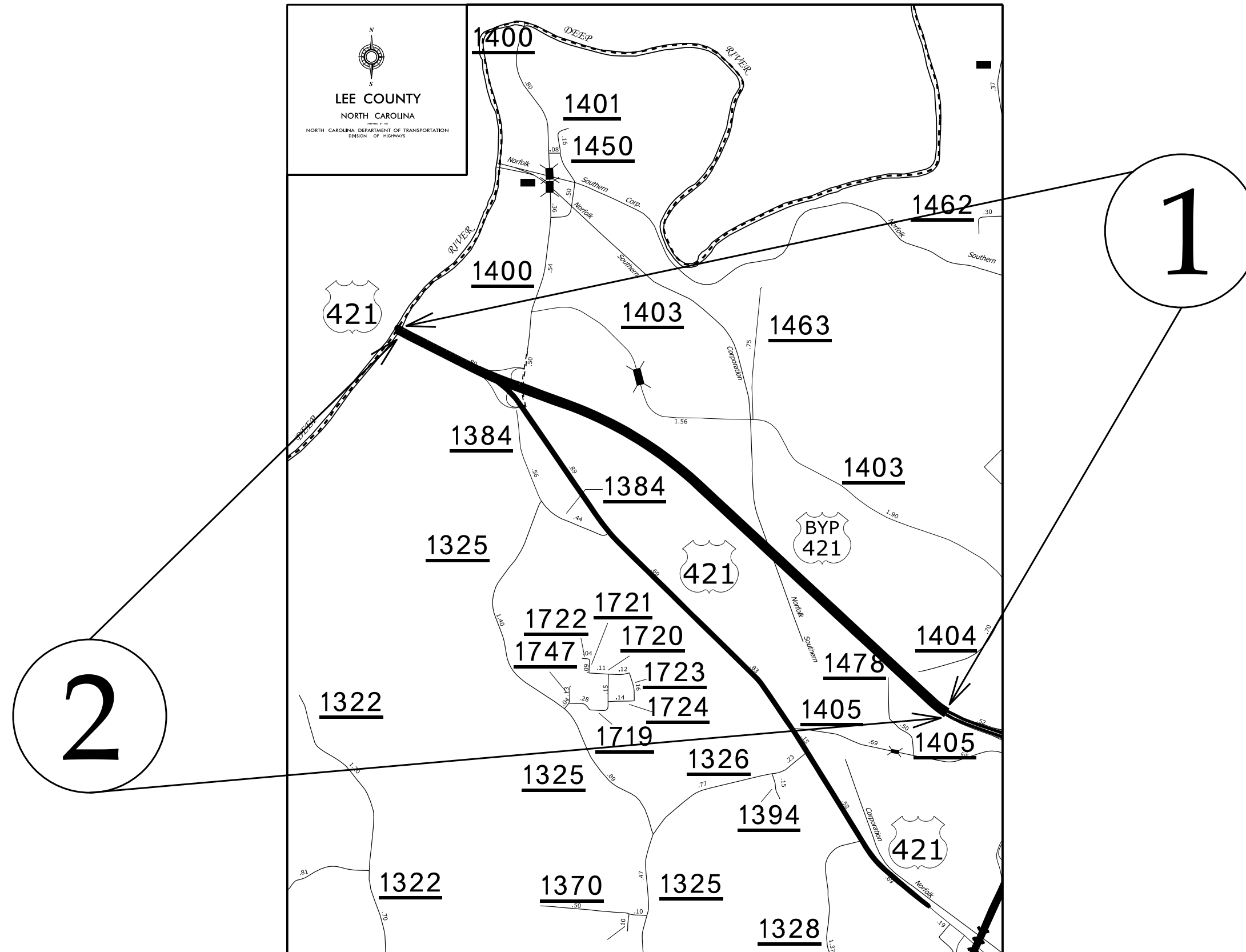
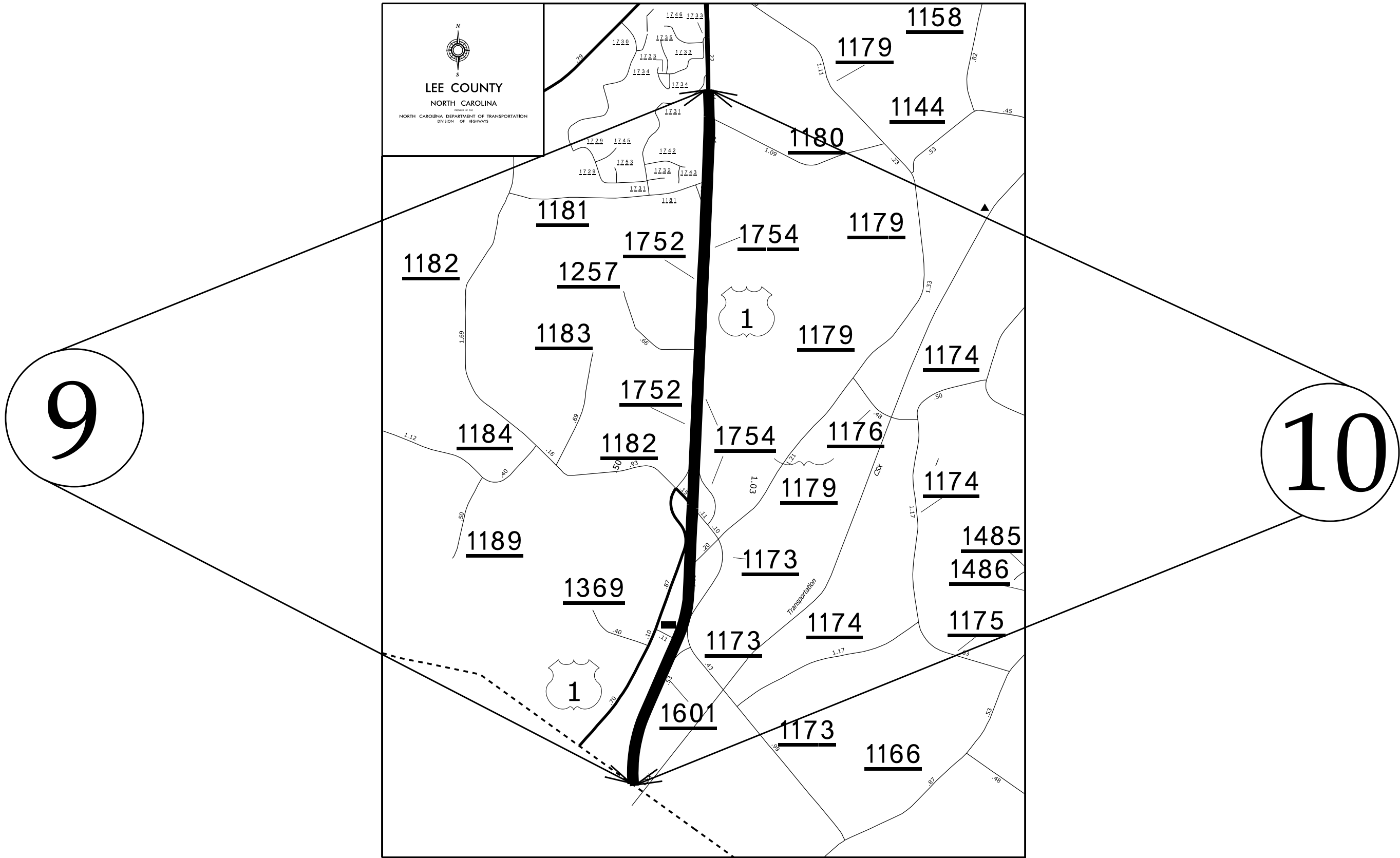


