

## MINIMUM CRITERIA DETERMINATION CHECKLIST

**TIP Project No.:** B-5765

**State Project No.:** 45721.1.1

**Project Location:** Bridge No. 115 on SR 1711 over Winston-Salem Southbound (WSSB) Railway in Davidson County.

**Project Description:** The North Carolina Department of Transportation (NCDOT) proposes to replace Bridge No. 115 on Gumtree Road (SR 1711) over WSSB Railway in the Town of Midway, Davidson County (refer to Appendix A, Figure 1). The purpose of the project is to replace a structurally deficient bridge.

Bridge No. 115 was constructed in 1975 and has a posted weight limit of 32 tons. The existing structure is a 3-span structural steel bridge that is 125 feet long with a clear roadway width of 27.75 feet. Bridge No. 115 carries 5,600 vehicles per day (2018 ADT), which is anticipated to increase to 8,400 vehicles by 2040. NCDOT Structure Management Unit records (May 2017) indicate that Bridge No. 115 has a Deck Condition rating of 4 (poor) out of 9, a sufficiency rating of 2, and was identified as a high priority bridge replacement due to sub-standard deck geometry and structural deficiencies.

Bridge No. 115 will be replaced in-place with an off-site detour. The proposed structure will be an approximately 156.5-foot long single-span welded steel-plate girder. The bridge typical section includes two, 12-foot lanes with 4-foot offsets for a minimum clear roadway width of 32 feet. The height of the structure will be increased approximately four feet, with a minimum vertical clearance over the existing railroad tracks of 23.5 feet. The use of sheet piles will allow WSSB Railway to construct an additional future track if needed. The project length is approximately 1,300 feet. The proposed design speed is 40 mph. Preliminary plans are included in Appendix A. The proposed project, B-5765, is state-funded with an estimated construction cost of \$1.7 Million.

**Anticipated Permit or Consultation Requirements:** No permits required. Railroad coordination is ongoing.

### **Special Project Information:**

**Environmental Commitments** – The list of project commitments (green sheet) is included at the end of the checklist.

**Design Exceptions** – There are no anticipated design exceptions for this project.

### **Alternatives Evaluation-**

- **No Build** – The No-Build Alternative would not eliminate the structural deficiencies of the existing bridge, and thus is not a viable option.
- **Build Alternatives** – The following Build Alternatives were considered:
  1. *Rehabilitation* - The bridge was constructed in 1975. Some temporary repairs have been made previously. The corrugated steel deck has corroded; the steel beams and piles have experienced section losses due to corrosion. Replacement of these structural elements would effectively constitute replacing the bridge.
  2. *Offsite Detour (Preferred Alternative)* - An approximately 2-mile off-site detour utilizing Hebron Church Road and Old US 52 was evaluated and determined to be acceptable. This alternative is preferred as it minimizes relocations, construction time, and overall project cost.
  3. *On-site Detour* - An on-site detour would require construction of a temporary detour bridge and result in a minimum of one additional relocatee, a longer construction schedule, and increased construction costs.
  4. *Staged Construction* - Staged construction was not evaluated due to the availability of an acceptable off-site detour.
  5. *New Alignment* - Given that the alignment for SR 1711 is acceptable, a new alignment alternative was considered infeasible as it would result in a higher overall project cost and would likely result in additional relocations.

**Relocations** - Due to the increased height and length requirements of the replacement bridge, one residential relocatee is anticipated southwest of Bridge No. 115 (Parcel 8; refer to Appendix A, Sheet No. 4).

**Pedestrian and Bicycle Accommodations** - Bridge No. 115 is located on the 700-mile NC 2 (Mountains to Sea) state bike route, which follows Old US Highway 52 onto Gumtree Road within the study area. NCDOT Division of Bicycle and Pedestrian Transportation (DBPT) recommended that the replacement bridge and approaches include 4-foot shoulders (minimum), as well as a 42-inch high railing on the bridge. The Davidson County Comprehensive Transportation Plan recommends minimum 4-foot paved shoulders and 54-inch railing heights. As shown in Appendix A (Sheet No. S-2), the proposed replacement bridge includes 4-foot shoulders and a 54-inch high (4-foot, 6-inch) two-bar metal rail.

**Detour** - Traffic would be managed with an off-site detour. The detour would begin from Gumtree Road (SR 1711) to Hebron Church Road (SR 1713), to Old US 52, and end on Gumtree Road, for a distance of approximately 2 miles.

**Threatened & Endangered Species** - The biological conclusion for the Northern long-eared bat (NLEB) is unresolved. No potential NLEB habitat or evidence of bats were observed by NCDOT biologists during a site visit on May 24, 2018. The USACE has developed a Standard Local Operating Procedure for Endangered Species (SLOPES) to address NLEB when they are the lead agency, which NCDOT will follow for this project. This procedure applies to projects in Divisions 9-14. The requirements of the SLOPES for NLEB will be completed prior to Let and will be submitted to the USACE.

**Cultural Resources** - NCDOT's cultural resources staff reviewed the project under a programmatic agreement with the State Historic Preservation Office and determined that there are no known National Register of Historic Places (NRHP) eligible or listed archaeological resources within or adjacent to the Area of Potential Effects (APE) and no archaeological survey is required (refer to Appendix B). The preliminary historic architecture and landscapes review indicated that a site survey was necessary to determine if a full National Register evaluation would be warranted. Based on the results of the site survey conducted on April 11, 2018, no historic properties will be affected by the proposed bridge replacement (refer to Appendix B).

**Agency Comments** - NCDOT sought input from resource and regulatory agencies via a Start of Study scoping letter emailed on July 30, 2018. Letters were sent to the following agencies (responding agencies are denoted by an asterisk [\*]):

- Federal Highway Administration\*
- US Army Corps of Engineers\*
- US Fish and Wildlife Service\*
- NC Department of Environmental Quality
- NC Department of Parks and Recreation\*
- NC State Historic Preservation Office\*
- NC Wildlife Resources Commission
- NCDOT-DBPT\*

Of the responses received, USFWS indicated that Schweinitz's sunflower may occur in the project area. Schweinitz's sunflower surveys were conducted for this project on August 15, 2018. In addition, the NCDOT-DBPT recommended that a minimum of 4-foot shoulders and 42-inch rail height be included in the design of the replacement bridge. As indicated previously, the proposed replacement bridge includes 4-foot shoulders and a 54-inch high, two-bar metal rail (refer to Appendix A, Sheet No. S-2).

Input forms were sent to the Midway Fire and Rescue Chief, Gumtree Fire Department Chief, Davidson County EMS Director, Davidson County Sheriff's Office, Davidson County Planning and Zoning Director, Winston Salem Urban Area MPO Deputy Director of Transportation, and Davidson County Schools Transportation Director. The Midway Fire and Rescue Chief noted that any potential detour route and bridge closure would have a moderate impact on response time. The Davidson County School District's Transportation Director noted that bus routes can be adjusted if advance notice of road closures is provided. In addition, it was noted that the proposed bridge closure would

likely conflict with the Midway Christmas Parade route, which occurs annually in the study area on the last Saturday of November.

