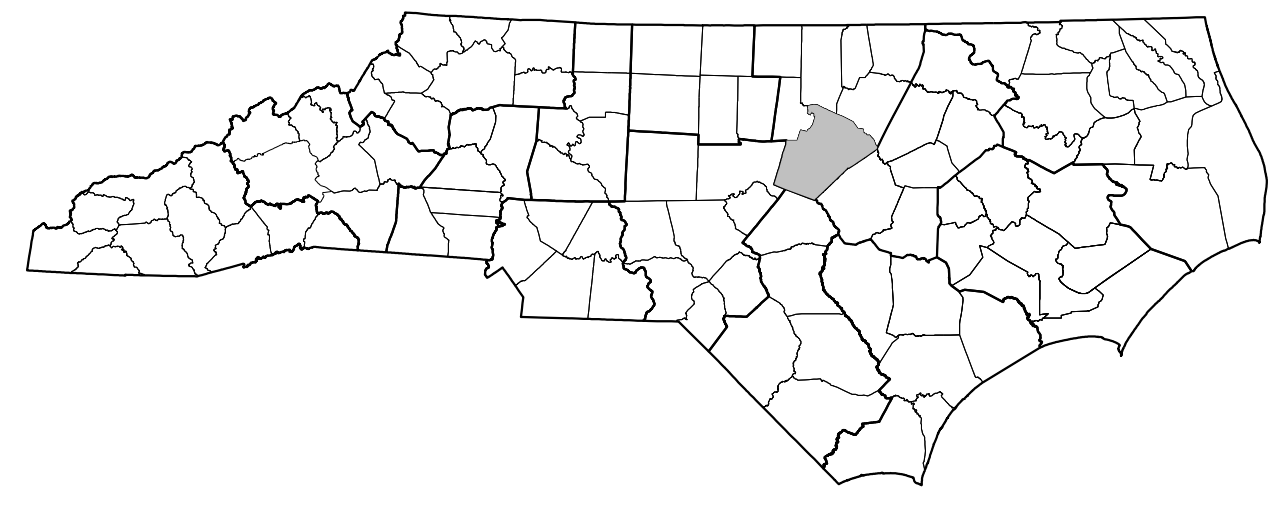


STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

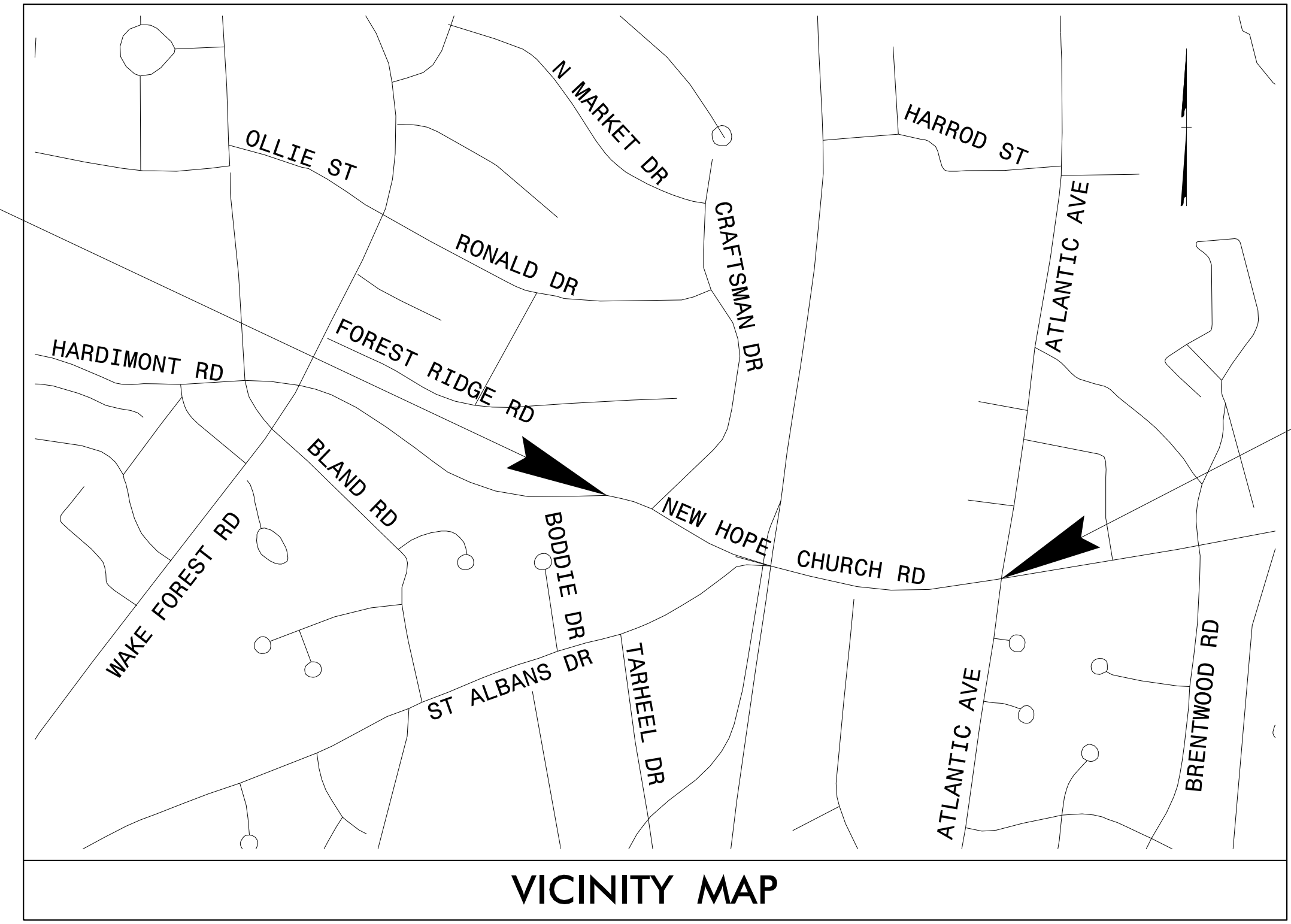
TRANSPORTATION MANAGEMENT PLAN

WAKE COUNTY
DIVISION 5



**RALEIGH - NEW HOPE CHURCH ROAD
GRADE SEPARATION**

BEGIN
PROJECT



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PROJECT

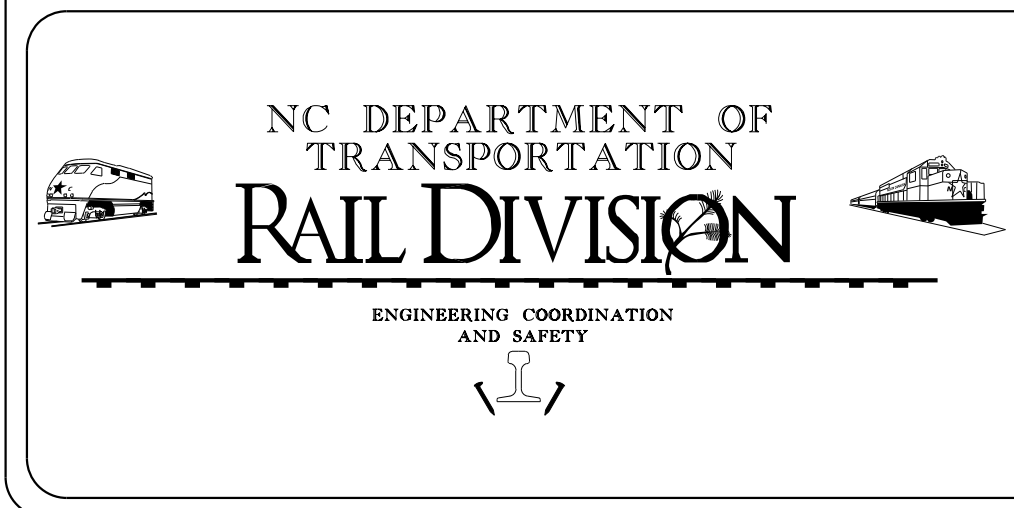
VICINITY MAP

INDEX OF SHEETS

SHEET NO.	TITLE
TMP-1	TITLE SHEET, VICINITY MAP, AND INDEX OF SHEETS
TMP-1A	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, AND LEGEND
TMP-1B, 1C	GENERAL NOTES
TMP-2	PORTABLE CONCRETE BARRIER AT TEMPORARY SHORING LOCATIONS
TMP-2A	TEMPORARY SHORING NOTES
TMP-3	TEMPORARY TRAFFIC CONTROL PHASING
TMP-4, 5, 6	-L- NEW HOPE CHURCH ROAD PHASE I
TMP-7, 8, 9	-L- NEW HOPE CHURCH ROAD PHASE II STEP 2
TMP-10	-Y1- CRAFTSMAN DRIVE PHASE II STEP 3
TMP-11, 12, 13	-L- NEW HOPE CHURCH ROAD PHASE II STEP 4
TMP-14	-Y2- ST ALBANS DRIVE DETOUR ROUTE
TMP-15	-Y1- CRAFTSMAN DRIVE DETOUR ROUTE
TMP-16	-L- NEW HOPE CHURCH ROAD DETOUR ROUTE
TMP-17	SPECIAL SIGN DESIGN

SHEET NO.
TMP-1

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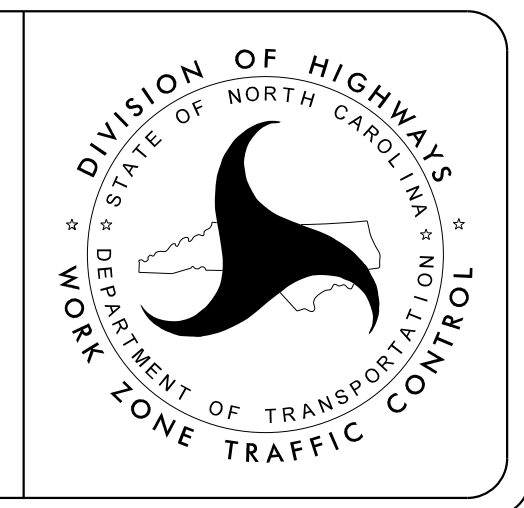
PLANS PREPARED BY:

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ANDREW N. GOOD
TRANSPORTATION DESIGNER

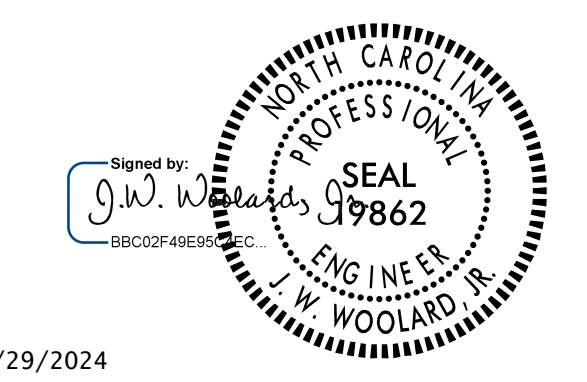
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BRIAN GACKSTETTER



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TIP PROJECT: P-5715

ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS" N.C. DEPARTMENT OF TRANSPORTATION - RALEIGH, N.C., DATED JANUARY 2024 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.01	WORK ZONE ADVANCE WARNING SIGNS
1101.02	TEMPORARY LANE CLOSURES
1101.03	TEMPORARY ROAD CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.06	WARNING SIGNS FOR BLASTING ZONES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW BOARDS
1130.01	DRUMS
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGING DEVICES
1160.01	TEMPORARY CRASH CUSHION
1165.01	WORK VEHICLE LIGHTING SYSTEMS AND TMA DELINEATION
1170.01	PORTABLE CONCRETE BARRIER
1180.01	SKINNY - DRUM
1205.01	PAVEMENT MARKINGS - LINE TYPES AND OFFSETS
1205.02	PAVEMENT MARKINGS - TWO LANE AND MULTILANE ROADWAYS
1205.12	PAVEMENT MARKINGS - BRIDGES
1261.01	GUARDRAIL AND BARRIER DELINEATORS - INSTALLATION SPACING
1261.02	GUARDRAIL AND BARRIER DELINEATORS - TYPES AND MOUNTING
1262.01	GUARDRAIL END DELINEATION
1264.01	OBJECT MARKERS - TYPES
1264.02	OBJECT MARKERS - INSTALLATION

TEMPORARY PAVEMENT MARKING SCHEDULE

PAINT PAVEMENT MARKING LINES (4")	
(P1)	WHITE EDGELINE
(P4)	3 FT. - 9 FT./SP WHITE MINISKIP
(P5)	2 FT. - 6 FT./SP WHITE MINISKIP
(P13)	YELLOW DOUBLE CENTER
PAINT PAVEMENT MARKING LINES (8")	
(P40)	WHITE GORELINE
(P41)	WHITE DIAGONAL
(P42)	YELLOW DIAGONAL
(P43)	WHITE SOLID LANE LINE
(P44)	3 FT. - 9 FT./SP WHITE MINISKIP
(P46)	3 FT. WHITE CROSSWALK LINE (8")
PAINT PAVEMENT MARKING LINES (24")	
(P60)	WHITE LINE, RR X
(P61)	WHITE STOPBAR
PAINT PAVEMENT MARKING SYMBOLS	
(P71)	RIGHT TURN ARROW
(P74)	COMBO. RIGHT/STRAIGHT ARROW
(P100)	ALPHANUMERIC CHAR
COLD APPLIED PLASTIC TYPE 4 PAVEMENT MARKING LINES (4")	
(C1)	WHITE EDGELINE
(C13)	YELLOW DOUBLE CENTER

LEGEND

GENERAL

- DIRECTION OF TRAFFIC FLOW
- EXIST. PVMT.
- PROPOSED PVMT.
- NORTH ARROW
- TEMP. SHORING (LOCATION PURPOSES ONLY)
- WORK AREA
- PREVIOUSLY STARTED / CONCURRENT CONSTRUCTION
- PAVEMENT REMOVAL
- TEMPORARY PAVEMENT

PAVEMENT MARKINGS

- EXISTING LINES
- TEMPORARY MARKINGS
- PREVIOUSLY PLACED
- WHITE EDGE LINE
- YELLOW EDGE LINE
- BROKEN LANE LINES
- MINISKIP LANE LINES
- DOUBLE YELLOW LINES
- GORELINE
- STOP BAR

PAVEMENT MARKING SYMBOLS

- EXISTING PAVEMENT MARKING SYMBOLS (HOLLOW)
- TEMPORARY SYMBOLS
- PREVIOUSLY PLACED
- PAVEMENT MARKING SYMBOLS
- ONLY
- ONLY
- PAVEMENT MARKING ALPHANUMERIC CHARACTERS

TRAFFIC CONTROL DEVICES

- | TEMPORARY DEVICES | PREVIOUSLY PLACED | |
|-------------------|-------------------|--------------------------------|
| | | BARRICADE (TYPE III) |
| | | CONE |
| | | DRUM |
| | | FLASHING ARROW BOARD |
| | | FLAGGER |
| | | LAW ENFORCEMENT |
| | | TRUCK MOUNTED ATTENUATOR (TMA) |
| | | CHANGEABLE MESSAGE SIGN |
| | | TEMPORARY CRASH CUSHION |
| | | PORTABLE CONCRETE BARRIER |

SIGNALS

- EXISTING
- PROPOSED
- TEMPORARY

TEMPORARY SIGNING

- | TEMPORARY SIGNS | PREVIOUSLY PLACED | |
|-----------------|-------------------|-----------------------------|
| | | PORTABLE SIGN |
| | | STATIONARY SIGN |
| | | STATIONARY OR PORTABLE SIGN |

PAVEMENT MARKERS

- CRYSTAL/CRYSTAL
- CRYSTAL/RED
- YELLOW/YELLOW

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DIVISION OF HIGHWAYS
STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
WORK ZONE TRAFFIC CONTROL

ROADWAY STANDARD DRAWINGS, AND LEGEND

GENERAL NOTES

PROJ. REFERENCE NO.	SHEET NO.
P-5715	TMP-1B

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS, AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS OR RESULT IN DUPLICATE OR UNDESIRE OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING, OR REMOVAL OF DEVICES AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT EXCEPT WHEN OTHERWISE NOTED IN THE PLAN OR DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
-L- (NEW HOPE CHURCH RD)	6:00 A.M.-8:00 P.M. MONDAY THRU FRIDAY 9:00 A.M.-7:00 P.M. SATURDAY THRU SUNDAY
-Y3- (ATLANTIC AVE)	6:00 A.M.-8:00 P.M. MONDAY THRU FRIDAY 9:00 A.M.-7:00 P.M. SATURDAY THRU SUNDAY

B) CONTRACTOR SHALL MAINTAIN A MINIMUM OF A TWO-LANE, TOW-WAY TRAFFIC PATTERN DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS:

ROAD NAME
-L- (NEW HOPE CHURCH ROAD)

HOLIDAY

- 1) FOR ANY UNEXPECTED OCCURRENCE THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
- 2) FOR NEW YEAR'S, BETWEEN THE HOURS OF 6:00 A.M. DECEMBER 31ST TO 7:00 P.M. JANUARY 2ND. IF NEW YEAR'S DAY IS ON A FRIDAY, SATURDAY, SUNDAY, OR MONDAY THEN UNTIL 7:00 P.M. THE FOLLOWING TUESDAY.
- 3) FOR EASTER, BETWEEN THE HOURS OF 6:00 A.M. THURSDAY AND 7:00 P.M. MONDAY.
- 4) FOR MEMORIAL DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY TO 7:00 P.M. TUESDAY.
- 5) FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 6:00 A.M. THE DAY BEFORE INDEPENDENCE DAY AND 7:00 P.M. THE DAY AFTER INDEPENDENCE DAY. IF INDEPENDENCE DAY IS ON A FRIDAY, SATURDAY, SUNDAY OR MONDAY; THEN BETWEEN THE HOURS OF 6:00 A.M. THE THURSDAY BEFORE INDEPENDENCE DAY AND 7:00 P.M. THE TUESDAY AFTER INDEPENDENCE DAY.
- 6) FOR LABOR DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY AND 7:00 P.M. TUESDAY.
- 7) FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 6:00 A.M. TUESDAY TO 7:00 P.M. MONDAY.
- 8) FOR CHRISTMAS, BETWEEN THE HOURS OF 6:00 A.M. THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 7:00 P.M. THE FOLLOWING TUESDAY AFTER THE WEEK OF CHRISTMAS.

