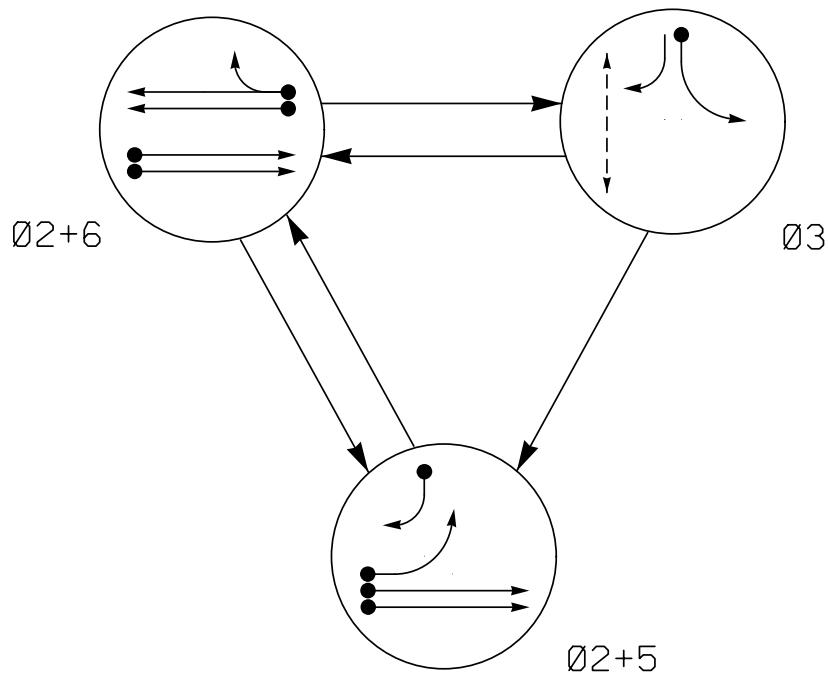


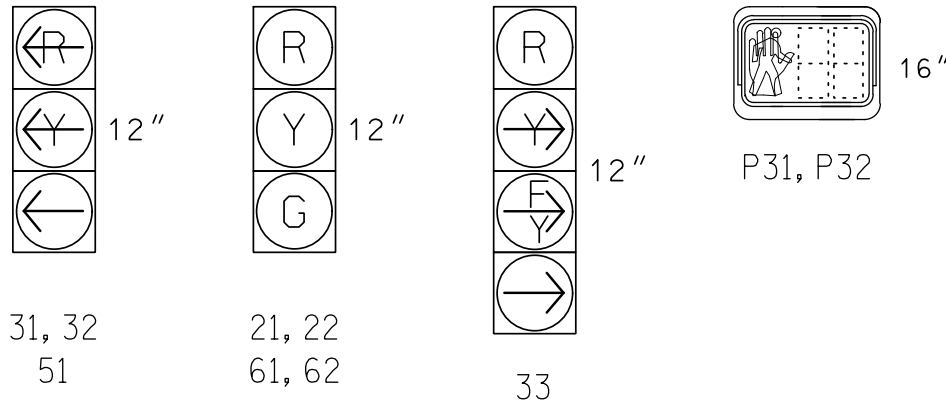
PHASING DIAGRAM



SIGNAL FACE	PHASE			
	02+5	02+6	03	FLASH
21, 22	G	G	R	R
31, 32	R	R	L	R
33	L	R	F	R
51	L	R	R	R
61, 62	R	G	R	R
P31, P32	DW	DW	W	DRK

SIGNAL FACE I.D.

All Heads L.E.D.



MAXTIME DETECTOR INSTALLATION CHART											
DETECTOR						PROGRAMMING					
LOOP	SIZE (FT)	DISTANCE FROM STOPBAR (FT)	TURNS	NEW LOOP	CALL PHASE	DELAY TIME	EXTEND TIME	EXTEND	ADDED INITIAL	CALL	DELAY DURING GREEN
3A	6X40	0	*	*	3	3	-	X	-	X	*
5A	6X40	0	*	*	5	-	-	X	-	X	*
5B	6X40	0	*	*	5	15	-	X	-	X	*