**Public Involvement** - A project newsletter was mailed on May 31, 2019, to 84 property owners within the project area and along the proposed off-site detour route. Written comments were submitted by four residents during the comment period. These comments were all in reference to the proposed off-site detour and included concerns over the additional traffic volume and travel speeds of detoured vehicles on Hebron Church Road. In response to these comments, NCDOT has verified that Hebron Church Road (SR 1713) was resurfaced in June 2018 and would sufficiently carry the added traffic volume from the proposed off-site detour. NCDOT Division 9 has offered to place orange advisory signs with a reduced speed limit along the detour route. The posted speed limit would remain in effect.

**PART A: MINIMUM CRITERIA**

	<b>YES</b>	<b>NO</b>
1. Is the proposed project listed as a type and class of activity allowed under the Minimum Criteria Rule in which environmental documentation is <u>not</u> required?	<input checked="" type="checkbox"/>	<input type="checkbox"/>

If the answer to number 1 is “no”, then the project does not qualify as a minimum criteria project. A state environmental assessment is required.

If yes, under which category?      **Category #9** – Reconstruction of existing crossroad or railroad separations and existing stream crossings, included but not limited to pipes, culverts, and bridges.

If either category #8, #12(i) or #15 is used complete Part D of this checklist.

**PART B: MINIMUM CRITERIA EXCEPTIONS**

2. Could the proposed activity cause significant changes in land use concentrations that would be expected to create adverse air quality impacts?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3. Will the proposed activity have secondary impacts or cumulative impacts that may result in a significant adverse impact to human health or the environment?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4. Is the proposed activity of such an unusual nature or does the proposed activity have such widespread implications, that an uncommon concern for its environmental effects has been expressed to the Department?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

- 5. Does the proposed activity have a significant adverse effect on wetlands; surface waters such as rivers, streams, and estuaries; parklands; prime or unique agricultural lands; or areas of recognized scenic, recreational, archaeological, or historical value?
- 6. Will the proposed activity endanger the existence of a species on the Department of Interior's threatened and endangered species list?
- 7. Could the proposed activity cause significant changes in land use concentrations that would be expected to create adverse water quality or ground water impacts?
- 8. Is the proposed activity expected to have a significant adverse effect on long-term recreational benefits or shellfish, finfish, wildlife, or their natural habitats?

**PART C: COMPLIANCE WITH STATE AND FEDERAL REGULATIONS**

- |  | <b>YES</b>                          | <b>NO</b>                           |
|--|-------------------------------------|-------------------------------------|
| 9. Is a federally protected threatened or endangered species, or its habitat, likely to be impacted by the proposed action?  | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| 10. Does the action require the placement of temporary or permanent fill in waters of the United States?   | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| 11. Does the project require the placement of a significant amount of fill in high quality or relatively rare wetland ecosystems, such as mountain bogs or pine savannahs? | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| 12. Is the proposed action located in an Area of Environmental Concern, as defined in the coastal Area Management Act?   | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| 13. Does the project require stream relocation or channel changes?   | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |

**Cultural Resources**

- 14. Will the project have an “effect” on a property or site listed on the National Register of Historic Places?
- 15. Will the proposed action require acquisition of additional right of way from publicly owned parkland or recreational areas?

**Response to Question 9** - Habitat for Schweinitz's sunflower is found along portions of the maintained communities and along the edges of the Basic Mesic Forest. A plant survey was conducted of all potential habitat in the study area on August 15, 2018, and no specimens of any sunflower were observed. A review of NHP records on August 16, 2018, indicated no known occurrences within 1.0 mile of the study area. The biological conclusion for this species is No Effect.

**PART D:( To be completed when either category #8, 12(i) or #15 of the rules are used.)**

- 16. Project length: \_\_\_\_\_
- 17. Right of Way width: \_\_\_\_\_
- 18. Project completion date: \_\_\_\_\_
- 19. Total acres of newly disturbed ground surface: \_\_\_\_\_
- 20. Total acres of wetland impacts: \_\_\_\_\_
- 21. Total linear feet of stream impacts: \_\_\_\_\_
- 22. Project purpose: \_\_\_\_\_

Prepared by: DocuSigned by:  
*Laura M. Stevens* \_\_\_\_\_ Date: 8/22/2019  
2ECB27C304754C4...  
Laura Stevens, AICP  
Parrish and Partners, LLC

Approved by: DocuSigned by:  
*Kevin Fischer* \_\_\_\_\_ Date: 8/23/2019  
ED19A18D98EC496...  
Kevin Fischer, PE  
Structures Management Unit  
North Carolina Department of Transportation

**PROJECT COMMITMENTS**  
**Davidson County**  
**Replace Bridge No. 115 on Gumtree Road (SR 1711) over**  
**Winston-Salem Southbound Railway**  
WBS No. 45721.1.1  
Project No. B-5765

**NCDOT Division 9, NCDOT Structures Management Unit (SMU)**  
**Continued Coordination and Outreach**

- The Division Project Manager and/or SMU Project Manager will coordinate with Winston-Salem Railway representatives prior to construction to secure the necessary access to railroad right-of-way.
- In order to have time to adequately reroute school buses, the Division Project Manager will contact the Davidson County School District's Transportation Director (336-249-8977) at least six months prior to road closure.
- The Division Project Manager will coordinate with emergency management officials (Midway Fire and Rescue [336-764-0920], Gumtree Fire Department [336-788-3544], Davidson County EMS [336-242-2270], Davidson County Sheriff's Office [336-242-2100]) to discuss detour planning for emergency vehicles.
- If construction requires closure of Gumtree Road during the month of November, the Division Project Manager will coordinate with the Midway Town Manager (336-764-5455) to discuss potential detour planning for the annual Midway Christmas Parade.
- NCDOT Division 9 will place orange advisory signs with a reduced speed limit along the off-site detour. The posted speed limit will remain in effect.