C) DO NOT CLOSE ROADS AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
-L- (NEW HOPE CHURCH RD)	5:00 A.M.-11:00 P.M. MONDAY THRU SUNDAY

LANE AND SHOULDER CLOSURE REQUIREMENTS

- D) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED OR AS DIRECTED BY THE ENGINEER.
- E) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 15 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR A LANE CLOSURE IS INSTALLED.
- F) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- G) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- H) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRANSPORTATION MANAGEMENT PLANS, ROADWAY STANDARD DRAWINGS, OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.
- I) DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY, RAMP, OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.
- J) PROVIDE TRAFFIC CONTROL FOR APPROPRIATE LANE CLOSURES FOR SURVEYING DONE BY THE DEPARTMENT.

PAVEMENT EDGE DROP OFF REQUIREMENTS

- J) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPEN TRAVEL LANE THAT HAS A DROP-OFF AS FOLLOWS:
 - BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.
 - BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.
 - BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.
- K) DO NOT EXCEED A DIFFERENCE OF 2 INCHES IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC FOR NOMINAL LIFTS OF 1.5 INCHES. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) 500 FT IN ADVANCE AND A MINIMUM OF ONCE EVERY HALF MILE THROUGHOUT THE UNEVEN AREA.



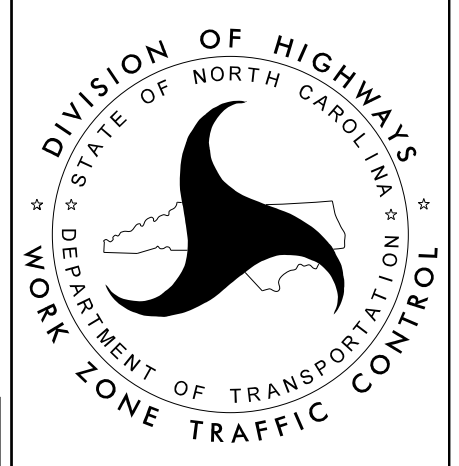
TRAFFIC PATTERN ALTERATIONS

- L) NOTIFY THE ENGINEER THIRTY (30) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

- M) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 40 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.
- N) PROVIDE SIGNING AND DEVICES REQUIRED TO CLOSE THE ROAD ACCORDING TO THE ROADWAY STANDARD DRAWINGS AND TRANSPORTATION MANAGEMENT PLANS.
 - AND
 - PROVIDE SIGNING REQUIRED FOR THE OFF-SITE DETOUR ROUTE AS SHOWN IN THE TRANSPORTATION MANAGEMENT PLANS.
- O) COVER OR REMOVE ALL SIGNS AND DEVICES REQUIRED TO CLOSE THE ROAD WHEN ROAD CLOSURE IS NOT IN OPERATION.
 - AND
 - COVER OR REMOVE ALL SIGNS REQUIRED FOR THE OFF-SITE DETOUR WHEN THE DETOUR IS NOT IN OPERATION.
- P) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.
- Q) INSTALL BLACK ON ORANGE "DIP" SIGNS (W8-2) AND/OR "BUMP" SIGNS (W8-1) 500 FT IN ADVANCE OF THE UNEVEN AREA, OR AS DIRECTED BY THE ENGINEER.

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 Stantec Consulting Services Inc. 801 Jones Franklin Road Suite 300 Raleigh, NC 27606 Tel. 919.851.6866 Fax. 919.851.7024 www.stantec.com License No. F-0672	Signed by:  8/29/2024 SEAL 19862 ENGINEER J. W. WOOLARD, P.E.		<p style="font-size: 24pt; margin: 0;">GENERAL NOTES</p>
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GENERAL NOTES

PROJ. REFERENCE NO.	SHEET NO.
P-5715	TMP-1C

TRAFFIC BARRIER

R) INSTALL TEMPORARY BARRIER ACCORDING TO THE TRANSPORTATION MANAGEMENT PLANS A MAXIMUM OF TWO (2) WEEKS PRIOR TO BEGINNING WORK IN ANY LOCATION. ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION PROCEED IN A CONTINUOUS MANNER TO COMPLETE THE PROPOSED WORK IN THAT LOCATION UNLESS OTHERWISE STATED IN THE TRANSPORTATION MANAGEMENT PLANS OR AS DIRECTED BY THE ENGINEER.

DO NOT PLACE BARRIER DIRECTLY ON ANY SURFACE OTHER THAN ASPHALT OR CONCRETE.

ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION AND NO WORK IS PERFORMED BEHIND THE TEMPORARY BARRIER FOR A PERIOD LONGER THAN TWO (2) MONTHS, REMOVE/RESET TEMPORARY BARRIER AT NO COST TO THE DEPARTMENT UNLESS OTHERWISE STATED IN THE TRANSPORTATION MANAGEMENT PLANS, TEMPORARY BARRIER IS PROTECTING A HAZARD, OR AS DIRECTED BY THE ENGINEER.

INSTALL TEMPORARY BARRIER WITH THE TRAFFIC FLOW BEGINNING WITH THE UPSTREAM SIDE OF TRAFFIC. REMOVE TEMPORARY BARRIER AGAINST THE TRAFFIC FLOW BEGINNING WITH THE DOWNSTREAM SIDE OF TRAFFIC.

INSTALL AND SPACE DRUMS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH) TO CLOSE OR KEEP THE SECTION OF THE ROADWAY CLOSED UNTIL THE TEMPORARY BARRIER CAN BE PLACED OR AFTER THE TEMPORARY BARRIER IS REMOVED.

S) PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER AT ALL TIMES DURING THE INSTALLATION AND REMOVAL OF THE BARRIER BY EITHER A TRUCK MOUNTED ATTENUATOR (MAXIMUM 72 HOURS) OR A TEMPORARY CRASH CUSHION.

PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER FROM ONCOMING TRAFFIC AT ALL TIMES BY A TEMPORARY CRASH CUSHION UNLESS THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER IS OFFSET FROM ONCOMING TRAFFIC AS FOLLOWS OR AS SHOWN IN THE PLANS:

POSTED SPEED LIMIT	MINIMUM OFFSET
40 OR LESS	15 FT
45-50	20 FT
55	25 FT
60 MPH or HIGHER	30 FT

TRAFFIC CONTROL DEVICES

T) WHEN LANE CLOSURES ARE NOT IN EFFECT SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER IN FEET THAN TWICE THE POSTED SPEED LIMIT (MPH), EXCEPT 10 FT ON-CENTER IN RADII, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY. REFER TO STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES SECTIONS 1130 (DRUMS) FOR ADDITIONAL REQUIREMENTS.

U) PLACE TYPE III BARRICADES WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY.

V) PLACE ADDITIONAL SETS OF THREE CHANNELIZING DEVICES (DRUMS, CONES OR SKINNY DRUMS) PERPENDICULAR TO THE EDGE OF TRAVELWAY ON (500 FT) CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC.

PAVEMENT MARKINGS AND MARKERS

W) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:

ROAD NAME	MARKING	MARKER
ALL ROADS	PAINT	TEMPORARY RAISED
BRIDGES	COLD APPLIED PLASTIC TYPE IV	TEMPORARY RAISED

X) PLACE ONE APPLICATION OF PAINT FOR TEMPORARY TRAFFIC PATTERNS. PLACE A SECOND APPLICATION OF PAINT SIX (6) MONTHS AFTER THE INITIAL APPLICATION AND EVERY SIX MONTHS AS DIRECTED BY THE ENGINEER.

Y) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.

Z) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS BY THE END OF EACH DAY'S OPERATION.

AA) TRACE THE (EXISTING AND/OR PROPOSED) MONOLITHIC ISLAND LOCATIONS WITH THE PROPER COLOR PAVEMENT MARKING PRIOR TO (REMOVAL AND/OR INSTALLATION). PLACE DRUMS TO DELINEATE ANY (EXISTING AND/OR PROPOSED) MONOLITHIC ISLANDS (AFTER REMOVAL AND/OR BEFORE INSTALLATION).

MISCELLANEOUS

BB) ALL CURB RAMP LOCATIONS SHALL BE DERIVED FROM STATIONING SHOWN ON PAVEMENT MARKING PLANS OR AS DIRECTED BY THE ENGINEER IN COORDINATION WITH THE SIGNING AND DELINEATION UNIT.

CC) MAINTAIN VEHICULAR ACCESS TO ALL DRIVEWAYS DURING THE LIFE OF THE CONTRACT, UNLESS OTHERWISE NOTED IN THE PHASING OR DIRECTED BY THE ENGINEER. USE INCIDENTAL STONE WHEN NECESSARY.

DD) ALL DIMENSIONS AND STATIONS IN THE TRANSPORTATION MANAGEMENT PLAN AND PHASING ARE APPROXIMATE (+/-); FIELD ADJUST AS NECESSARY OR AS DIRECTED BY THE ENGINEER.

EE) USE LAW ENFORCEMENT TO MAINTAIN TRAFFIC THROUGH THE WORK AREA AND/OR INTERSECTIONS AS SHOWN IN THE PLANS OR AS DIRECTED BY THE ENGINEER. LOCATIONS SHOWN IN THE PLANS ARE APPROXIMATE AND MAY BE REVISED AS THE OFFICER OR THE ENGINEER DEEM NECESSARY.

FF) COMPLETE ANY PROPOSED OR TEMPORARY WIDENING IN SUCH A MANNER THAT PONDING OF WATER WILL NOT OCCUR IN THE TRAVEL LANE.


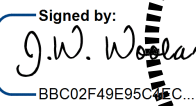
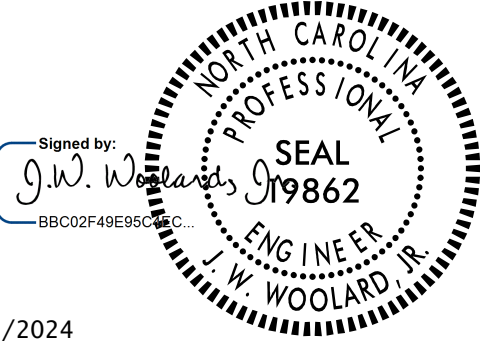
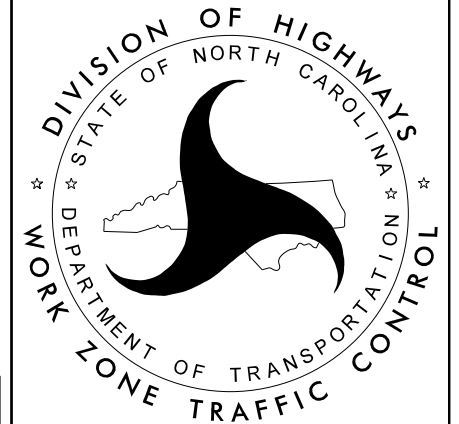
GG) ENSURE THE OVERSIZE/OVERWEIGHT PERMIT UNIT (919) 814-3700 HAS BEEN ADVISED OF THE ONGOING TRAFFIC OPERATIONS THROUGH THE DIVISION OFFICE.

HH) MAINTAIN EXISTING GUARDRAIL UNTIL PROPOSED FILL SLOPES ARE COMPLETED TO THE POINT THAT FIELD CONDITIONS NO LONGER MEET GUARDRAIL WARRANTS AS DIRECTED BY THE ENGINEER.

II) MAINTAIN ALL EXISTING SIGNING ON PROJECT (WARNING, REGULATORY AND GUIDE SIGNS). WHERE CONSTRUCTION AFFECTS THE LOCATION OF A SIGN, RELOCATE AS NECESSARY, OR INSTALL REPLACEMENT SUCH THAT THE FUNCTION OF THE SIGN IS MAINTAINED AT ALL TIMES. DURING RELOCATION OF STOP SIGNS PROVIDE FLAGGERS WITH "FLAGGER AHEAD" (W20-7a) AND "BE PREPARED TO STOP" (W3-4) SIGNS AS NECESSARY TO MAINTAIN INTERSECTION TRAFFIC.

JJ) CONTRACTOR SHALL MAINTAIN SIDEWALK ACCESS AT ALL TIMES AS STATED IN THE PHASING. CONTRACTOR SHALL BE RESPONSIBLE TO PROVIDE TEMPORARY SIDEWALKS (CONCRETE, ASPHALT, OR OTHER SUITABLE MATERIAL AS APPROVED BY THE ENGINEER) AT ALL LOCATIONS WHERE THE OPEN PEDESTRIAN TRAVELWAY HAS BEEN REMOVED FOR CONSTRUCTION OPERATIONS (UTILITIES, DRAINAGE, ETC.).

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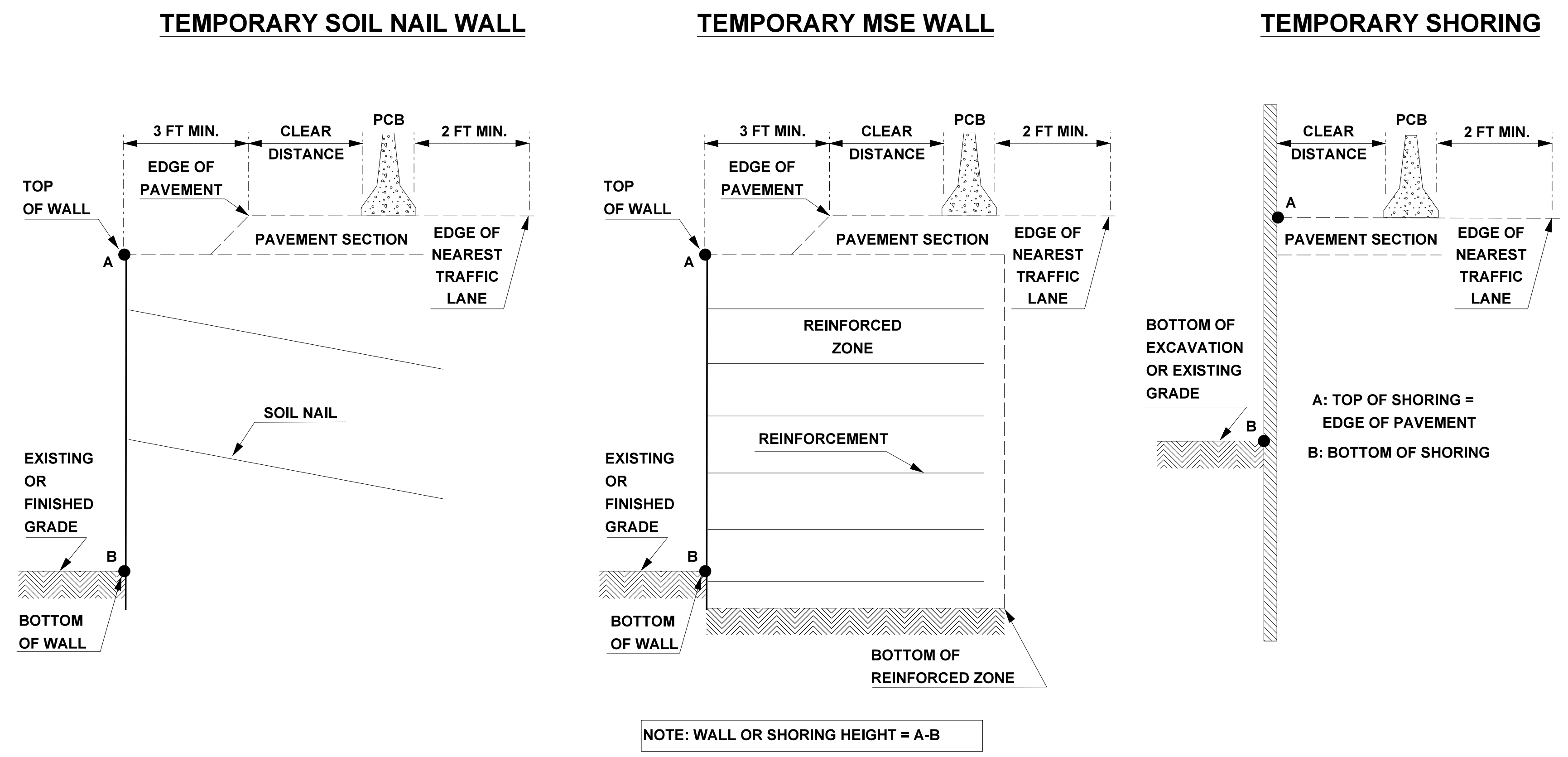


FIGURE A

NOTES

- 1- REFER TO THE TRAFFIC CONTROL PLANS FOR TEMPORARY SHORING LOCATIONS AND NOTES.
- 2- REFER TO THE "TEMPORARY SHORING" STANDARD PROVISION FOR INFORMATION ABOUT TEMPORARY SHORING AND PORTABLE CONCRETE BARRIER (PCB).
- 3- PCB IS REQUIRED IF TEMPORARY SHORING/WALL IS LOCATED WITHIN THE CLEAR ZONE IN ACCORDANCE WITH THE AASHTO ROADSIDE DESIGN GUIDE. DO NOT PLACE BARRIER DIRECTLY ON ANY SURFACE OTHER THAN ASPHALT OR CONCRETE. (CONTACT NCDOT PAVEMENT MANAGEMENT FOR APPLICABLE PAVEMENT DESIGN).
- 4- BASED ON THE CLEAR DISTANCE, OFFSET, DESIGN SPEED AND PAVEMENT TYPE, CHOOSE AN UNANCHORED OR ANCHORED PCB FROM THE TABLE SHOWN IN FIGURE B. CLEAR DISTANCE IS DEFINED AS SHOWN IN FIGURE A AND OFFSET IS DEFINED AS SHOWN IN FIGURE B.
- 5- AT THE CONTRACTOR'S OPTION OR IF THE MINIMUM REQUIRED CLEAR DISTANCE IS NOT AVAILABLE, SET PCB NEXT TO AND UP AGAINST THE TRAFFIC SIDE OF THE TEMPORARY SHORING/WALLS EXCEPT FOR BARRIER ABOVE TEMPORARY WALLS. PCB WITH THE MINIMUM REQUIRED CLEAR DISTANCE IS REQUIRED ABOVE TEMPORARY WALLS.
- 6- USE NCDOT PORTABLE CONCRETE BARRIER (PCB) IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1170.01 AND SECTION 1170 OF THE STANDARD SPECIFICATIONS.
- 7- SET PCB WITH A MINIMUM HORIZONTAL DISTANCE OF 2 FT BETWEEN THE FRONT FACE OF THE BARRIER AND THE EDGE OF THE NEAREST TRAFFIC LANE AS SHOWN IN FIGURE A UNLESS OTHERWISE SHOWN IN THE PLANS OR APPROVED BY THE ENGINEER.
- 8- FOR PCB ABOVE AND BEHIND TEMPORARY WALLS, PROVIDE A MINIMUM DISTANCE OF 3 FT BETWEEN THE EDGE OF PAVEMENT AND THE WALL FACE AS SHOWN IN FIGURE A. IF THIS MINIMUM REQUIRED DISTANCE IS NOT AVAILABLE, CONTACT THE ENGINEER.
- 9- TABLE SHOWN IN FIGURE B IS BASED ON NCDOT RESEARCH PROJECT NO. 2005-010 WITH VEHICLE TYPE USED FOR NCHRP 350 CRASH TESTS.

