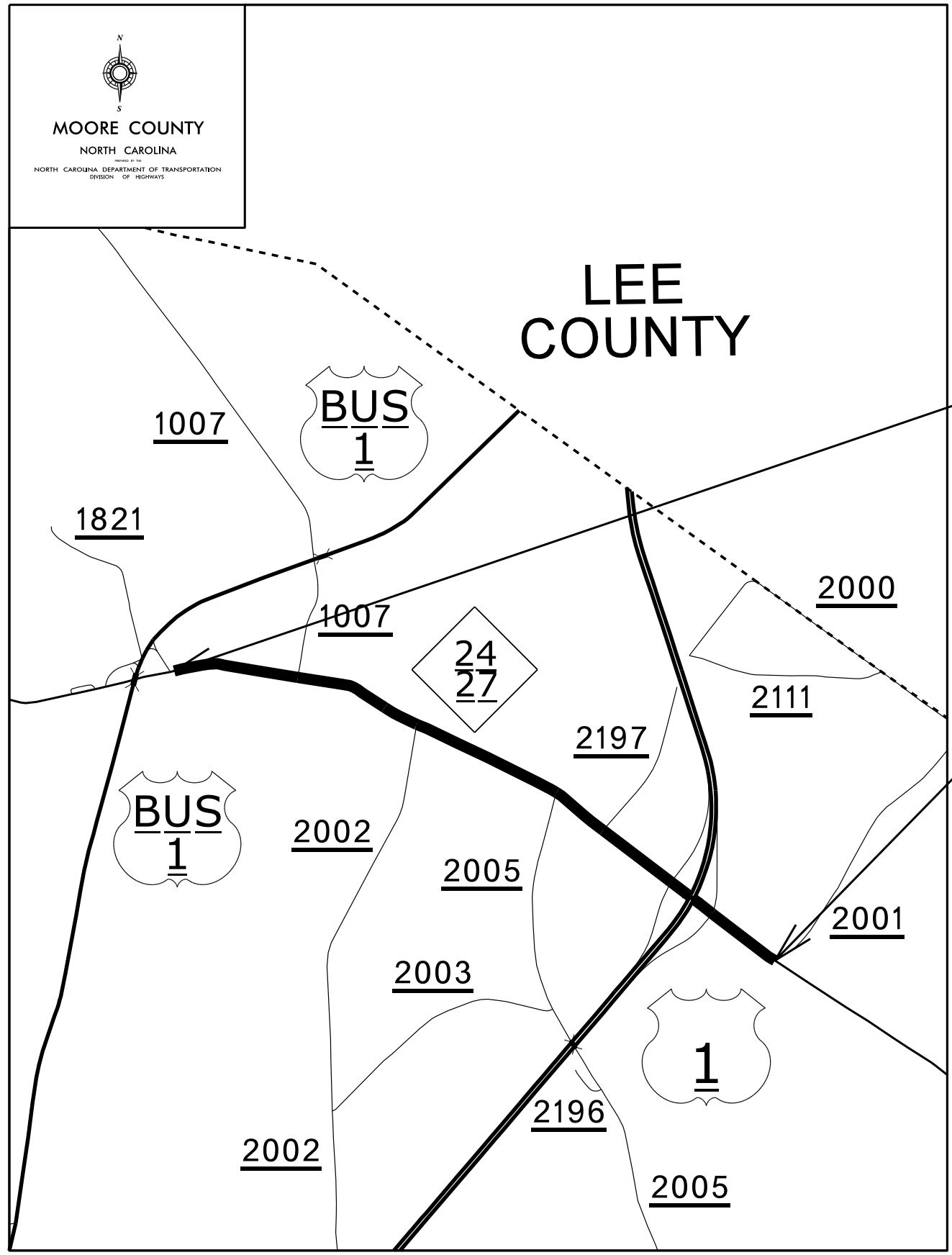


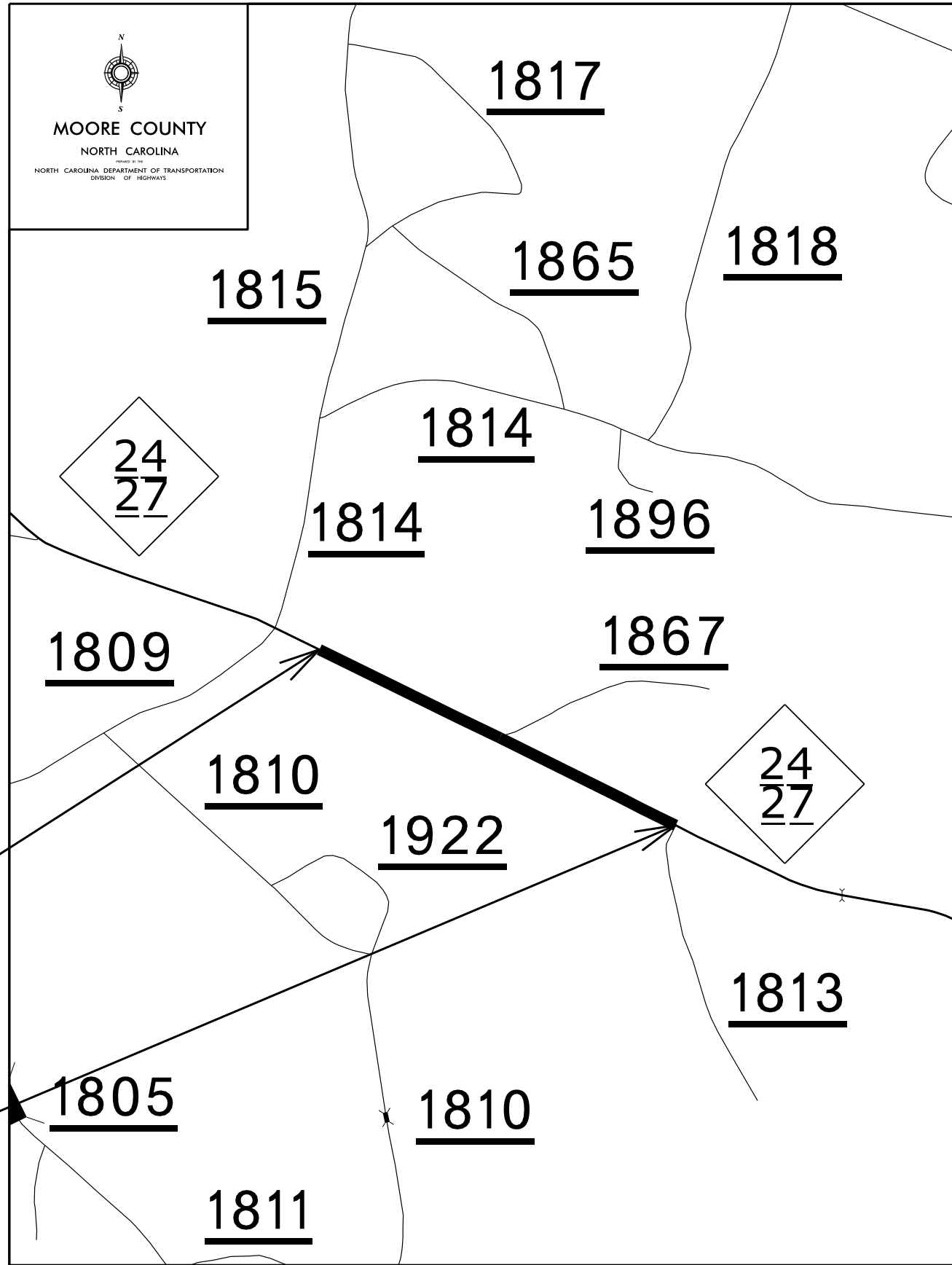
# MOORE COUNTY RESURFACING

17-SEP-2024 16:44  
R:\Resurfacing\2025\2025CPT.08.03.10631.dwg  
17-SEP-2024 16:44  
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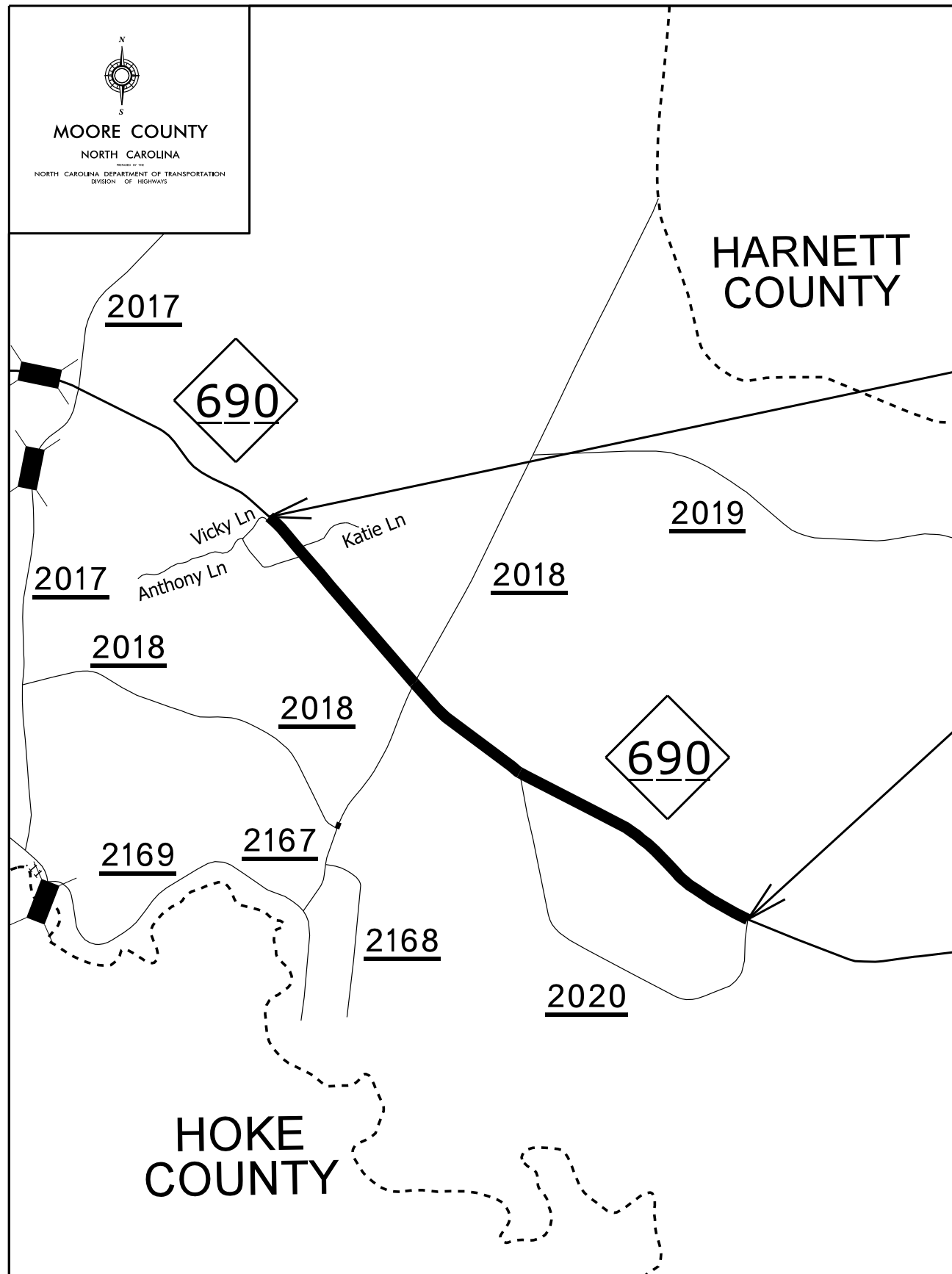
# Map 1