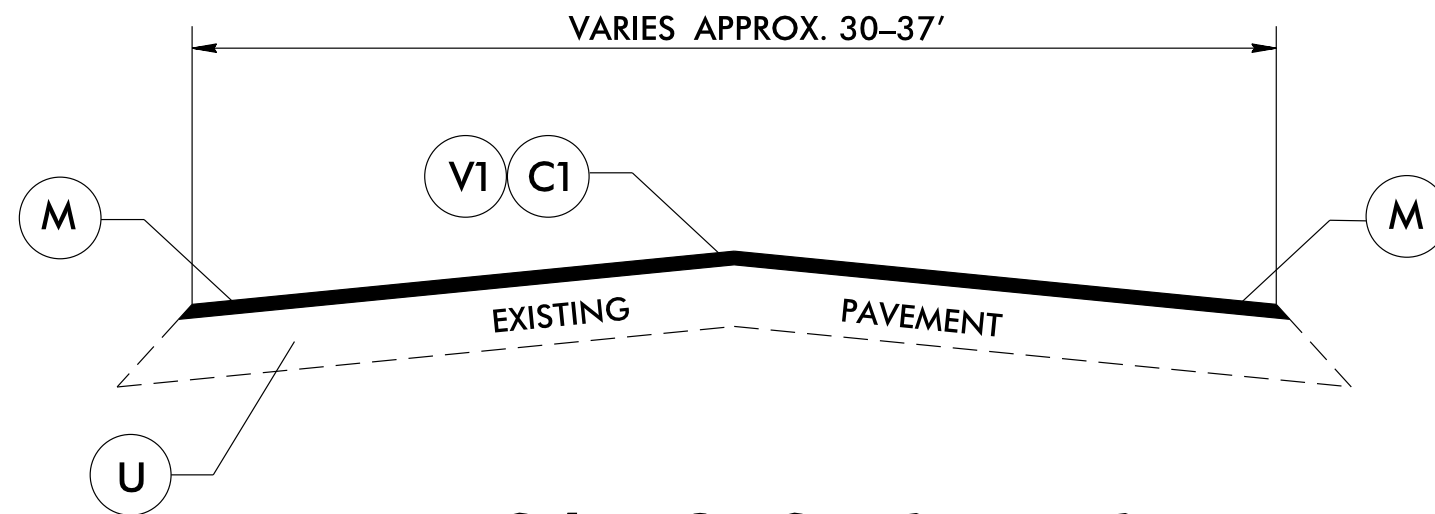
LEE COUNTY RESURFACING

Map 1 & 2



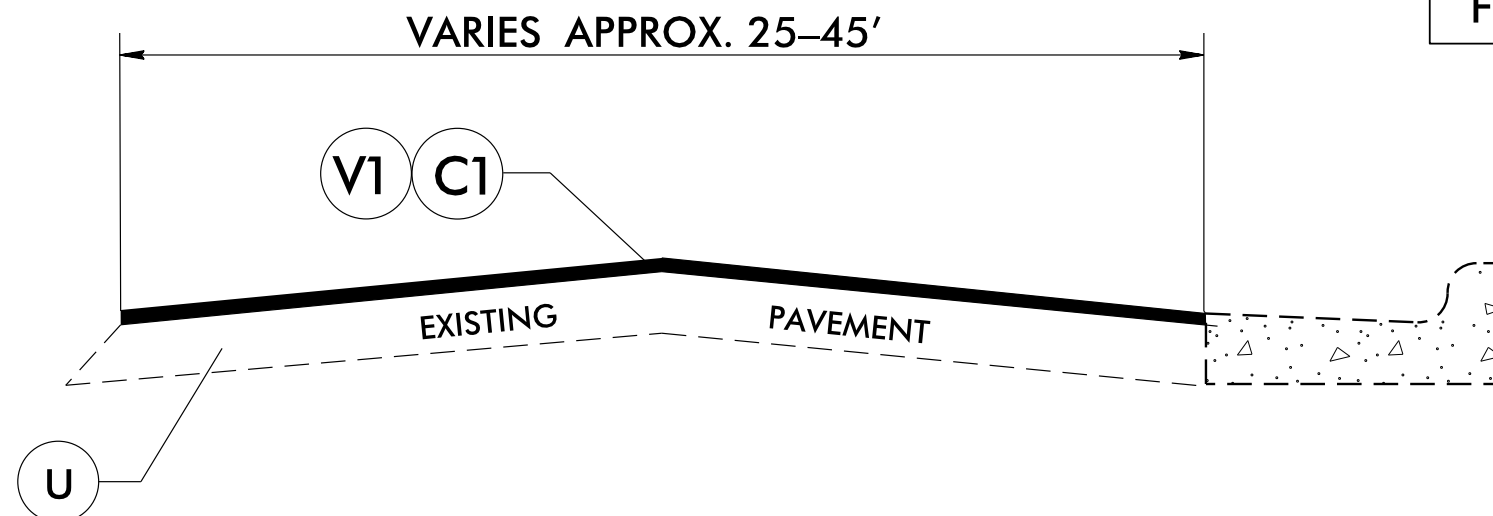
Map 9, 10



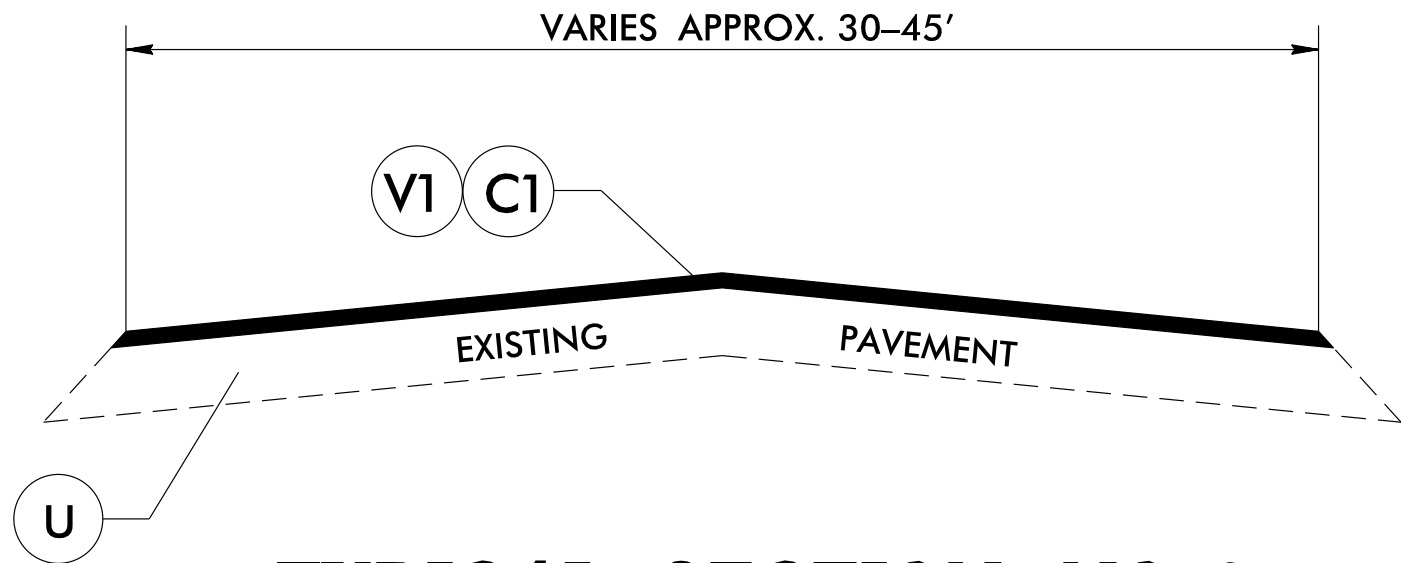


TYPICAL SECTION NO. 1
USE FOR MAPS 1-2, 9-10

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
U	EXISTING PAVEMENT.
V1	MILLING 1.5" IN DEPTH