MINIMUM REQUIRED CLEAR DISTANCE, inches

Barrier Type	Pavement Type	Offset * ft	Design Speed, mph					
			<30	31-40	41-50	51-60	61-70	71-80
Unanchored PCB	Asphalt	<8	24	26	29	32	36	40
		8-14	26	28	31	35	38	42
		14-20	27	29	34	36	39	43
		20-26	28	31	35	38	40	44
		26-32	29	32	36	39	42	45
		32-38	30	34	38	41	43	46
		38-44	31	34	41	43	45	48
	Concrete	44-50	31	35	41	43	46	49
		50-56	32	36	42	44	47	50
		>56	32	36	42	45	47	51
		<8	17	18	21	22	25	26
		8-14	19	20	23	25	26	29
		14-20	22	22	24	26	28	31
		20-26	23	24	26	27	30	34
Anchored PCB	Asphalt	All Offsets	24 for All Design Speeds					
		Concrete (including bridge approach slabs)	All Offsets	12 for All Design Speeds				

* See Figure Below

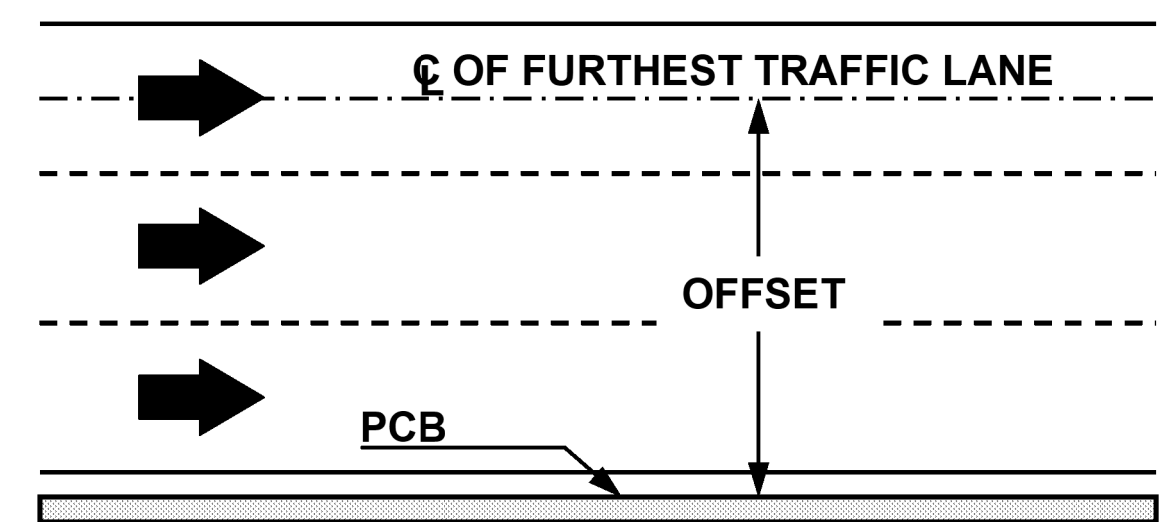
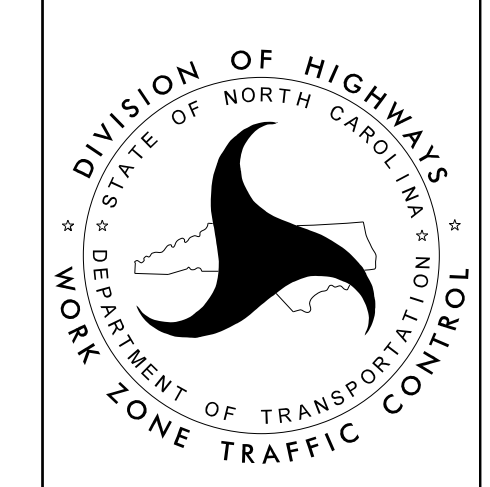


FIGURE B

DETAIL PROVIDED BY NCDOT



PORTABLE CONCRETE BARRIER AT TEMPORARY SHORING LOCATIONS

TEMPORARY SHORING LOCATION NO. 01 SEE SHEET TMP-4,5
 ESTIMATED QUANTITY = 10,368 SF

-L- STA. 13+50, CL TO -L- STA. 19+94, CL
 LENGTH=644' AVERAGE HEIGHT = 16.1 FT MAXIMUM HEIGHT = 33.6 FT

1. FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.
2. BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.
3. DESIGN TEMPORARY SHORING FROM STATION -L- 13+50, CL, TO STATION -L- 19+94, CL FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:
 UNIT WEIGHT, (γ) = 120 LB/CF
 FRICTION ANGLE, (ϕ) = 29 DEGREES
 COHESION, c = 0 LB/SF
 GROUNDWATER ELEVATION = 277 FT
4. WHEN BACKFILL FOR RETAINING WALLS AND BRIDGE APPROACH FILLS OVERLAPS WITH THE REINFORCED ZONE OF TEMPORARY WALLS, USE SHORING BACKFILL OR BACKFILL MATERIAL REQUIRED FOR RETAINING WALLS, WHICHEVER IS BETTER, IN THE REINFORCED ZONE OF TEMPORARY WALLS.
5. DO NOT USE CANTILEVER, BRACED, OR ANCHORED SHORING FOR TEMPORARY SHORING FROM STATION -L- 13+50, CL, TO STATION -L- 19+94, CL.

TEMPORARY SHORING LOCATION NO. 02 SEE SHEET TMP-5
 ESTIMATED QUANTITY = 1,039 SF

-L- STA. 19+94, CL TO -L- STA. 20+25, 3.0' LT
 LENGTH=31' AVERAGE HEIGHT = 33.5 FT MAXIMUM HEIGHT = 33.6 FT

1. FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.
2. BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.
3. DESIGN TEMPORARY SHORING FROM STATION -L- 19+94, CL, TO STATION -L- 20+25, 3.0 FT LT FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:
 UNIT WEIGHT, (γ) = 120 LB/CF
 FRICTION ANGLE, (ϕ) = 28 DEGREES
 COHESION, c = 0 LB/SF
 GROUNDWATER ELEVATION = 277 FT
4. WHEN BACKFILL FOR RETAINING WALLS AND BRIDGE APPROACH FILLS OVERLAPS WITH THE REINFORCED ZONE OF TEMPORARY WALLS, USE SHORING BACKFILL OR BACKFILL MATERIAL REQUIRED FOR RETAINING WALLS, WHICHEVER IS BETTER, IN THE REINFORCED ZONE OF TEMPORARY WALLS.
5. DO NOT USE CANTILEVER, BRACED, OR ANCHORED SHORING FOR TEMPORARY SHORING FROM STATION -L- 19+94, CL, TO STATION -L- 20+25, 3.0 FT LT.

TEMPORARY SHORING LOCATION NO. 03 SEE SHEET TMP-5
 ESTIMATED QUANTITY = 960 SF

-L- STA. 20+25, 3.0' LT TO -L- STA. 20+54, 3.0' LT
 LENGTH=29' AVERAGE HEIGHT = 33.1 FT MAXIMUM HEIGHT = 33.6 FT

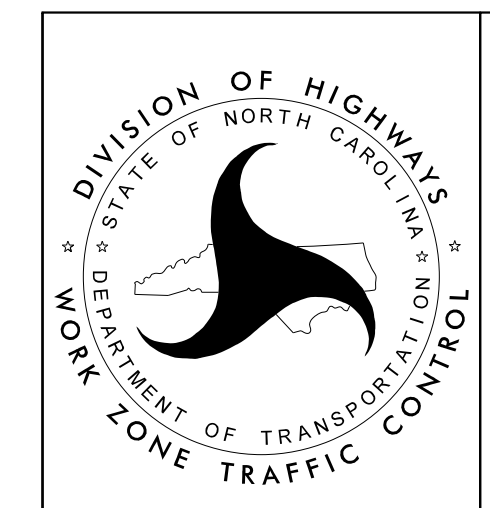
1. FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.
2. BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.
3. DESIGN TEMPORARY SHORING FROM STATION -L- 20+25, 3.0 FT LT, TO STATION -L- 20+54, 3.0 FT LT FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:
 UNIT WEIGHT, (γ) = 120 LB/CF
 FRICTION ANGLE, (ϕ) = 28 DEGREES
 COHESION, c = 0 LB/SF
 GROUNDWATER ELEVATION = 277 FT
4. WHEN BACKFILL FOR RETAINING WALLS AND BRIDGE APPROACH FILLS OVERLAPS WITH THE REINFORCED ZONE OF TEMPORARY WALLS, USE SHORING BACKFILL OR BACKFILL MATERIAL REQUIRED FOR RETAINING WALLS, WHICHEVER IS BETTER, IN THE REINFORCED ZONE OF TEMPORARY WALLS.
5. DO NOT USE CANTILEVER, BRACED, OR ANCHORED SHORING FOR TEMPORARY SHORING FROM STATION -L- 20+25, 3.0 FT LT, TO STATION -L- 20+54, 3.0 FT LT.

TEMPORARY SHORING LOCATION NO. 04 SEE SHEET TMP-5,6
 ESTIMATED QUANTITY = 6,111 SF

-L- STA. 21+68, CL TO -L- STA. 27+50, CL
 LENGTH=582' AVERAGE HEIGHT = 10.5 FT MAXIMUM HEIGHT = 29.5 FT

1. FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.
2. BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.
3. DESIGN TEMPORARY SHORING FROM STATION -L- 21+68, CL, TO STATION -L- 27+50, CL FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:
 UNIT WEIGHT, (γ) = 120 LB/CF
 FRICTION ANGLE, (ϕ) = 28 DEGREES
 COHESION, c = 0 LB/SF
 GROUNDWATER ELEVATION = 270 FT
4. WHEN BACKFILL FOR RETAINING WALLS AND BRIDGE APPROACH FILLS OVERLAPS WITH THE REINFORCED ZONE OF TEMPORARY WALLS, USE SHORING BACKFILL OR BACKFILL MATERIAL REQUIRED FOR RETAINING WALLS, WHICHEVER IS BETTER, IN THE REINFORCED ZONE OF TEMPORARY WALLS.
5. DO NOT USE CANTILEVER, BRACED, OR ANCHORED SHORING FOR TEMPORARY SHORING FROM STATION -L- 21+68, CL, TO STATION -L- 27+50, CL.

THE TEMPORARY SHORING NOTES SHOWN ON THIS SHEET WERE PROVIDED THROUGH A SEALED DOCUMENT FROM THE GEOTECHNICAL ENGINEER. THE DOCUMENT WAS SUBMITTED TO STANTEC CONSULTING ON (MARCH 08, 2019) AND SEALED BY A PROFESSIONAL ENGINEER, JEREMY R. HAMM, LICENSE #039779.



TEMPORARY SHORING NOTES

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PHASING

PHASE I

STEP 1:

PRIOR TO ANY WORK OPERATIONS, INSTALL WORK ZONE ADVANCE WARNING SIGNS ON -L- (NEW HOPE CHURCH RD.), -Y1- (CRAFTSMAN DR.), -Y2- (ST ALBANS DR.), PER ROADWAY STANDARD DRAWING 1101.01, SHEET 3.

STEP 2A: (NOT SHOWN)

AWAY FROM TRAFFIC, COMPLETE AS MUCH UTILITY CONSTRUCTION AS POSSIBLE.

INTERMEDIATE CONTRACT TIME

NOTE: COMPLETE THE WORK REQUIRED OF PHASE I, STEP 2B BETWEEN FRIDAY AT 11:00 P.M. AND THE FOLLOWING MONDAY AT 5:00 A.M. (SEE SPECIAL PROVISIONS).

STEP 2B:

CLOSE -L- AND PLACE TRAFFIC ON THE OFF-SITE DETOUR SHOWN ON TMP-16. CONSTRUCT ENCASED WATERLINE CROSSING OF -L- AT STA. 18+55±. REPLACE PAVEMENT, AND ANY NEEDED PAVEMENT MARKINGS, AND REOPEN -L- TO TRAFFIC.

INTERMEDIATE CONTRACT TIME

NOTE: COMPLETE THE WORK REQUIRED OF PHASE I, STEP 2C BETWEEN FRIDAY AT 11:00 P.M. AND THE FOLLOWING MONDAY AT 5:00 A.M. (SEE SPECIAL PROVISIONS).

STEP 2C:

CLOSE -L- AND PLACE TRAFFIC ON THE OFF-SITE DETOUR SHOWN ON TMP-16. CONSTRUCT ENCASED WATERLINE CROSSING OF -L- AT STA. 29+65±. REPLACE PAVEMENT, AND ANY NEEDED PAVEMENT MARKINGS, AND REOPEN -L- TO TRAFFIC.

STEP 2D: (NOT SHOWN)

USING NIGHTLY ROAD CLOSURES, CLOSE -L- AND PLACE TRAFFIC ON THE OFF-SITE DETOUR SHOWN ON TMP-16. CONSTRUCT SEWER LINE ON -L- FROM STA. 28+00± TO STA. 29+90±. REPEAT UNTIL SEWER LINE CONSTRUCTION IS COMPLETE.

STEP 2E: (TMP-4,5,6)

USING TEMPORARY LANE CLOSURES PER ROADWAY STANDARD DRAWING 1101.02, SHEET 3 PERFORM THE FOLLOWING:

PLACE TEMPORARY PAVEMENT MARKINGS AND TRAFFIC CONTROL DEVICES ON -L- FROM STA. 8+63+/- TO STA. 31+55+/- AS SHOW ON SHEETS TMP-4, 5, AND 6. TEMPORARY RAILROAD GATE TO BE INSTALLED BY OTHERS PRIOR TO SHIFTING TRAFFIC. SHIFT TRAFFIC TO A TWO LANE TWO WAY PATTERN.

PLACE OFF-SITE DETOUR SIGNS FOR -Y2- (ST ALBANS DR.). USING ROADWAY STANDARD DRAWING 1101.03, SHEET 2, CLOSE -Y2- TO TRAFFIC AND PLACE TRAFFIC ON THE OFF-SITE DETOUR (SEE TMP-14).

OPEN PARCELL 11'S ACCESS TO ATLANTIC DRIVE PRIOR TO CLOSING -DRWY4- AT -L-.

CLOSE -DRWY4- AND -DRWY5- AS SHOWN ON SHEET TMP-5.

PHASE I CONT.

STEP 3: (TMP-4,5,6)

USING A FLAGGING OPERATION AND RSD 1101.02, SHEET 1, CONSTRUCT TEMPORARY DRAINAGE (SEE TMP-4,5).

USING TRENCHLESS METHODS, INSTALL 36" WELDED STEEL PIPE AT -L- STA. 20+20+/- (SEE TMP-5).

AFTER TEMPORARY DRAINAGE IS INSTALLED, USING A FLAGGING OPERATION AND RSD 1101.02, SHEET 1, PLACE PCB ALONG -L- (SEE TMP-4,5,6).

STEP 4: (TMP-5)

CONSTRUCT REMAINDER OF 36" WELDED STEEL PIP AT -L- STA. 20+20+/- (SEE TMP-5).