# Map 2

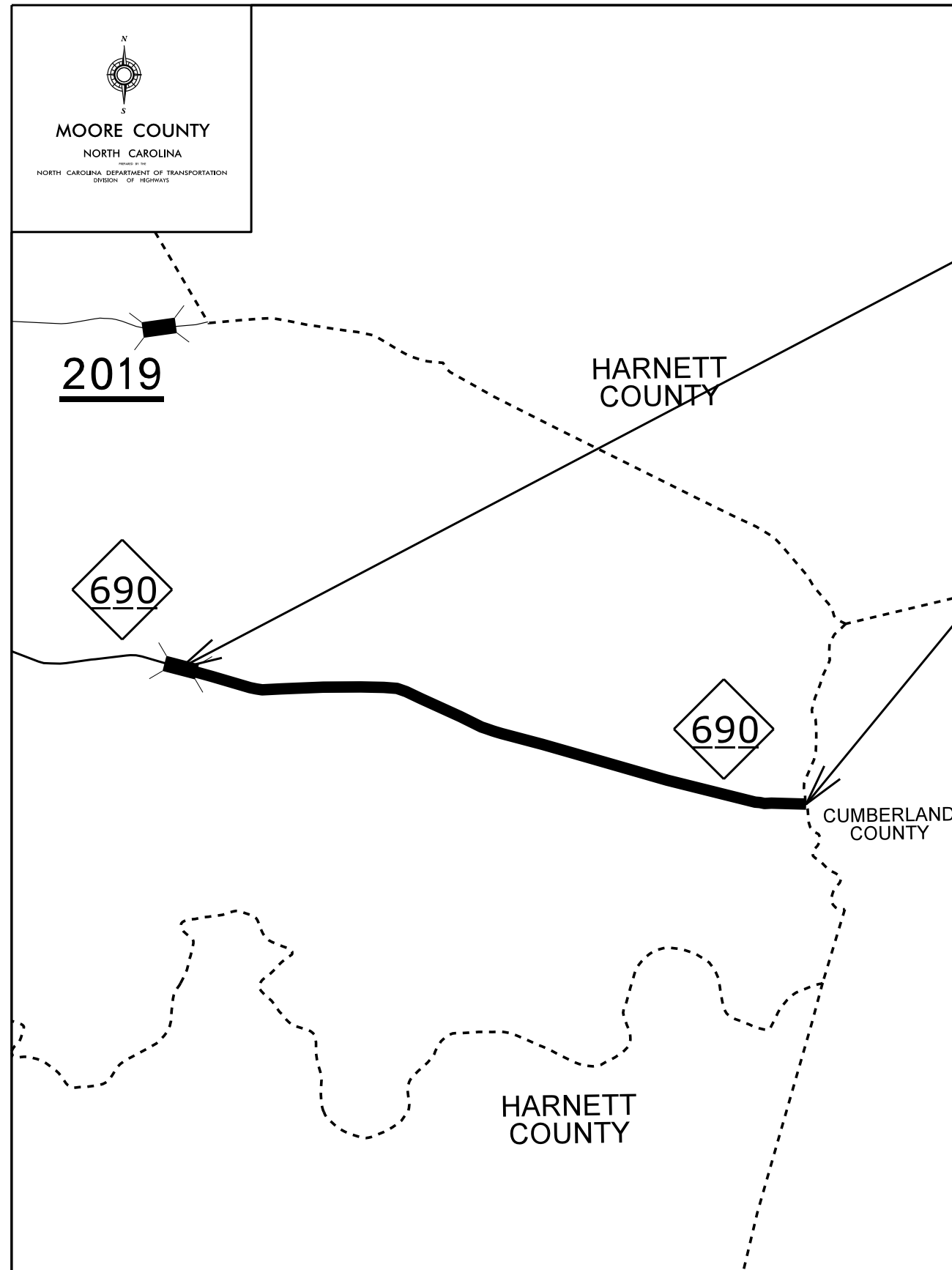


# Map 3

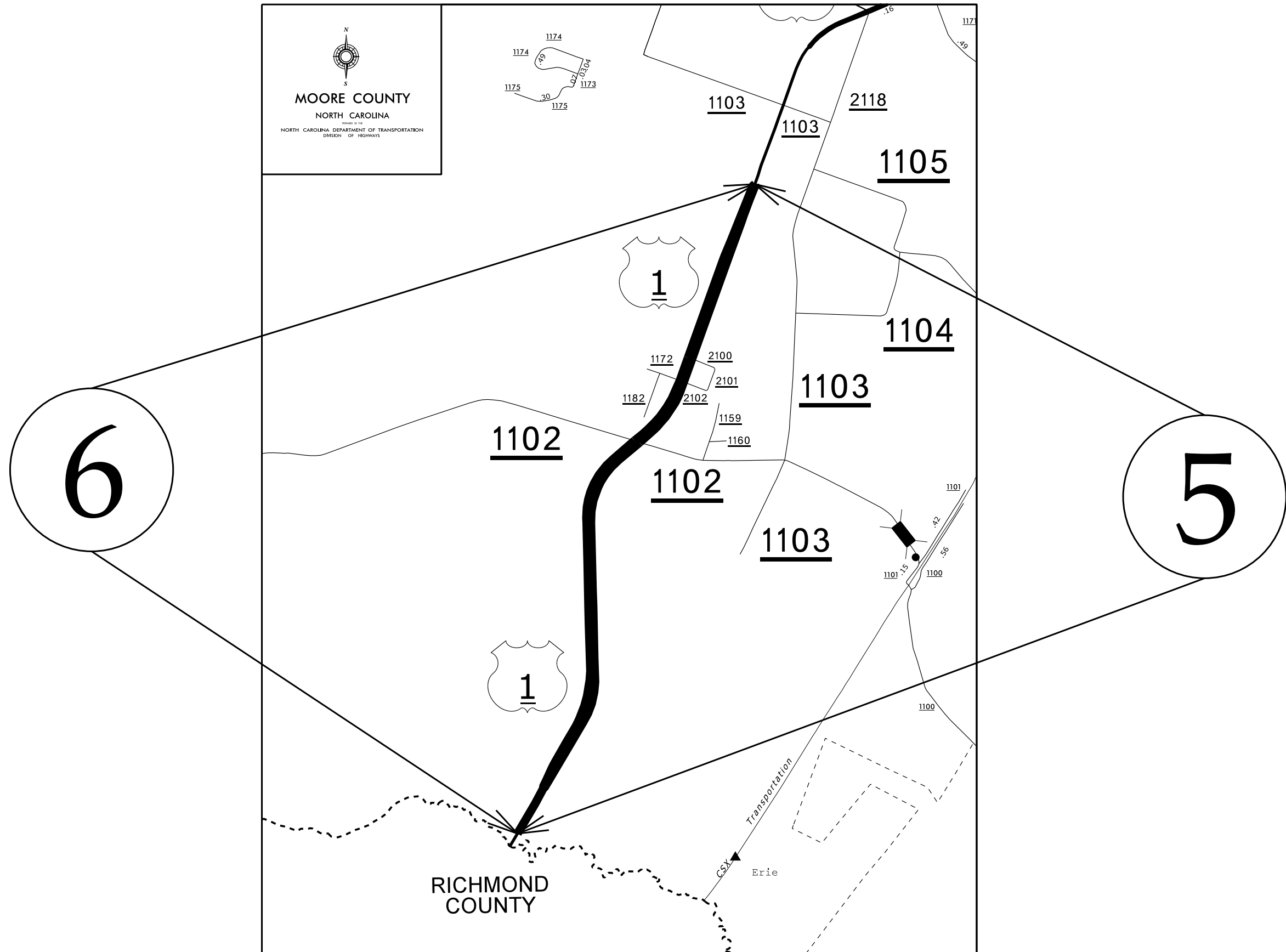


3

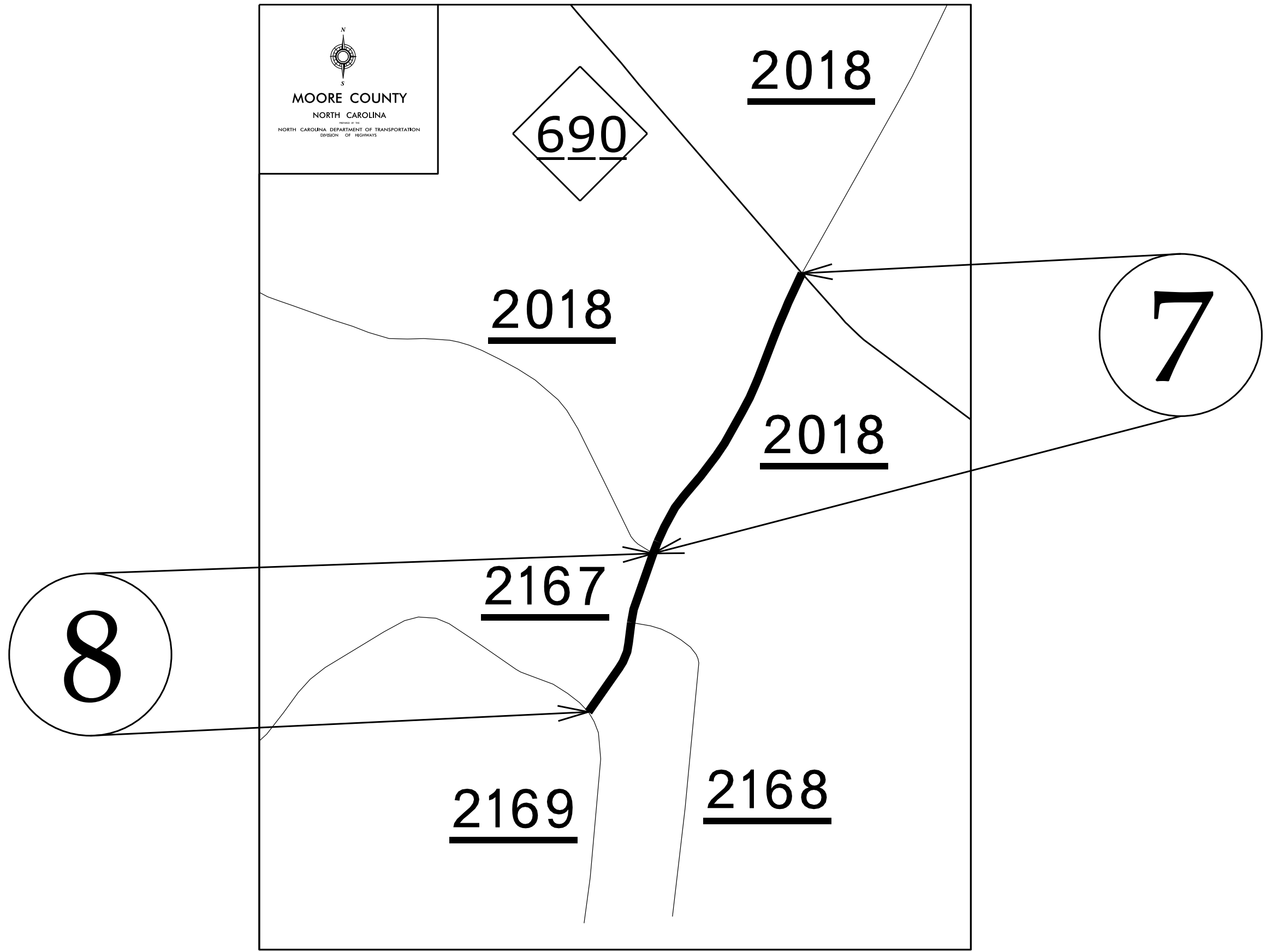
# Map 4



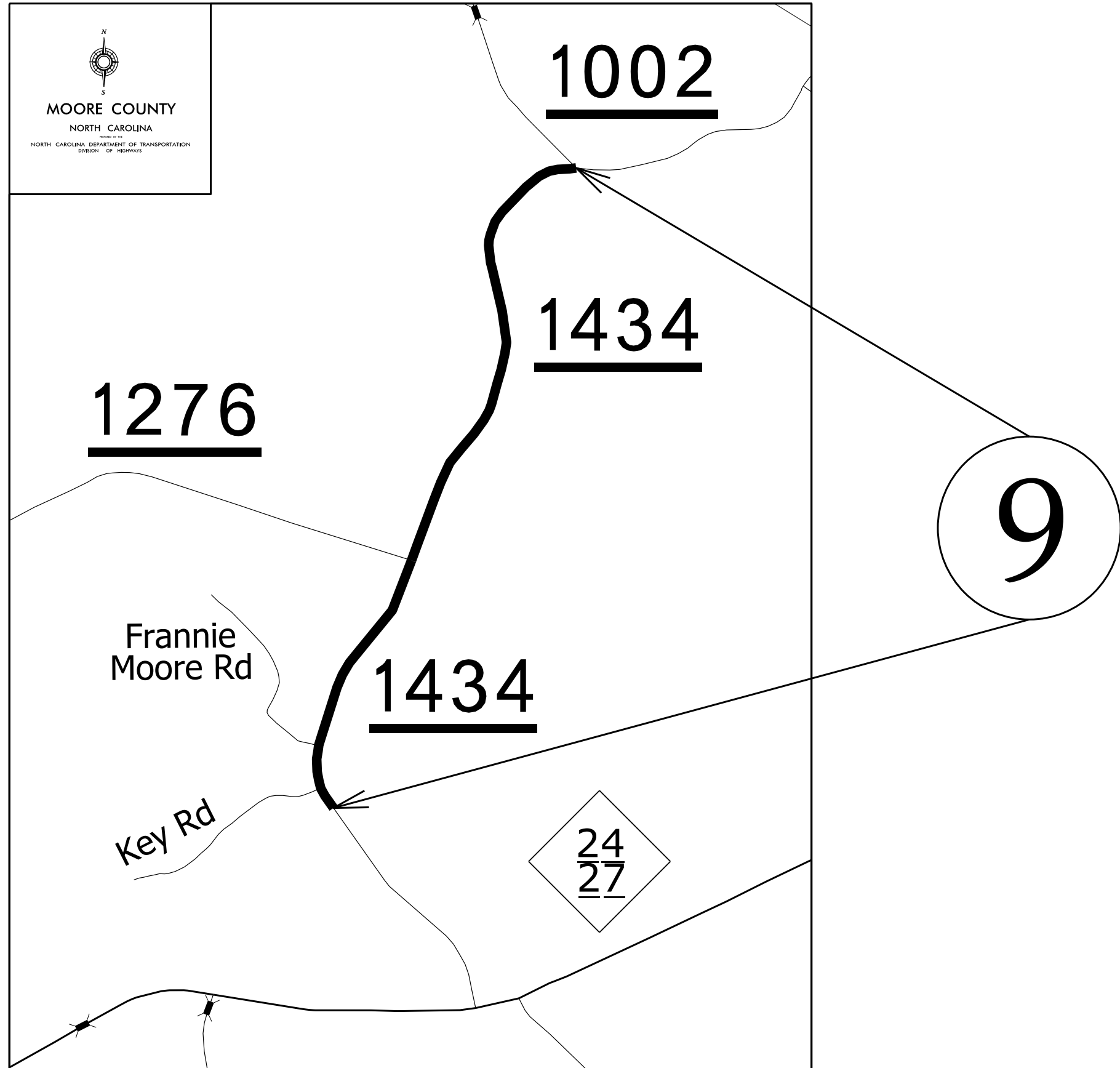
# Maps 5 and 6



# Maps 7 and 8

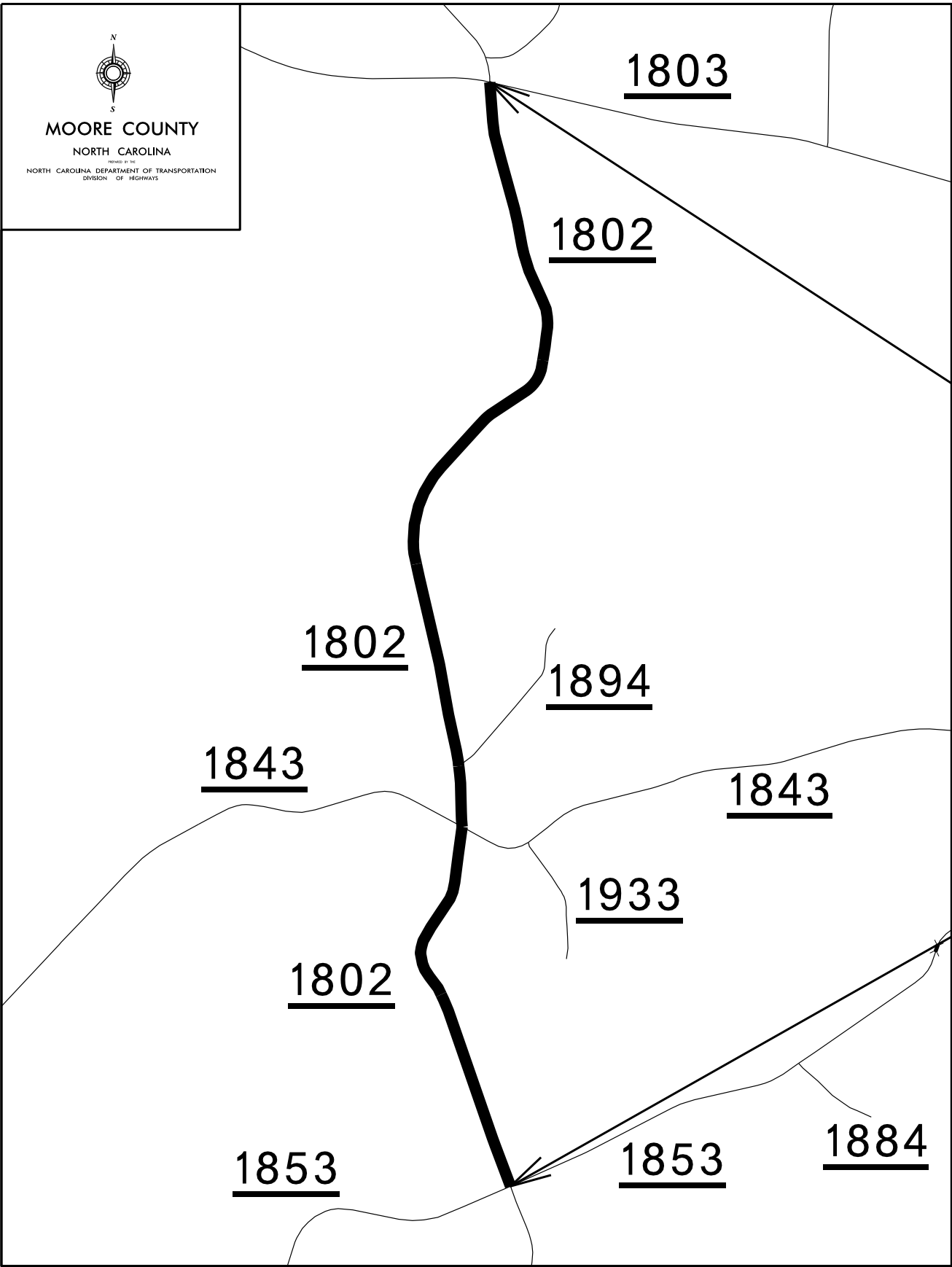