M	MILLED RUMBLE STRIPS
F	FOG SEAL

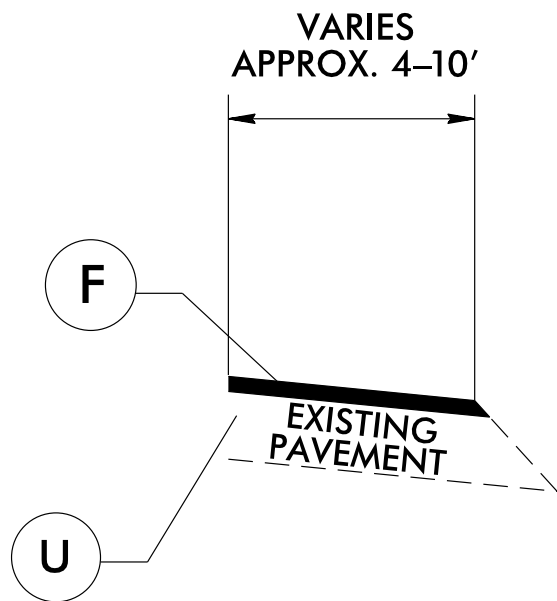


TYPICAL SECTION NO. 2
USE FOR MAP 1-6, 8-10



TYPICAL SECTION NO. 3
USE FOR MAPS 5-7

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
U	EXISTING PAVEMENT.
V1	MILLING 1.5" IN DEPTH
M	MILLED RUMBLE STRIPS
F	FOG SEAL