STEP 5: (TMP-4,5,6)

CONSTRUCT AWAY FROM TRAFFIC THE FOLLOWING UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE:

- RIGHT HALF OF PROPOSED -L- (INCLUDING TEMPORARY SHORING) FROM STA. 11+10± TO STA. 27+50+/-, -Y2- FROM STA. 14+25+/- TO STA. 20+30+/-, -DRWY4-, -DRWY5-, AND -DRWYTMP2-.

PHASE II

STEP 1:

PRIOR TO SHIFING TRAFFIC USING TEMPORARY LANE CLOSURES PER ROADWAY STANDARD DRAWING 1101.02, SHEET 3 PERFORM THE FOLLOWING:

-L- (NEW HOPE CHURCH RD.), WEDGE UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE FROM STA. 27+50 TO 31+55.

NOTE: USE INCIDENTAL STONE AS NECESSARY TO MAINTAIN ACCESS TO ALL DRIVEWAYS.

PHASE II CONT.

INTERMEDIATE CONTRACT TIME

COMPLETE THE WORK REQUIRED OF PHASE II-STEP 2 AND 3 IN 75 CONSECUTIVE CALENDAR DAYS. (SEE SPECIAL PROVISIONS)

STEP 2: (TMP-7,8,9)

USING TEMPORARY LANE CLOSURES PER ROADWAY STANDARD DRAWING 1101.02, SHEET 3 PERFORM THE FOLLOWING:

PLACE OFF-SITE DETOUR SIGNS FOR -Y1- (CRAFTSMAN DR.). USING ROADWAY STANDARD DRAWING 1101.03, SHEET 2, CLOSE -Y1- TO TRAFFIC (SEE TMP-7) AND PLACE TRAFFIC ON THE OFF-SITE DETOUR (SEE TMP-15).

-L- (NEW HOPE CHURCH RD.) PLACE TEMPORARY PAVEMENT MARKINGS AND PCB. SHIFT TRAFFIC TO A TWO LANE TWO WAY PATTERN ON THE PROPOSED PAVEMENT. (SEE TMP-7,8,9).

-Y2- (ST ALBANS DR.) PLACE TEMPORARY PAVEMENT MARKINGS, REMOVE OFF-SITE DETOUR, PLACE, AND OPEN TO TRAFFIC.

-DRWY4- AND -DRWY5- OPEN TO TRAFFIC.

-L- CONTRACTOR MAY START STA. 11+10+/- TO STA. 14+00+/- AND STA. 16+50+/- TO STA. 27+50+/- . CONTRACTOR MAY START -DRWY1-, -DRWY2-, -DRWY3-.

STEP 3: (TMP-10)

1) CONSTRUCT -Y1- UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE FROM STA. 10+00+/- TO STA. 13+50+/-.

2) CONSTRUCT AWAY FROM TRAFFIC LEFT SIDE OF -L- UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE FROM STA. -L- 14+00 TO STA. -L- 16+50.

3) PLACE PAVEMENT MARKINGS ON -Y1-, REMOVE OFF-SITE DETOUR, AND OPEN -Y1- TO TRAFFIC. (SEE TMP-11)

STEP 4: (TMP-11,12,13)

CONSTRUCT PROPOSED -L- STA. 11+10+/- TO STA. 14+00+/- AND STA. 16+50+/- TO STA 27+50+/-, -DRWY1-, -DRWY2-, AND -DRWY3-, UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE.

NOTE: CONTRACTOR IS TO MAINTAIN ALL DRIVEWAY ACCESS FOR THE DURATION OF THE PROJECT.

PHASE III (NOT SHOWN)

STEP 1:

USING TEMPORARY LANE CLOSURES PER ROADWAY STANDARD DRAWING 1101.02, SHEET 3 PERFORM THE FOLLOWING:

PLACE THE FINAL LAYER OF SURFACE COURSE ON -L-, -Y1-, -Y2-, -DRWY1-, -DRWY2-, -DRWY3-, -DRWY4-, AND -DRWY5-. PLACE FINAL PAVEMENT MARKINGS AS SHOWN IN THE PAVEMENT MARKING PLAN.

STEP 2:

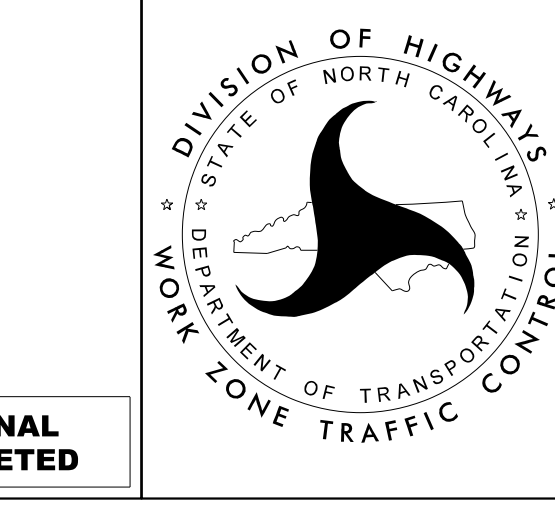
REMOVE ALL TEMPORARY TRAFFIC CONTROL DEVICES AND PLACE TRAFFIC IN THE FINAL PATTERN.

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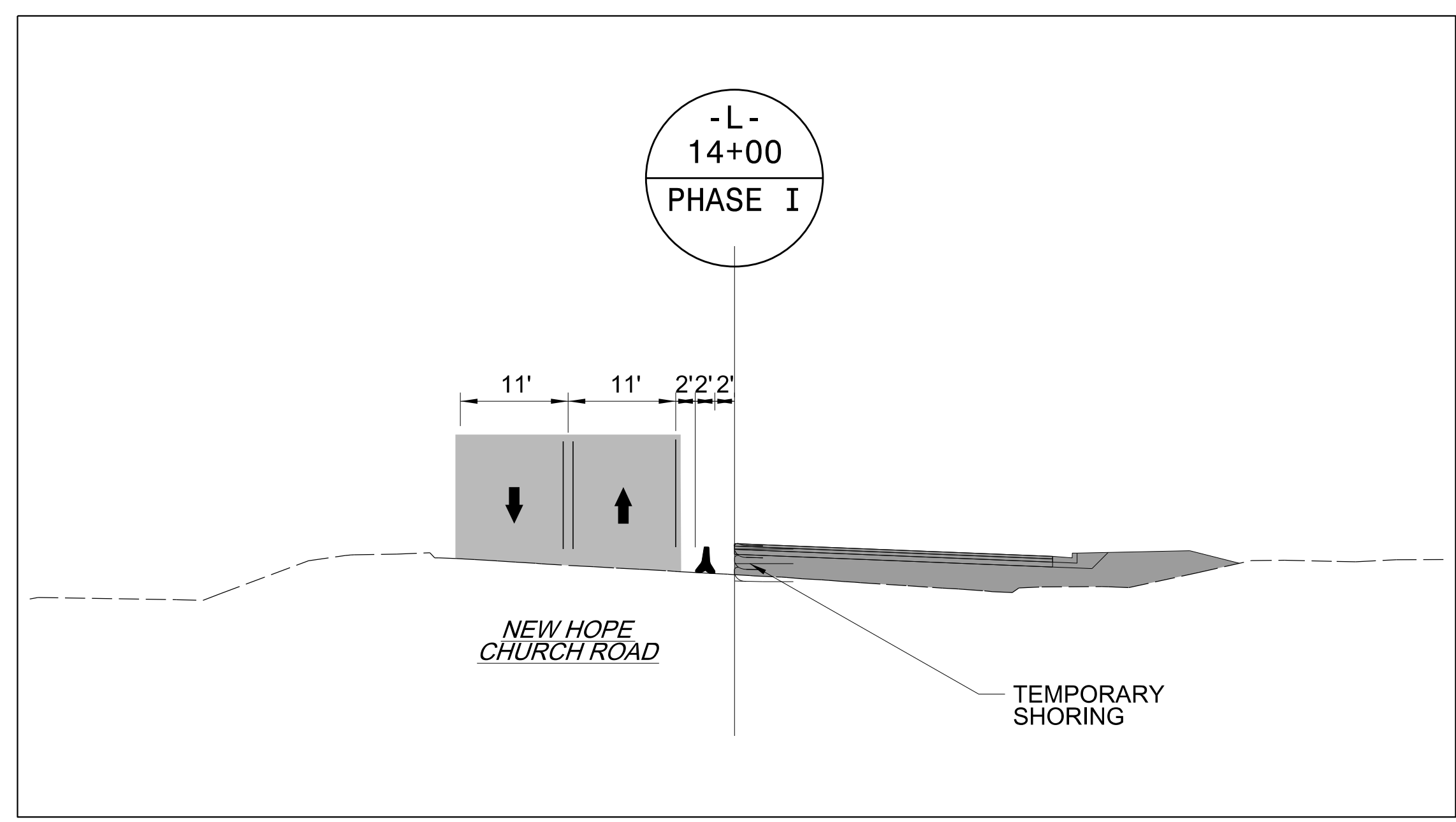
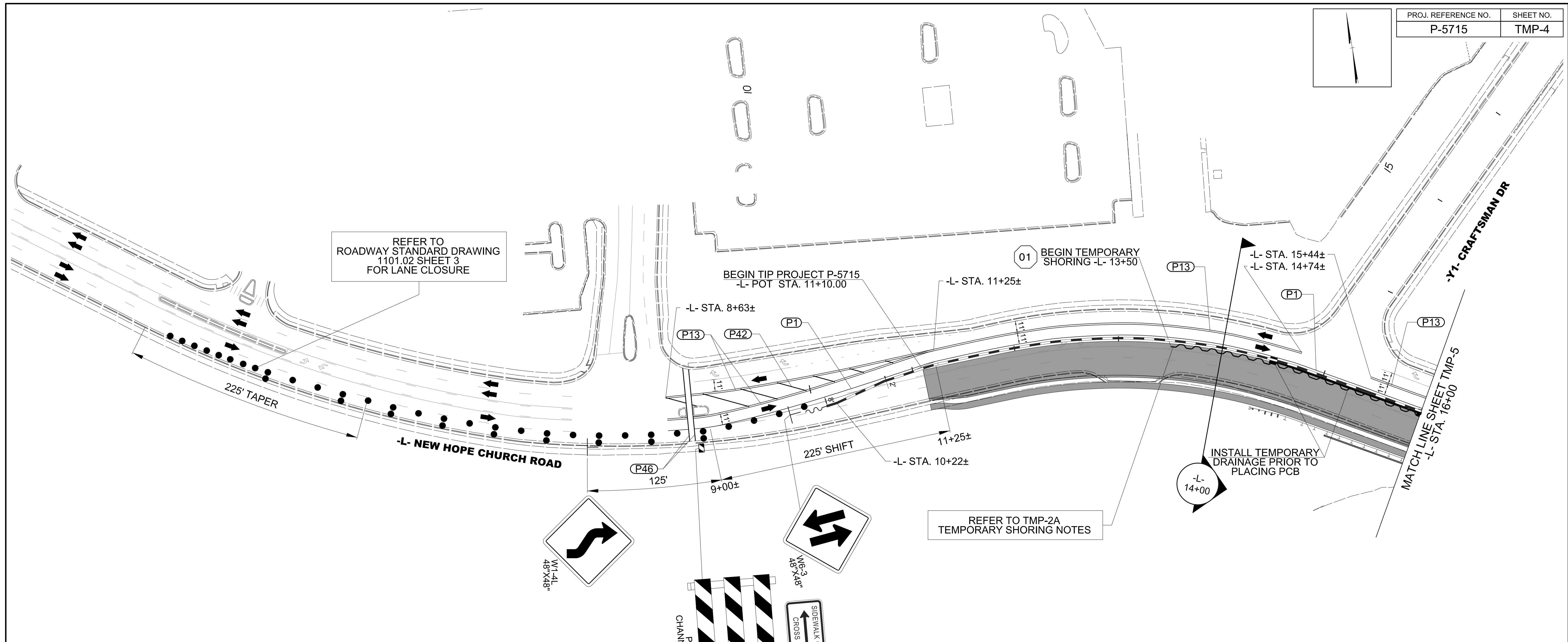
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Signed by: *J.W. Woolard*
SEAL
19862
ENGINEER
W. WOOLARD, R.
9/30/2024



DIVISION OF HIGHWAYS
DEPARTMENT OF TRANSPORTATION
STATE OF NORTH CAROLINA
WORK ZONE TRAFFIC CONTROL

PHASING



PAINT PAVEMENT MARKING LEGEND	
(P1)	WHITE EDGELINE (4")
(P13)	YELLOW DOUBLE CENTER (4")
(P42)	YELLOW DIAGONAL (8")
(P46)	WHITE CROSSWALK LINE (8")

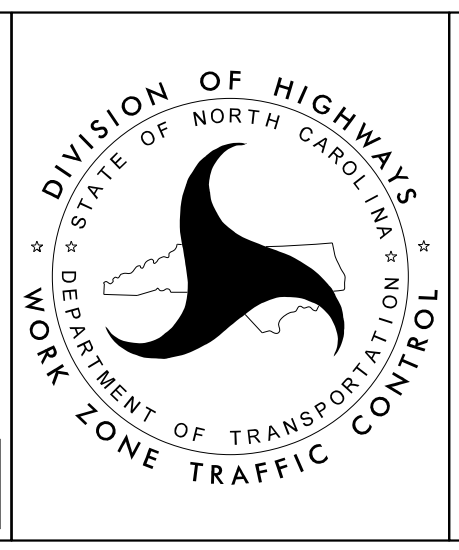
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 W. WOOLLARD, R.

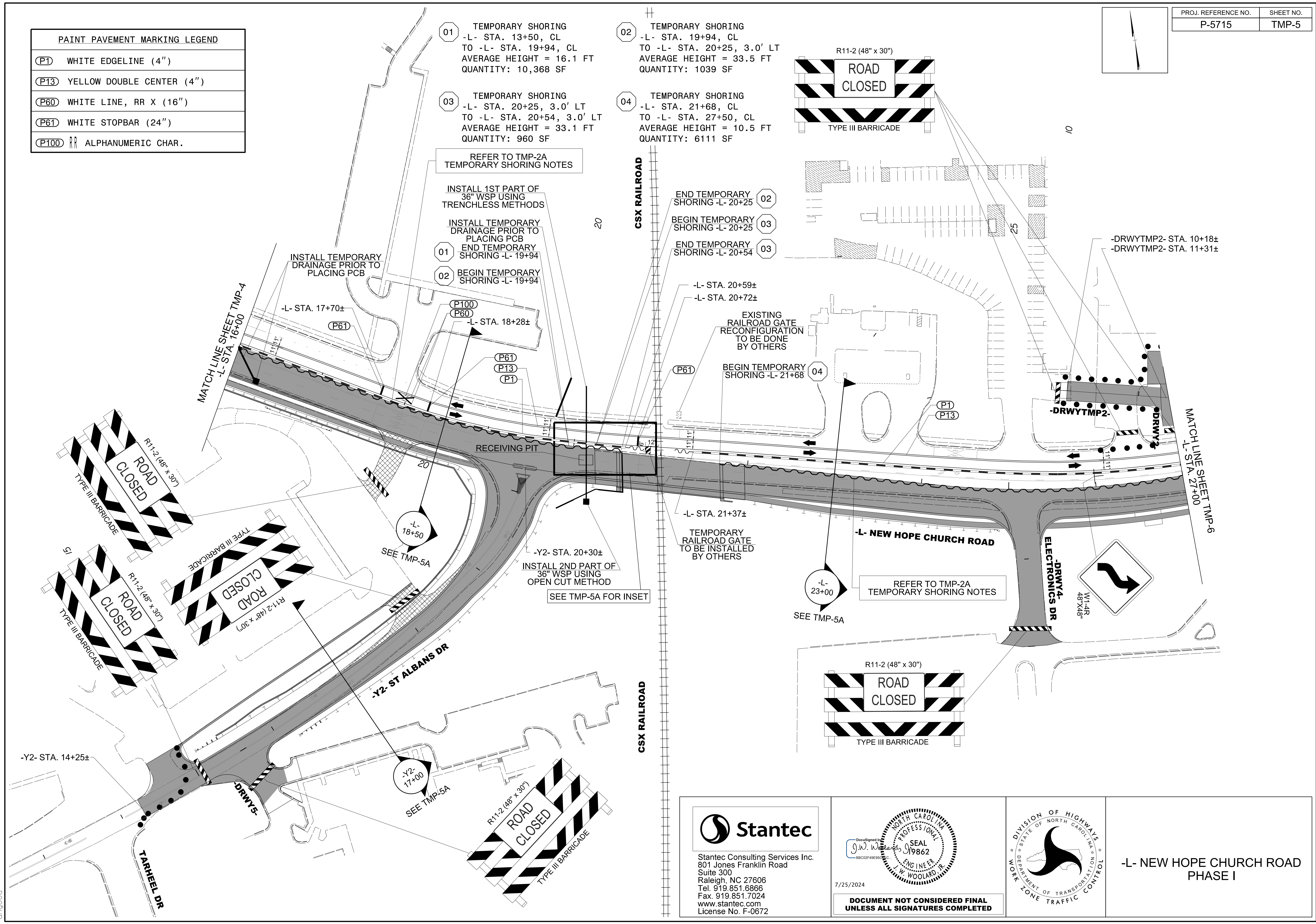
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**-L- NEW HOPE CHURCH ROAD
 PHASE I**

PAINT PAVEMENT MARKING LEGEND	
(P1)	WHITE EDGELINE (4")
(P13)	YELLOW DOUBLE CENTER (4")
(P60)	WHITE LINE, RR X (16")
(P61)	WHITE STOPBAR (24")
(P100)	RR ALPHANUMERIC CHAR.