# Map 9

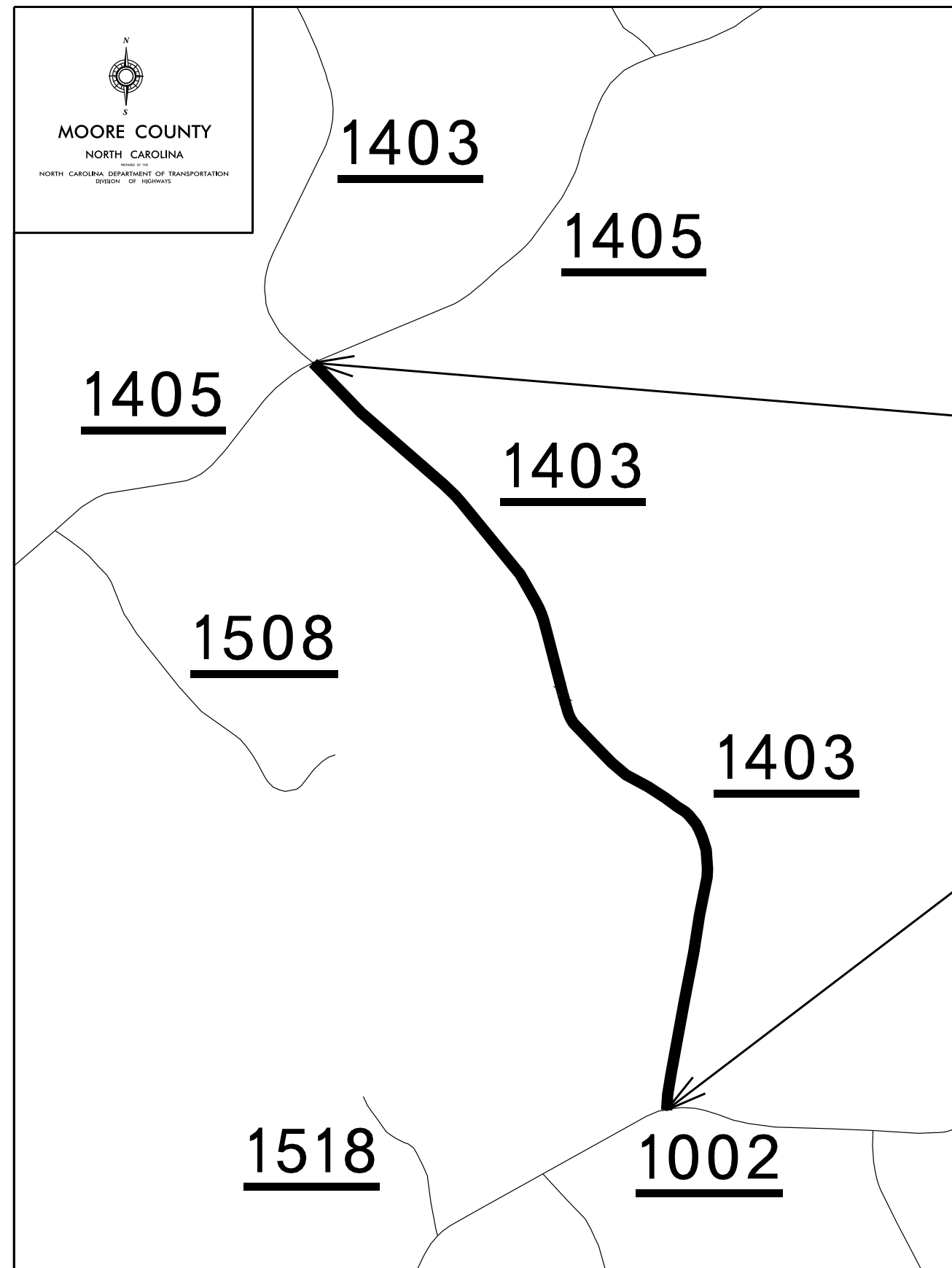




# Map 10

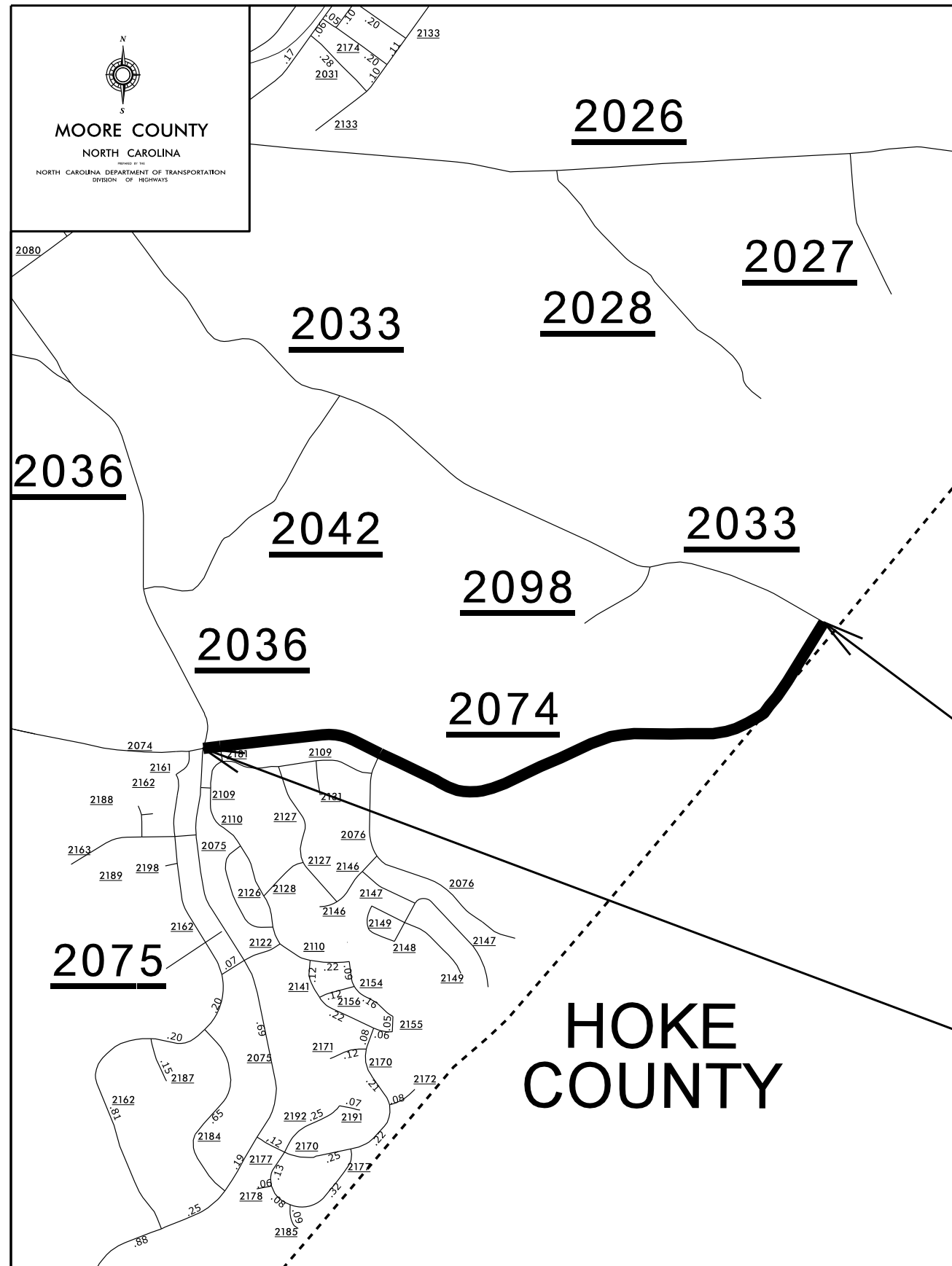


# Map 11



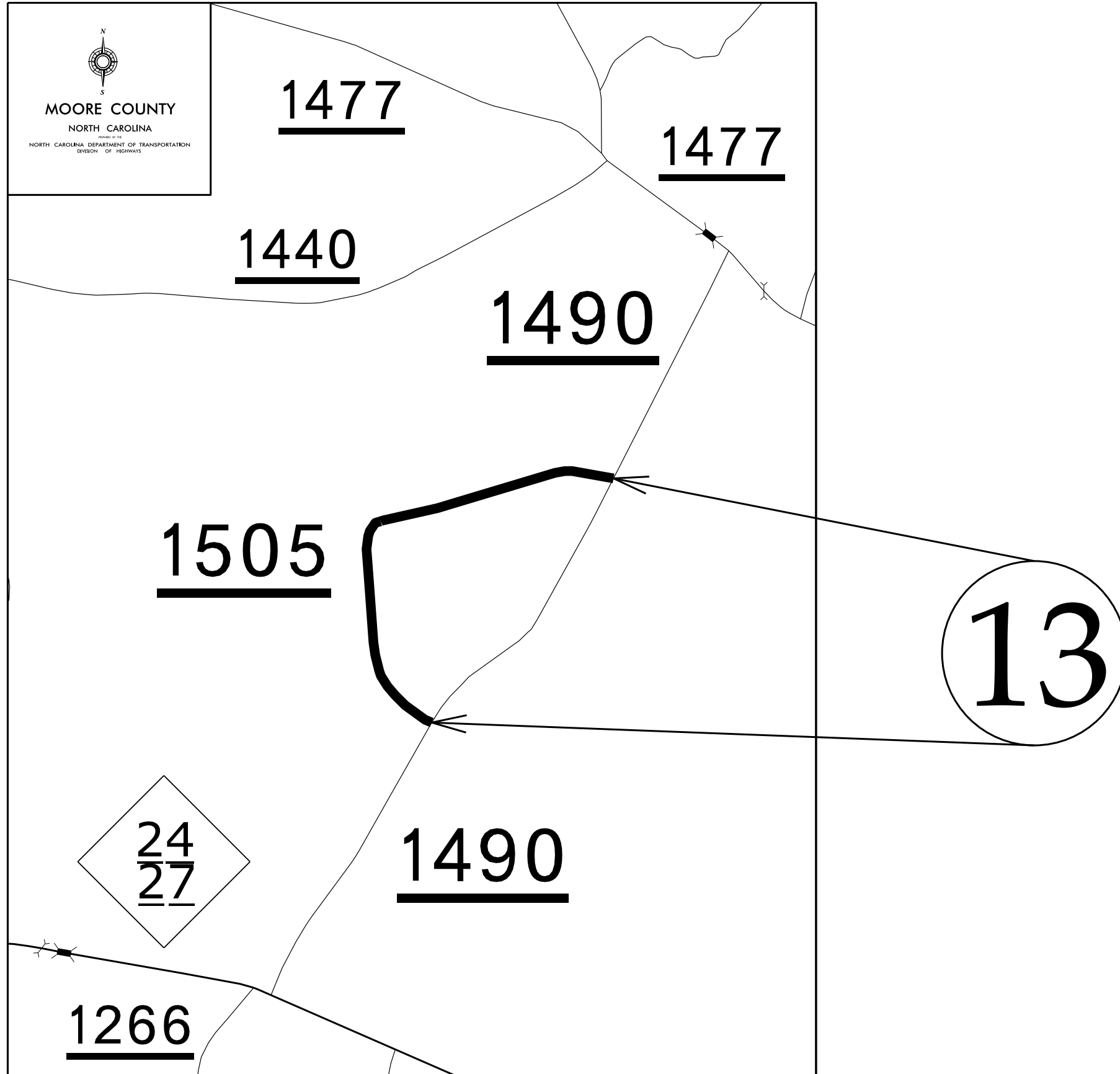
MOORE COUNTY  
NORTH CAROLINA  
DIVISION OF HIGHWAYS

