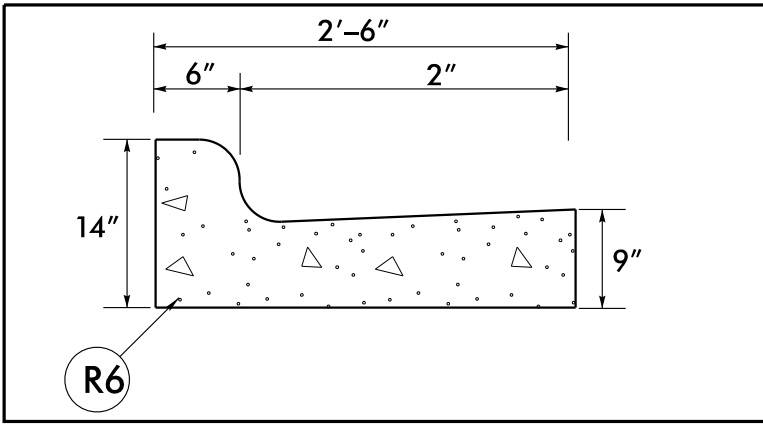


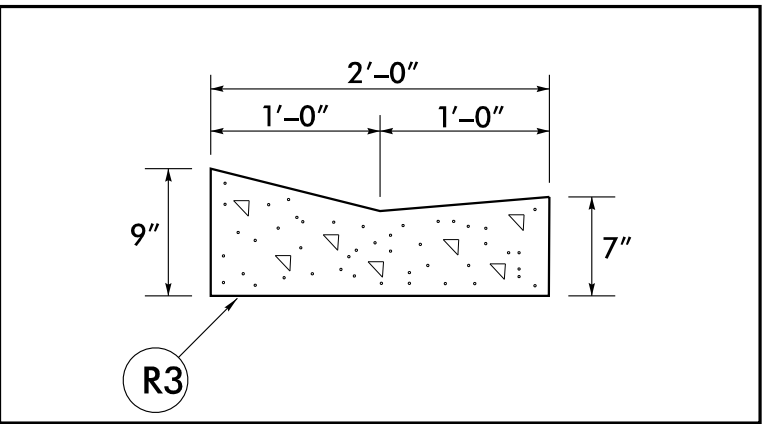
FINAL PAVEMENT SCHEDULE			
A1	9" CONCRETE TRUCK APRON WITH WIRE MESH 4x4-W3.5 OR 6x6-W5 x W5 15' MINIMUM RADIAL JOINT SPACING	R3	2' MODIFIED VALLEY GUTTER (SEE DETAIL NO.2)
C1	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.	R4	5" MONOLITHIC CONCRETE ISLAND, KEYED IN
C2	PROP. APPROX. 3" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. IN EACH OF TWO LAYERS.	R5	2' VALLEY GUTTER
C3	PROP. VAR. DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 112 LBS. PER SQ. YD. PER 1" DEPTH. TO BE PLACED IN LAYERS NOT TO EXCEED 2" IN DEPTH.	R6	2'-6" MODIFIED CURB AND GUTTER (SEE DETAIL NO. 1)
D1	PROP. APPROX. 4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YD.	S	4" CONCRETE SIDEWALK
D2	PROP. VAR. DEPTH ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 114 LBS. PER SQ. YD. PER 1" DEPTH, TO BE PLACED IN LAYERS NOT LESS THAN 2½" IN DEPTH OR GREATER THAN 4" IN DEPTH.	T	EARTH MATERIAL
E1	PROP. APPROX. 4" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YD.	U	EXISTING PAVEMENT
E2	PROP. VAR. DEPTH ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 114 LBS. PER SQ. YD. PER 1" DEPTH. TO BE PLACED IN LAYERS NOT LESS THAN 3" IN DEPTH OR GREATER THAN 5½" IN DEPTH.	V1	INCIDENTAL MILLING (VARIABLE DEPTH 0" TO 1.5" DEPTH)
R1	2'-6" CONCRETE CURB AND GUTTER.	V2	1.5" MILLING OF EXISTING ASPHALT PAVEMENT
R2	2'-9" CONCRETE CURB AND GUTTER.	W	VARIABLE DEPTH ASPHALT PAVEMENT (SEE WEDGING DETAIL).

NOTES: 1.PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.

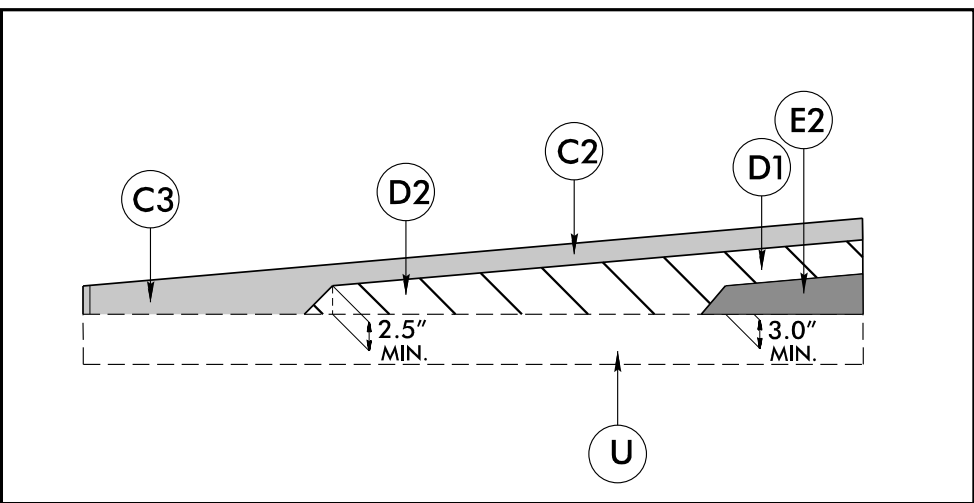
2.SAWCUT EXISTING PAVEMENT AS NEEDED SUCH THAT PAVEMENT  
WIDENING IS 2' MINIMUM