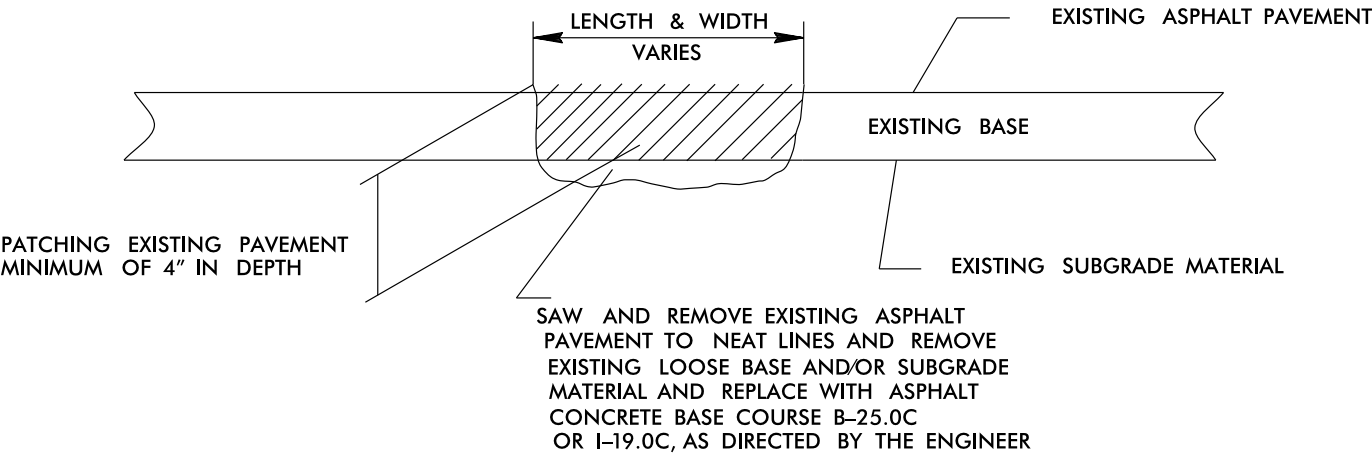


SHOULDER DETAIL
USE FOR MAPS 1-3, 5-8

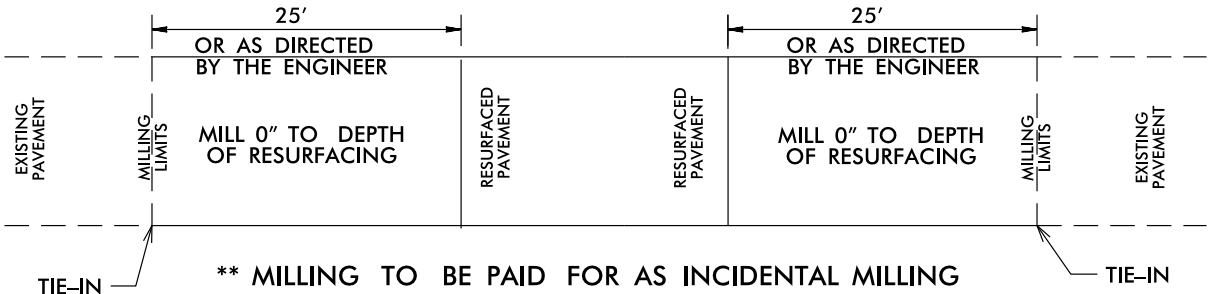
NOTE* – CURB SECTIONS SHALL BE FULL WIDTH RESURFACING.