# Map 12

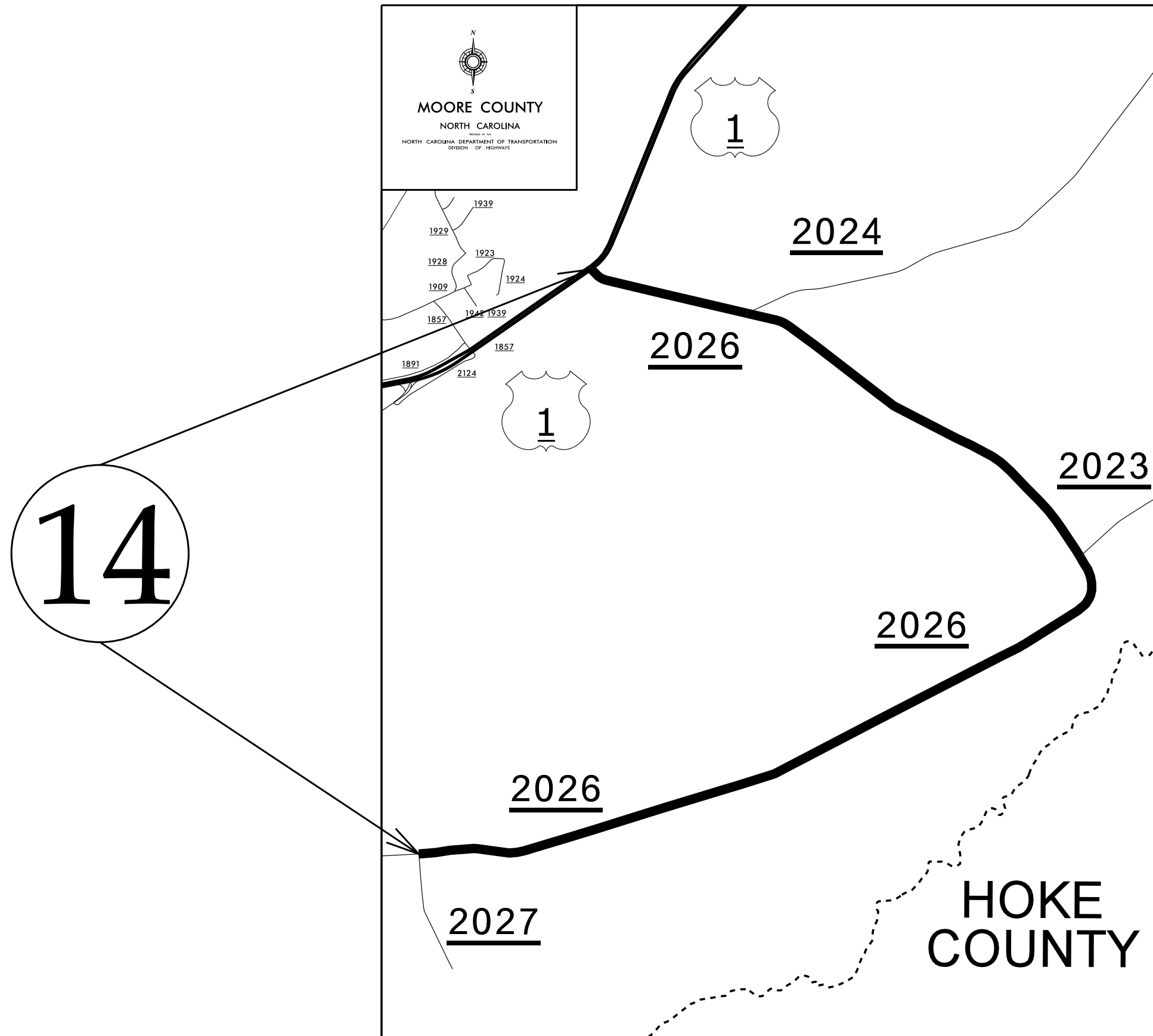


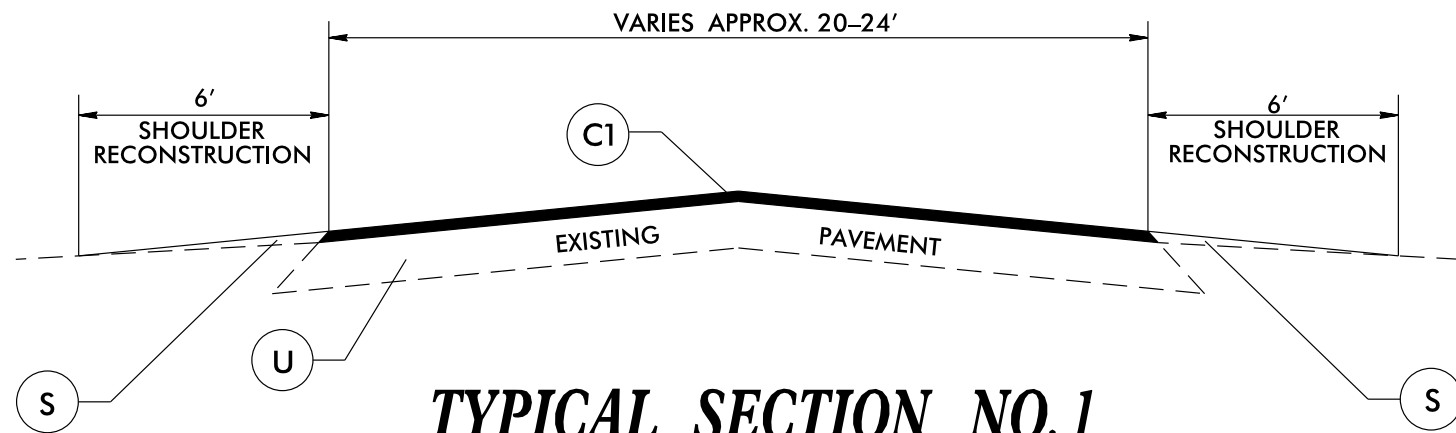
12

# Map 13



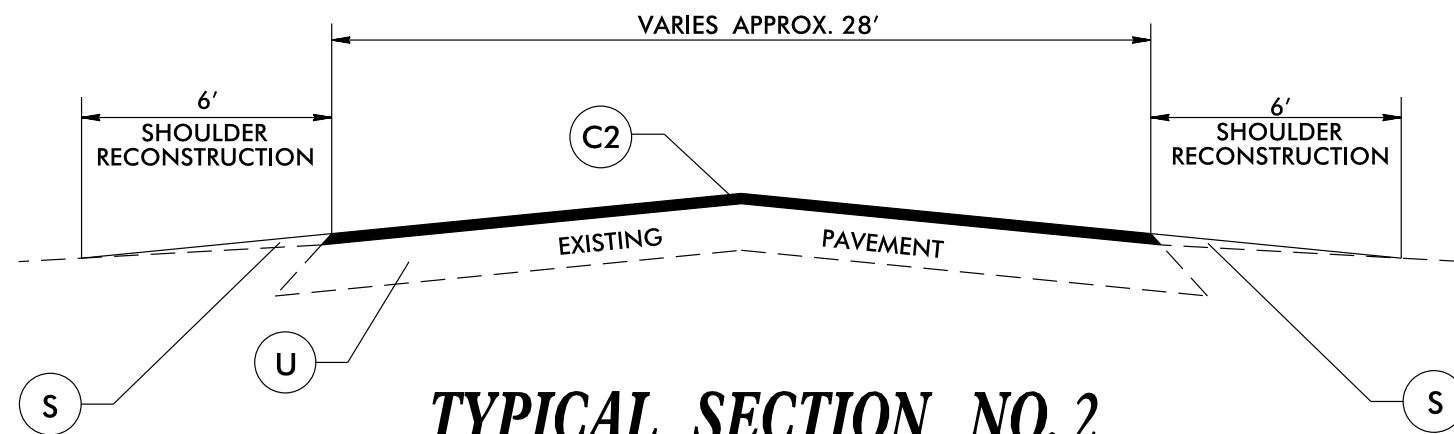
# Map 14





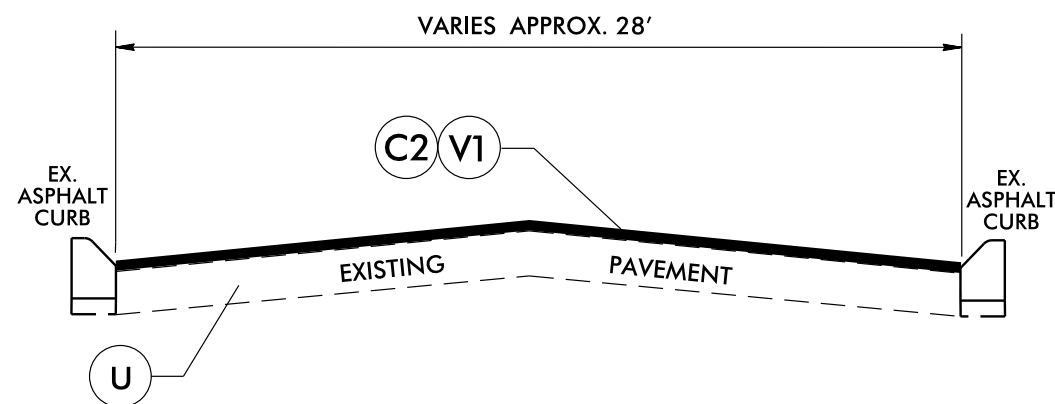
**TYPICAL SECTION NO. 1**

USE FOR MAPS 7-13



**TYPICAL SECTION NO. 2**

USE FOR MAPS 1,2,5, AND 6

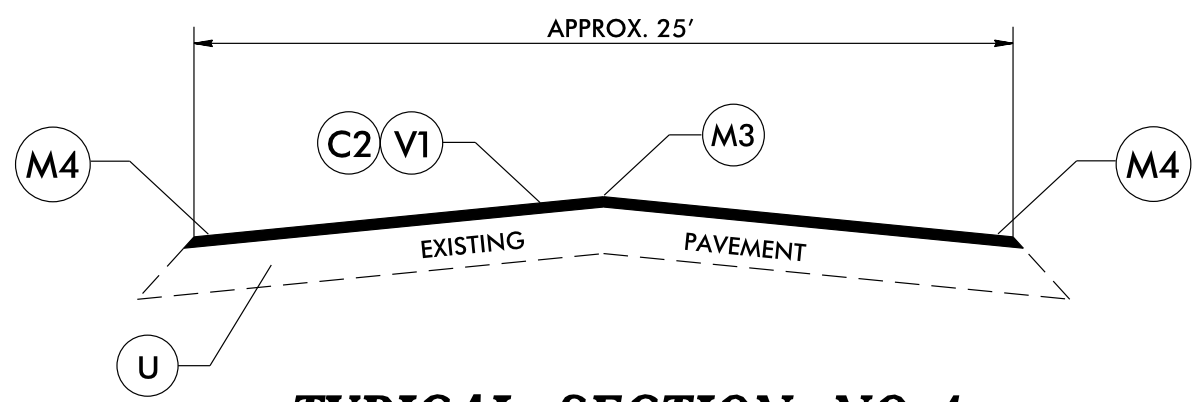


**TYPICAL SECTION NO. 3**

USE FOR MAP 1 FROM  
 100 FT EAST OF RAMP TO US 1 BUS  
 TO PAVEMENT JOINT AT RAILROAD

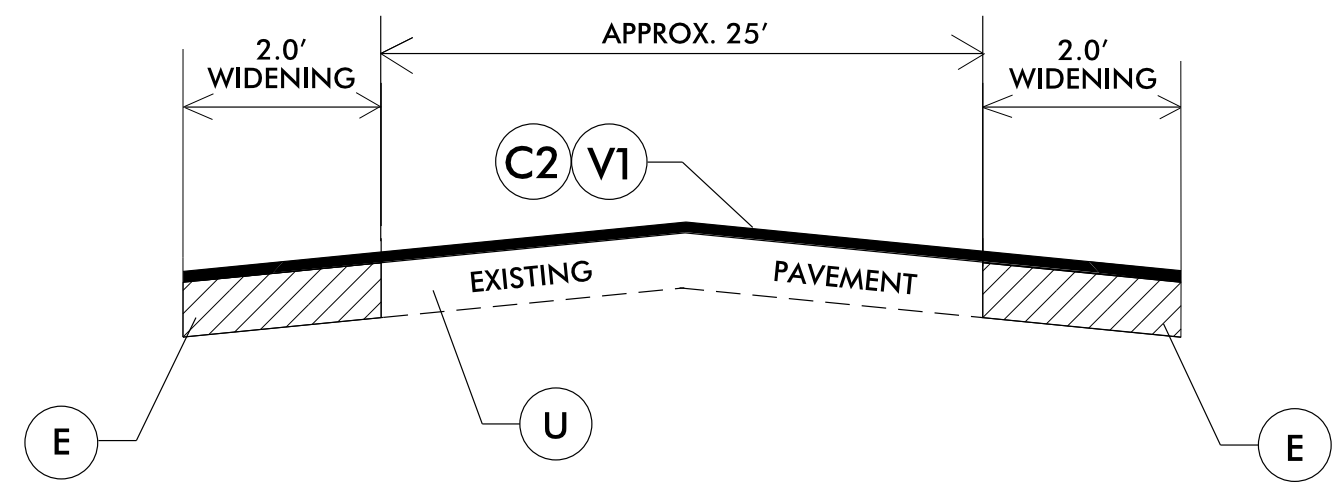
**PAVEMENT SCHEDULE**

C1	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
E	PROP. APPROX. 4.0" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YD. (TRENCHING FOR WIDENING IS INCIDENTAL TO BASE, SEE S.P.)
M1	MILLED RUMBLE STRIPES, CENTERLINE
M2	MILLED RUMBLE STRIPS, EDGELINE
M3	SINUSOIDAL MILLED RUMBLE STRIPES, CENTERLINE
M4	SINUSOIDAL MILLED RUMBLE STRIPES, EDGELINE
S	AGGREGATE SHOULDER BORROW
U	EXISTING PAVEMENT.
V1	MILLING 1.5" IN DEPTH



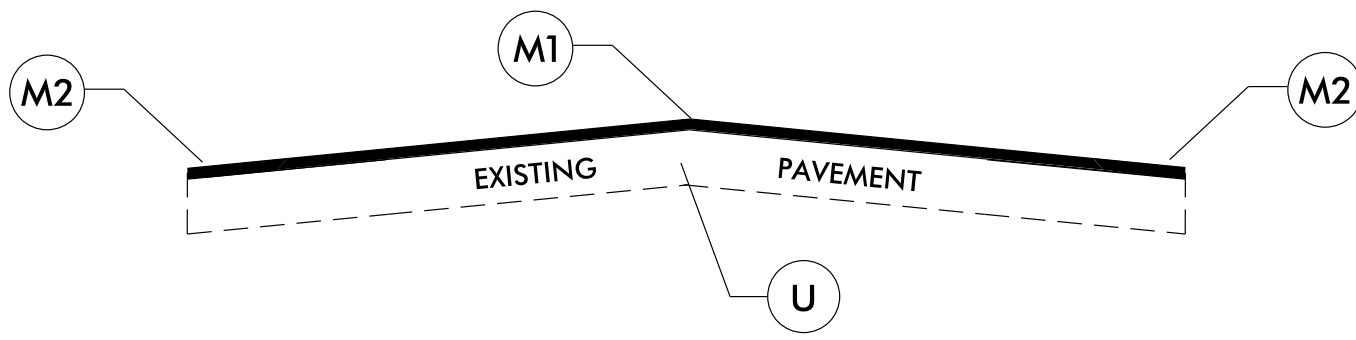
**TYPICAL SECTION NO. 4**

USE FOR MAP 3  
RESTRIPE WITH 11' LANE WIDTH



**TYPICAL SECTION NO. 5**

USE FOR MAP 4  
RESTRIPE WITH 12' LANE WIDTH

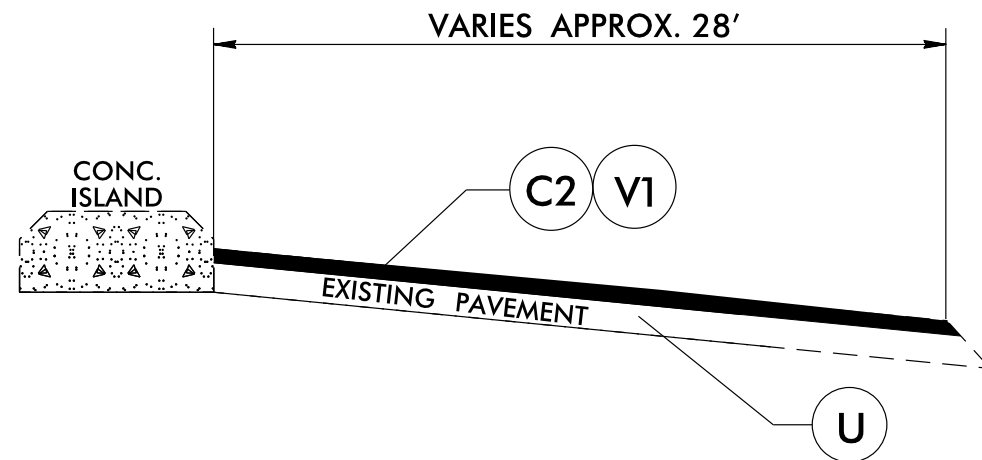


**TYPICAL SECTION NO. 6**

USE FOR MAP 4  
RESTRIPE WITH 12' LANE WIDTH

**PAVEMENT SCHEDULE**

C1	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
E	PROP. APPROX. 4.0" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YD. (TRENCHING FOR WIDENING IS INCIDENTAL TO BASE, SEE S.P.)
M1	MILLED RUMBLE STRIPES, CENTERLINE
M2	MILLED RUMBLE STRIPS, EDGELINE
M3	SINUSOIDAL MILLED RUMBLE STRIPES, CENTERLINE
M4	SINUSOIDAL MILLED RUMBLE STRIPES, EDGELINE
S	AGGREGATE SHOULDER BORROW
U	EXISTING PAVEMENT.
V1	MILLING 1.5" IN DEPTH