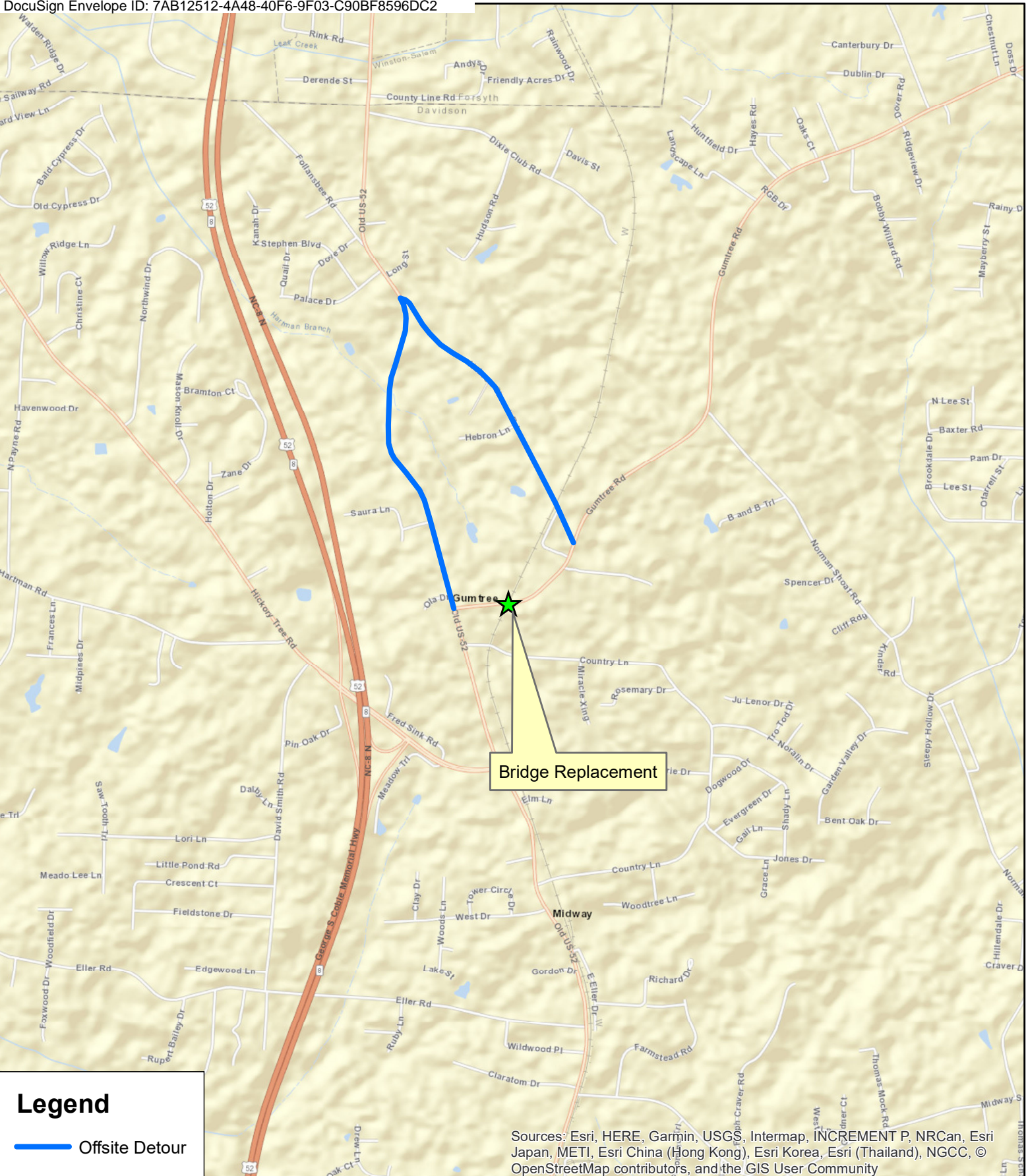
**NCDOT Environmental Analysis Unit**  
**Northern-Long Eared Bat (NLEB)**

- The USACE has developed a Standard Local Operations Procedure for Endangered Species (SLOPES) to address NLEB when they are the lead agency, which NCDOT will follow for this project. This procedure applies to projects in Divisions 9-14. The requirements of the SLOPES for the NLEB will be completed prior to Let and will be submitted to the USACE. A Biological Conclusion is not required.

# **APPENDIX A**

## Preliminary Design





Sources: Esri, HERE, Garmin, USGS, Intermap, INCREMENT P, NRCan, Esri Japan, METI, Esri China (Hong Kong), Esri Korea, Esri (Thailand), NGCC, © OpenStreetMap contributors, and the GIS User Community



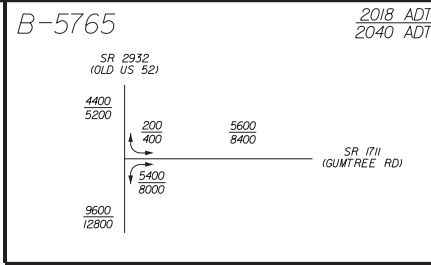
0 1,000 2,000 4,000 Feet

**Figure 1**  
**Vicinity Map**  
 B-5765, Replace Bridge 115 on SR 1711 over  
 Winston Salem SB RR  
 Davidson County, North Carolina