DETAILS

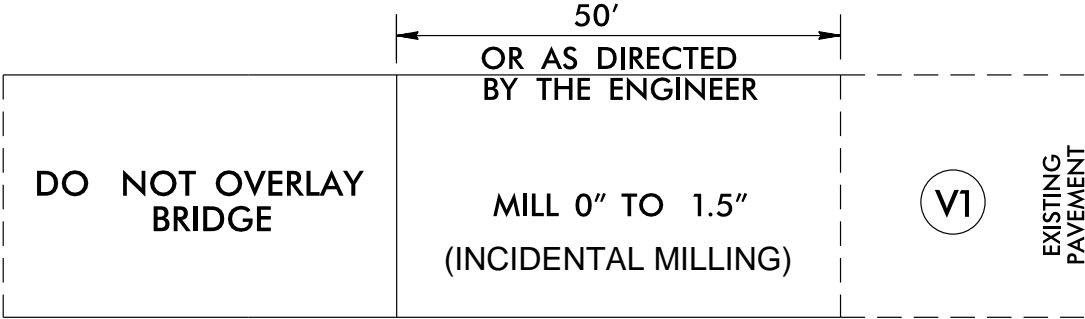
DETAILS OF PATCHING EXISTING PAVEMENT PRIOR TO RESURFACING



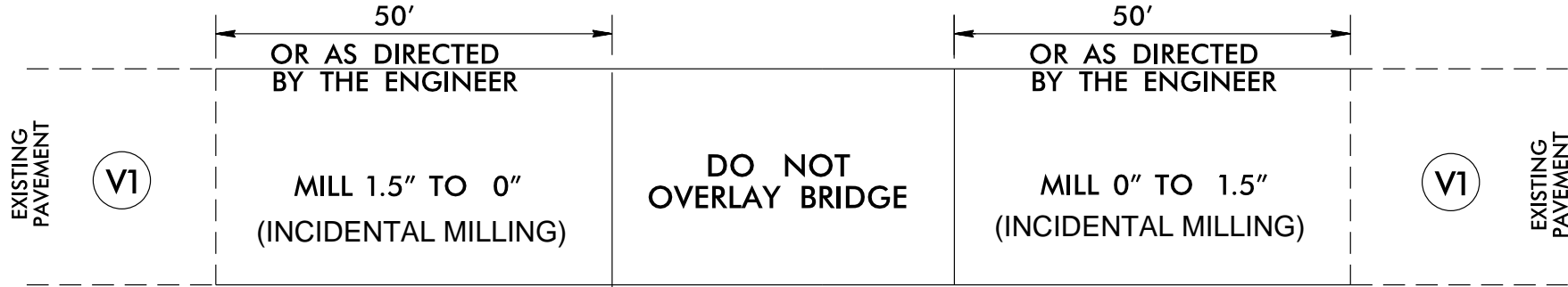
PAVEMENT TIE-IN DETAIL



BRIDGE DETAILS

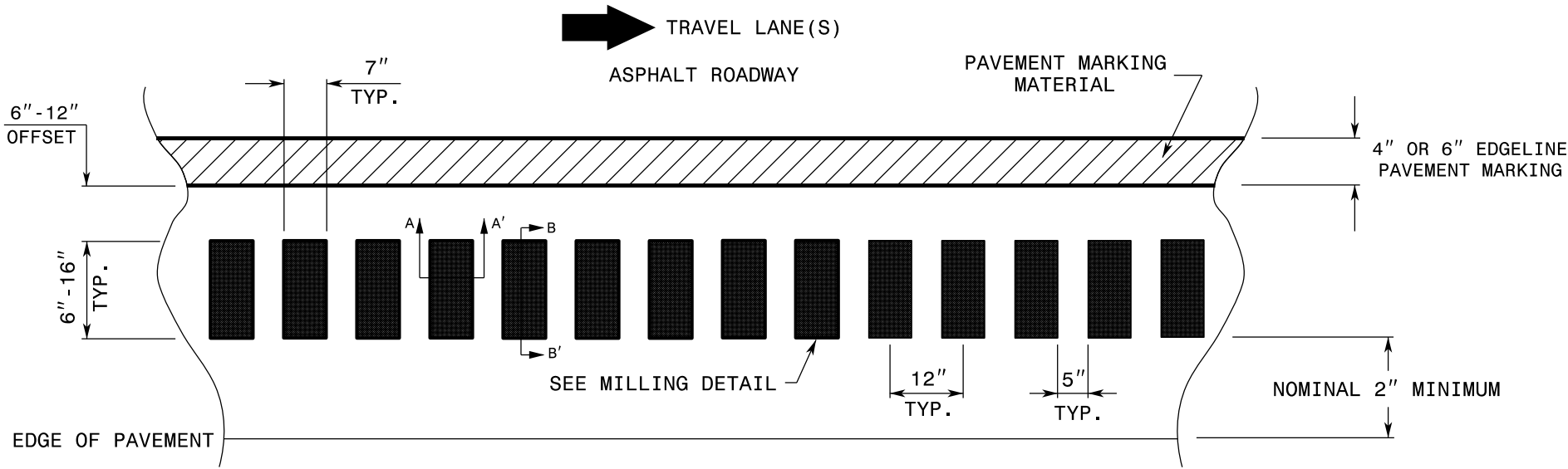


BRIDGE DRAWING FOR
MAP #1-2 421 BRIDGES, 9-10 US-1 BRIDGES

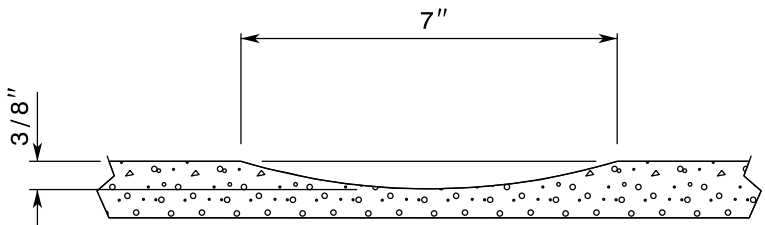


BRIDGE DRAWING FOR
MAP #1-2 US 421 BRIDGES

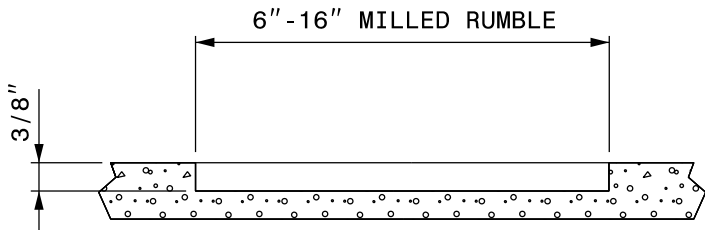
See Table 1 within Rumble Strip
Policy for Design Guidance



MILLING DETAIL:



SECTION A-A'



SECTION B-B'