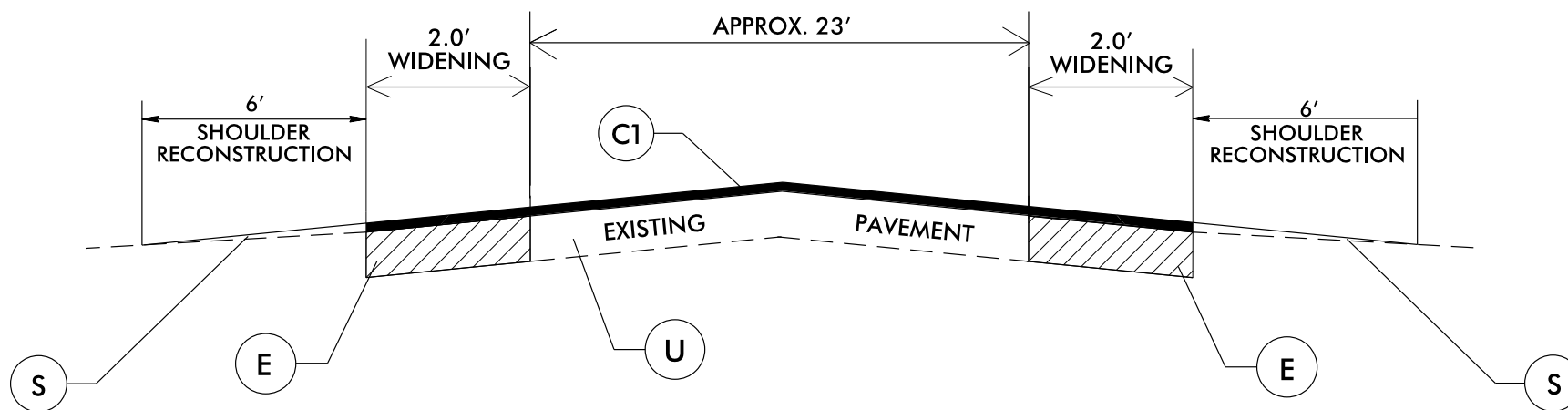


**TYPICAL SECTION NO. 7**

USE FOR ISLAND SECTIONS ON MAP 1  
 APPROACHING EACH END OF  
 BRIDGE #248 OVER US 1

USE FOR MAPS 5 AND 6 FROM  
 PINEBLUFF FIRE DEPT ENTRANCE  
 TO END OF MAPS 800 FT NORTH OF ENTRANCE  
 (DO NOT RESURFACE BULBOUT ON MAP 6)

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
E	PROP. APPROX. 4.0" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YD. (TRENCHING FOR WIDENING IS INCIDENTAL TO BASE, SEE S.P.)
M1	MILLED RUMBLE STRIPES, CENTERLINE
M2	MILLED RUMBLE STRIPS, EDGELINE
M3	SINUSOIDAL MILLED RUMBLE STRIPES, CENTERLINE
M4	SINUSOIDAL MILLED RUMBLE STRIPES, EDGELINE
S	AGGREGATE SHOULDER BORROW
U	EXISTING PAVEMENT.
V1	MILLING 1.5" IN DEPTH



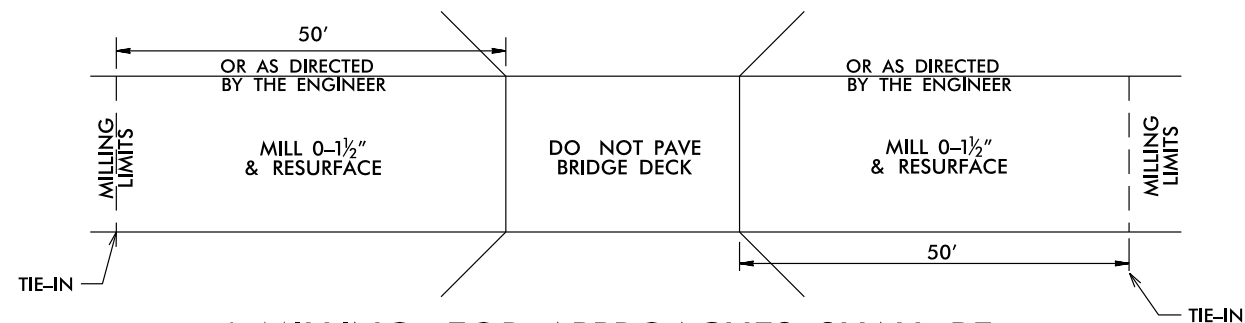
**TYPICAL SECTION NO. 8**

USE FOR MAP 14



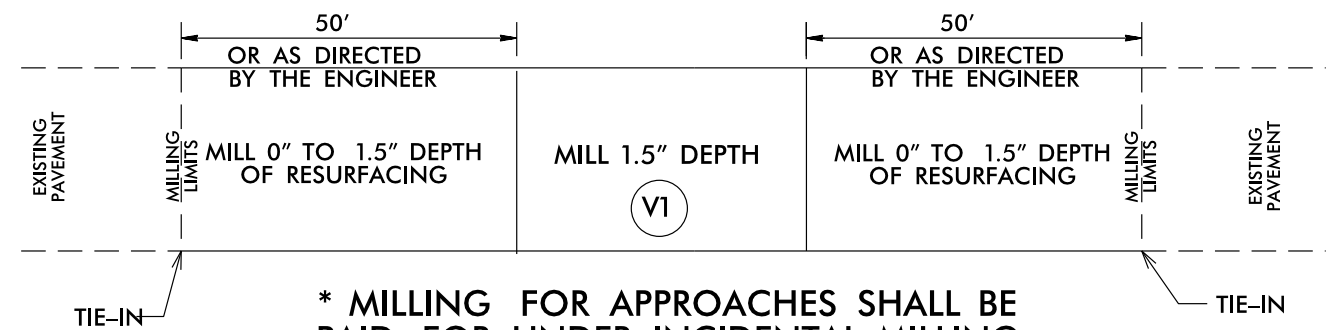


# BRIDGE DETAILS



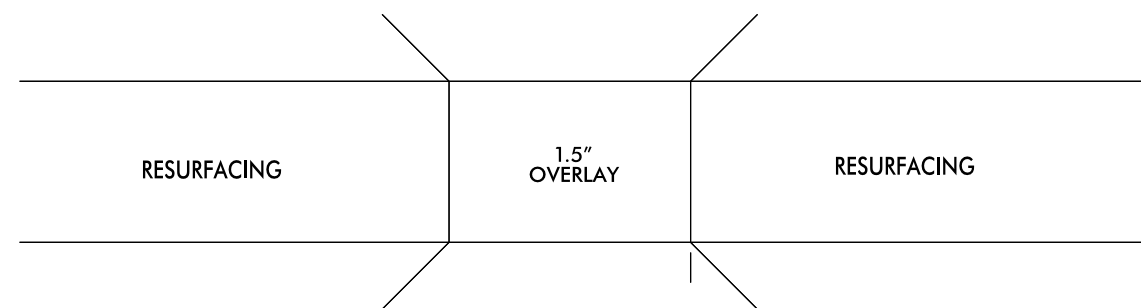
\* MILLING FOR APPROACHES SHALL BE PAID FOR UNDER INCIDENTAL MILLING

**BRIDGE DRAWING FOR  
MAP #1 NC-24 E BRIDGE #248**



\* MILLING FOR APPROACHES SHALL BE PAID FOR UNDER INCIDENTAL MILLING

**BRIDGE DRAWING FOR  
MAP #7 MCPHERSON RD BRIDGE #198  
MAP #10 NIAGARA CARTHAGE RD BRIDGE #33**

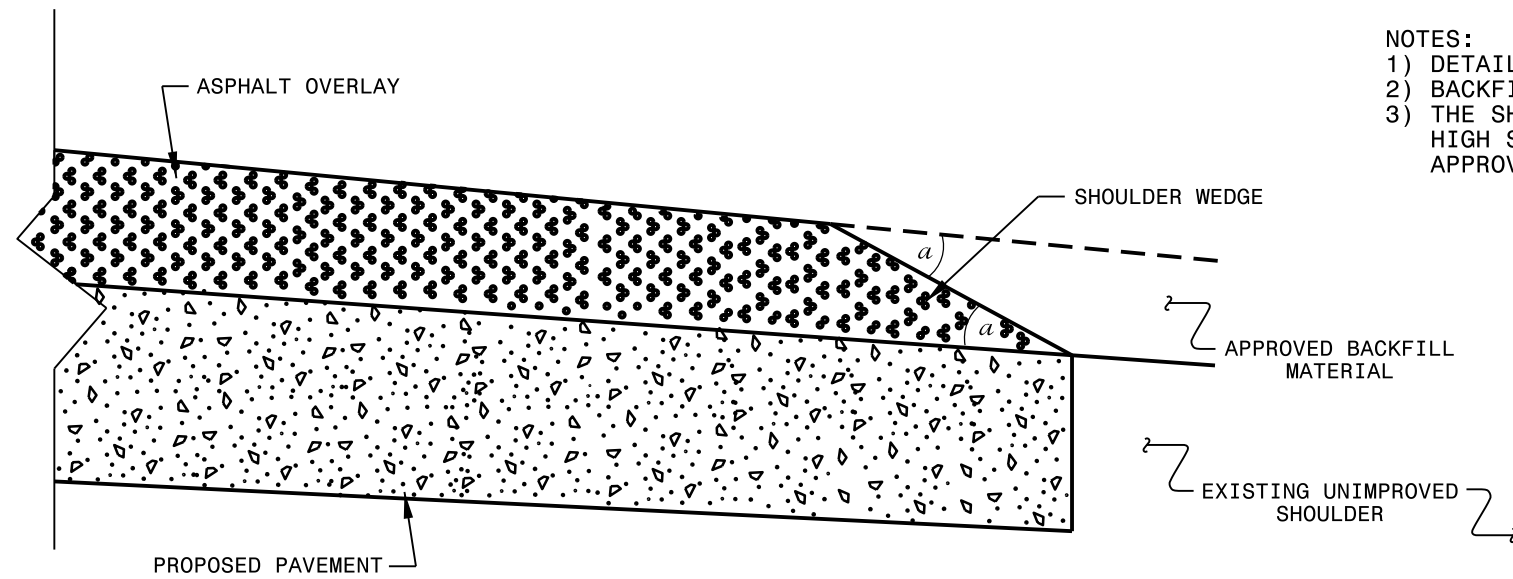


**BRIDGE DRAWING FOR  
MAP #11 JASON RD BRIDGE #173**



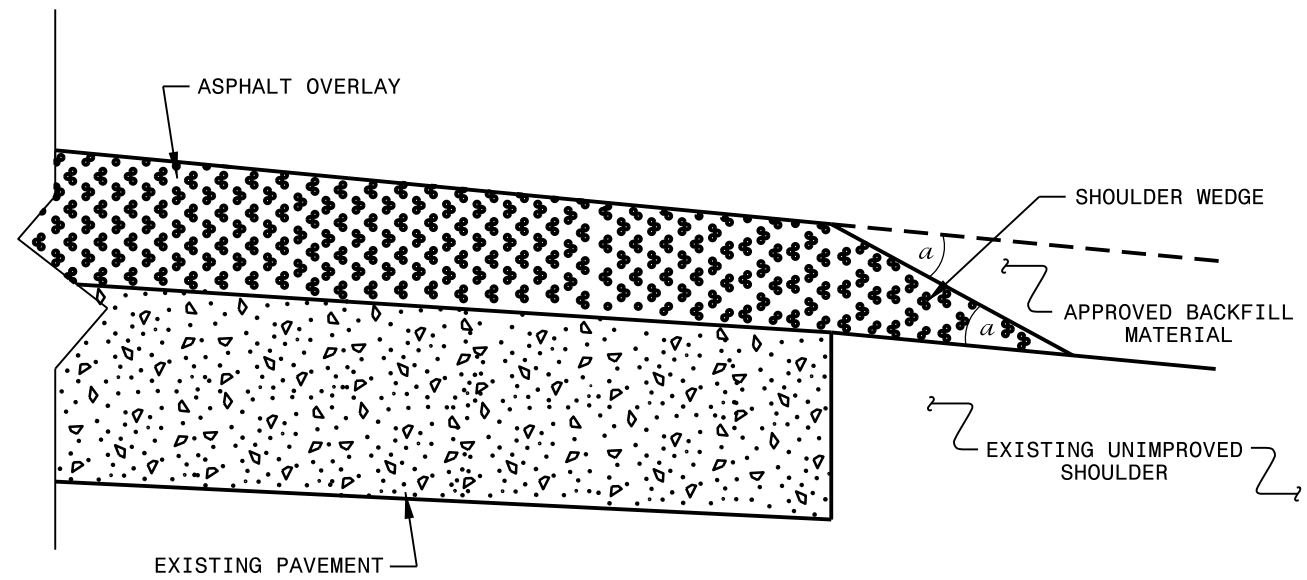
**NOTES:**

- 1) DETAIL DOES NOT APPLY TO OGAFB AND ULTRA-THIN BONDED WEARING COURSE.
- 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
- 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS, SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS APPROVED BY THE ENGINEER.



