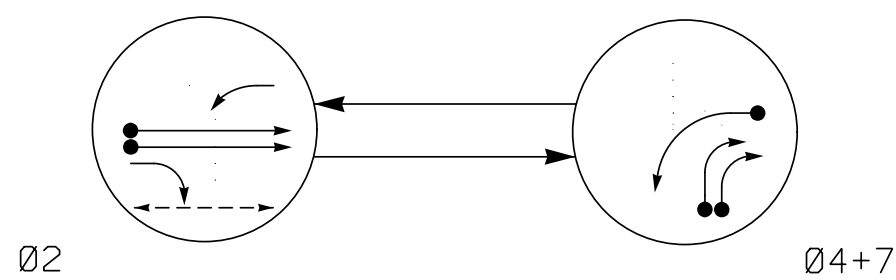


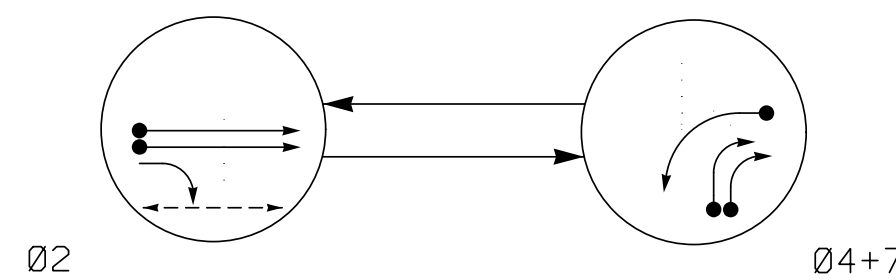
DEFAULT PHASING DIAGRAM



DEFAULT PHASING TABLE OF OPERATION

SIGNAL FACE	PHASE		
	Ø 2	Ø 4 + 7	FLASH
21	↑	R R	
22	G	R R	
23	↓	R R	
41, 42, 43	R	→ R	
71, 72	←	← R	
P21, P22	W	DW DRK	

ALTERNATE PHASING DIAGRAM



ALTERNATE PHASING TABLE OF OPERATION

SIGNAL FACE	PHASE		
	Ø 2	Ø 4 + 7	FLASH
21	↑	R R	
22	G	R R	
23	↓	R R	
41, 42	R	→ R	
71, 72	←	← R	
P21, P22	W	DW DRK	

MAXTIME DETECTOR INSTALLATION CHART

LOOP	DETECTOR			PROGRAMMING								
	SIZE (FT)	DISTANCE FROM STOPBAR (FT)	TURNS	NEW LOOP	CALL PHASE	DELAY TIME	EXTEND TIME	EXTEND	ADDED INITIAL	CALL	DELAY DURING GREEN	NEW CARD
2A	6X6	420	6	X	2	-	-	X	X	X	-	X
2B	6X6	420	6	X	2	-	-	X	X	X	-	X
4A	6X40	0	2-4-2	X	4	15.0	-	X	-	X	-	X
4B	6X40	0	2-4-2	X	4	15.0	-	X	-	X	-	X
7A	6X40	0	2-4-2	X	7	15.0*	-	X	-	X	-	X

* Disable Delay during Alternate Phasing operation.