REFERENCE DRAWING ID: Trad.Strip

STATE OF
NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

ROADWAY DETAIL DRAWING FOR
RUMBLE STRIPS / STRIPES
TRADITIONAL SHOULDER RUMBLE STRIP

SHEET 1 OF 9

PROJECT NO.	SHEET NO.	TOTAL NO.
2026CPT.08.01.10531.1	11	13

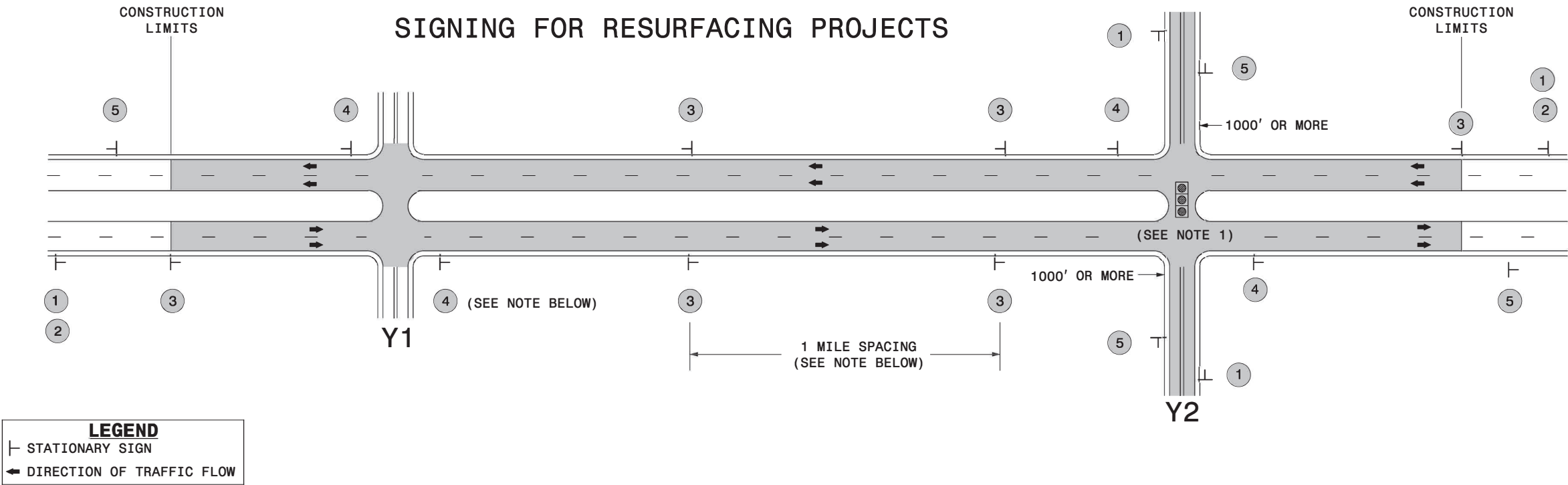
SUMMARY OF QUANTITIES

												1297000000-E	1330000000-E	1523000000-E	1524000000-E	1575000000-E	1704000000-E	1820000000-E	1841000000-E	5255000000-N
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	BEGIN MP	END MP	1.5" MILLING	INCIDENTAL MILLING	SURFACE COURSE, S9.5C	LEVELING COURSE, S9.5C	ASPHALT BINDER FOR PLANT MIX	PATCHING EXISTING PAVEMENT	ASPHALT SURFACE TREATMENT, FOG SEAL	MILLED RUMBLE STRIPS	PORTABLE LIGHTING
								MI	FT			SY	SY	TONS	TONS	TONS	TONS	SY	LF	LS
2026CPT.08.01.10531.1	Lee	1	US-421 N	FROM PVMT JOINT NORTH OF US-421 / US-1 INTERCHANGE TO BRIDGE AT CHATHAM COUNTY LINE	1,2	2	MD	3.63	38	11.345	14.975	63,890	970	5,455	200	337	80	17,400.00	19,166	*
TOTAL FOR MAP NO. 1								3.63				63,890	970	5,455	200	337	80	17,400.00	19,166	
2026CPT.08.01.10531.1	Lee	2	US-421 S	FROM PVMT JOINT NORTH OF US-421 / US-1 INTERCHANGE TO BRIDGE AT CHATHAM COUNTY LINE	1,2	2	MD	3.72	38	0.055	3.775	65,475	970	5,585		333	70	18,215.00	19,642	*
TOTAL FOR MAP NO. 2								3.72				65,475	970	5,585		333	70	18,215.00	19,642	
2026CPT.08.01.10531.1	Lee	3	LP-6504 OI	FROM BEGINNING OF RAMP US-421 N TO INTERSECTION AT BUS 421	2	2		0.38	25	0	0.38	5,350	140	465		28	5	420.00		
TOTAL FOR MAP NO. 3								0.38				5,350	140	465		28	5	420.00		
2026CPT.08.01.10531.1	Lee	4	LP-6505 OI	FROM RAMP TO INTERSECTION AT BUS 421	2	2		0.04	25	0.34	0.38	590	140	65		4				
TOTAL FOR MAP NO. 4								0.04				590	140	65		4				
2026CPT.08.01.10531.1	Lee	5	RMP-6506 OI	FROM INTERSECTION AT BUS 421 TO END OF RAMP AT US-421 N	2,3	2		0.56	35	0	0.56	8,545	250	745		44		2,925.00		
TOTAL FOR MAP NO. 5								0.56				8,545	250	745		44		2,925.00		
2026CPT.08.01.10531.1	Lee	6	RMP-6501 OI	FROM BEGINNING OF RAMP US-421 S TO INTERSECTION AT BUS 421	2,3	2		0.46	35	0	0.46	7,290	195	635		37		2,165.00		
TOTAL FOR MAP NO. 6								0.46				7,290	195	635		37		2,165.00		
2026CPT.08.01.10531.1	Lee	7	RMP-6502 OI	FROM RAMP TO INTERSECTION AT BUS 421 JUST PAST GORE	3	2		0.17	30	0.38	0.55	2,395	170	220		13		645.00		
TOTAL FOR MAP NO. 7								0.17				2,395	170	220		13		645.00		
2026CPT.08.01.10531.1	Lee	8	LP-6503 OI	FROM BEGINNING OF RAMP AT BUS 421 TO END OF RAMP AT US-421 S	2	2		0.37	25	0	0.37	4,995	170	440		26		370.00		
TOTAL FOR MAP NO. 8								0.37				4,995	170	440		26		370.00		
2026CPT.08.01.10531.1	Lee	9	US-1 S	FROM MOORE COUNTY LINE TO PVMT JOINT NORTH OF WILDLIFE ROAD (SR 1180)	1,2	2	MD	4.022	32	18.028	22.05	75,510	11,580	7,320		436	80		42,475	*
TOTAL FOR MAP NO. 9								4.022				75,510	11,580	7,320		436	80		42,475	
2026CPT.08.01.10531.1	Lee	10	US-1 N	FROM MOORE COUNTY LINE TO PVMT JOINT NORTH OF WILDLIFE ROAD (SR 1180)	1,2	2	MD	4.038	32	0	4.038	75,810	8,785	7,110		423	80		42,641	*
TOTAL FOR MAP NO. 10								4.038				75,810	8,785	7,110		423	80		42,641	
TOTAL FOR PROJ NO. 2026CPT.08.01.10531.1								17.39				309,850	23,370	28,040	200	1,681	315	42,140.00	123,924	1
GRAND TOTAL								17.39				309,850	23,370	28,040	200	1,681	315	42,140.00	123,924	1