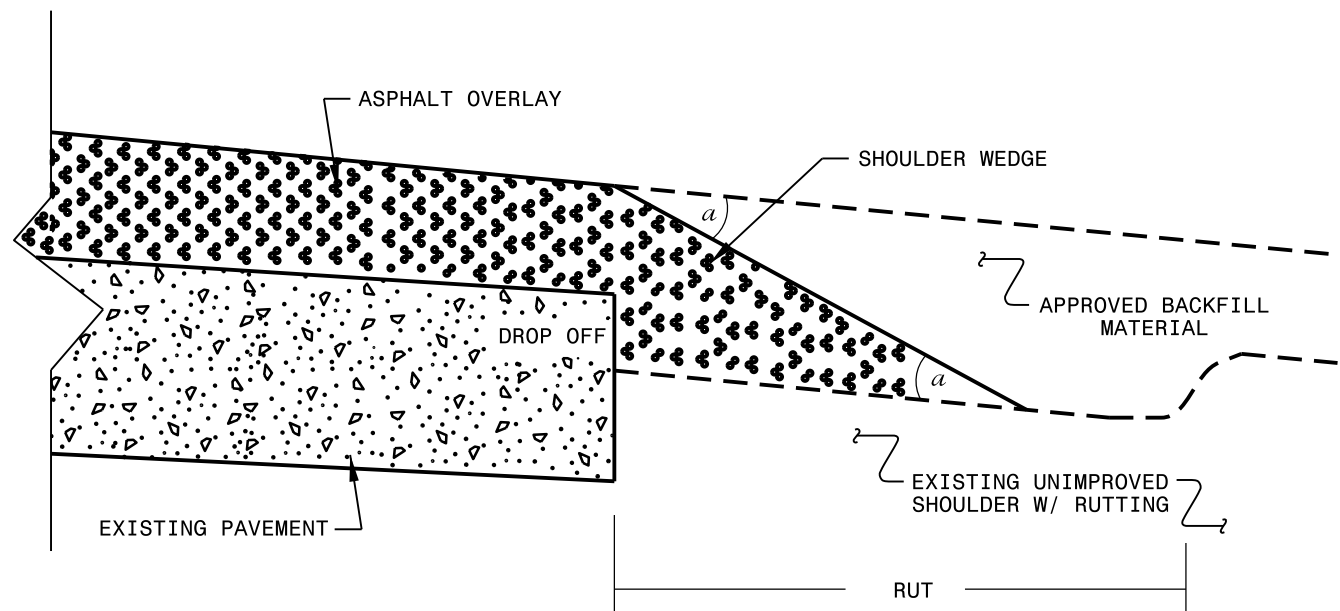
**SHOULDER WEDGE DETAIL**

(Resurfacing Projects w/ Widening or with Existing Paved Shoulder having no dropoffs)



**SHOULDER WEDGE DETAIL**

(Resurfacing Projects w/ NO Widening)



**SHOULDER WEDGE DETAIL**

(Resurfacing Adjacent to Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

<b>CONTRACT STANDARDS AND DEVELOPMENT UNIT</b>		
Office 919-707-6950 FAX 919-250-4119		
<b>SHOULDER WEDGE DETAILS</b>		
ORIGINAL BY: T.SPELL	DATE: 7-19-11	
MODIFIED BY:	DATE: 2/2/16	
CHECKED BY:	DATE:	
FILE SPEC.: szusr/details/stand/shoulderwedgedetail.dgn		

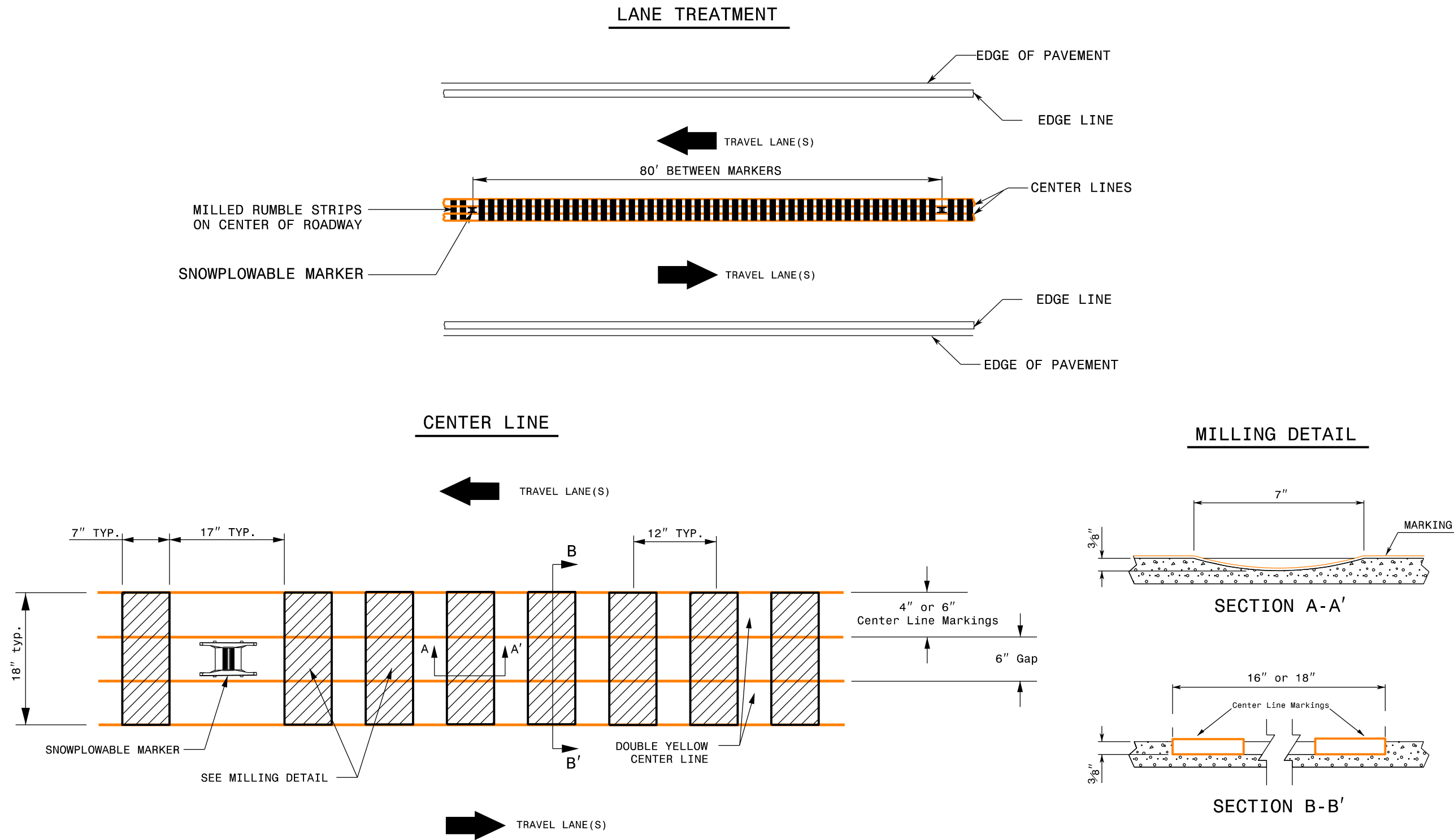
22 JAN-2018 09:41  
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 P:\piper\41\_CSD-215342

STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

2-23

ENGLISH DETAIL DRAWING FOR  
**RUMBLE STRIPS / STRIPES**  
TRADITIONAL CENTERLINE RUMBLE STRIPE WITH SNOWPLOWABLE MARKERS

See Table 2 within Rumble Strip Policy for Design Guidance



REFERENCE DRAWING ID: Trad.CL with Snowplowable Markers

**NOTES:**

- 1) USING A VACUUM, REMOVE ALL DEBRIS FROM THE MILLINGS JUST PRIOR TO PLACING ANY PAVEMENT MARKINGS.
- 2) ENSURE GLASS BEADS ARE SPREAD UNIFORMLY OVER THE ENTIRE SURFACE OF THE PAVEMENT MARKING MATERIAL.
- 3) INSTALL SNOWPLOWABLE MARKERS AT APPROXIMATELY 80' INCREMENTS. DO NOT MILL RUMBLE STRIPS IN SECTION WHERE SNOWPLOWABLE MARKERS ARE INSTALLED.

STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

2-23

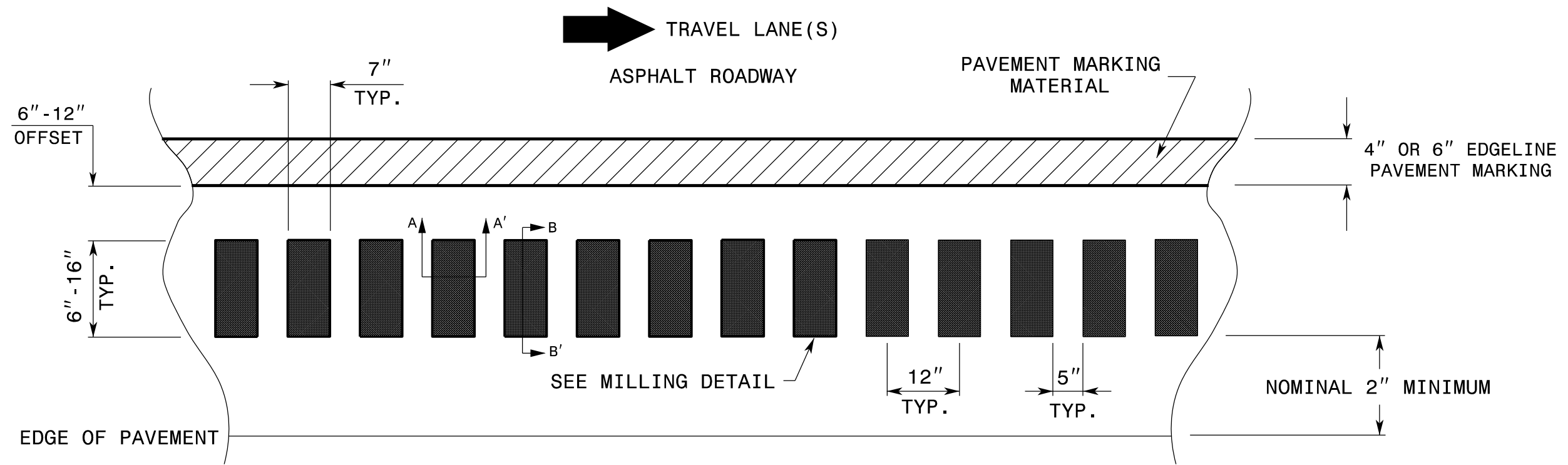
ENGLISH DETAIL DRAWING FOR  
**RUMBLE STRIPS / STRIPES**  
TRADITIONAL CENTERLINE RUMBLE STRIPE WITH SNOWPLOWABLE MARKERS

STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

2-23

ENGLISH DETAIL DRAWING FOR  
**RUMBLE STRIPS / STRIPES**  
TRADITIONAL SHOULDER RUMBLE STRIP

See Table 1 within Rumble Strip  
Policy for Design Guidance

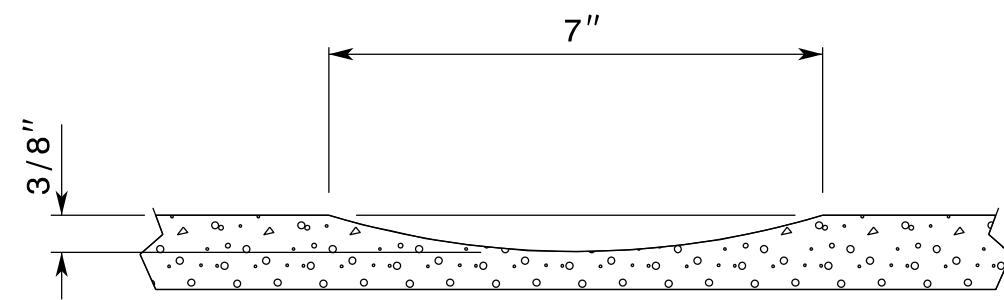


STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

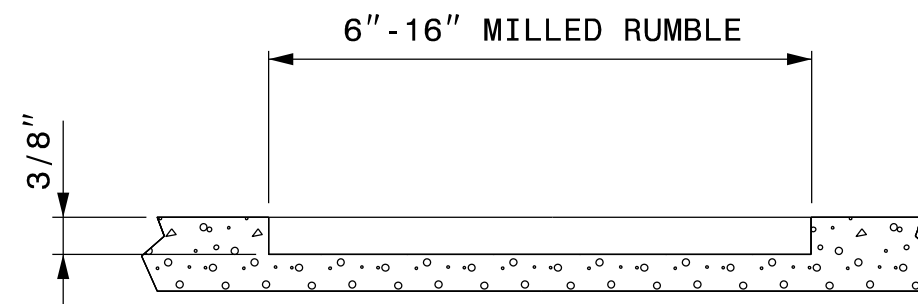
2-23

ENGLISH DETAIL DRAWING FOR  
**RUMBLE STRIPS / STRIPES**  
TRADITIONAL SHOULDER RUMBLE STRIP

MILLING DETAIL:

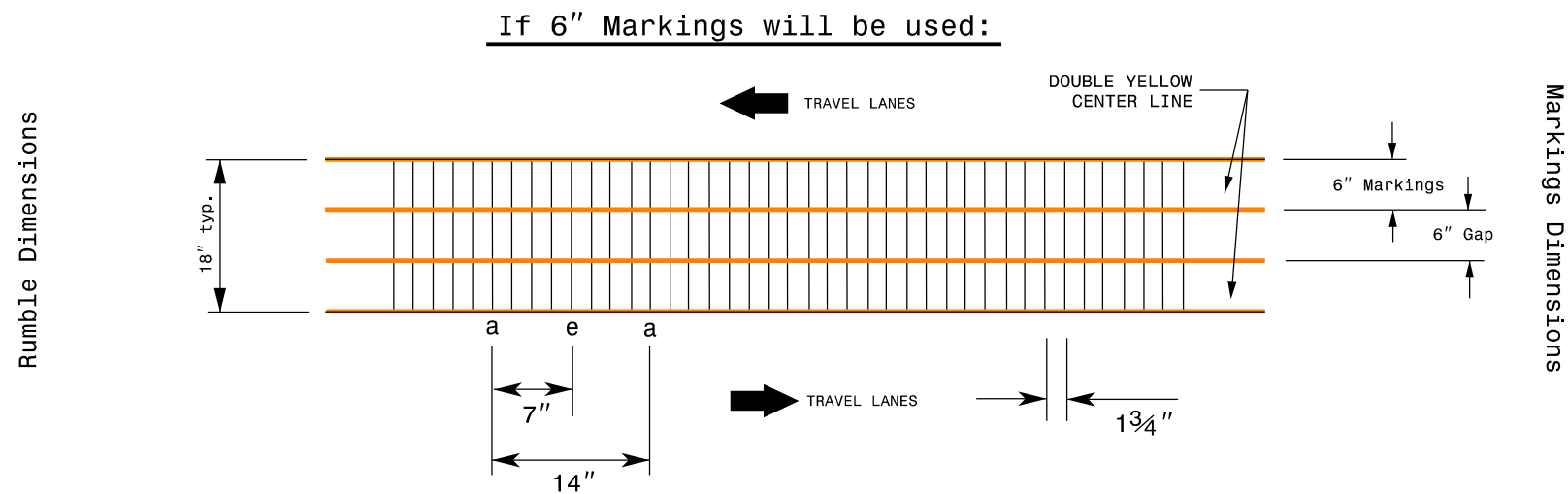
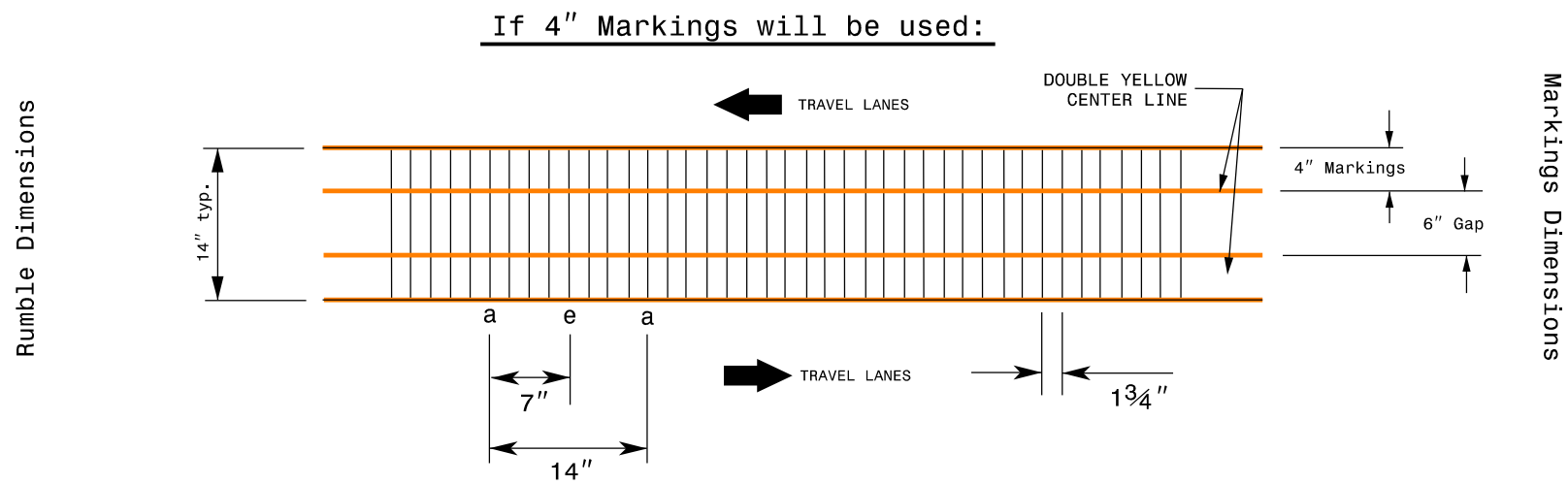