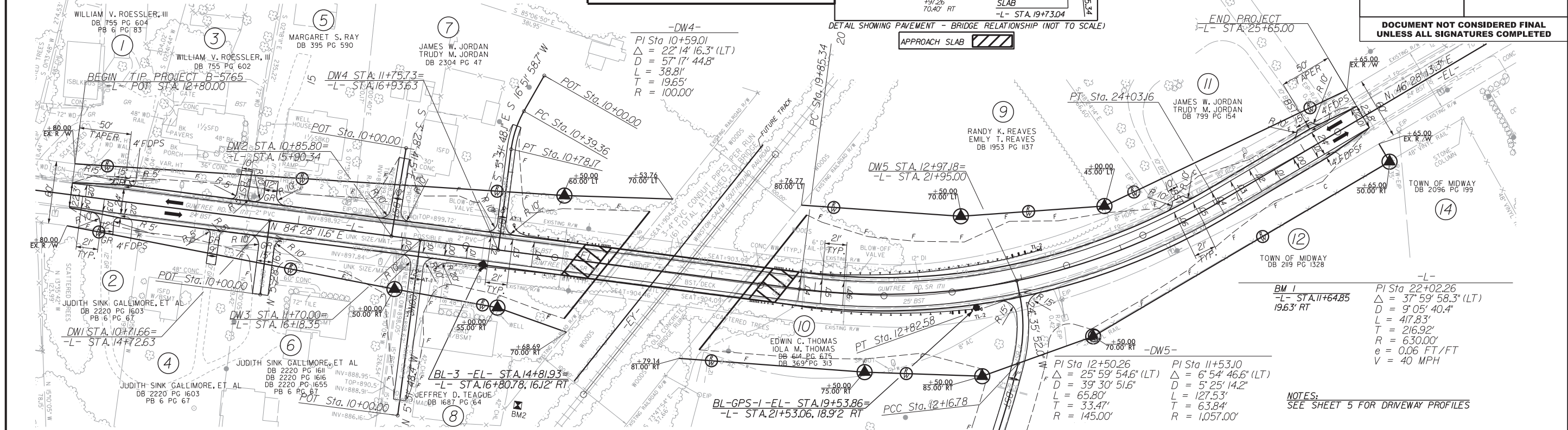




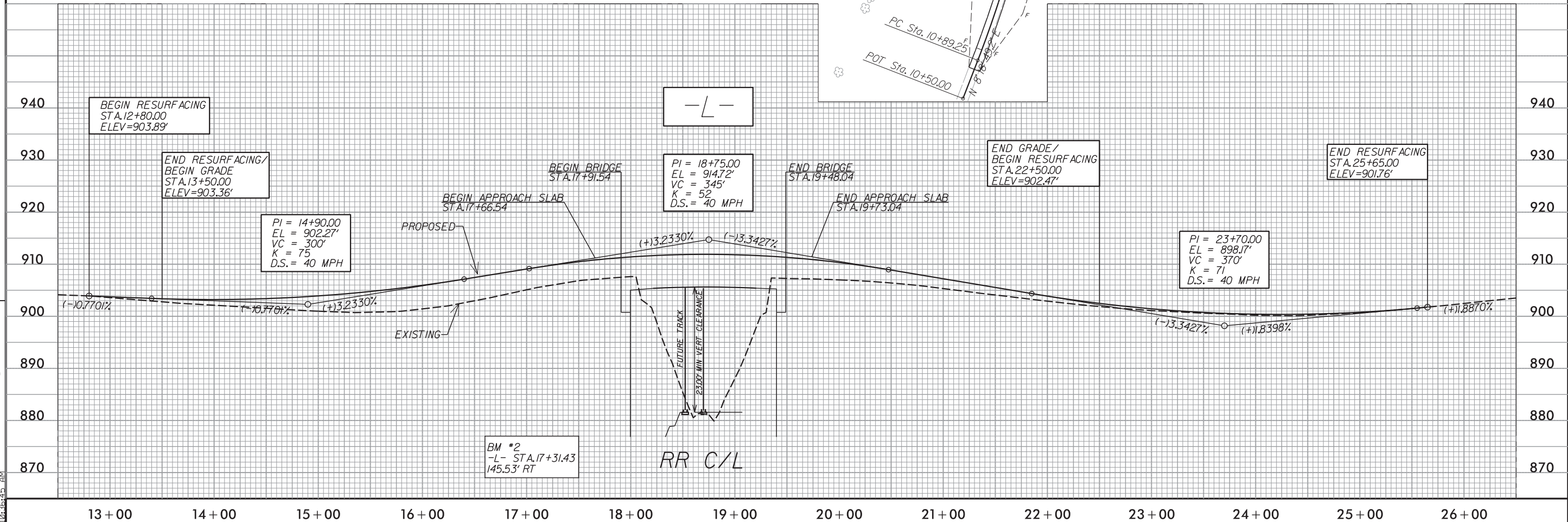
B.17/9C



PROJECT REFERENCE NO.	SHEET NO.
B-5765	4
RW SHEET NO.	
ROADWAY DESIGN ENGINEER	HYDRAULICS ENGINEER
<b>INCOMPLETE PLANS</b> DO NOT USE FOR R/W ACQUISITION	
<b>DOCUMENT NOT CONSIDERED FINAL</b> UNLESS ALL SIGNATURES COMPLETED	

