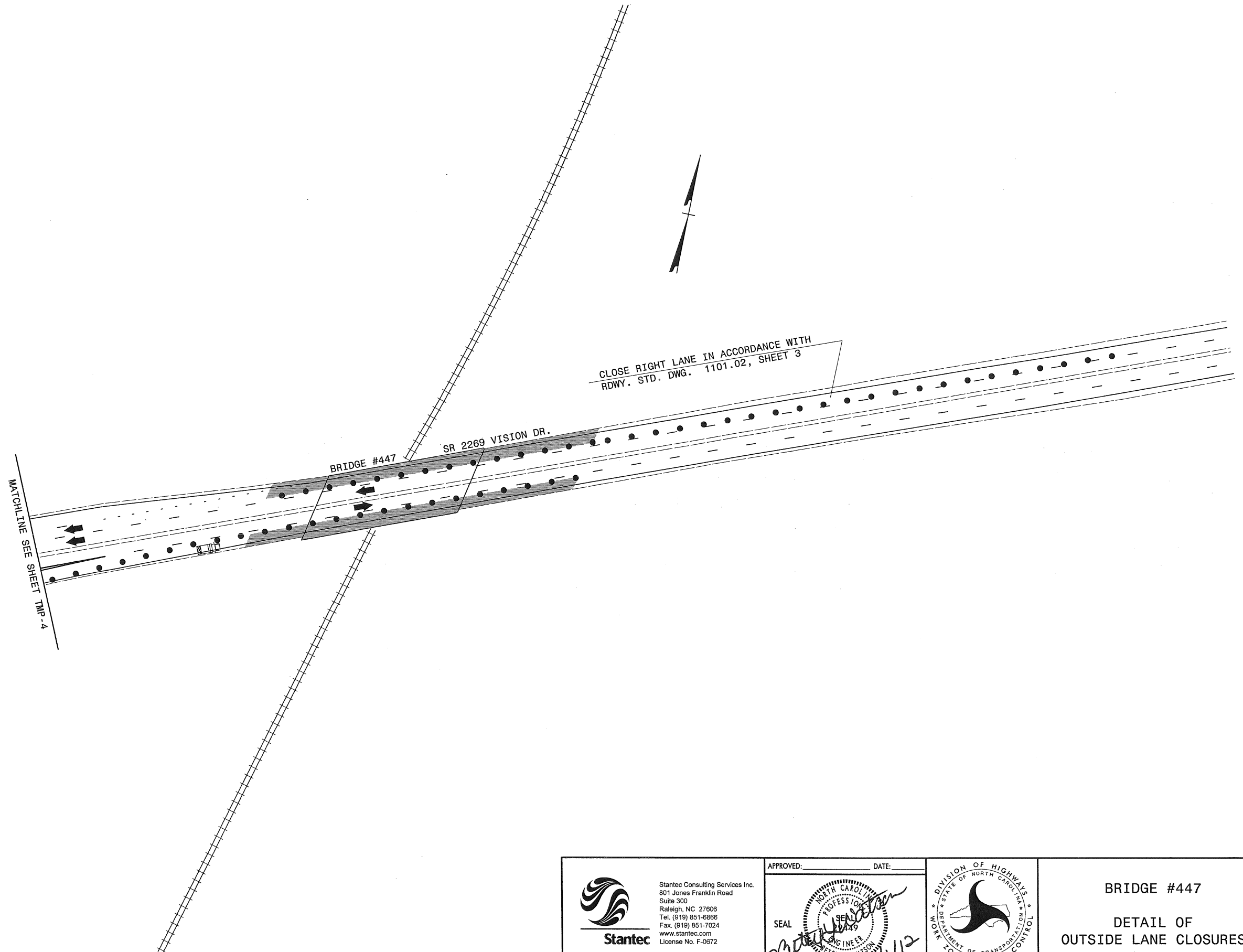


PHASING






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 Stantec	Stantec Consulting Services Inc. 801 Jones Franklin Road Suite 300 Raleigh, NC 27606 Tel. (919) 851-6866 Fax. (919) 851-7024 www.stantec.com License No. F-0672	APPROVED: _____ DATE: _____ 		BRIDGE #447 DETAIL OF OUTSIDE LANE CLOSURES



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 <p>Stantec Consulting Services Inc. 801 Jones Franklin Road Suite 300 Raleigh, NC 27606 Tel. (919) 851-0868 Fax. (919) 851-7024 www.stantec.com License No. F-0572</p>	APPROVED: _____ DATE: _____			<p align="center">BRIDGE #447</p> <p align="center">DETAIL OF OUTSIDE LANE CLOSURES</p>
	