SECTION A-A'

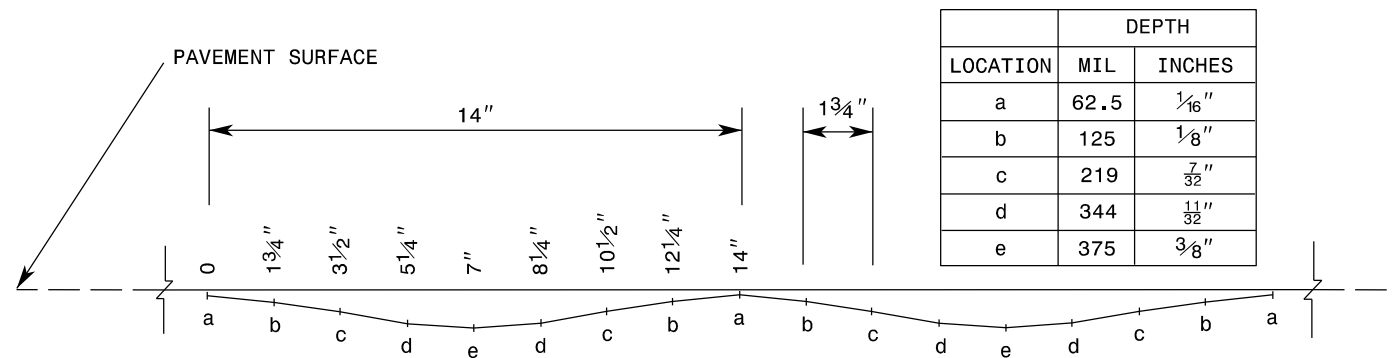


SECTION B-B'

See Table 2 within Rumble Strip Policy for Design Guidance



PROFILE:



REFERENCE DRAWING ID: Sin.CL

NOTES:

- 1) Specification in table taken from MNDOT Research Project Final Report 2016-23: *Sinusoidal Rumble Strip Design Optimization Study By: Terhaar et. al. June 2016*
- 2) USING A VACUUM, REMOVE ALL DEBRIS FROM THE MILLINGS JUST PRIOR TO PLACING ANY PAVEMENT MARKINGS.
- 3) ENSURE GLASS BEADS ARE SPREAD UNIFORMLY OVER THE ENTIRE SURFACE OF THE PAVEMENT MARKING MATERIAL.

STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

2-23

ENGLISH DETAIL DRAWING FOR  
**RUMBLE STRIPS / STRIPES**  
SINUSOIDAL EDGELINE RUMBLE STRIPE

See Table 1 within Rumble Strip Policy for Design Guidance

PLAN VIEW

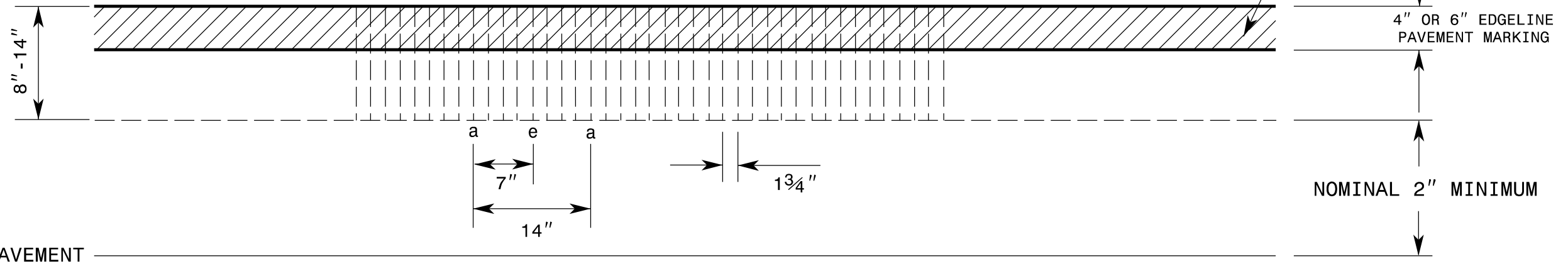
TRAVEL LANE(S)  
ASPHALT ROADWAY

PAVEMENT MARKING MATERIAL

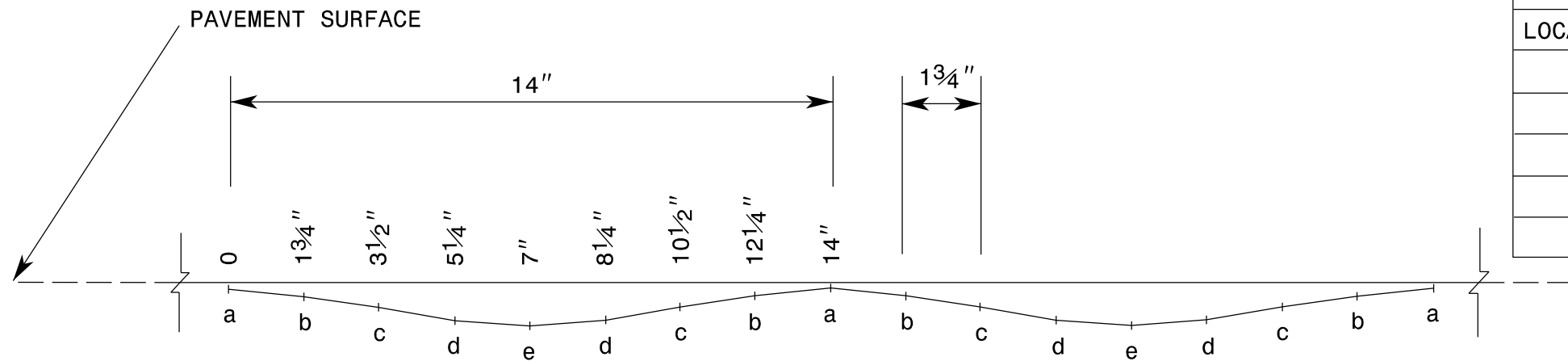
4" OR 6" EDGELINE PAVEMENT MARKING

NOMINAL 2" MINIMUM

EDGE OF PAVEMENT



PROFILE VIEW



LOCATION	DEPTH	
	MIL	INCHES
a	62.5	1/16"
b	125	1/8"
c	219	7/32"
d	344	11/32"
e	375	3/8"

REFERENCE DRAWING ID: Sin.Stripe

NOTES:

- 1) Specification in table taken from MNDOT Research Project Final Report 2016-23: *Sinusoidal Rumble Strip Design Optimization Study By Terhaar et. al, June 2016*
- 2) USING A VACUUM, REMOVE ALL DEBRIS FROM THE MILLINGS JUST PRIOR TO PLACING ANY PAVEMENT MARKINGS.
- 3) ENSURE GLASS BEADS ARE SPREAD UNIFORMLY OVER THE ENTIRE SURFACE OF THE PAVEMENT MARKING MATERIAL.

STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

2-23

ENGLISH DETAIL DRAWING FOR  
**RUMBLE STRIPS / STRIPES**  
SINUSOIDAL EDGELINE RUMBLE STRIPE



PROJECT NO.	SHEET NO.	TOTAL NO.
2025CPT.08.03.10631, 2025CPT.08.03.20631, 51406	<b>23</b>	

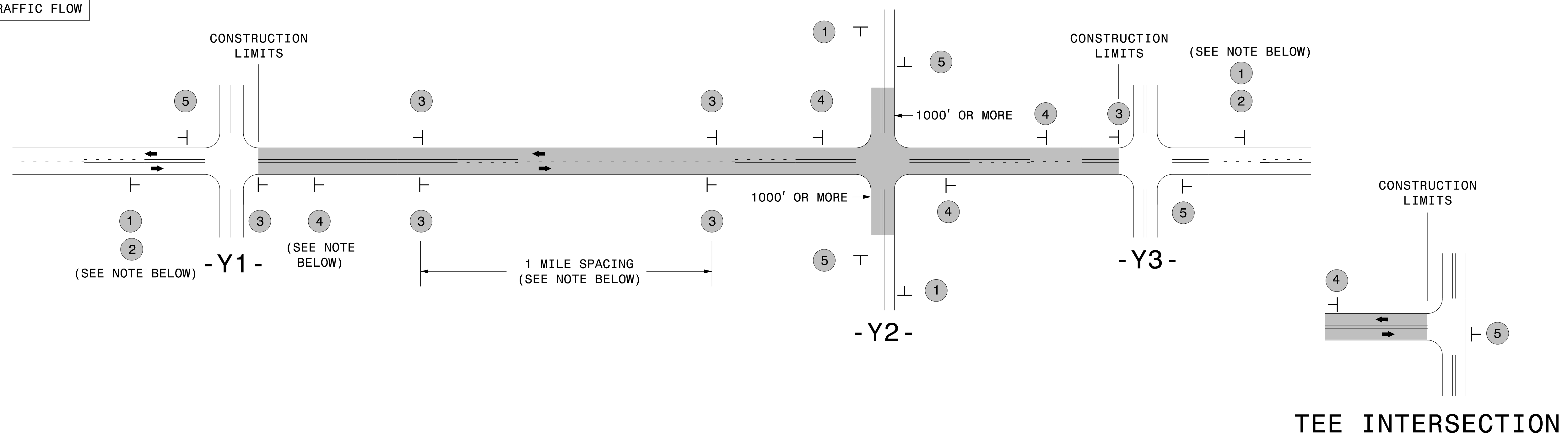
**SUMMARY OF QUANTITIES**

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	BEGIN MP	END MP	1245000000-E	1260000000-E	1297000000-E	1330000000-E	1491000000-E	1519000000-E	1523000000-E	1575000000-E	1704000000-E	1881000000-E		1881000000-E	5255000000-N		
												SHOULDER RECONSTRUCTION	AGGREGATE SHOULDER BORROW	1.5" MILLING	INCIDENTAL MILLING	BASE COURSE, B25.0C	SURFACE COURSE, S9.5B	SURFACE COURSE, S9.5C	ASPHALT BINDER FOR PLANT MIX	PATCHING EXISTING PAVEMENT	MILLED RUMBLE STRIPS (ASPHALT CONCRETE), 12"	CENTERLINE MILLED RUMBLE STRIPS (ASPHALT CONCRETE), 18"	SINUSOIDAL RUMBLE STRIPS (18" CENTERLINE)	SINUSOIDAL RUMBLE STRIPS (8" EDGELINE)	PORTABLE LIGHTING	
												SMI	TON	SY	SY	TONS	TONS	TONS	TONS	TONS	TONS	LF	LF	LF	LF	LS
								MI	FT																	
2025CPT.08.03.10631	Moore	1	NC-24 E	FROM PVMT JOINT 100 FT EAST OF RAMP TO US 1 BUS. TO PVMT JOINT AT SR 2001 (BASS RD)	2,3,7	2	2WU	1.91	28	29.59	31.5	2.48	347.00	10,800	3,032			2,964	177	50						
<b>TOTAL FOR MAP NO. 1</b>								<b>1.91</b>																		
2025CPT.08.03.10631	Moore	2	NC-24 E	FROM PVMT JOINT 900 FT EAST OF SR 1814 (BRYANT RD) TO SR 1813 (JAMES H RD)	2	2	2WU	1.07	28	25.48	26.55	2.14	300.00		625			1,602	96	30						
<b>TOTAL FOR MAP NO. 2</b>								<b>1.07</b>																		
2025CPT.08.03.10631	Moore	3	NC-690	FROM MP 7.73 50' WEST OF VICKY LN. TO MP 9.46 50' EAST OF SR 2020 (WRIGHT RD)	4	2	2WU	1.73	25	7.73	9.46			25,373	1,410			2,314	141	100			9,074	18,148	0.5	
<b>TOTAL FOR MAP NO. 3</b>								<b>1.73</b>																		
2025CPT.08.03.10631	Moore	4	NC-690	FROM EAST JOINT OF BRIDGE #200 OVER BUFFALO CREEK TO CUMBERLAND CO. LINE	5,6	2	2WU	2.2	25	10.26	12.46			32,267				2,943	177	75	23,232	11,616			0.5	
<b>TOTAL FOR MAP NO. 4</b>								<b>2.2</b>																		
2025CPT.08.03.10631	Moore	5	US-1 N	FROM PVMT JOINT 500 FT NORTH OF RICHMOND CO. LINE AT DROWNING CREEK BRIDGE #42 TO PVMT JOINT 800 FT NORTH OF PINEBLUFF FIRE DEPT.	2,7	2	MD	2.79	28	0.47	3.26	5.42	759.00	2,089	1,890			4,178	250	75					0.5	
<b>TOTAL FOR MAP NO. 5</b>								<b>2.79</b>																		
2025CPT.08.03.10631	Moore	6	US-1 S	FROM PVMT JOINT 800 FT NORTH OF PINEBLUFF FIRE DEPT. TO PVMT JOINT 500 FT NORTH OF RICHMOND CO. LINE AT DROWNING CREEK BRIDGE #231	2,7	2	MD	2.8	28	22.64	25.44	5.44	762.00	2,239	1,250			4,193	251	75					0.5	
<b>TOTAL FOR MAP NO. 6</b>								<b>2.8</b>																		
<b>TOTAL FOR PROJ NO. 2025CPT.08.03.10631</b>								<b>12.5</b>																		
																						<b>34,848</b>	<b>11,616</b>	<b>9,074</b>	<b>18,148</b>	<b>1</b>
2025CPT.08.03.20631	Moore	7	SR-2018 / MCPHERSON RD	FROM PAVEMENT JOINT AT NC 690 TO SR 2018 (CABINS CREEK RD)	1	2	2WU	0.44	21	1.49	1.05	0.88	123.00		475			495		33	13					
<b>TOTAL FOR MAP NO. 7</b>								<b>0.44</b>																		
2025CPT.08.03.20631	Moore	8	SR-2167 / MCPHERSON RD	FROM SR 2018 (CABINS CREEK RD) TO SR 2169 (RIVERVIEW DR)	1	2	2WU	0.23	21	0	0.23	0.46	64.00		135			259		17	7					
<b>TOTAL FOR MAP NO. 8</b>								<b>0.23</b>																		
2025CPT.08.03.20631	Moore	9	SR-1434 / TALC MINE RD	FROM PAVEMENT JOINT AT SR 1002 (SPIES RD) TO PAVEMENT JOINT 60 FT SOUTH OF KEY RD	1	2	2WU	1.5	20	0	1.5	3.00	420.00		445			1,607		106	38					
<b>TOTAL FOR MAP NO. 9</b>								<b>1.5</b>																		
2025CPT.08.03.20631	Moore	10	SR-1802 / NIAGARA-CARTHAGE RD	FROM SR 1803 (VASS-CARTHAGE RD) TO SR 1853 (CAMP EASTER RD)	1	2	2WU	3.99	24	5.44	9.43	7.98	1,117.00		1,295			5,125		338	100					
<b>TOTAL FOR MAP NO. 10</b>								<b>3.99</b>																		
2025CPT.08.03.20631	Moore	11	SR-1403 / JASON RD	FROM SR 1002 (SPIES RD) TO SR 1405 (DOVER CHURCH RD)	1	2	2WU	2.19	20	0	2.19	4.38	613.00		270			2,346		155	55					
<b>TOTAL FOR MAP NO. 11</b>								<b>2.19</b>																		
2025CPT.08.03.20631	Moore	12	SR-2074 / FORT BRAGG RD	FROM SR 2033 (E CONNECTICUT AVE) TO SR 2036 (E INDIANA AVE)	1	2	2WU	2.32	20	0.01	2.33	4.64	650.00		805			2,486		164	60					
<b>TOTAL FOR MAP NO. 12</b>								<b>2.32</b>																		
2025CPT.08.03.20631	Moore	13	SR-1505 / WYATT RD	FROM SR 1490 (FLINT HILL CHURCH RD) TO SR 1490 (FLINT HILL CHURCH RD)	1	2	2WU	0.96	20	0	0.96	1.92	269.00		270			1,029		68	25					
<b>TOTAL FOR MAP NO. 13</b>								<b>0.96</b>																		
2025CPT.08.03.20631	Moore	14	SR-2026 / YOUNGS RD	FROM US 1 TO PAVEMENT JOINT AT SR 2027 (OLD MAIL RD)	8	2	2WU	5.545	23	7.6	2.055	11.09	1,553.00		1,000	3,214		6,206		550	50					
<b>TOTAL FOR MAP NO. 14</b>								<b>5.545</b>																		
<b>TOTAL FOR PROJ NO. 2025CPT.08.03.20631</b>								<b>17.175</b>																		
51406	Moore	4	NC-690	FROM EAST JOINT OF BRIDGE #200 OVER BUFFALO CREEK TO CUMBERLAND CO. LINE	5,6	2	2WU	2.2	25	10.26	12.46							1,244		56						
<b>TOTAL FOR MAP NO. 4</b>								<b>2.2</b>																		
<b>TOTAL FOR PROJ NO. 51406</b>								<b>2.2</b>																		
<b>GRAND TOTAL</b>								<b>31.875</b>																		
												<b>49.83</b>	<b>6,977.00</b>	<b>72,768</b>	<b>12,902</b>	<b>4,458</b>	<b>19,553</b>	<b>18,194</b>	<b>2,579</b>	<b>753</b>	<b>23,232</b>	<b>11,616</b>	<b>9,074</b>	<b>18,148</b>	<b>1</b>	
																					<b>34,848</b>	<b>27,222</b>				