PROJECT NO.	SHEET NO.	TOTAL NO.
2026CPT.08.01.10531.1	13	13

THERMOPLASTIC AND PAINT QUANTITIES

												4775000000-E	4810000000-E		4835000000-E	4840000000-N		4845000000-N								4850000000-E	4895000000-N		
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	BEGIN MP	END MP	COLD APPLIED PLASTIC PAVEMENT MARKING LINES, TYPE II (6") LF	4" WHITE PAINT	4" YELLOW PAINT	24" WHITE PAINT	PAINT MSG STOP	PAINT MSG ONLY	PAINT RAMP ARROW	PAINT LT ARROW	PAINT RT ARROW	PAINT YIELD LINETRIANGLE	PAINT MERGE ARROW	PAINT STR ARROW	PAINT STR & RT ARROW	4" LINE REMOVAL	GENERIC PAVEMENT MARKING ITEM POLYCARBONATE H-SHAPED MARKERS C&R	GENERIC PAVEMENT MARKING ITEM POLYCARBONATE H-SHAPED MARKERS Y&Y		
								MI	FT				LF	LF	LF	EA	EA	EA	EA	EA	EA	EA	EA	EA				EA	EA
2026CPT.08.01.10531.1	Lee	1	US-421 N	FROM PVMT JOINT NORTH OF US-421 / US-1 INTERCHANGE TO BRIDGE AT CHATHAM COUNTY LINE	1,2	2	MD	3.63	38	11.345	14.975	1,175	18,138	19,166											1,175	401			
TOTAL FOR MAP NO. 1								3.63				1,175	18,138	19,166											1,175	401			
2026CPT.08.01.10531.1	Lee	2	US-421 S	FROM PVMT JOINT NORTH OF US-421 / US-1 INTERCHANGE TO BRIDGE AT CHATHAM COUNTY LINE	1,2	2	MD	3.72	38	0.055	3.775	1,175	18,355	19,700											1,175	371			
TOTAL FOR MAP NO. 2								3.72				1,175	18,355	19,700											1,175	371			
2026CPT.08.01.10531.1	Lee	3	LP-6504 OI	FROM BEGINNING OF RAMP US-421 N TO INTERSECTION AT BUS 421	2	2		0.38	25	0	0.38		1,695	900	20	4		4	1						2	4			
TOTAL FOR MAP NO. 3								0.38					1,695	900	20	4		4	1						2	4			
2026CPT.08.01.10531.1	Lee	4	LP-6505 OI	FROM RAMP TO INTERSECTION AT BUS 421	2	2		0.04	25	0.34	0.38		30			4				1	10				3				
TOTAL FOR MAP NO. 4								0.04					30			4				1	10				3				
2026CPT.08.01.10531.1	Lee	5	RMP-6506 OI	FROM INTERSECTION AT BUS 421 TO END OF RAMP AT US-421 N	2,3	2		0.56	35	0	0.56		3,455	1,260							5	7			65				
TOTAL FOR MAP NO. 5								0.56					3,455	1,260							5	7			65				
2026CPT.08.01.10531.1	Lee	6	RMP-6501 OI	FROM BEGINNING OF RAMP US-421 S TO INTERSECTION AT BUS 421	2,3	2		0.46	35	0	0.46		3,385	2,015	30								1		20	4			
TOTAL FOR MAP NO. 6								0.46					3,385	2,015	30								1		20	4			
2026CPT.08.01.10531.1	Lee	7	RMP-6502 OI	FROM RAMP TO INTERSECTION AT BUS 421 JUST PAST GORE	3	2		0.17	30	0.38	0.55		1,050	415								3			13				
TOTAL FOR MAP NO. 7								0.17					1,050	415								3			13				
2026CPT.08.01.10531.1	Lee	8	LP-6503 OI	FROM BEGINNING OF RAMP AT BUS 421 TO END OF RAMP AT US-421 S	2	2		0.37	25	0	0.37		2,150	940							6	2			17				
TOTAL FOR MAP NO. 8								0.37					2,150	940							6	2			17				
2026CPT.08.01.10531.1	Lee	9	US-1 S	FROM MOORE COUNTY LINE TO PVMT JOINT NORTH OF WILDLIFE ROAD (SR 1180)	1,2	2	MD	4.022	32	18.028	22.05		32,615	22,435	40		4		15	5			8		560	15			
TOTAL FOR MAP NO. 9								4.022					32,615	22,435	40		4		15	5			8		560	15			
2026CPT.08.01.10531.1	Lee	10	US-1 N	FROM MOORE COUNTY LINE TO PVMT JOINT NORTH OF WILDLIFE ROAD (SR 1180)	1,2	2	MD	4.038	32	0	4.038		31,200	22,195	70		12		8	9			8	2	435	35			
TOTAL FOR MAP NO. 10								4.038					31,200	22,195	70		12		8	9			8	2	435	35			
TOTAL FOR PROJ NO. 2026CPT.08.01.10531.1								17.39				2,350	112,073	89,026	160	8	16	4	24	15	21	12	17	2	2,350.000	1,887	58		
													201,099			24					95					1,945			
GRAND TOTAL								17.39				2,350	112,073	89,026	160	8	16	4	24	15	21	12	17	2	2,350.000	1,887	58		
													201,099			24					95					1,945			



MAINLINE (-L-) SIGNING

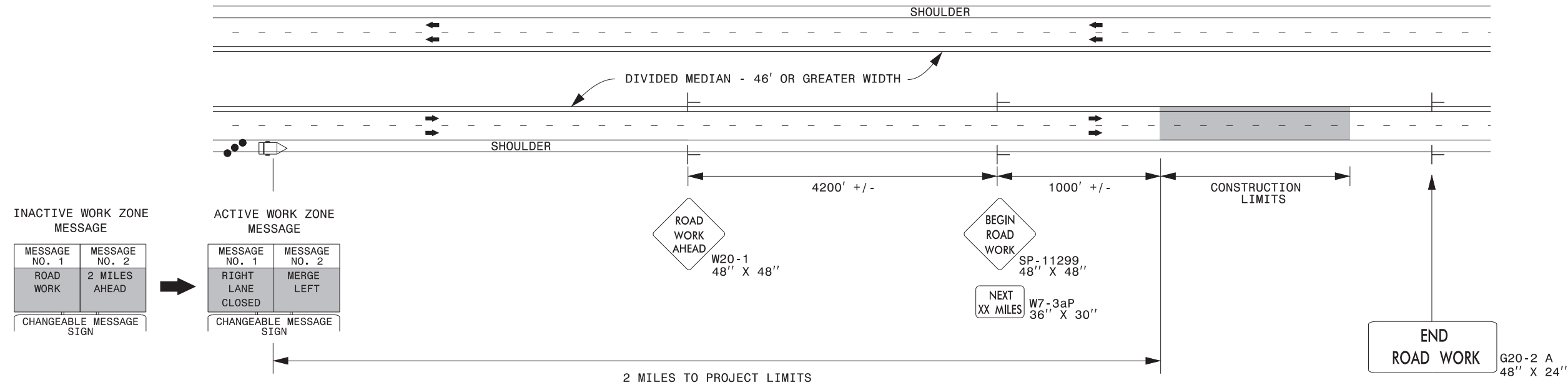
-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	<div>1</div> <div>2</div> <div><div>ROAD WORK AHEAD</div><div>W20-1 48" X 48"</div><div>NEXT XX MILES</div><div>W7-3aP 24" X 18"</div></div> <div>PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</div> <div>#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER.(NO FRACTIONAL OR DECIMAL NUMBERS)</div>	<div>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</div> <div>1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS</div> <div>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</div> <div><div><div>ROAD WORK AHEAD</div><div>W20-1 48" X 48"</div></div><div><div>ROAD WORK AHEAD</div><div>W20-7 A 48" X 48"</div></div></div> <div>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</div> <div>NOTES:</div> <div>1) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION.</div>
	<div>3</div> <div><div>LOWSOFT SHOULDER</div><div>SP 13107 48" X 48"</div></div> <div>PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. IF NO -Y- LINES EXIST, PLACE 2ND SET 1/2 MILE FROM THE CONSTRUCTION LIMITS AND THEN SPACE 1 MILE THEREAFTER.</div>	
	<div>4</div> <div><div>ROAD UNDER CONST</div><div>SP 13106 48" X 48"</div></div> <div>THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</div>	
	<div>5</div> <div><div>END ROAD WORK</div><div>G20-2 A 48" X 24"</div></div> <div>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.</div>	