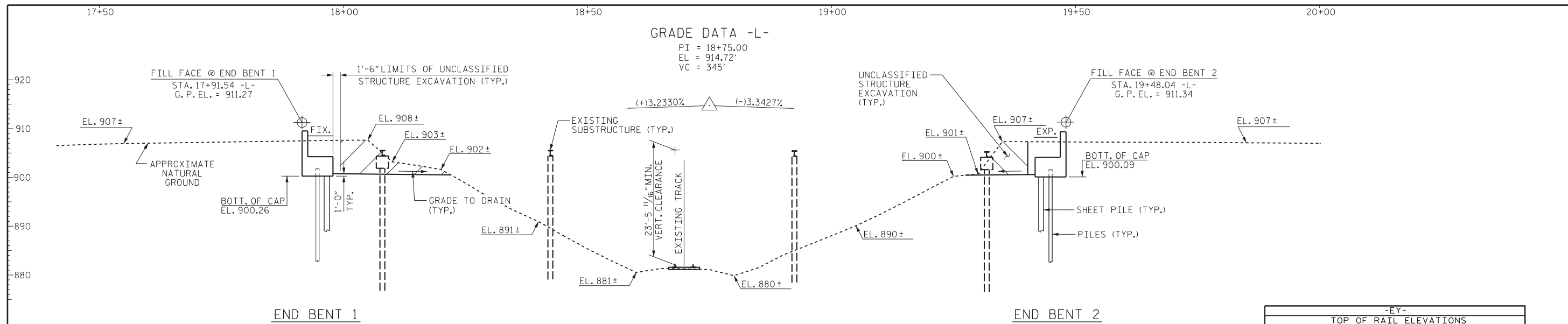


NOTES:  
SEE SHEET 5 FOR DRIVEWAY PROFILES



REVISIONS

11/13/2018 10:38:45 AM  
PLAN\_PROFILE\_SHT\_1.dgn



SECTION ALONG -L-

SECTIONS @ END BENTS ARE AT RIGHT ANGLES

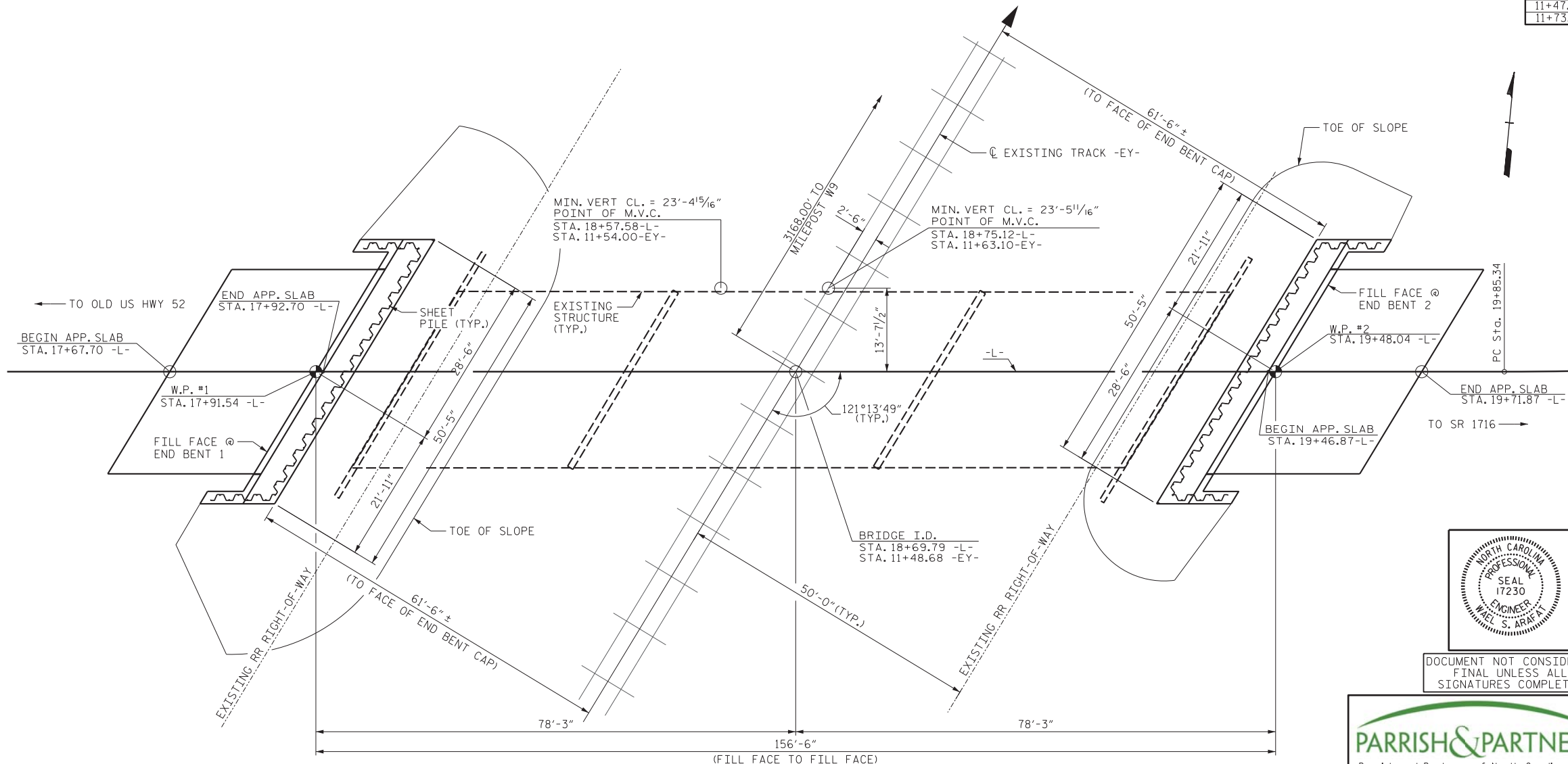
-EY-

TOP OF RAIL ELEVATIONS  
(LOOKING UPSTATION ALONG RAILROAD)

LEFT RAIL		RIGHT RAIL	
STATION	ELEVATION	STATION	ELEVATION
11+00.12	881.92	11+00.20	881.92
11+28.85	881.75	11+28.83	881.74
11+47.33	881.67	11+47.77	881.66
11+73.77	881.54	11+74.00	881.48

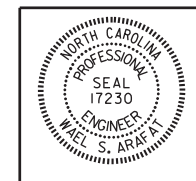
HORIZONTAL CURVE DATA -L-

PI Sta 22+02.26  
 $\Delta = 37^{\circ} 59' 58.3''$  (LT)  
 $D = 9^{\circ} 05' 40.4''$   
 $L = 417.83'$   
 $T = 216.92'$   
 $R = 630.00'$



PLAN

PILES NOT SHOWN FOR CLARITY



DOCUMENT NOT CONSIDERED  
FINAL UNLESS ALL  
SIGNATURES COMPLETED

**PARRISH & PARTNERS**  
 Parrish and Partners of North Carolina, PLLC  
 421 Fayetteville St., #1100  
 Raleigh, NC 27601  
 NC License #P-1212

PROJECT NO. B-5765  
 DAVIDSON COUNTY  
 STATION: STA. 18+69.79-L-  
STA. 11+48.68-EY-  
 SHEET 1 OF 2 REPLACES BRIDGE NO. 115  
 MILE POST #W9.6

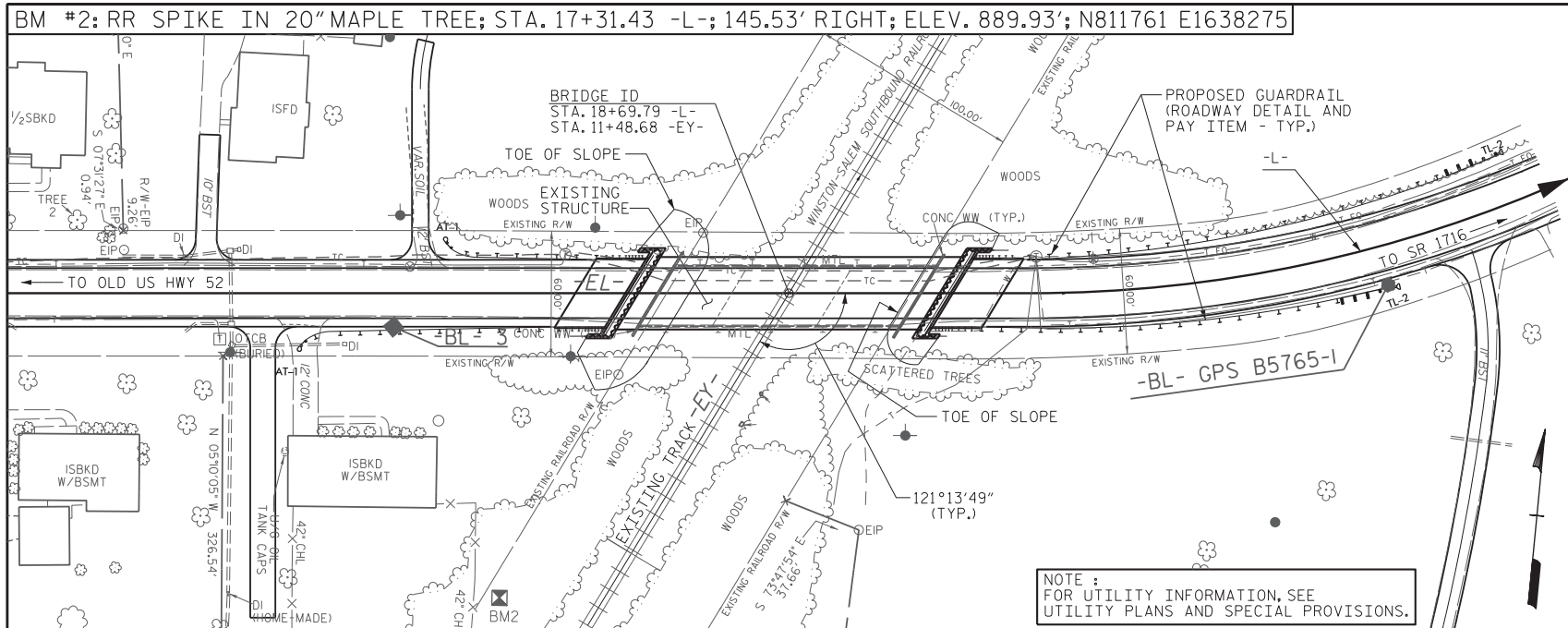
STATE OF NORTH CAROLINA  
 DEPARTMENT OF TRANSPORTATION  
 RALEIGH  
 PRELIMINARY  
 GENERAL DRAWING  
 FOR BRIDGE ON SR 1711  
 OVER WINSTON-SALEM SOUTHBOND  
 RAILROAD BETWEEN OLD US 52  
 AND SR 1716