# SIGNING FOR RESURFACING PROJECTS

**LEGEND**  
 ┆ STATIONARY SIGN  
 ← DIRECTION OF TRAFFIC FLOW



## MAINLINE (-L-) SIGNING

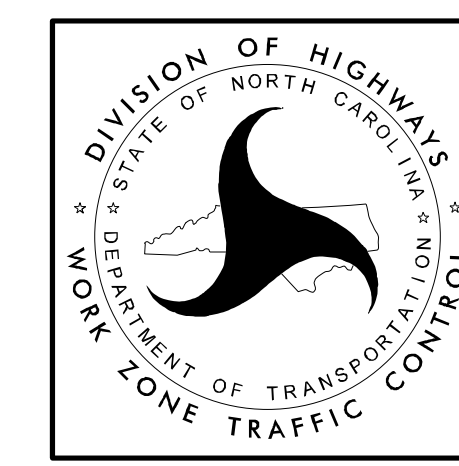
## -Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	1		PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> <li>1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE</li> <li>2) SUBDIVISION ROADS</li> <li>3) DEAD END ROADS</li> </ol> <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;">             W20-1 48" X 48"            PLACED 500' IN ADVANCE OF FLAGGER.         </div> <div style="text-align: center;">             W20-7 A 48" X 48"            PLACED 250' IN ADVANCE OF FLAGGER.         </div> </div>
	2		#2 SIGN ONLY USED WHEN CONSTRUCTION LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)	
	3		- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.	
	4		- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.	
	5		PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.	

THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.

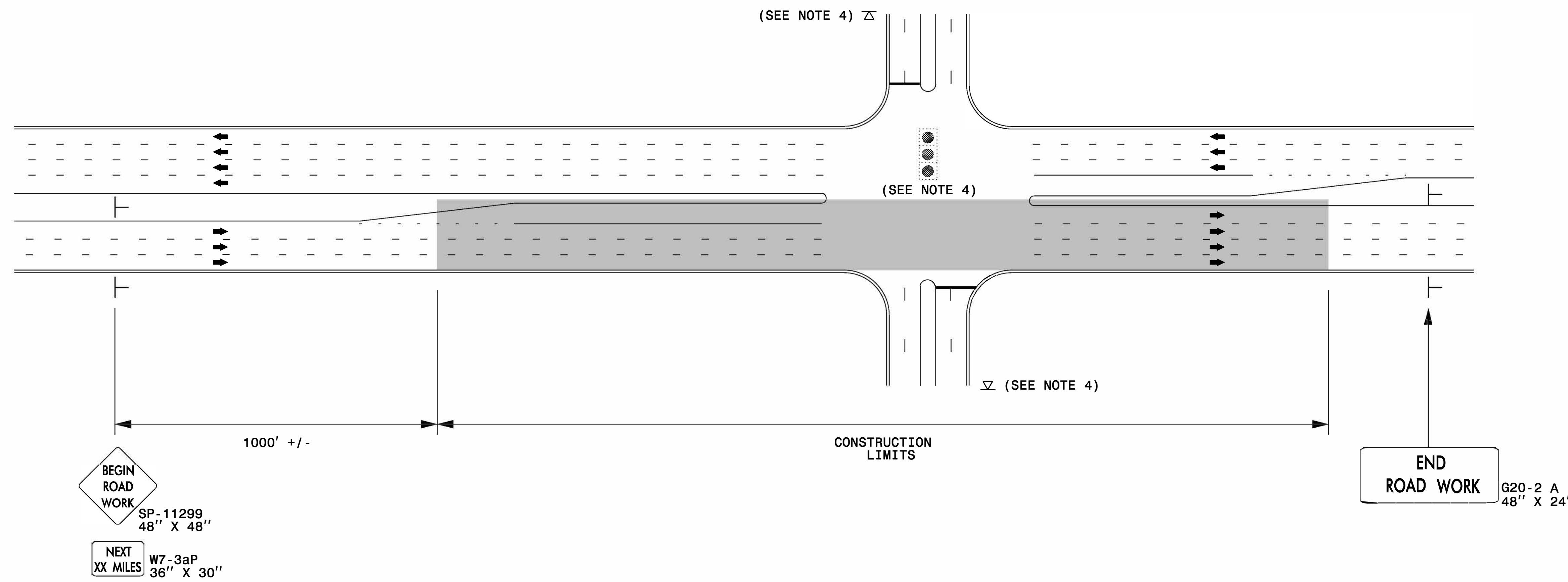
**MAPS LESS THAN 2 MILES**

FOR RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, NO STATIONARY SIGNS ARE REQUIRED. USE PORTABLE "ROAD UNDER CONSTRUCTION" OR "ROAD WORK AHEAD" SIGNS IN LIEU OF STATIONARY ADVANCE WARNINGS SIGNS.



**ADVANCE WARNING SIGNS FOR RURAL AND SUBURBAN 2-LANE ROADWAY RESURFACING**

## URBAN / SUBURBAN WORKZONES



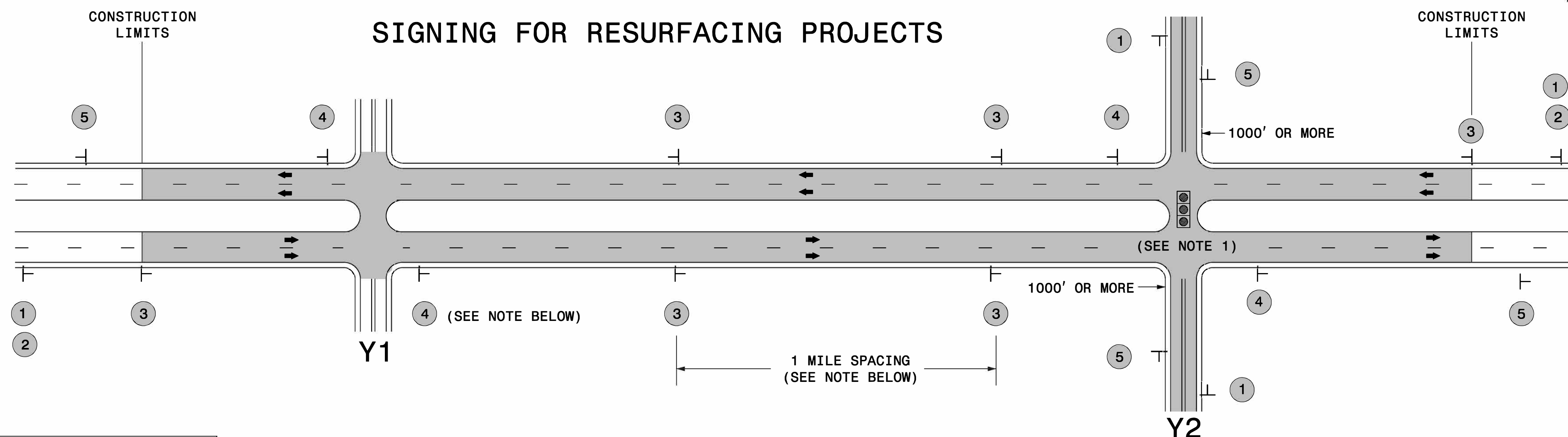
**NOTES:**

- 1) 48" x 48" SIZED SIGNS (SP- 11299) MAY BE REDUCED TO 36" X 36" ON ROADWAYS WITH SPEED LIMITS OF 40 MPH OR LESS.
- 2) MOUNT SIGNS THAT ARE LARGER THAN 10 SQUARE FEET IN AREA ON TWO OR MORE WOOD OR U-CHANNEL SUPPORTS. PERFORATED SQUARE TUBING SUPPORT SYSTEMS MAY SUPPORT LARGER AREAS ON A SINGLE SUPPORT. FOLLOW MANUFACTURER'S RECOMMENDATIONS. THESE SYSTEMS SHALL BE NCHRP 350 COMPLIANT AND NCDOT APPROVED.
- 3) ADVANCE WARNING SIGNS NOT REQUIRED ON NON-SIGNALIZED SIDE STREETS.
- 4) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION.
- 5) LATERAL CLEARANCE AT ALL SIGN LOCATIONS SHALL BE 2' AS MEASURED FROM THE EDGE OF PAVEMENT OR THE FACE OF THE CURB. WHEN UNABLE TO OBTAIN THE LATERAL CLEARANCE WITHIN THE MEDIAN AREA USE SHOULDER MOUNTS ONLY.
- 6) SIGN MOUNT LOCATIONS SHALL NOT BLOCK SIDEWALKS OR DRIVEWAYS.
- 7) IF STATIONARY GENERAL WARNING SIGNS ARE USED, THEY WILL BE PAID FOR PER SECTION 104 OF THE NCDOT STANDARD SPECIFICATIONS AS EXTRA WORK.
- 8) IF MILLED AREAS ARE NOT PAVED BACK BY THE END OF THE WORK DAY, PORTABLE SIGNS SHALL BE USED TO WARN DRIVERS OF THE PRESENT CONDITIONS. THESE ARE TO INCLUDE, BUT NOT LIMITED TO "ROUGH ROAD" W8-8, "UNEVEN LANES" W8-11, "GROOVED PAVEMENT" W8-15 w/MOTORCYCLE PLAQUE MOUNTED BELOW. THESE ARE TO BE DOUBLE INDICATED ON MULTI-LANE ROADWAYS WITH SPEED LIMITS 45 MPH AND GREATER WHERE LATERAL CLEARANCE CAN BE OBTAINED WITHIN THE MEDIAN AREAS. THESE PORTABLE SIGNS ARE INCIDENTAL TO THE OTHER ITEMS OF WORK INCLUDED IN THE TEMPORARY TRAFFIC CONTROL (LUMP SUM) PAY ITEM.

LEGEND	
├	STATIONARY SIGN
➔	DIRECTION OF TRAFFIC FLOW

	<p><b>RESURFACING ADVANCE WARNING SIGNS FOR URBAN / SUBURBAN FACILITIES</b></p>
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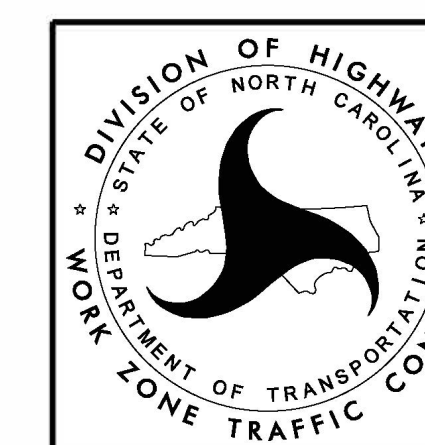


LEGEND	
┆	STATIONARY SIGN
←	DIRECTION OF TRAFFIC FLOW

### MAINLINE (-L-) SIGNING

### -Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	MAINLINE (-L-) SIGNING		-Y- LINE SIGNING		
	1	 W20-1 48" X 48"	PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> <li>1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE</li> <li>2) SUBDIVISION ROADS</li> <li>3) DEAD END ROADS</li> </ol> <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;">             W20-1            48" X 48"         </div> <div style="text-align: center;">             W20-7 A            48" X 48"         </div> </div> <p>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p> <p>NOTES:</p> <ol style="list-style-type: none"> <li>1) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION.</li> </ol>	
	2	 W7-3gP 24" X 18"	#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)		
	3	 SP 13107 48" X 48"	PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. IF NO -Y- LINES EXIST, PLACE 2ND SET 1/2 MILE FROM THE CONSTRUCTION LIMITS AND THEN SPACE 1 MILE THEREAFTER.		
	4	 SP 13106 48" X 48"	THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.		
5	 G20-2 A 48" X 24"	PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.			



RESURFACING  
ADVANCE WARNING SIGNS  
FOR RURAL AND SUBURBAN  
MULTI-LANE ROADWAYS  
W/ SHOULDER SECTIONS