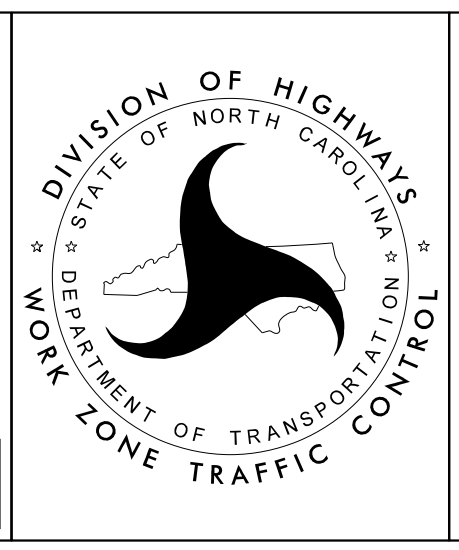
- 01 TEMPORARY SHORING
-L- STA. 13+50, CL
TO -L- STA. 19+94, CL
AVERAGE HEIGHT = 16.1 FT
QUANTITY: 10,368 SF
- 02 TEMPORARY SHORING
-L- STA. 19+94, CL
TO -L- STA. 20+25, 3.0' LT
AVERAGE HEIGHT = 33.5 FT
QUANTITY: 1039 SF
- 03 TEMPORARY SHORING
-L- STA. 20+25, 3.0' LT
TO -L- STA. 20+54, 3.0' LT
AVERAGE HEIGHT = 33.1 FT
QUANTITY: 960 SF
- 04 TEMPORARY SHORING
-L- STA. 21+68, CL
TO -L- STA. 27+50, CL
AVERAGE HEIGHT = 10.5 FT
QUANTITY: 6111 SF



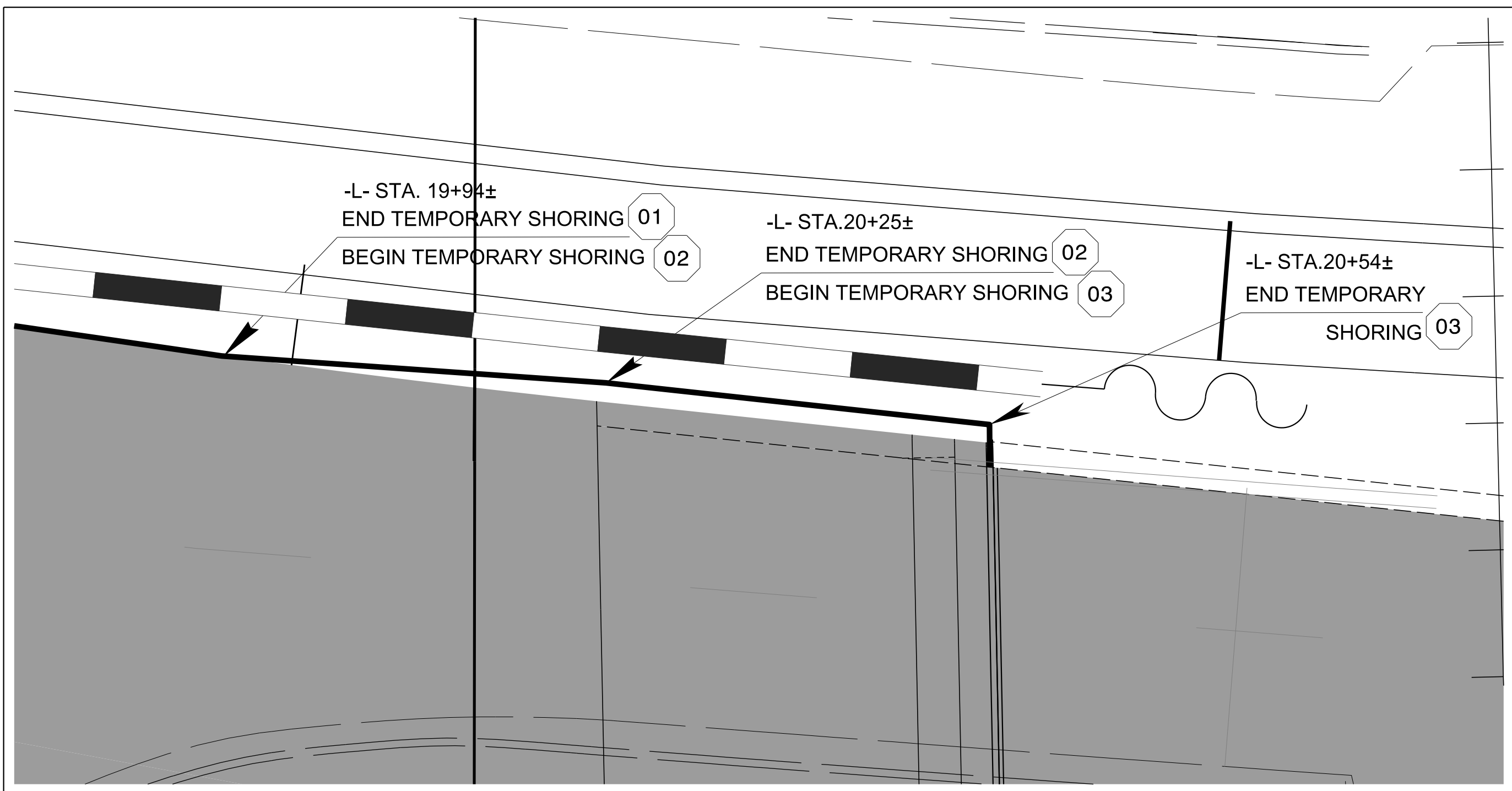
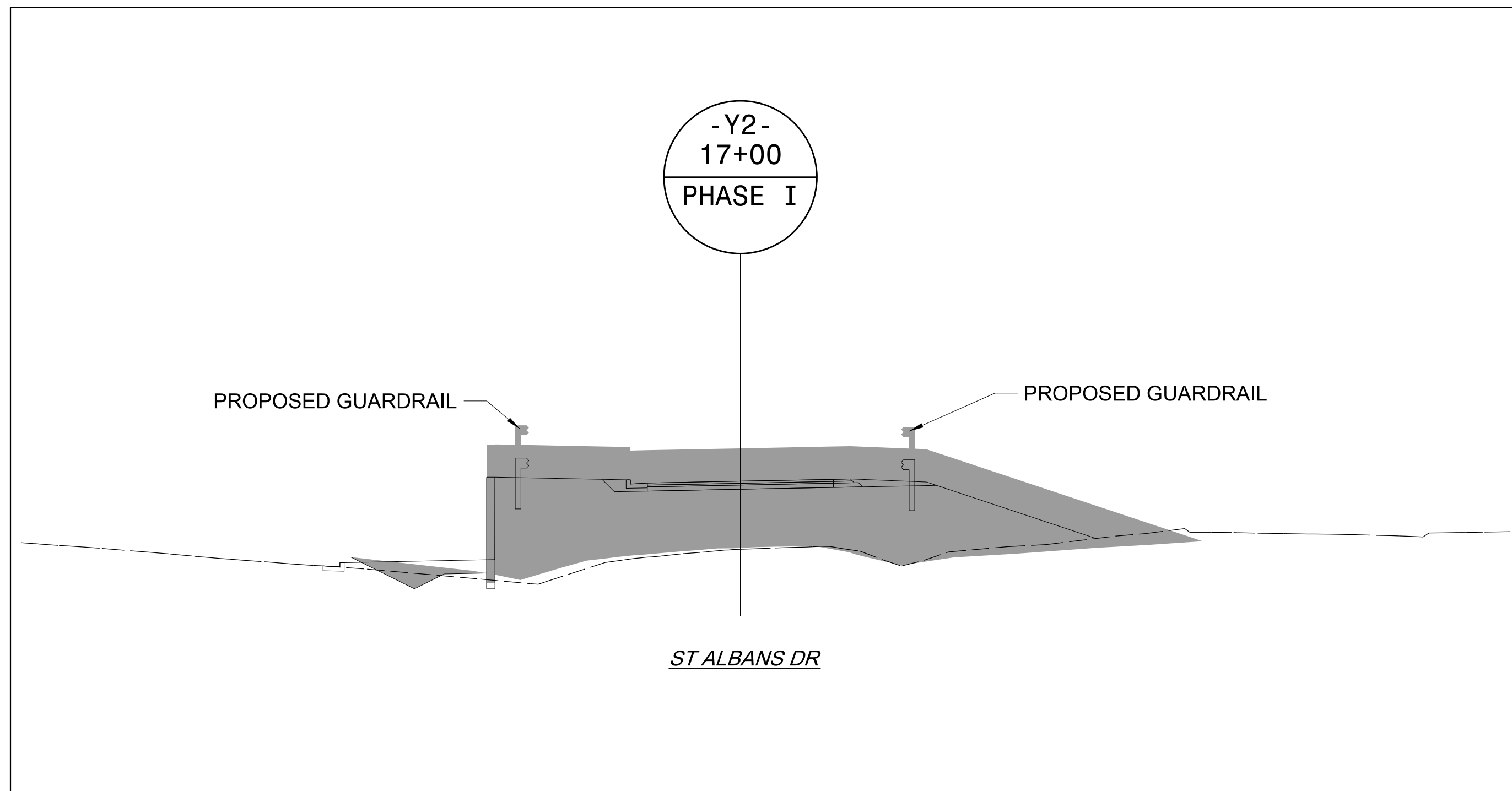
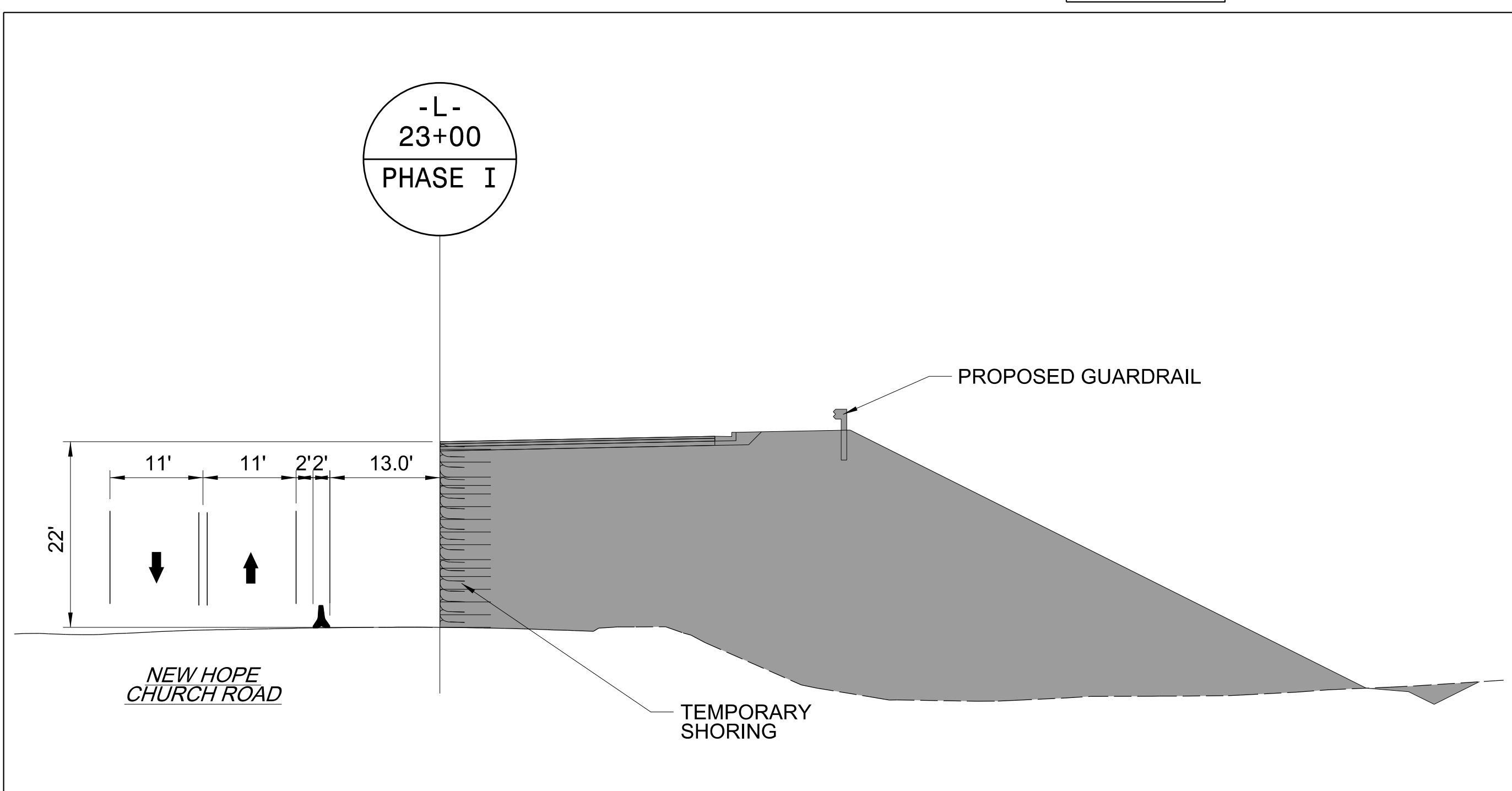
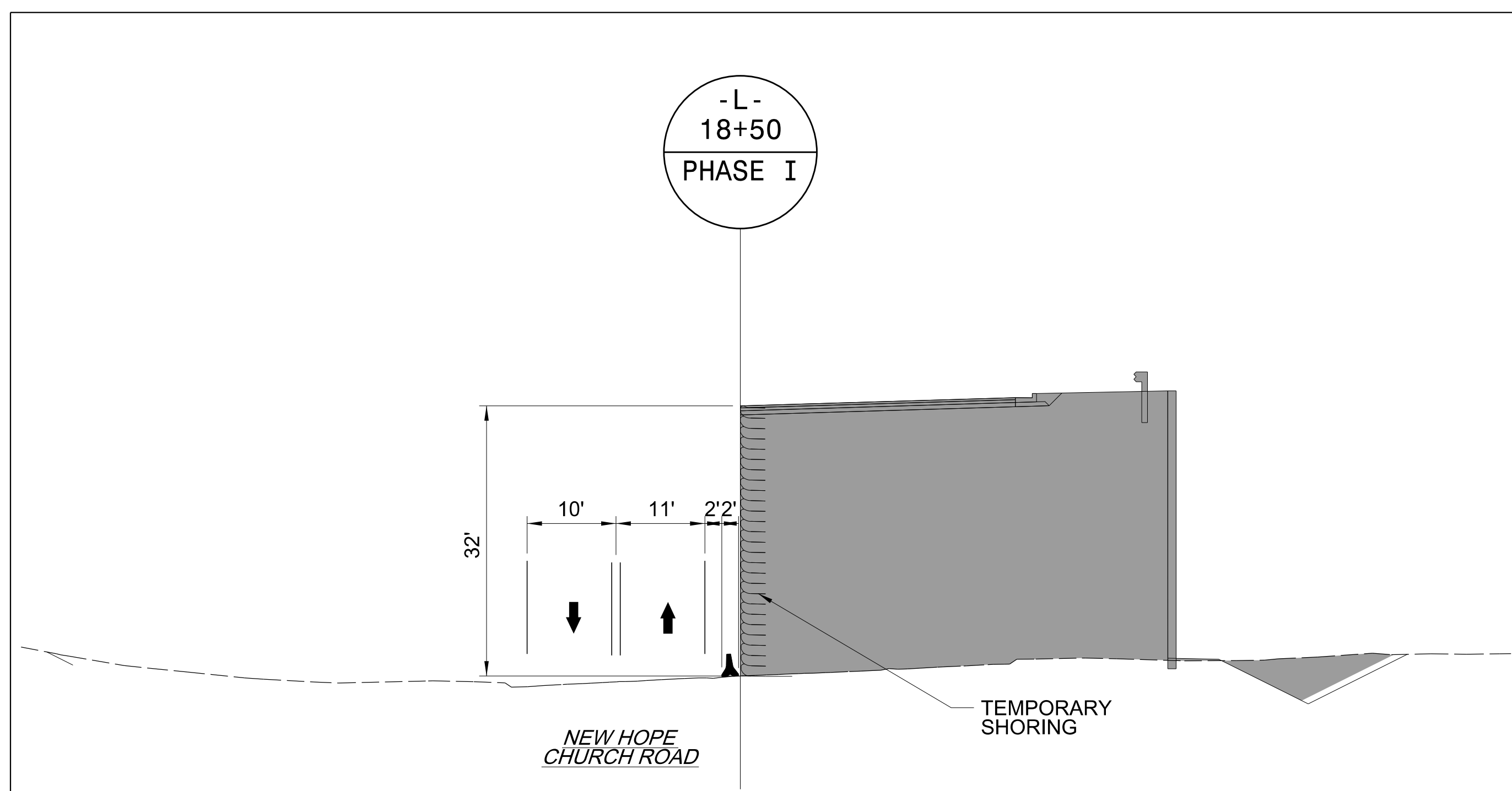
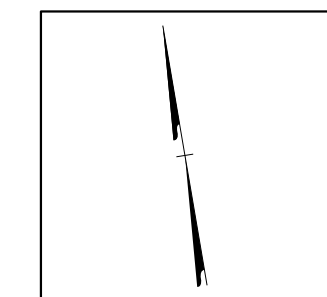
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 -L- NEW HOPE CHURCH ROAD
 PHASE I