REVISIONS					SHEET NO.
NO.	BY:	DATE:	NO.	DATE:	S-1
1			3		TOTAL SHEETS
2			4		2

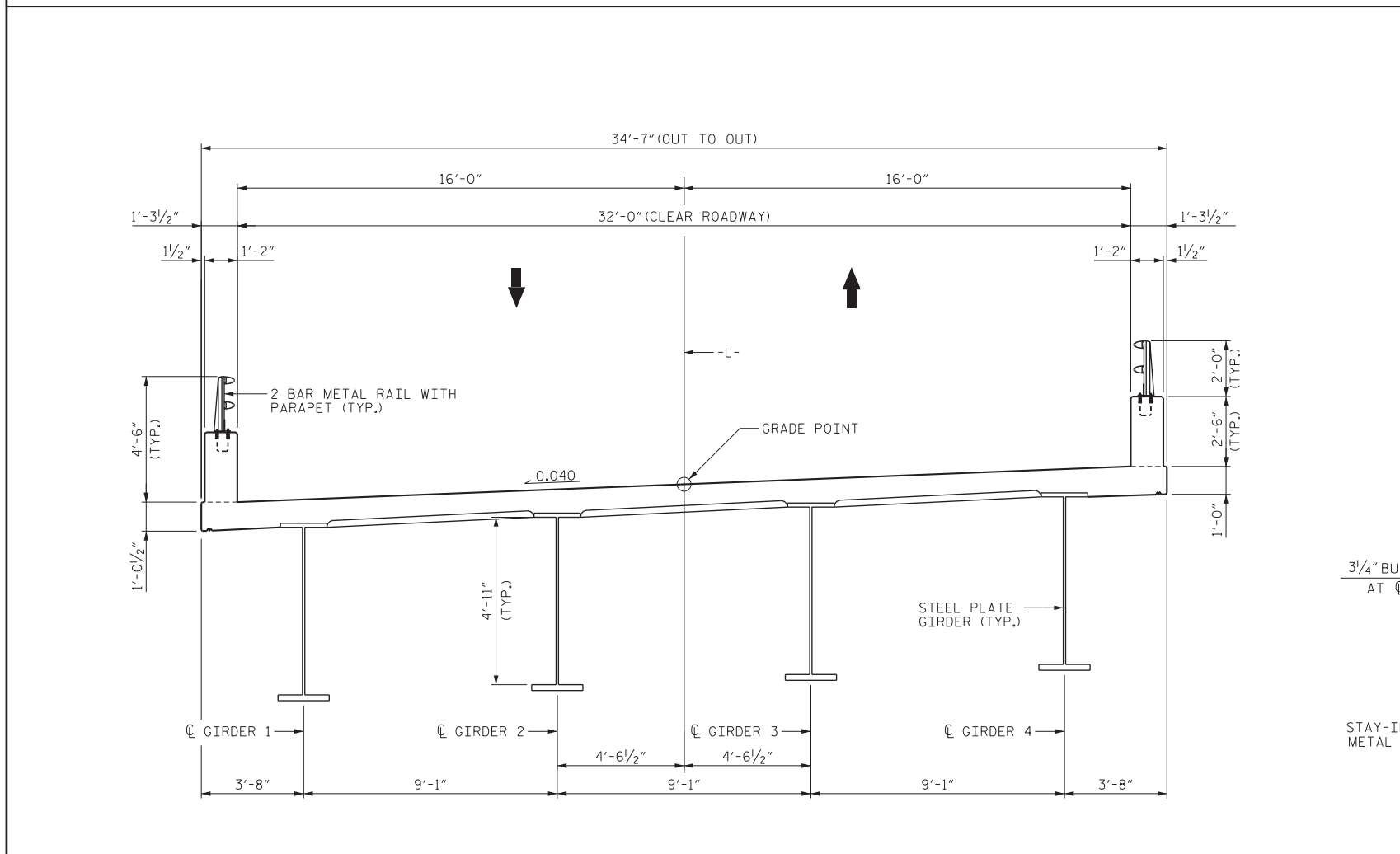
DRAWN BY : M.W. BREELAND DATE : 05-19  
 CHECKED BY : W.S. ARAFAT DATE : 05-19  
 DESIGN ENGINEER OF RECORD: O. PUIGCERVER DATE : 05-19

PRELIMINARY PLANS  
DO NOT USE FOR CONSTRUCTION





LOCATION SKETCH



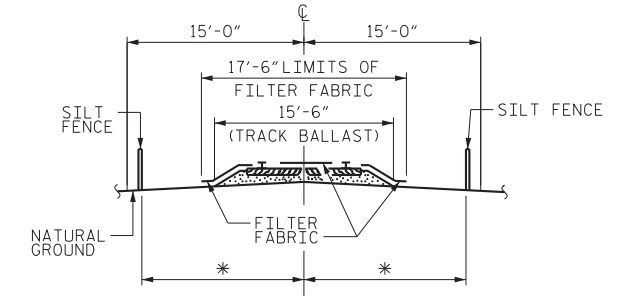
TYPICAL SECTION

SIMPLE SPAN PLATE GIRDERS WITH COMPOSITE DECK

DRAWN BY : M.W. BREELAND DATE : 05-19  
 CHECKED BY : W.S. ARAFAT DATE : 05-19  
 DESIGN ENGINEER OF RECORD : O. PUIGCERVER DATE : 05-19

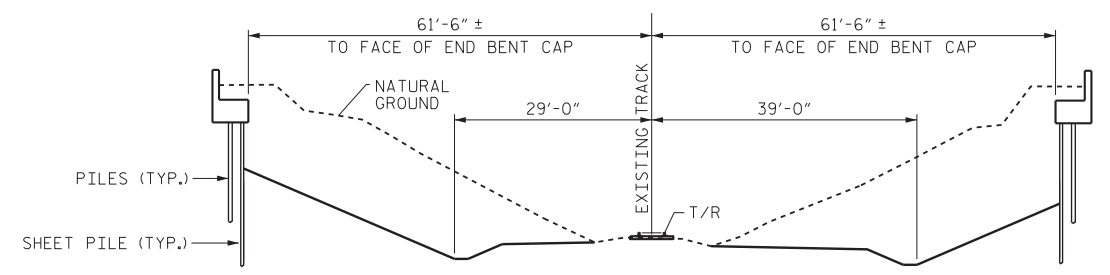
NOTES

ASSUMED LIVE LOAD = HL-93 OR ALTERNATE LOADING.  
 THIS BRIDGE HAS BEEN DESIGNED IN ACCORDANCE WITH THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS.  
 THIS BRIDGE IS LOCATED IN SEISMIC ZONE 1.

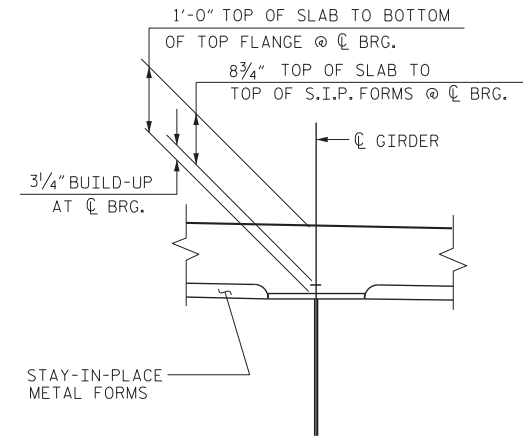


RAILROAD EROSION CONTROL DETAIL

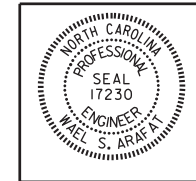
\* TO BE DETERMINED BY RESIDENT ENGINEER IN CONSULTATION WITH RAILROAD ENGINEER.  
 NOTES  
 RAILROAD EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO PERFORMING ANY WORK IN THE RAILROAD RIGHT-OF-WAY.  
 ADDITIONAL EROSION CONTROL MEASURES FOR PROTECTION OF RAILROAD DITCHES MAY BE REQUIRED AS DIRECTED BY THE ENGINEER.  
 NO SEPARATE PAYMENT WILL BE MADE FOR RAILROAD EROSION CONTROL MEASURES.  
 LIMITS OF SILT FENCE AND FILTER FABRIC PARALLEL TO THE RAILROAD SHALL EXTEND A MINIMUM OF 25'-0" OUTSIDE OF SUPERSTRUCTURE OR TOE OF SLOPE ON CONSTRUCTION. A GREATER LENGTH OF SILT FENCE OR FILTER FABRIC MAY BE REQUIRED IF SO DIRECTED BY THE ENGINEER.  
 FILTER FABRIC TO BE NAILED TO TIMBER RAIL TIES WITH PRIME SOURCE "GRIP CAP" OR EQUIVALENT. FILTER FABRIC ON SHOULDER TO BE SECURED AS DIRECTED BY THE ENGINEER AND RAILROAD.



