PAVEMENT SCHEDULE

FINAL PAVEMENT DESIGN: NOVEMBER 21, 2024

PROP. APPROX. 2.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 110 LBS. PER SQ. YD. IN EACH OF TWO LAYERS.

PROP. APPROX. 3.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD. IN EACH OF TWO LAYERS.

PROP. VAR. DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 110 LBS. PER SQ. YD. PER 1" DEPTH. TO BE PLACED IN LAYERS NOT LESS THAN 1" IN DEPTH OR GREATER THAN 1.5" IN DEPTH.

PROP. APPROX. 2.5" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 285 LBS PER SQUARE YARD.

PROP. APPROX. 4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 456 LBS PER SQUARE YARD.

PROP. VARIABLE DEPTH ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 114 LBS PER SQUARE YARD PER 1" DEPTH, TO BE PLACED IN LAYERS NOT LESS THAN 2.5" IN DEPTH OR GREATER THAN 4" IN DEPTH.

PROP. APPROX. 4" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YD.

PROP. VAR. DEPTH ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 114 LBS. PER SQ. YD. PER 1" DEPTH. TO BE PLACED IN LAYERS NOT LESS THAN 3" IN DEPTH OR GREATER THAN $5\frac{1}{2}$ " IN DEPTH.

PROPOSED 6" AGGREGATE BASE COURSE

PRIME COAT AT THE RATE OF 0.50 GAL. PER SQ. YARD.

CONCRETE EXPRESSWAY GUTTER

2'-6" CONCRETE CURB & GUTTER

EARTH MATERIAL

EXISTING PAVEMENT

INCIDENTAL MILLING

MILLING ASPHALT PAVEMENT, 2.5" DEPTH

VARIABLE DEPTH ASPHALT PAVEMENT (SEE WEDGING DETAIL)

VARIABLE DEPTH ASPHALT PAVEMENT (SEE WEDGING DETAIL)

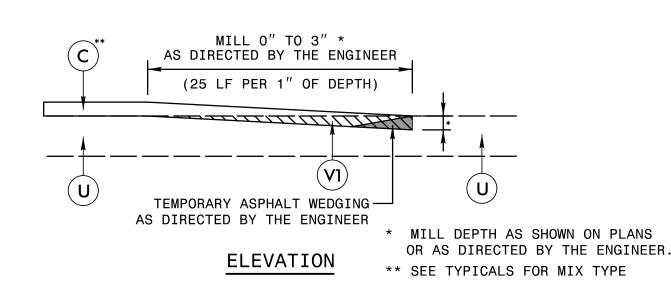
(SEE SINUSOIDAL SHOULDER RUMBLE STRIP DETAIL)

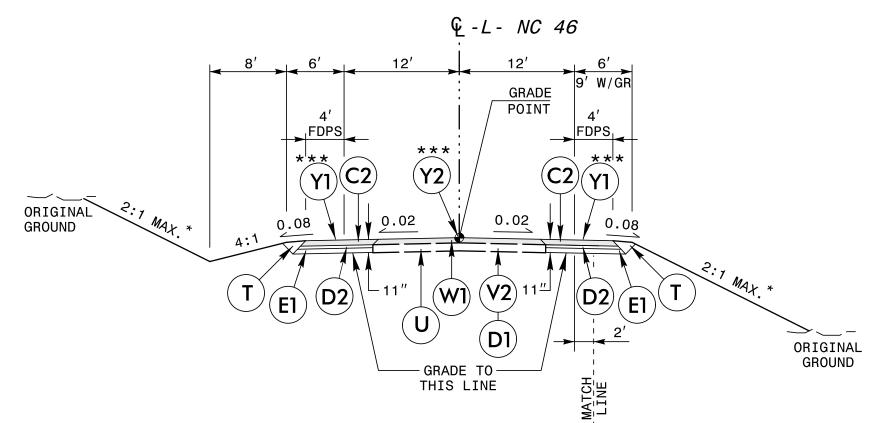
(SEE SINUSOIDAL CENTERLINE RUMBLE STRIPE DETAIL)

PROP. APPROX. 12" SINUSOIDAL RUMBLE STRIPS

PROP. APPROX. 18" SINUSOIDAL RUMBLE STRIPS

NOTE: PAVEMENT EDGE SLOPES AND TRENCH SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.