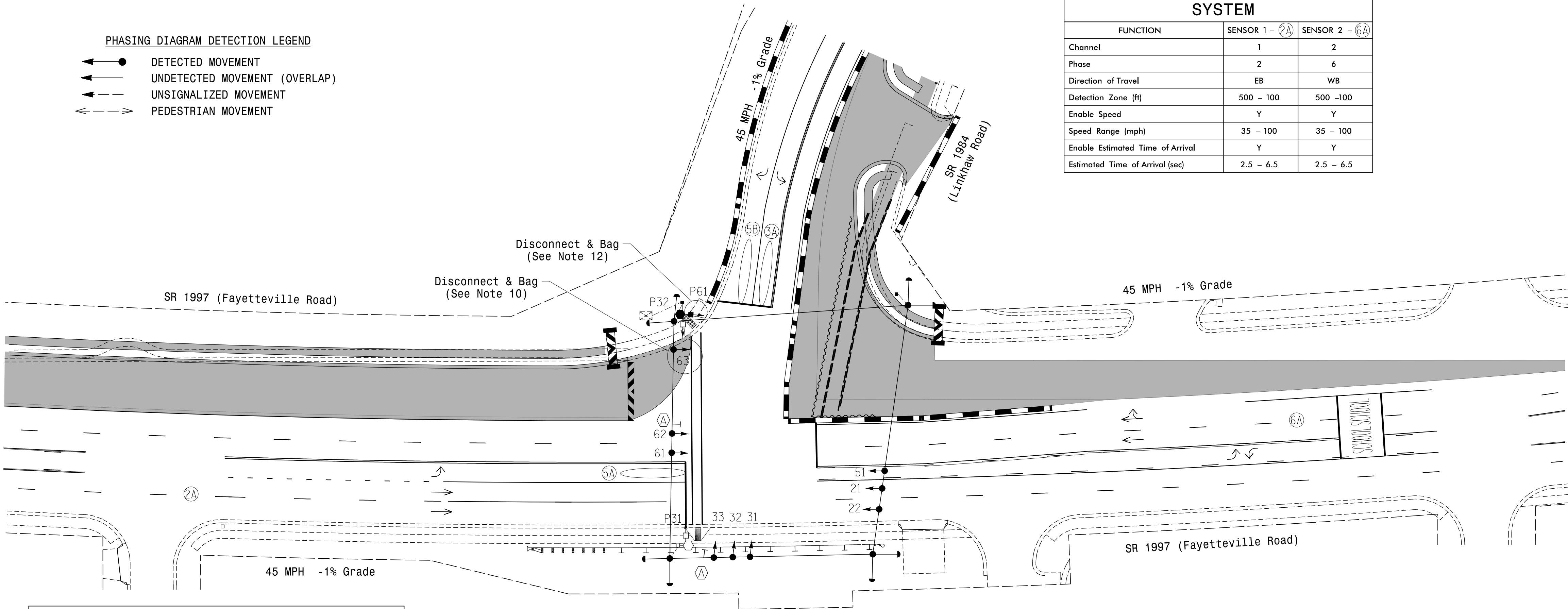
* Multi-zone Microwave Detection Zone

RADAR DETECTION SYSTEM

FUNCTION	SENSOR 1 - 2A	SENSOR 2 - 6A
Channel	1	2
Phase	2	6
Direction of Travel	EB	WB
Detection Zone (ft)	500 - 100	500 - 100
Enable Speed	Y	Y
Speed Range (mph)	35 - 100	35 - 100
Enable Estimated Time of Arrival	Y	Y
Estimated Time of Arrival (sec)	2.5 - 6.5	2.5 - 6.5

PHASING DIAGRAM DETECTION LEGEND

- DETECTED MOVEMENT
- UNDETECTED MOVEMENT (OVERLAP)
- UNSIGNALIZED MOVEMENT
- PEDESTRIAN MOVEMENT



MAXTIME TIMING CHART

FEATURE	PHASE			
	2	3	5	6
Walk *	-	14	-	-
Ped Clear *	-	31	-	-
Min Green	12	7	12	12
Passage *	2.0	2.0	2.0	2.0
Max 1 *	90	36	20	90
Yellow Change	4.6	3.0	3.0	4.6
Red Clear	1.3	3.9	1.9	1.1
Added Initial *	-	-	-	-
Maximum Initial *	-	-	-	-
Time Before Reduction *	-	-	-	-
Time To Reduce *	-	-	-	-
Minimum Gap	-	-	-	-
Advance Walk	-	7	-	-
Non Lock Detector	-	X	X	-
Vehicle Recall	MIN RECALL	-	-	MIN RECALL
Dual Entry	-	-	-	-

* These values may be field adjusted. Do not adjust Min Green and Extension times for phases 2 and 6 lower than what is shown. Min Green for all other phases should not be lower than 4 seconds.

3 Phase
Fully Actuated
Signal System #D06-17 Lumberton
US 301 and SR 1997 (Fayetteville Road)

NOTES

- Refer to "Roadway Standard Drawings NCDOT" dated January 2024 and "Standard Specifications for Roads and Structures" dated January 2024.
- Do not program signal for late night flashing operation unless otherwise directed by the Engineer.
- Remove existing signal head numbered 34.
- Reposition existing signal heads numbered 31, 32, 33, 51, 61, and 62.
- Set all detector units to presence mode.
- Omit "WALK" and flashing "DON'T WALK" with no pedestrian calls.
- Program pedestrian heads to countdown the flashing "Don't Walk" time only.
- This intersection uses multi-zone microwave detection. Install detectors according to the manufacturer's instructions to achieve the desired detection.
- Maximum times shown in timing chart are for free-run operation only. Coordinated signal system timing values supersede these values.
- Bag and disconnect existing signal head numbered 63.
- Remove bags and reconnect signal heads numbered 31, 32 and 51.
- Bag and disconnect existing pedestrian signal head numbered P61.

LEGEND

- | PROPOSED | EXISTING |
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LEGEND

- | PROPOSED | EXISTING |
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Signal Upgrade
Temporary Design 4 - TMP Phase IV, Step 1

stv
STV Engineers, Inc.
900 West Trade St., Suite 715
Charlotte, NC 28202
(704) 372-1885
NC License Number F-0991

Prepared for the Offices of:

750 N. Greenfield Pkwy, Garner, NC 27529
SCALE
0 40
1" = 40'

SR 1997 (Fayetteville Road)
at
SR 1984 (Linkhaw Road)
Division 6 Robeson County Lumberton
PLAN DATE: Feb 2025 REVIEWED BY: H.M. Surti
PREPARED BY: R.L. Aristondo REVIEWED BY: T.M. Woody
REVISIONS INIT. DATE

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED
SEAL
NORTH CAROLINA PROFESSIONAL ENGINEER
RENT M. MOODY
DocuSigned by: Trent Moody
03/19/2025
DATE
SIC. INVENTORY NO. 06-0537T4