THEORETICAL RAILROAD CROSS SECTION



DETAIL (TYP. EACH GIRDER)



DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

**PARRISH & PARTNERS**  
 Parrish and Partners of North Carolina, PLLC  
 421 Fayetteville St., #1100  
 Raleigh, NC 27601  
 NC License #P-1212

PROJECT NO. B-5765  
 DAVIDSON COUNTY  
 STATION: STA. 18+69.79-L-  
 STA. 11+48.68-EY-  
 SHEET 2 OF 2 REPLACES BRIDGE NO. 115  
 MILE POST #W9.6

STATE OF NORTH CAROLINA  
 DEPARTMENT OF TRANSPORTATION  
 RALEIGH  
 PRELIMINARY GENERAL DRAWING  
 FOR BRIDGE ON SR 1711  
 OVER WINSTON-SALEM SOUTHBOUND  
 RAILROAD BETWEEN OLD US 52  
 AND SR 1716

REVISIONS					SHEET NO. S-2
NO.	BY:	DATE:	NO.	DATE:	
1			3		TOTAL SHEETS 2
2			4		

PRELIMINARY PLANS  
 DO NOT USE FOR CONSTRUCTION

# **APPENDIX B**

## Cultural Resources Correspondence

18-01-0004



## NO ARCHAEOLOGICAL SURVEY REQUIRED FORM

This form only pertains to ARCHAEOLOGICAL RESOURCES for this project. It is not valid for Historic Architecture and Landscapes. You must consult separately with the Historic Architecture and Landscapes Group.



### PROJECT INFORMATION

Project No: **B-5765** County: **Davidson**  
 WBS No: **45721.1.1** Document: **M C C**  
 F.A. No: Funding:  State  Federal

Federal Permit Required?  Yes  No Permit Type:

**Project Description:** NCDOT proposes to replace Bridge No. 115 on SR 1711 (Gumtree Road) over the Winston-Salem railroad in Davidson County. The state-funded project crosses a railroad rather than a stream, creek or river and does not require a federal USACE permit, therefore NC GS 121-12(a) applies for this archaeological review. The proposed bridge would be somewhat longer and taller than the existing structure. An offsite detour has been identified for use during construction. Design plans were available at the time of the review and guided the establishment of the following Area of Potential Effects (APE). For purposes of this archaeological review, the APE is the project length of about 1300 feet by a maximum width of about 150 feet to allow for any necessary easements. A large portion of the APE may be considered disturbed by the existing and previous roadway, bridge and associated soil disruption.

### SUMMARY OF CULTURAL RESOURCES REVIEW

#### **Brief description of review activities, results of review, and conclusions:**

The project area along SR 1711 in Davidson County is populated with twentieth century houses and crosses open lawns and fields. An established railroad is cross by the structure to be replaced. Utilities are present along the margins of the APE according to virtual drive by viewing that was accomplished using Google Maps. No cemeteries were noted during the virtual viewing or on the USGS mapping at the project location, nor were any noted on cemetery data maintained by NCDOT archaeologist, Paul Mohler.

The Office of State Archaeology was visited for research and to determine if any previous archaeological reviews, surveys or sites are associated with the nearby vicinity. There are no recorded archaeological sites near the project area, including any on the National Register of Historic Places, and no archaeological surveys have been conducted nearby, save TIP U-5786 (Hickory Tree Road widening, PA 17-11-0028) where soils were found to be generally eroded and no NRHP-eligible archaeological sites were identified.

Historic mapping was examined. Older mapping shows that the SR 1711 roadway used to dip towards the south on the eastern side of the railway before crossing westward. After 1975, when the existing bridge was reportedly constructed, USGS mapping shows the current alignment. This change is present also in aerial photography (1955, 1965, 1971 then after construction, 1980). The southeastern quadrant of the APE has been previously disturbed by the construction then dismantling of the previous roadbed and bridge launches over the railroad.

As much of the existing APE has been modified for the current roadway, bridge and drainage, expectations are low for encountering new archaeological sites, especially any that may be intact and significant. Further, as there are no documented NRHP eligible or listed archaeological resources within or adjacent to the APE, no further work is recommended under NC GS 121-12(a).

18-01-0004

***Brief Explanation of why the available information provides a reliable basis for reasonably predicting that there are no unidentified historic properties in the APE:***

The scale and nature of the project is limited to the replacement of an existing bridge with a new structure at the same location. An offsite detour will be used. The APE of the project overlays the current transportation facility and is heavily disturbed by the original roadway and bridge construction. New impacts will be necessary with ROW and easements extending beyond the current limits.

Review of background archaeological information, examination of mapping and virtual drive-by suggests low probability for the presence of significant, intact archaeological resources within the APE. Since there are no known NRHP eligible or listed archaeological resources within the APE, no further investigation is warranted under NC GS 121-12(a).

This project falls within a North Carolina County in which the Catawaba Nation has expressed an interest. It is recommended that you contact each federal agency involved with this project to determine their Section 106 Tribal Consultaion requirements.

**SUPPORT DOCUMENTATION**

See attached:  Map(s)  Previous Survey Info  Photos  Correspondence  
 Photocopy of County Survey Notes Other:

**FINDING BY NCDOT ARCHAEOLOGIST**NO ARCHAEOLOGY SURVEY REQUIRED


NCDOT ARCHAEOLOGIST

6/16/2019

Date



18-01-0004



## HISTORIC ARCHITECTURE AND LANDSCAPES NO HISTORIC PROPERTIES PRESENT OR AFFECTED FORM

This form only pertains to Historic Architecture and Landscapes for this project. It is not valid for Archaeological Resources. You must consult separately with the Archaeology Group.

### PROJECT INFORMATION

<b>Project No:</b>	B-5765	<b>County:</b>	Davidson
<b>WBS No.:</b>	45721.3.1	<b>Document Type:</b>	MCC
<b>Fed. Aid No:</b>		<b>Funding:</b>	<input checked="" type="checkbox"/> State <input type="checkbox"/> Federal
<b>Federal Permit(s):</b>	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<b>Permit Type(s):</b>	USACE
<b><u>Project Description:</u></b> Replace Bridge No 115 on SR 1711 (Gumtree Rd) over Winston-Salem Railroad.			