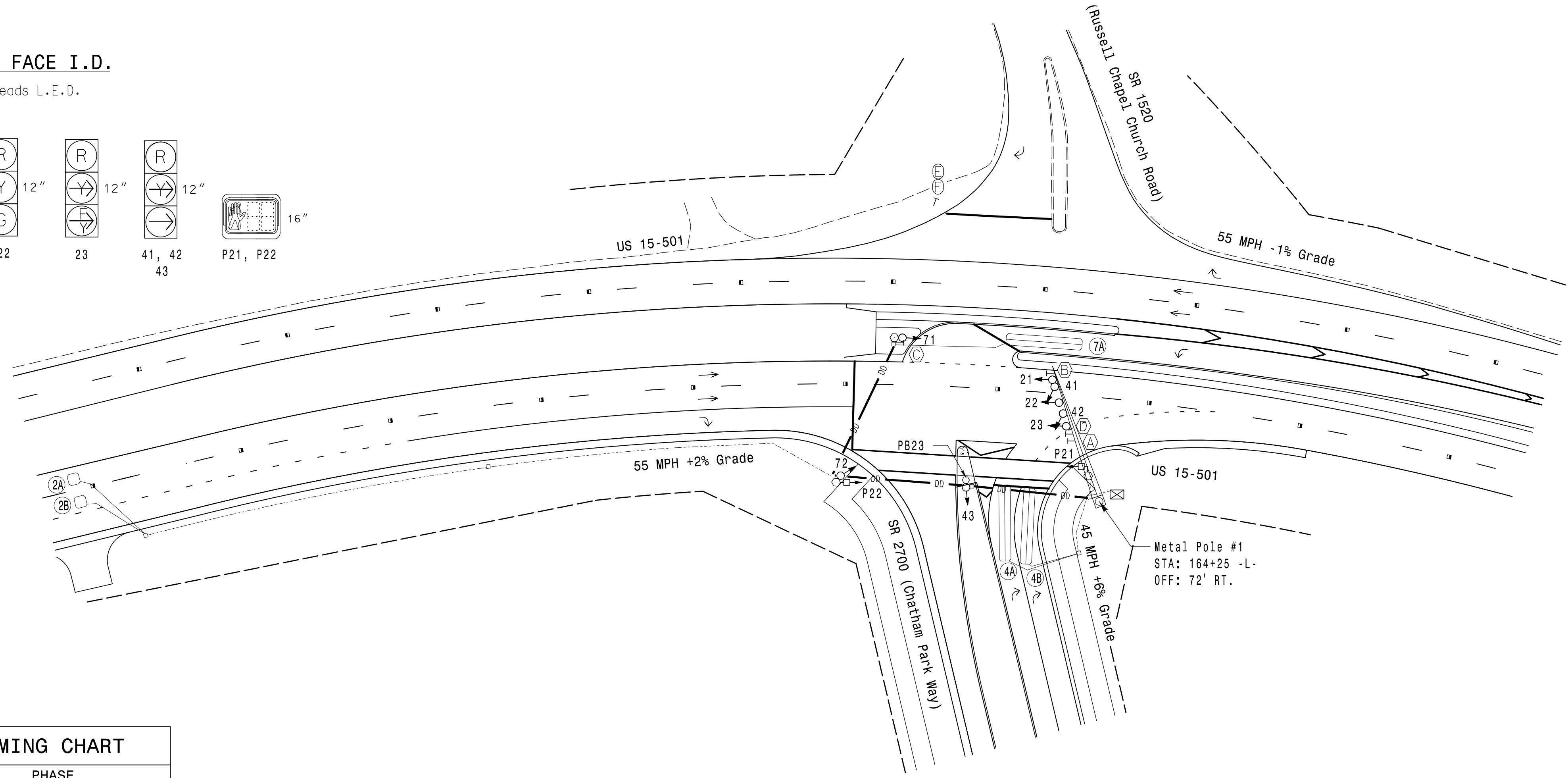
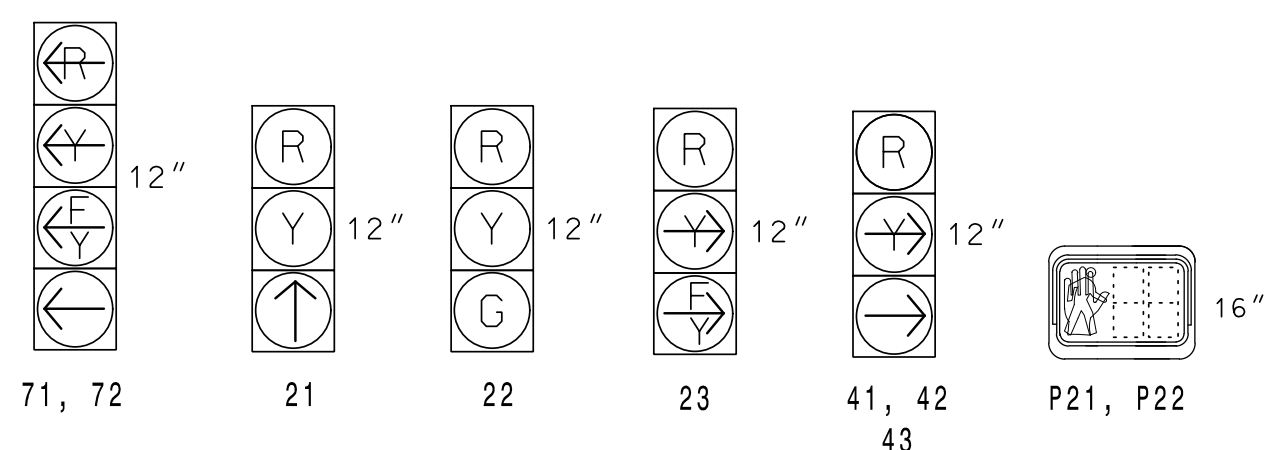
2 Phase Fully Actuated (Isolated)