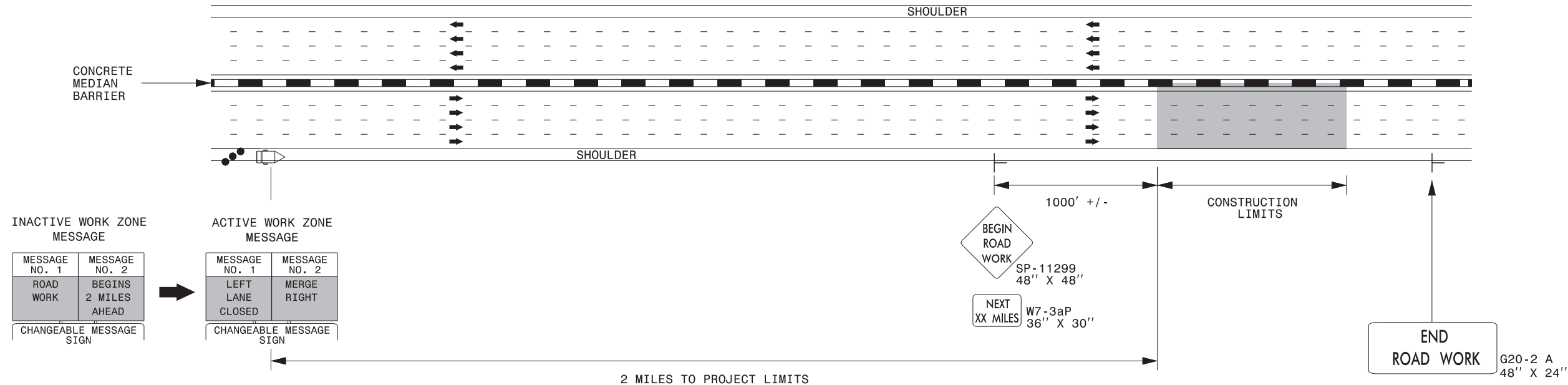


RESURFACING
ADVANCE WARNING SIGNS
FOR RURAL AND SUBURBAN
MULTI-LANE ROADWAYS
W/ SHOULDER SECTIONS

DIVIDED MEDIANS WITH WIDTHS 46' OR GREATER



DIVIDED MEDIANS WITH WIDTHS LESS THAN 46' OR WITH PERMANENT MEDIAN BARRIER



NOTES:

- 1) LATERAL CLEARANCE AT ALL SIGN LOCATIONS SHALL BE 6' AS MEASURED FROM THE EDGE OF PAVEMENT.
- 2) MOUNT SIGNS THAT ARE LARGER THAN 10 SQUARE FEET IN AREA ON TWO OR MORE WOOD OR U-CHANNEL SUPPORTS. PERFORATED SQUARE TUBING SUPPORT SYSTEMS MAY SUPPORT LARGER AREAS ON A SINGLE SUPPORT. FOLLOW MANUFACTURER'S RECOMMENDATIONS. THESE SYSTEMS SHALL BE NCHRP 350 COMPLIANT AND NCDOT APPROVED.
- 3) FOR MEDIAN WIDTHS LESS THAN 46' (MEASURED EDGELINE TO EDGELINE) USE THE BOTTOM DRAWING.
- 4) IF STATIONARY GENERAL WARNING SIGNS ARE USED, THEY WILL BE PAID FOR PER SECTION 104 OF THE NCDOT STANDARD SPECIFICATIONS AS EXTRA WORK.
- 5) INSTALL "ROAD WORK AHEAD" (W20-1) ALONG ENTRANCE RAMP 500' PRIOR TO RAMP TERMINAL, AND "END ROAD WORK" (G20-2a) AT THE END OF EXIT RAMP WITHIN THE WORK ZONE.
- 6) IF MILLED AREAS ARE NOT PAVED BACK BY THE END OF THE WORK DAY, PORTABLE SIGNS SHALL BE USED TO WARN DRIVERS OF THE PRESENT CONDITIONS. THESE ARE TO INCLUDE, BUT NOT LIMITED TO "ROUGH ROAD" W8-8, "UNEVEN LANES" W8-11, "GROOVED PAVEMENT" W8-15 w/MOTORCYCLE PLAQUE MOUNTED BELOW. THESE ARE TO BE DOUBLE INDICATED ON MULTI-LANE ROADWAYS WITH SPEED LIMITS 45 MPH AND GREATER AND WITH DIVIDED MEDIANS OF 46' OR GREATER. THESE PORTABLE SIGNS ARE INCIDENTAL TO THE OTHER ITEMS OF WORK INCLUDED IN THE TEMPORARY TRAFFIC CONTROL (LUMP SUM) PAY ITEM.

LEGEND

- CHANGEABLE MESSAGE SIGN (CMS)
- STATIONARY SIGN
- DIRECTION OF TRAFFIC FLOW
- TRAFFIC DRUM



RESURFACING ADVANCE
WARNING SIGNS FOR
HIGH SPEED FACILITIES
≥ 60 MPH