TMP - 5 INSET

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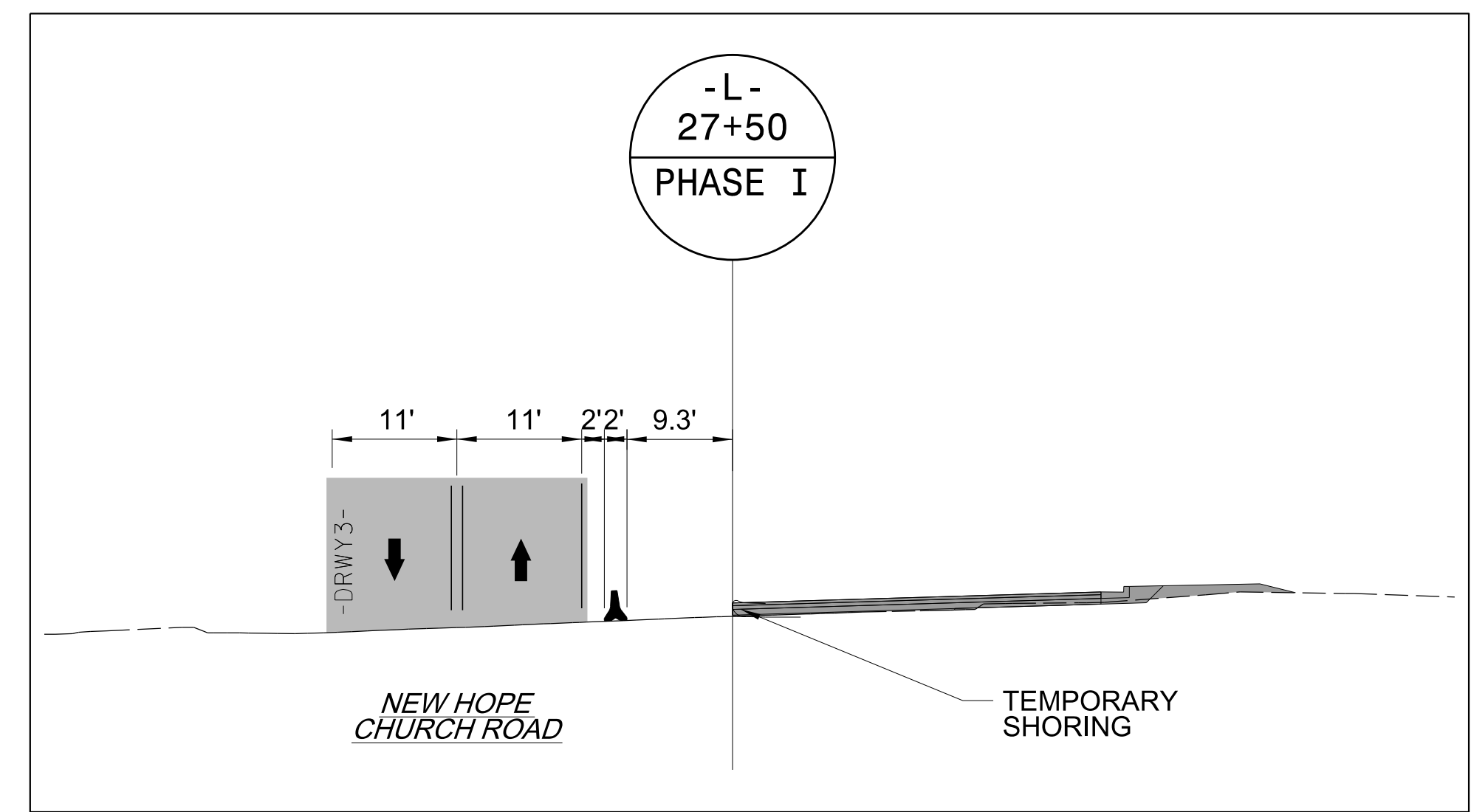
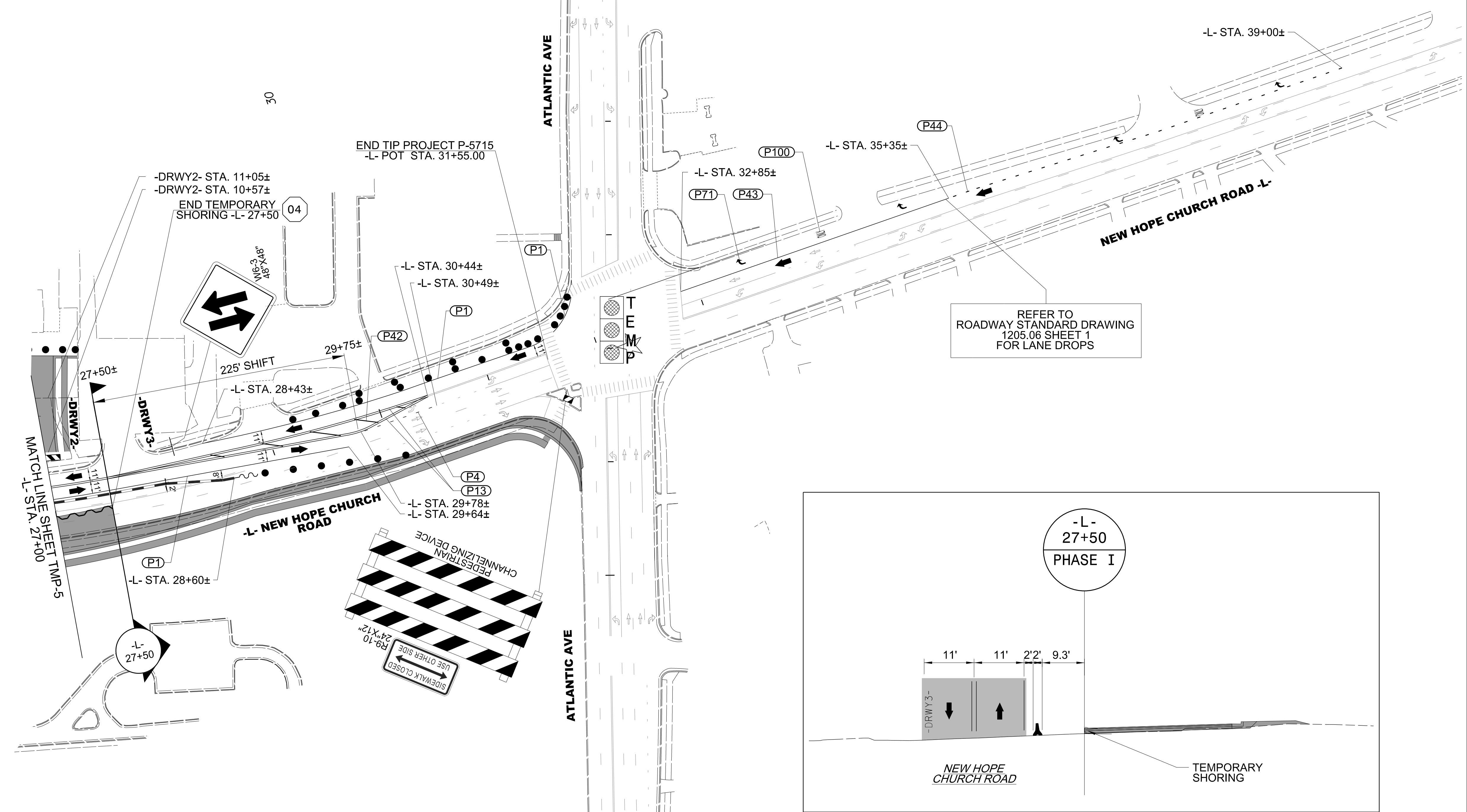
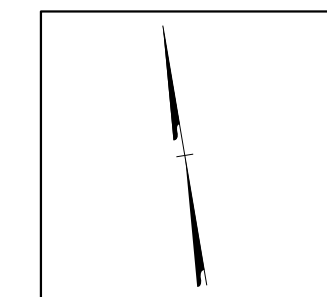
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 J.W. Woolard
 88002F48E950

5/10/2024

**-L- NEW HOPE CHURCH ROAD
 PHASE I**

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PAINT PAVEMENT MARKING LEGEND	
(P1)	WHITE EDGELINE (4")
(P4)	3 FT - 9 FT/SP WHITE MINISKIP (4")
(P13)	YELLOW DOUBLE CENTER (4")
(P42)	YELLOW DIAGONAL (8")
(P43)	WHITE SOLID LANE LINE (8")
(P44)	3 FT - 9 FT/SP WHITE MINISKIP (8")
(P74)	COMBO. RIGHT/STRAIGHT ARROW
(P100)	ALPHANUMERIC CHARACTER

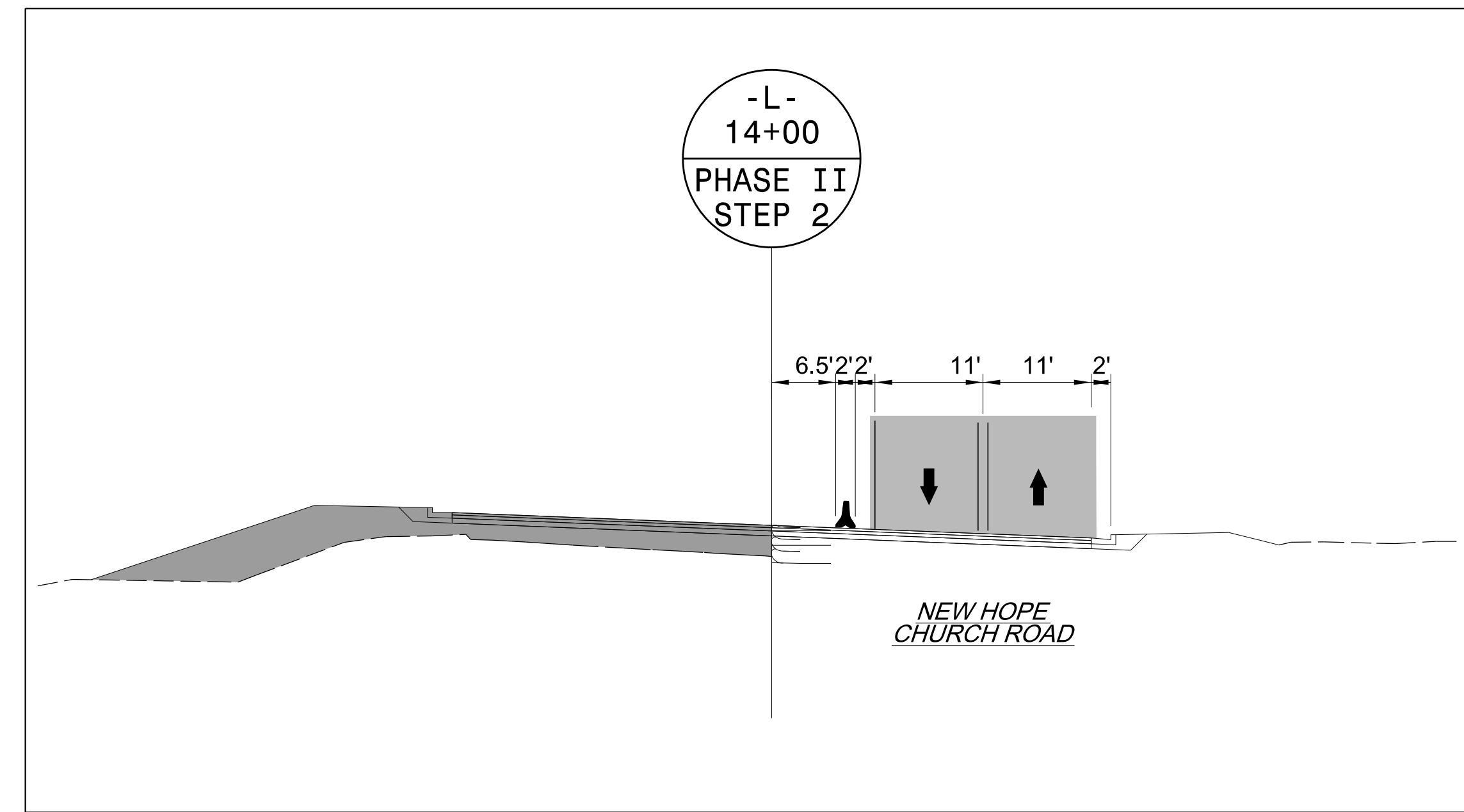
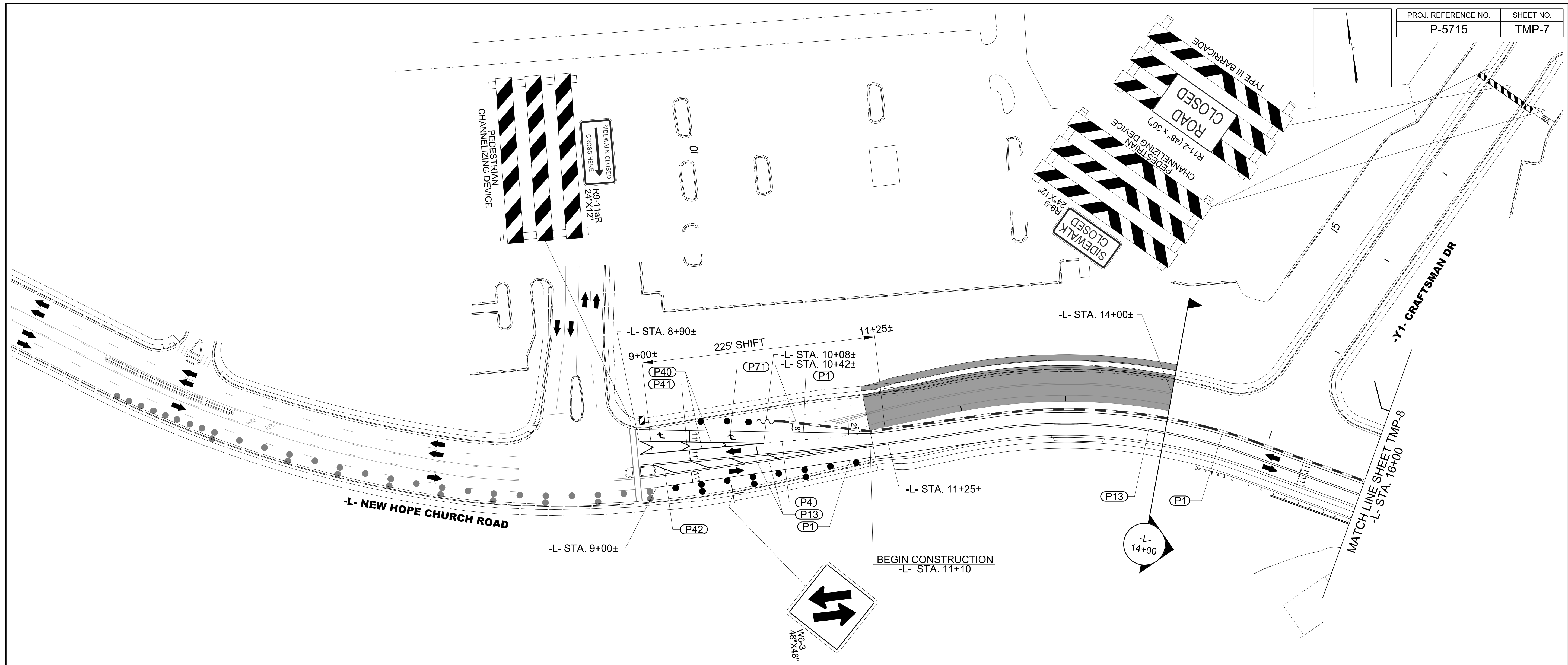
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 DEPARTMENT OF TRANSPORTATION
 WORK ZONE TRAFFIC CONTROL

-L- NEW HOPE CHURCH ROAD
PHASE I



PAINT PAVEMENT MARKING LEGEND	
(P1)	WHITE EDGELINE (4")
(P4)	3 FT - 9 FT/SP WHITE MINISKIP (4")
(P5)	2 FT - 6 FT/SP WHITE MINISKIP (4")
(P13)	YELLOW DOUBLE CENTER (4")
(P40)	WHITE GORELINE (4")
(P41)	WHITE DIAGONAL (4")
(P42)	YELLOW DIAGONAL (8")
(P61)	WHITE STOPBAR (24")
(P71)	↗ RIGHT TURN ARROW

