

PHASING DIAGRAM DETECTION LEGEND

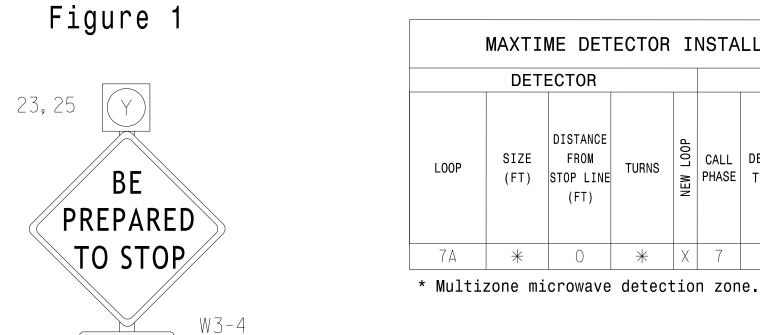
DETECTED MOVEMENT

UNDETECTED MOVEMENT (OVERLAP) UNSIGNALIZED MOVEMENT $<\!\!\!<\!\!\!--\!\!\!>$ PEDESTRIAN MOVEMENT

TABLE OF OPERATION PHASE FACE √R √ √R 71,72

SIGNAL FACE I.D. All Heads L.E.D.

> 71,72 21,22



| MAXTIME DETECTOR INSTALLATION CHART | | | | | | | | | | | | |
|-------------------------------------|--------------|---------------------------------------|-------|-------------|---------------|---------------|----------------|--------|---------------|------|--------------------|----------|
| DETECTOR | | | | PROGRAMMING | | | | | | | | |
| LOOP | SIZE (FT) | DISTANCE FROM STOP LINE (FT) | TURNS | NEW LOOP | CALL PHASE | DELAY TIME | EXTEND TIME | EXTEND | ADDED INITIAL | CALL | DELAY DURING GREEN | NEW CARD |
| 7 A | * | 0 | * | Χ | 7 | _ | - | Χ | - | Χ | - | * |

W16-13 TABLE OF OPERATION INTERVAL SIGNAL FACE

23,25

24,26

55 Mph 0% Grade

-450' from Stop Line-

ON OFF

OFF ON

55 Mph 0% Grade _ (2A) US 17 (Ocean Hwy West)

See Note 9

WHEN

FLASHING

See notes 6 and 7

24, 26

| MAXTIME T | IMING | CHART | | |
|-------------------------|-------|-------|--|--|
| CEATURE | PHASE | | | |
| FEATURE | 2 | 7 | | |
| Walk * | _ | _ | | |
| Ped Clear * | _ | _ | | |
| Min Green * | 14 | 7 | | |
| Passage * | 2.0 | 2.0 | | |
| Max 1 * | 100 | 25 | | |
| Yellow Change | 5.2 | 3.0 | | |
| Red Clear | 1.2 | 4.6 | | |
| Added Initial * | - | - | | |
| Maximum Initial * | _ | _ | | |
| Time Before Reduction * | _ | - | | |
| Time To Reduce * | _ | _ | | |
| Minimum Gap | _ | _ | | |
| Advance Walk | _ | _ | | |
| | 1 | | | |

| Dual Entry | _ | _ |
|-----------------------------|------------------|----------------|
| * These values may be field | adjusted. Do n | ot adjust Min |
| Green and Extension times t | for phases 2 and | d 6 |
| lower than what is shown. M | Nin Green for al | l other phases |
| should not be lower than 4 | seconds. | |

MIN RECALL

Χ

| ADVANCED MICROWAVE E | EXTENDED RA | ANGE DETI | ECTION | | |
|----------------------------------|-------------|-----------|---------|--|--|
| FUNCTION Sensor 1 (A) | | | | | |
| Channel | | 1 | | | |
| Phase | | 2 | | | |
| Direction of Travel | | NB | | | |
| Туре | | PRIORITY | | | |
| Level | 1 | 2 | QUEUE | | |
| Discovery Zone (ft) | >=750 | <750 | N/A | | |
| Range (ft) | 100-900 | 100-600 | 100-150 | | |
| Enable Speed | Y | Y | Y | | |
| Speed Range (mph) | 35-100 | 35-100 | 1-35 | | |
| Enable Estimated Time of Arrival | Y | Y | N | | |
| Estimated Time of Arrival (sec) | 2.5-10.0 | 2.5-6.5 | - | | |

Sig. 8.0 R-5857

PROJECT REFERENCE NO.

2 Phase Fully Actuated US 17 (Old Shallotte Rd) Signal System #: D03-38_Shallotte

NOTES

- 1. Refer to "Roadway Standard Drawings NCDOT" dated January 2024 and "Standard Specifications for Roads and Structures" dated January 2024.
- 2. Do not program signal for late night flashing operation unless otherwise directed by the Engineer.
- 3. Set all detector units to presence mode.
- 4. This intersection uses multi-zone microwave detection. Install detectors according to the manufacturer's instructions to achieve the desired detection.
- 5. Maximum times shown in timing chart are for free-run operation only. Coordinated signal system timing values shall supersede these values.
- 6. Activate flashers 3 seconds prior to end of phase 2 green.
- 7. Flash vertically-mounted beacons alternately. 8. Route conduit back to signal cabinet 03-1244
- for electrical service drop. 9. Install new conduit as close as possible to edge of pavement.
- 10. Refer to the Pavement Marking Plans for pavement marking details.

LEGEND

| PROPOSED | | EXISTING |
|---------------------|--|---|
| ○→ | Traffic Signal Head Modified Signal Head | ●─► N/A |
| Si | Sign Pedestrian Signal Head With Push Button & Sign Signal Pole with Guy gnal Pole with Sidewalk Guy | |
| N/A | Type III Signal Pedestal Detection zone Controller & Cabinet Junction Box Curb Ramp 2-in Underground Conduit | |
| — DD — N/A | Directional Drill Right of Way | N/A —— —— —— |
| \longrightarrow | Directional Arrow | $\longrightarrow\hspace{-0.5cm}\longrightarrow$ |
| | Metal Pole with Mastarm | |
| $\langle A \rangle$ | Signal Ahead Sign (W3-3) | |
| ⟨B⟩ ″BE F w/ ″v | PREPARED TO STOP" Sign (W3-4 WHEN FLASHING" Plaque (W16-3 (See Figure 1) | 4) B 3) |

New Installation

Prepared for the Offices of:

∽ Metal Pole #1 Sta. 30+59 -L-

94′ Rt.

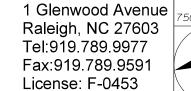
See Note 9

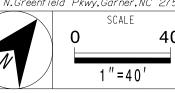
US 17 NB (Ocean Highway W) at U-Turn South of US

17 Bus/Old Shallotte Rd Division 3 Brunswick County PLAN DATE: March 2025 REVIEWED BY: G. G. Murr, Jr.

750 N.Greenfield Pkwy, Garner, NC 27529 PREPARED BY: Nadia Degbotse REVIEWED BY:







DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

Pre-Clearance

Vehicle Recall

Non Lock Detector