### SUMMARY OF HISTORIC ARCHITECTURE AND LANDSCAPES REVIEW

- There are no National Register-listed or Study Listed properties within the project's area of potential effects.
- There are no properties less than fifty years old which are considered to meet Criteria Consideration G within the project's area of potential effects.
- There are no properties within the project's area of potential effects.
- There are properties over fifty years old within the area of potential effects, but they do not meet the criteria for listing on the National Register.
- There are no historic properties present or affected by this project. (Attach any notes or documents as needed.)

**Date of field visit:** April 11, 2018

#### **Description of review activities, results, and conclusions:**

Review of HPO quad maps, relevant background reports, historic designations roster, and indexes was undertaken on December 18, 2017. Based on this review there are no NR, DE, LL, SS, or SL in the project area; however, a few structures over 50 years old were revealed. An NCDOT Architectural Historian conducted a site visit on April 11, 2018 and determined that no properties in the project study area warrant further investigation. Most date from the mid-1960s to a few circa 1940s houses. None of these houses rise to the level of National Register eligibility due to their lack of architectural integrity and significance due to their common style and type as well as significant alterations. No historic properties will be affected by this bridge replacement.

### SUPPORT DOCUMENTATION

- Map(s)     Previous Survey Info.     Photos     Correspondence     Design Plans

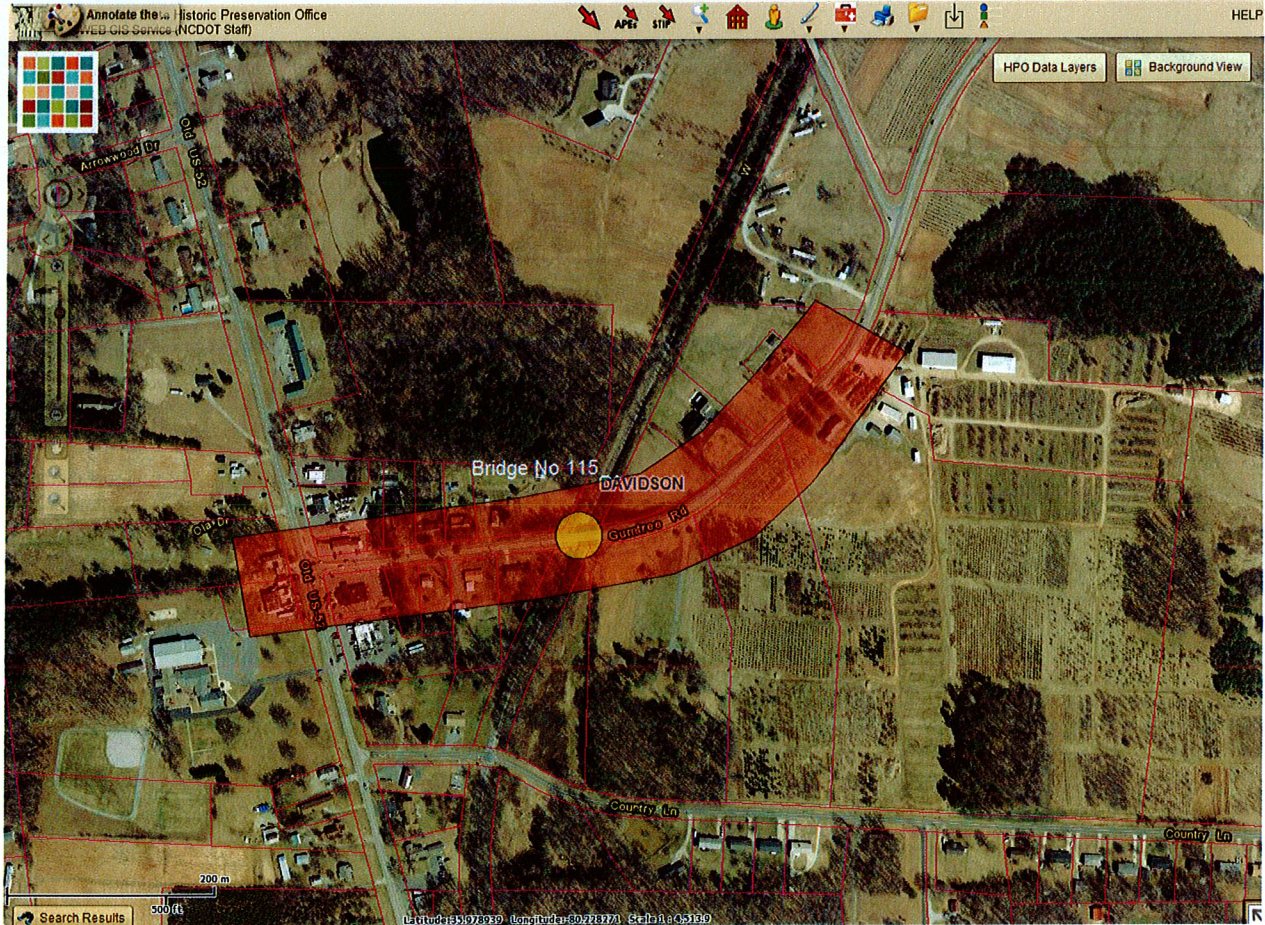


## FINDING BY NCDOT ARCHITECTURAL HISTORIAN

Historic Architecture and Landscapes – NO HISTORIC PROPERTIES PRESENT OR AFFECTED

*Shelby Reap*  
NCDOT Architectural Historian

*April 12, 2018*  
Date



18-01-0004



**HISTORIC ARCHITECTURE AND LANDSCAPES**  
**\*\*SURVEY REQUIRED FORM\*\***

This form only pertains to Historic Architecture and Landscapes for this project. It is not valid for Archaeological Resources. You must consult separately with the Archaeology Group.

**PROJECT INFORMATION**

<b>Project No:</b>	B-5765	<b>County:</b>	Davidson
<b>WBS No.:</b>	45721.3.1	<b>Document Type:</b>	MCC
<b>Fed. Aid No:</b>		<b>Funding:</b>	<input checked="" type="checkbox"/> State <input type="checkbox"/> Federal
<b>Federal Permit(s):</b>	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<b>Permit Type(s):</b>	USACE
<b><u>Project Description:</u></b> Replace Bridge No 115 on SR 1711 (Gumtree Rd) over Winston-Salem Railroad			

**SUMMARY OF HISTORIC ARCHITECTURE AND LANDSCAPES REVIEW**

**Description of review activities, results, and conclusions:**

On January 24, 2018 a search of NC HPOWEB GIS Service map reveals that the in the Area of Potential Effects (APE) contains a number of properties over 50 years of age. An architectural historian will need to conduct a site survey to determine if any of these properties warrant a full National Register evaluation.

**SUPPORT DOCUMENTATION**

Map(s)    Previous Survey Info.    Photos    Correspondence    Design Plans

**FINDING BY NCDOT ARCHITECTURAL HISTORIAN**

Historic Architecture and Landscapes -- **\*\*SURVEY REQUIRED\*\***

**Shelby Reap**

**January 24, 2018**

NCDOT Architectural Historian

Date

**Anticipated Fieldwork Completion Date:** July 24, 2018