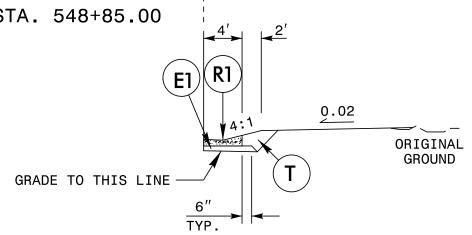


TYPICAL SECTION NO. 1

USE TYPICAL SECTION NO. 1

-L- STA. 12+09.79 TO STA. 162+29.36

-L- STA. 168+86.52 TO STA. 548+85.00

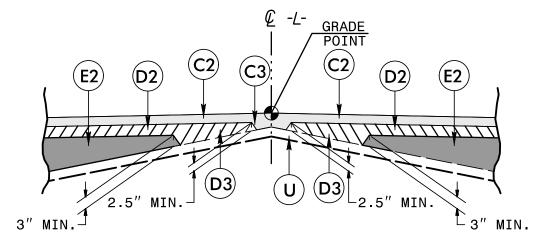


TYPICAL SECTION NO. 1A

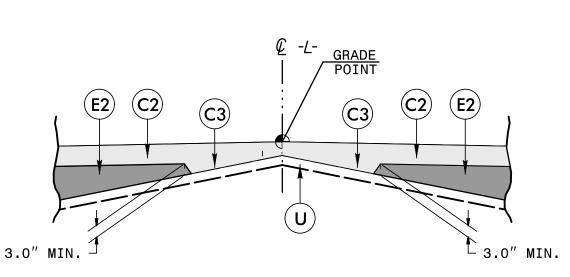
USE TYPICAL SECTION NO. 1A IN CONJUNCTION WITH TYPICAL SECTION NO 1

-L- STA. 109+35.00 TO STA. 111+95.00

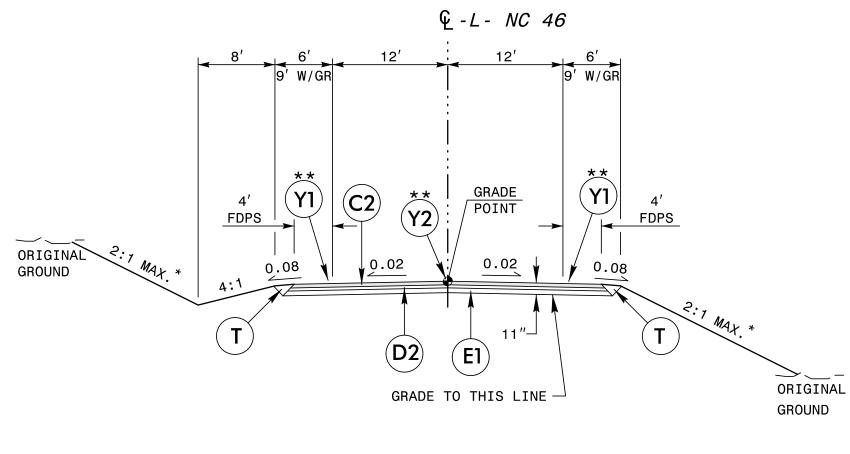
-L- STA. 317+20.00 TO STA. 318+25.00 (MIRROR)



DETAIL SHOWING METHOD OF WEDGING W



DETAIL SHOWING METHOD OF WEDGING W2



TYPICAL SECTION NO. 2

USE TYPICAL SECTION NO. 2

-L- STA. 162+29.36 TO STA. 168+86.52

PROJECT REFERENCE NO. SHEET NO. R-5739 2A-/ R/W SHEET NO.

ROADWAY DESIGN ENGINEER 19724

044590 8/13/2025

PAVEMENT DESIGN ENGINEER

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

706 HILLSBOROUGH ST. TGS ENGINEERS SUITE 200 RALEIGH, NC 27603 (919) 733-8887 | tgsengineers.com CORP. LICENSE NO.: C-0275

TYPICAL SECTION NO. 1 NOTES:

- * USE 3:1 MAXIMUM FILL SLOPE ON LOW SIDE OF SUPERELEVATED CURVES. SEE CROSS-SECTIONS FOR LOCATIONS.
- ** PROPOSED GUARDRAIL FROM -L- STA. 498+00.00 TO 499+50.00 TO BE CONSTRUCTED AT THE OFFSETS SHOWN ON THE PLANS.

LIMITS OF SINUSOIDAL RUMBLE STRIPS					
ROADWAY	Υ	LOCATION	STATION	STATION	
-L-	Y1	CL	12+09.79	28+85 +/-	
-L-	Y2	LT & RT	12+09.79	28+62 +/-	
-L-	Y2	LT	28+62 +/-	31+00 +/-	
-L-	Y1	CL	29+88 +/-	53+81 +/-	
-L-	Y2	LT & RT	31+00 +/-	56+42 +/-	
-L-	Y2	LT & RT	31+00 +/-	56+42 +/-	
-L-	Y2	LT	56+42 +/-	58+79 +/-	
-L-	Y2	LT & RT	58+79 +/-	161+79 +/	
-L-	Y1	CL	65+20 +/-	162+29.36	
-L-	Y2	RT	161+79 +/-	162+28 +/	
-L-	Y1	CL	168+86.52	491+70 +/	
-L-	Y1	CL	492+25 +/-	547+18 +/	
-L-	Y2	LT & RT	168+86.52	548+85.00	
- L -	Y2	LT	548+85.00	549+75.00	

NOTE: SINUSOIDAL RUMBLE STRIP LOCATIONS ARE THE RESPONSIBILITY OF THE ROADWAY DESIGN ENGINEER ONLY. THE PAVEMENT DESIGN ENGINEER'S DOES NOT APPLY TO THE RUMBLE STRIP LOCATION TABLES.

TYPICAL SECTION NO. 2 NOTES:

* USE 3:1 MAXIMUM FILL SLOPE ON LOW SIDE OF SUPERELEVATED CURVES.SEE CROSS-SECTIONS FOR LOCATIONS.

*	LIN	MITS	OF SINUSOI	SINUSOIDAL RUMBLE STRIPS		
	ROADWAY	Υ	LOCATION	STATION	STATION	
	-L-	Y1	CL	162+29.36	162+50 +/-	
	-L-	Y1	CL	163+64 +/-	168+86.52	
	-L-	Y2	RT	163+80 +/-	164+87 +/-	
	-L-	Y2	LT & RT	164+87 +/-	168+86.52	

TIE-IN MILLING DETAIL