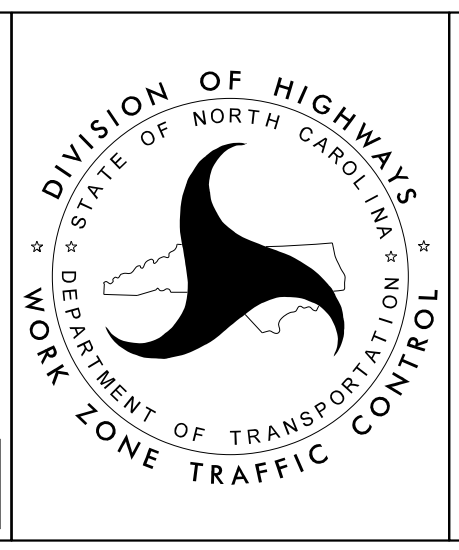
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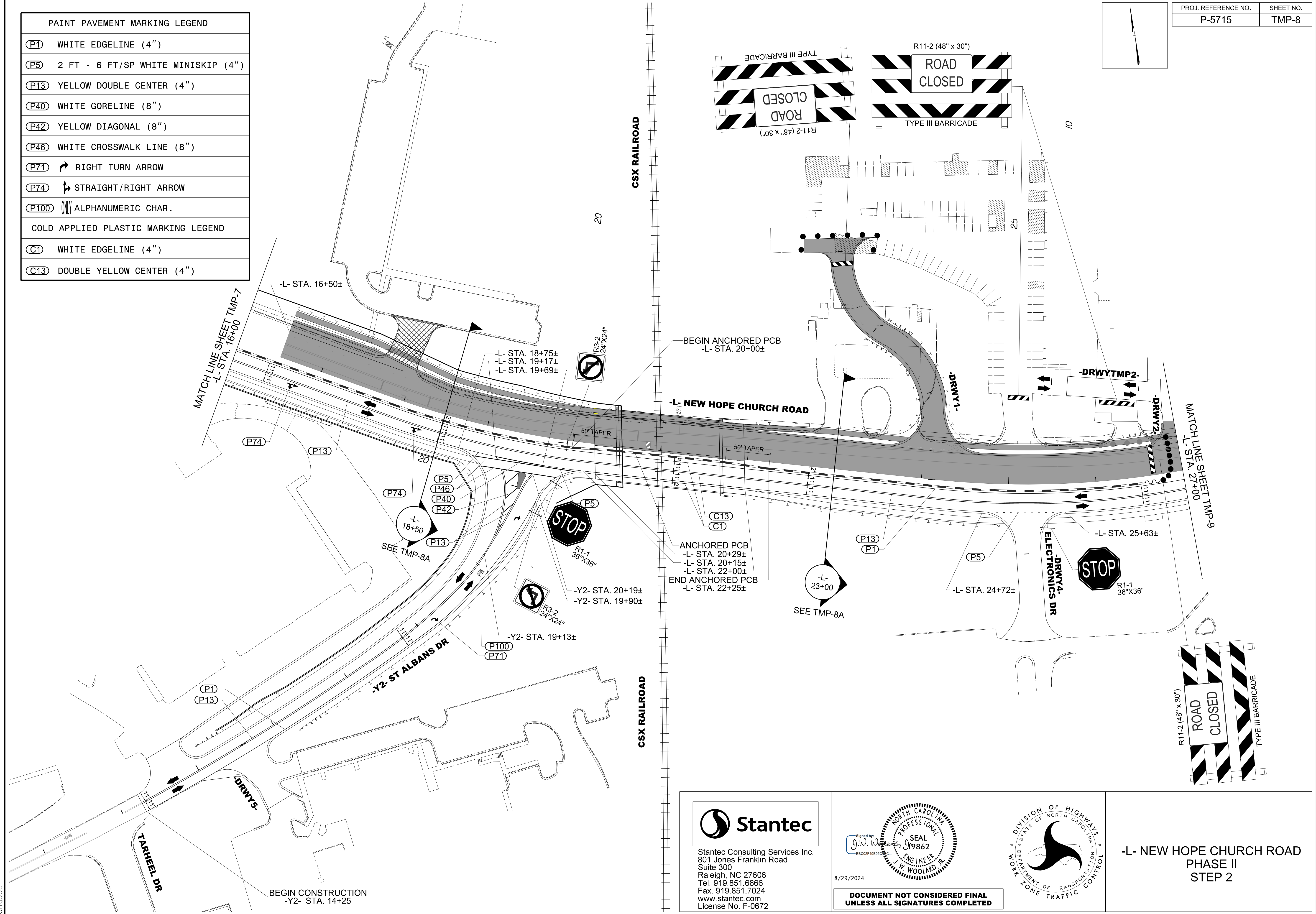
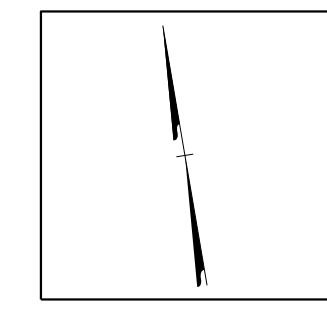
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 ENGINEER
 J. W. WOOLLARD, P.E.

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**-L- NEW HOPE CHURCH ROAD
 PHASE II
 STEP 2**

PAINT PAVEMENT MARKING LEGEND	
(P1)	WHITE EDGELINE (4")
(P5)	2 FT - 6 FT/SP WHITE MINISKIP (4")
(P13)	YELLOW DOUBLE CENTER (4")
(P40)	WHITE GORELINE (8")
(P42)	YELLOW DIAGONAL (8")
(P46)	WHITE CROSSWALK LINE (8")
(P71)	RIGHT TURN ARROW
(P74)	STRAIGHT/RIGHT ARROW
(P100)	ONLY ALPHANUMERIC CHAR.
COLD APPLIED PLASTIC MARKING LEGEND	
(C1)	WHITE EDGELINE (4")
(C13)	DOUBLE YELLOW CENTER (4")

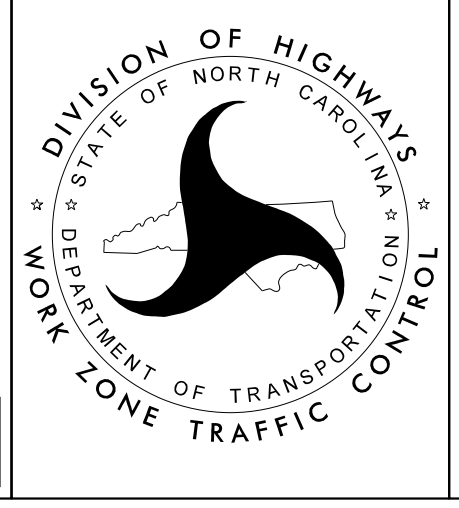


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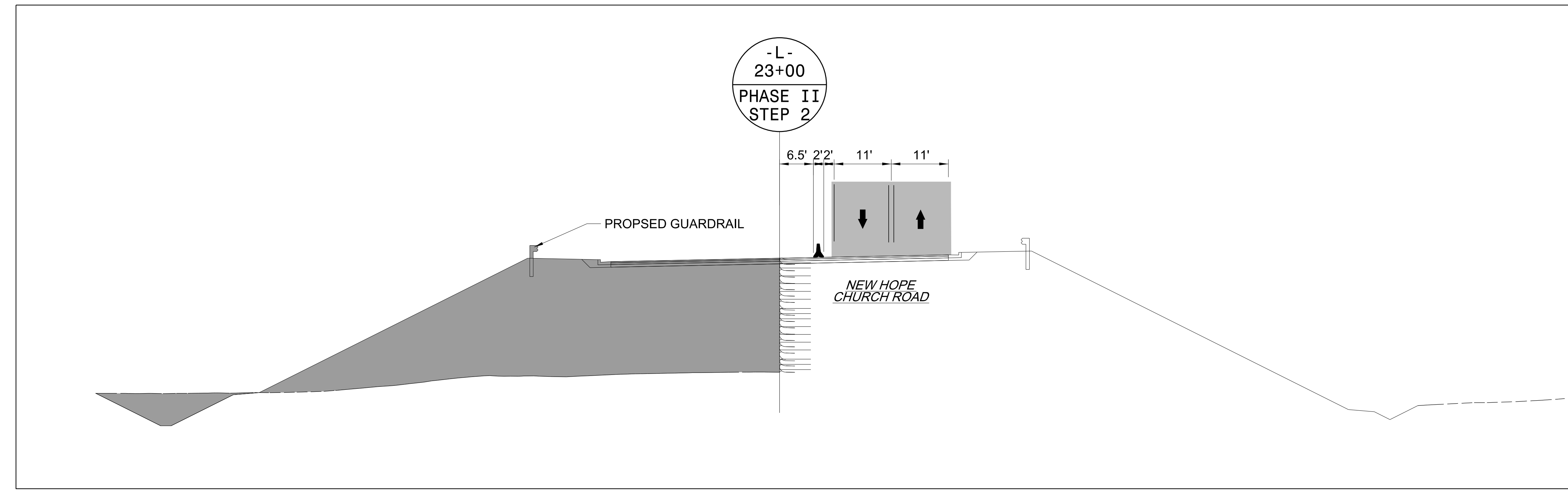
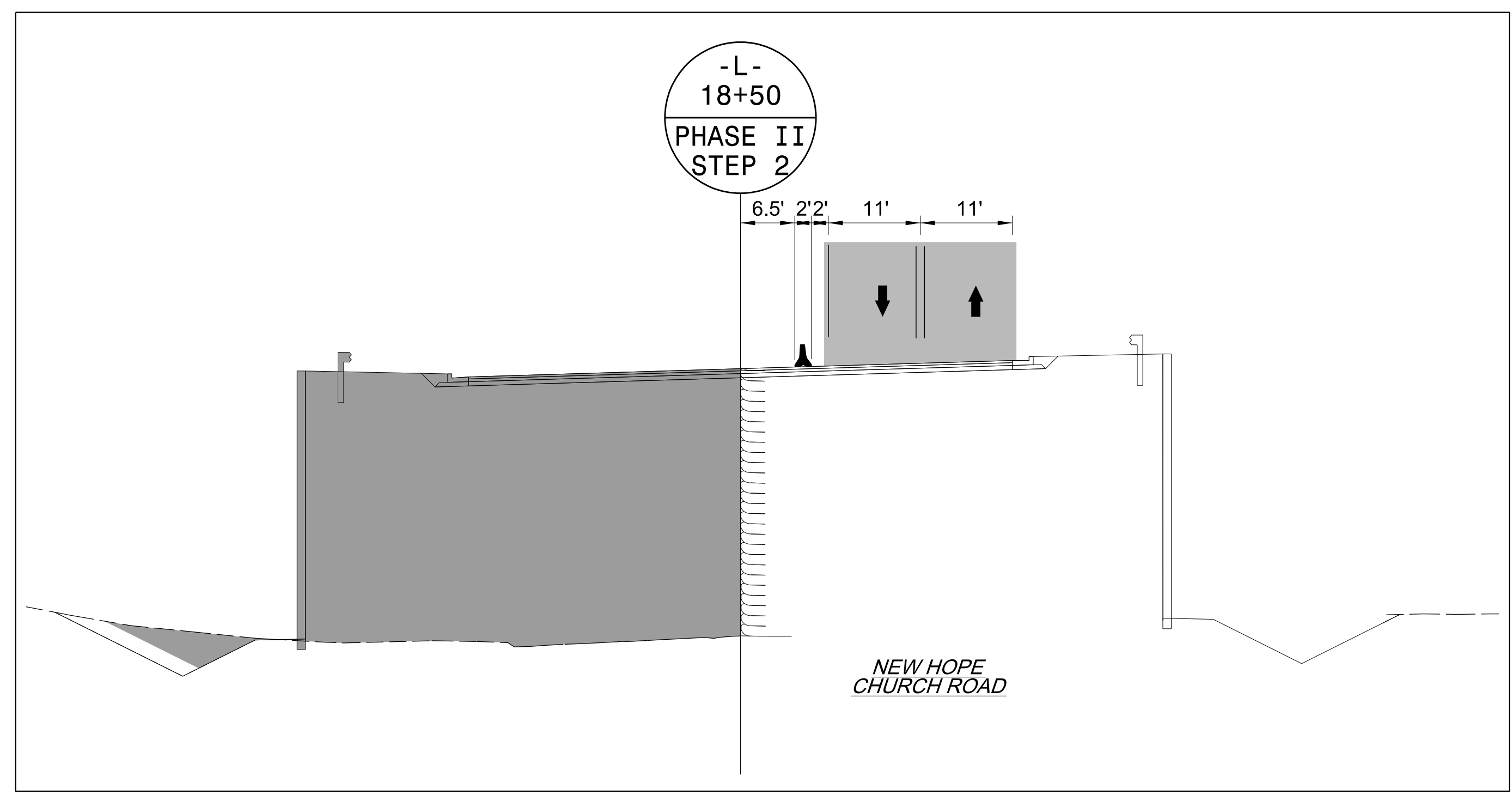
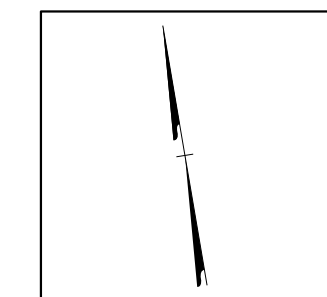
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**-L- NEW HOPE CHURCH ROAD
 PHASE II
 STEP 2**



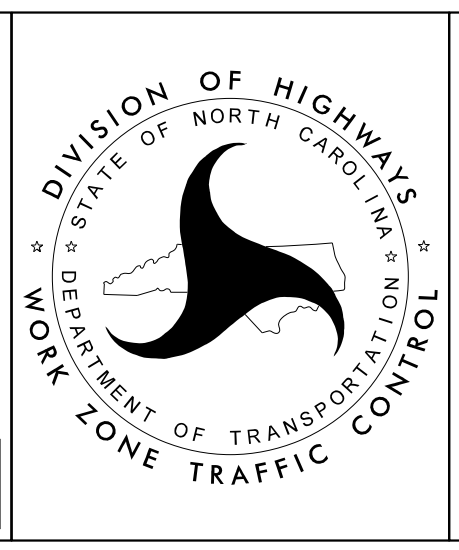
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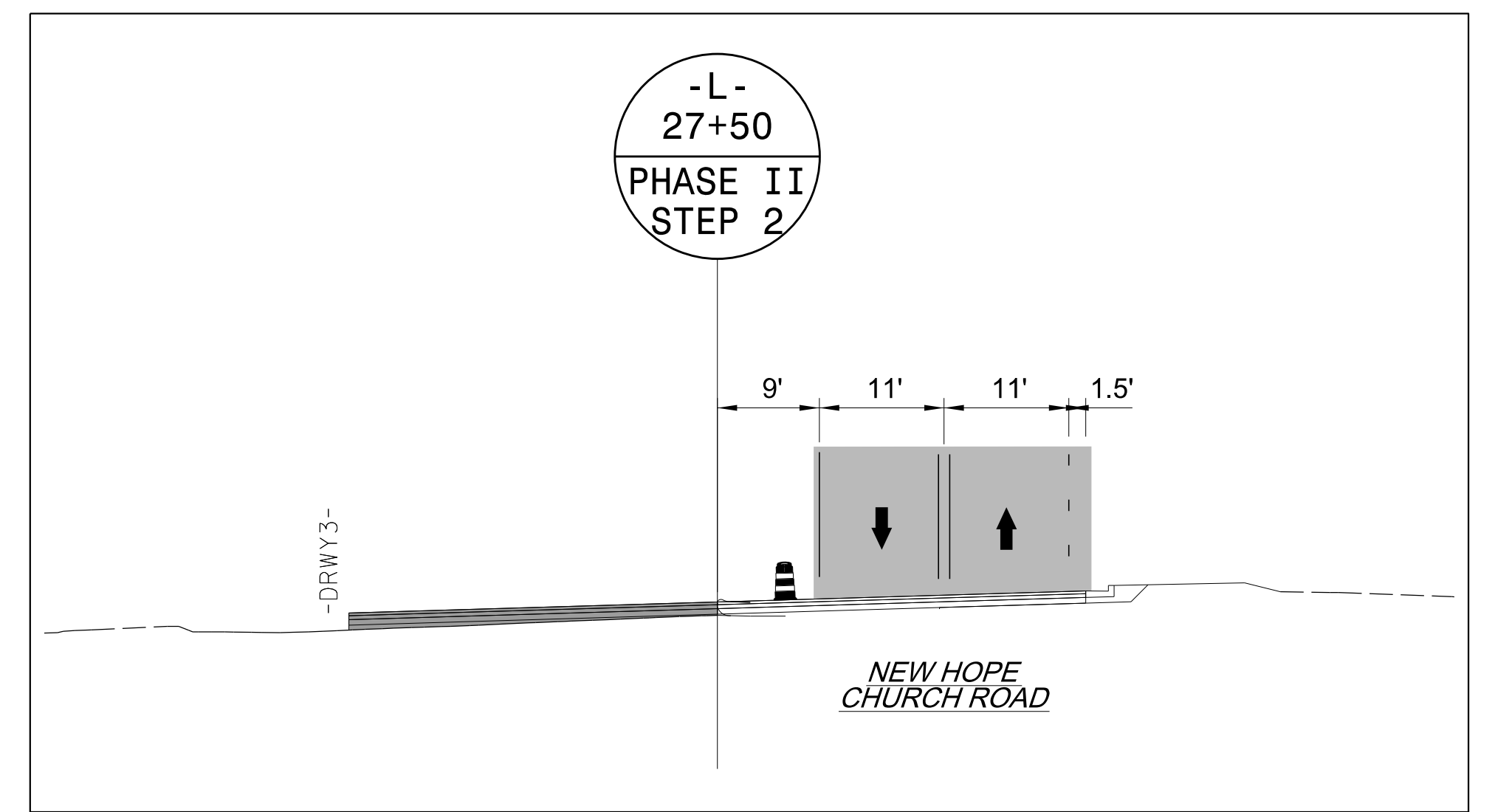
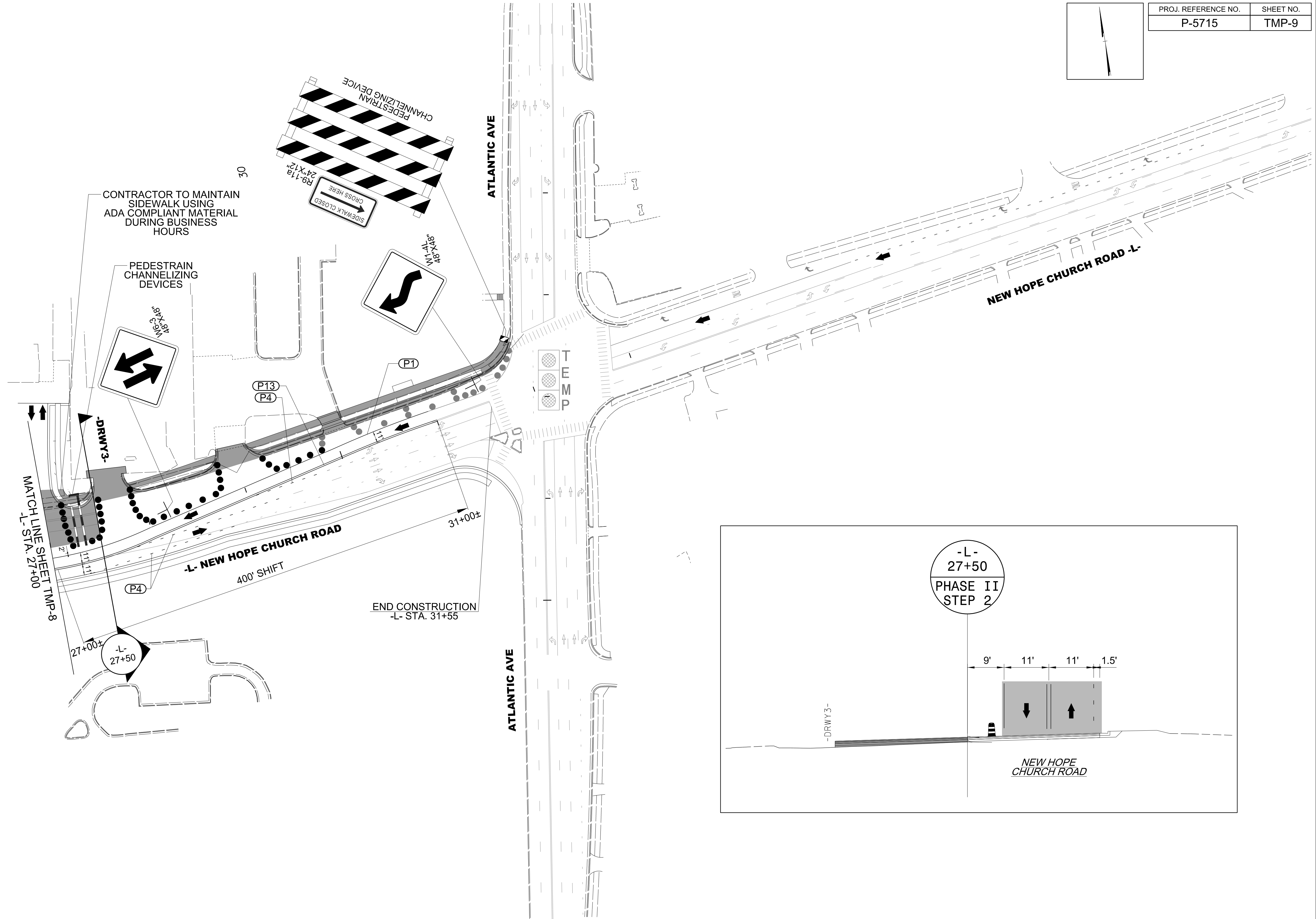
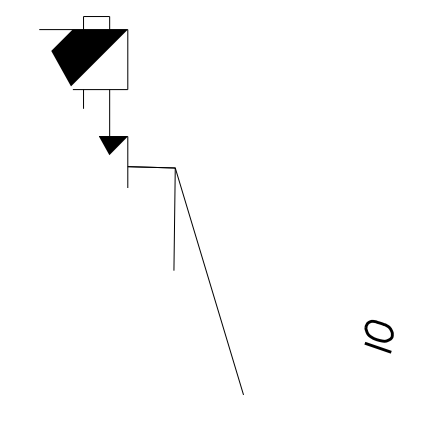
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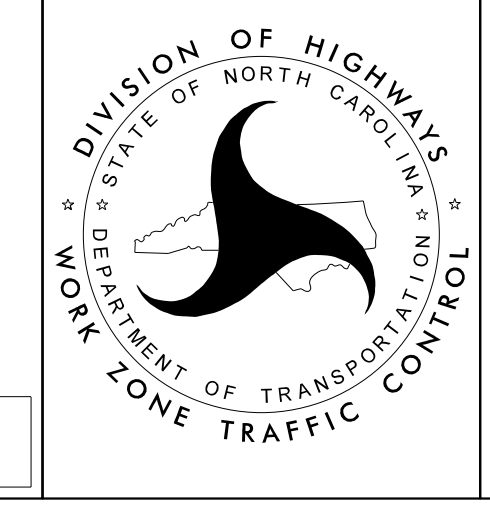
**-L- NEW HOPE CHURCH ROAD
 PHASE II
 STEP 2**