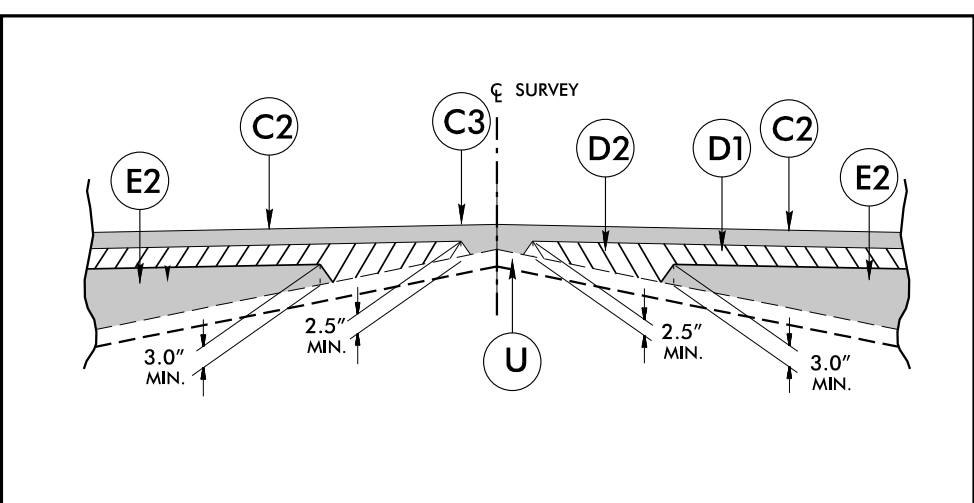
DETAIL NO. 1  
2'-6" MODIFIED CURB & GUTTER DETAIL  
See Typical No. 7 for Location



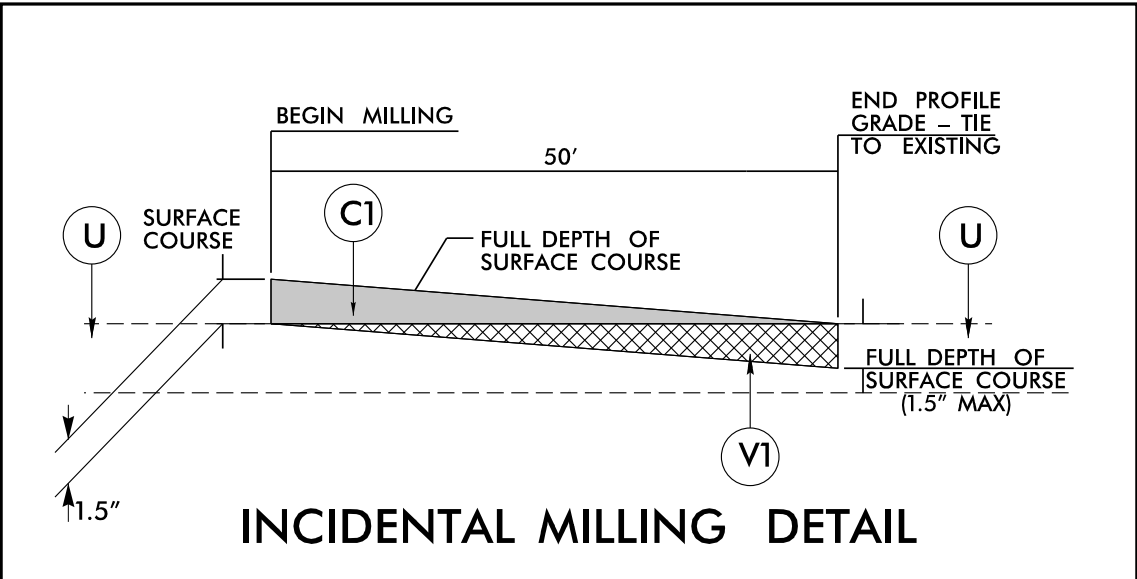
DETAIL NO. 2  
2' MODIFIED VALLEY GUTTER  
See Typical No. 7 for Location



WEDGING DETAIL FOR RESURFACING



METHOD OF WEDGING



- Y2- STA. 11+40.00 to 11+90.00
- Y3- STA. 10+50.00 to 11+00.00
- Y4- STA. 12+40.00 to 12+90.00
- Y5- STA. 10+80.00 to 11+30.00
- Y8- STA. 11+00.00 to 11+50.00
- Y9- STA. 11+65.00 TO 12+15.00
- Y6Rev1- STA. 10+30.00 TO 10+80.00
- Y7Rev- STA. 11+50.00 to 12+00.00
- Y10- STA. 10+50.00 to 11+00.00

PROJECT REFERENCE NO.  
U-6005

SHEET NO.  
2A-1

ROADWAY DESIGN  
ENGINEER  
NORTH CAROLINA  
PROFESSIONAL  
SEAL  
JAMES L. JOHNSON  
5/7/2025

PAVEMENT DESIGN  
ENGINEER  
NORTH CAROLINA  
PROFESSIONAL  
SEAL  
RYAN C. NEWCOMB  
5/7/2025

DOCUMENT NOT CONSIDERED FINAL  
UNLESS ALL SIGNATURES COMPLETED

Johnson, Miramiran, & Thompson, Inc.  
4700 Falls of Neuse Rd. Suite 100  
Raleigh, NC 27609  
License No. C-3097