NOTES

- Refer to "Roadway Standard Drawings NCDOT" dated January 2018 and "Standard Specifications for Roads and Structures" dated January 2018.
- Do not program signal for late night flashing operation unless otherwise directed by the Engineer.
- Set all detector units to presence mode.
- Locate new cabinet so as not to obstruct sight distance of vehicles turning right on red.
- Omit "WALK" and flashing "DON'T WALK" with no pedestrian calls.
- Program pedestrian heads to countdown the flashing "Don't Walk" time only.
- The Division Traffic Engineer will determine the hours of use for each phasing plan.
- To provide a leading pedestrian interval on phase 2, program FYA heads 23, 71 and 72 to delay for 7 seconds after the start of the phase 2 Walk Interval. See electrical details.
- All metal poles and pedestals to be painted agate gray.

SIGNAL FACE I.D.

All Heads L.E.D.



MAXTIME TIMING CHART

FEATURE	PHASE		
	2	4	7
Walk *	14	-	-
Ped Clear *	25	-	-
Min Green	14	7	7
Passage *	6.0	2.0	2.0
Max 1 *	90	40	40
Yellow Change	5.0	3.0	3.0
Red Clear	1.4	2.4	2.4
Added Initial *	1.5	-	-
Maximum Initial *	46	-	-
Time Before Reduction *	15	-	-
Time To Reduce *	45	-	-
Minimum Gap	3.4	-	-
Advance Walk	**	-	-
Non Lock Detector	-	X	X
Vehicle Recall	MIN RECALL	-	-
Dual Entry	-	X	X

* These values may be field adjusted. Do not adjust Min Green and Extension times for phase 2 lower than what is shown. Min Green for all other phases should not be lower than 4 seconds.
** See note #8.

LEGEND

PROPOSED	EXISTING
	N/A
	N/A

New Installation

Prepared for the Offices of:

 TRANSPORTATION MOBILITY AND SAFETY DIVISION
 STATE OF NORTH CAROLINA
 TRANSPORTATION SIGNAL DESIGN SECTION

PLANS PREPARED IN THE OFFICE OF:
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 Raleigh, NC 27601
 (919) 677-2000

750 N. Greenfield Pkwy, Garner, NC 27529
 SCALE: 0 40
 1"=40'

US 15-501 NB at SR 2700 (Chatham Park Way)

Division 8 Chatham County Pittsboro

PLAN DATE: April 2024	REVIEWED BY: KP Baumann
PREPARED BY: SP Pennington	REVIEWED BY:

REVISIONS: INIT. DATE

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

SEAL
 NORTH CAROLINA PROFESSIONAL ENGINEER
 SEAL 044434
 KEVIN P. BAUMANN

Signature:
 DATE: 12/12/2024
 SIG. INVENTORY NO. 08-0522

12/11/2024 3:44:16 PM susan.pennington K:\MAIL\TPTD\SIGNALS\01036584-R-5930-N-CPMS4-SIGNAL-DESIGN\5.0_08-0522_2024.dgn