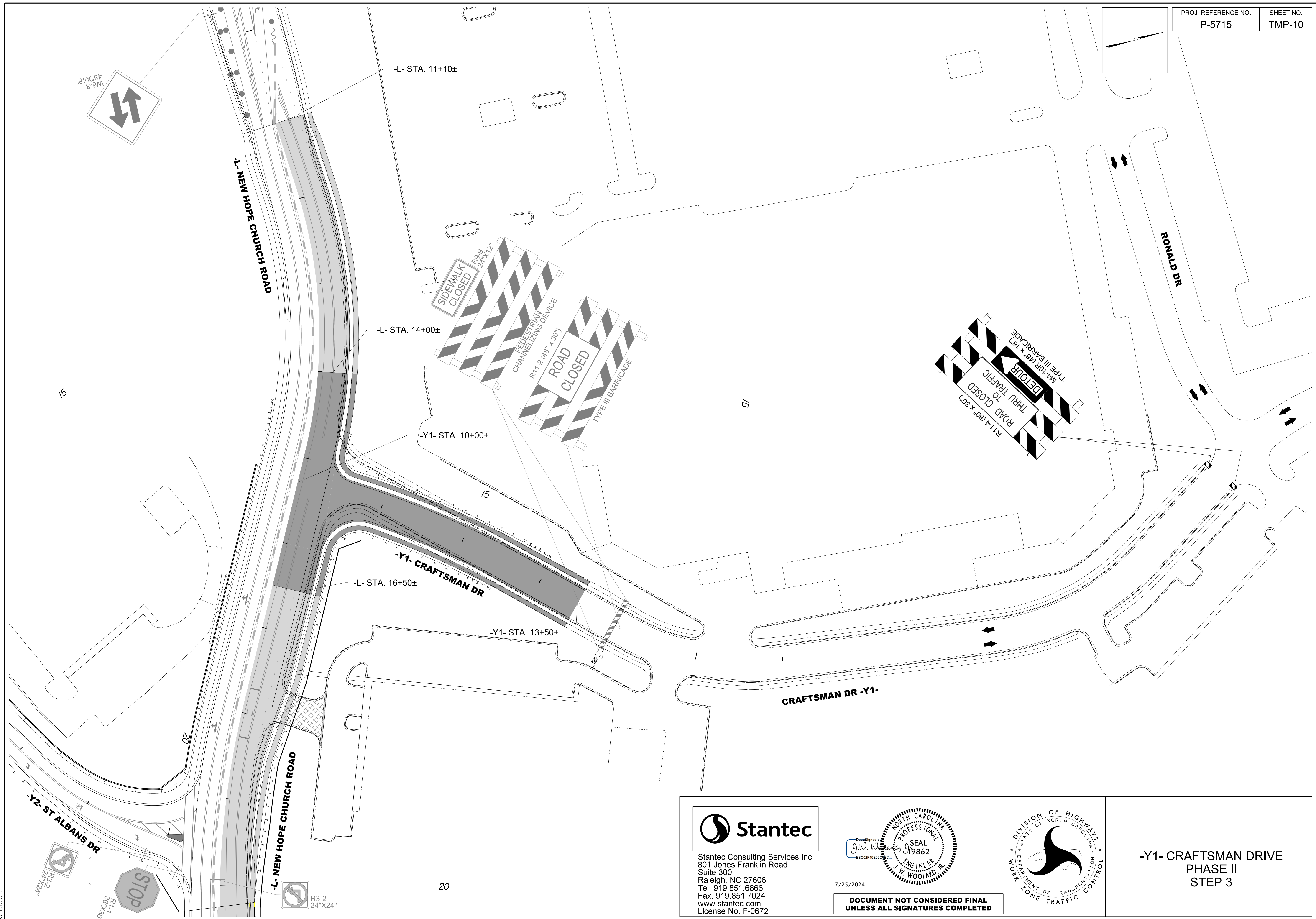
PAINT PAVEMENT MARKING LEGEND	
(P1)	WHITE EDGELINE (4")
(P4)	3 FT - 9 FT/SP WHITE MINISKIP (4")
(P13)	YELLOW DOUBLE CENTER (4")

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**-L- NEW HOPE CHURCH ROAD
 PHASE II
 STEP 2**



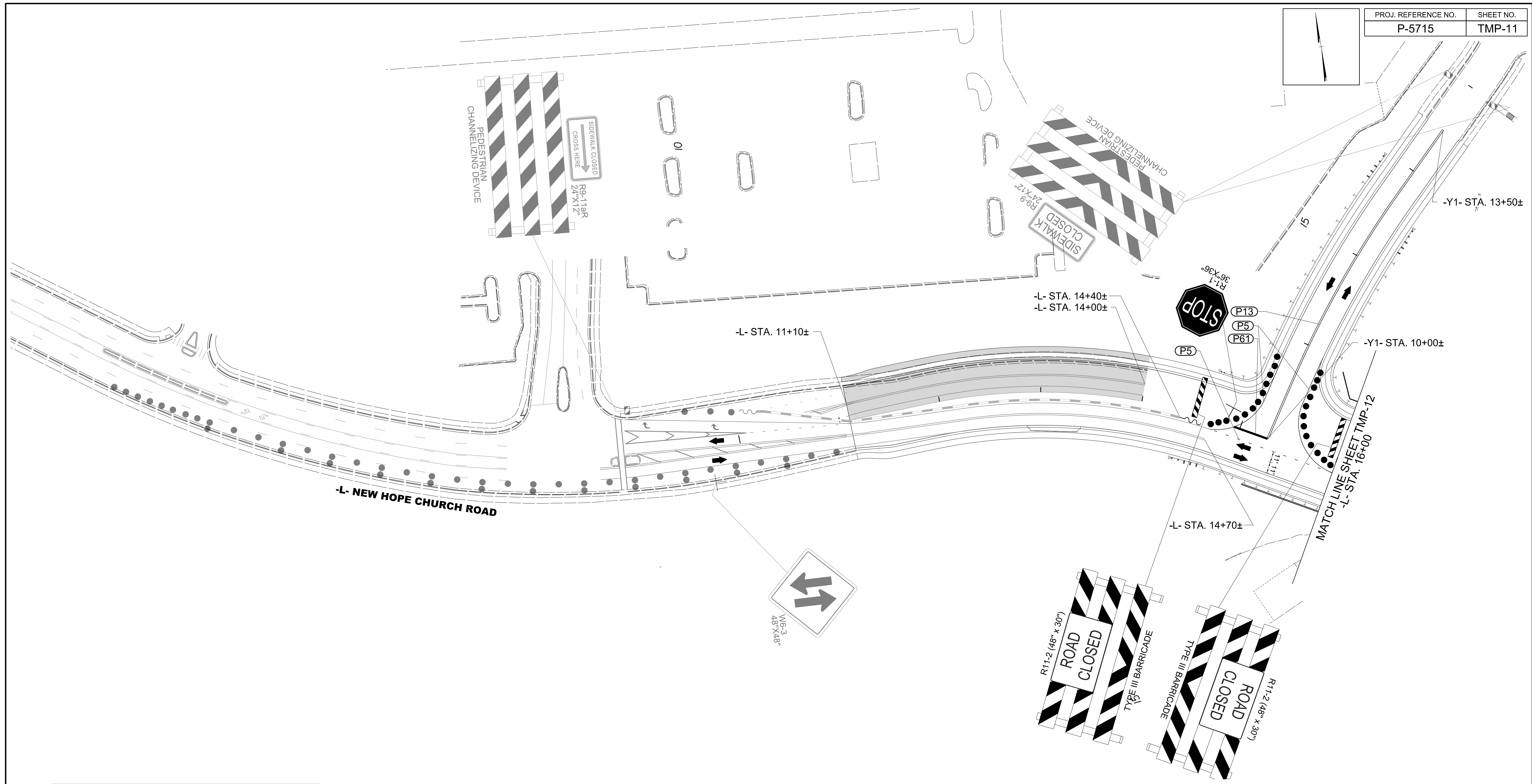
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 WORK ZONE TRAFFIC CONTROL

**-Y1- CRAFTSMAN DRIVE
 PHASE II
 STEP 3**



PAINT PAVEMENT MARKING LEGEND	
(P61)	WHITE STOPBAR (24")
(P5)	2 FT - 6 FT/SP WHITE MINISKIP (4")
(P1)	WHITE EDGELINE (4")
(P4)	3 FT - 9 FT/SP WHITE MINISKIP (4")
(P13)	YELLOW DOUBLE CENTER (4")
(P42)	YELLOW DIAGONAL (8")
(P40)	WHITE GORELINE (4")
(P41)	WHITE DIAGONAL (4")
(P71)	↗ RIGHT TURN ARROW

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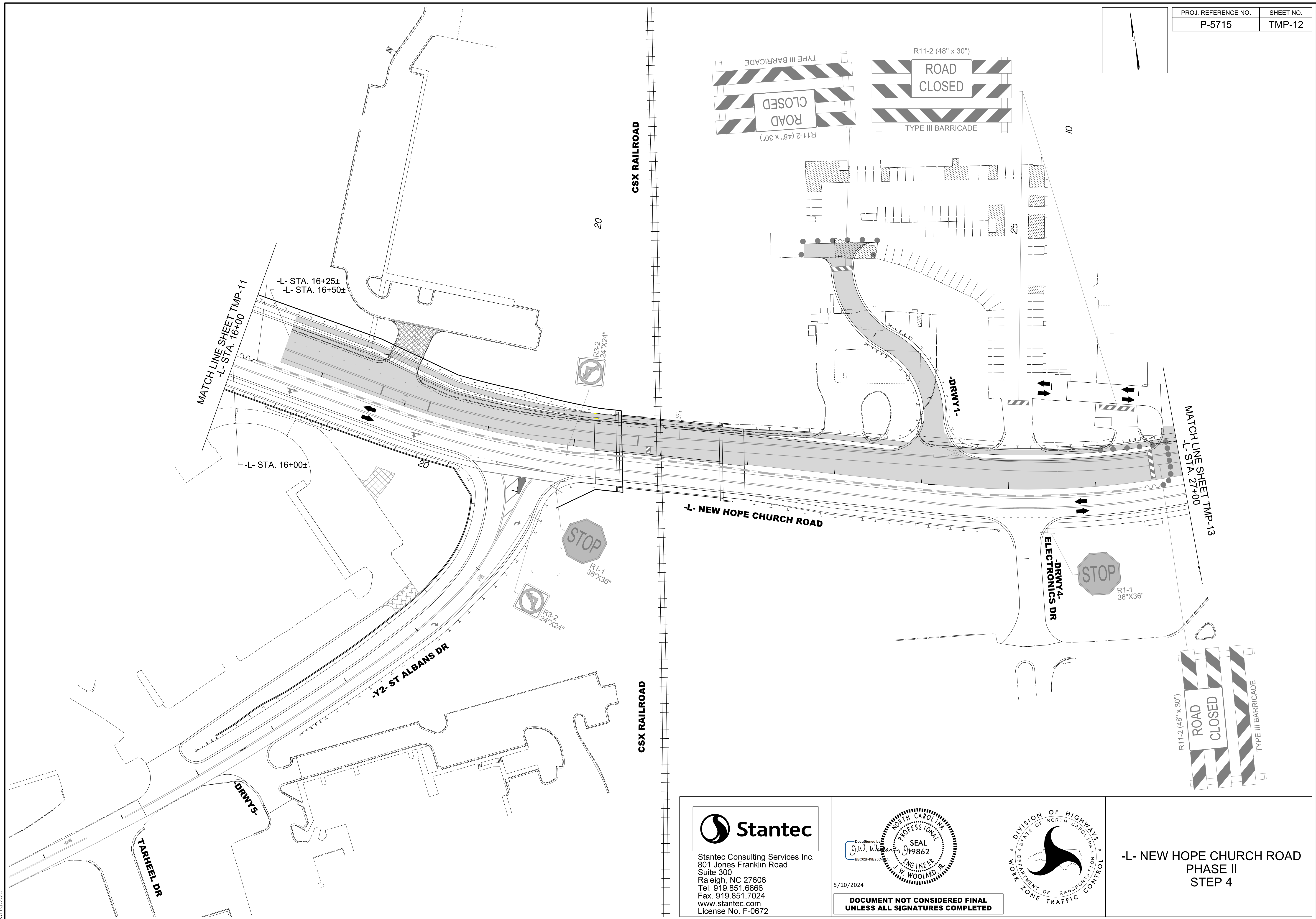
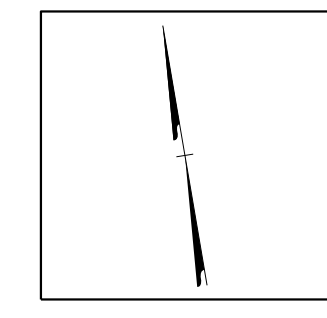
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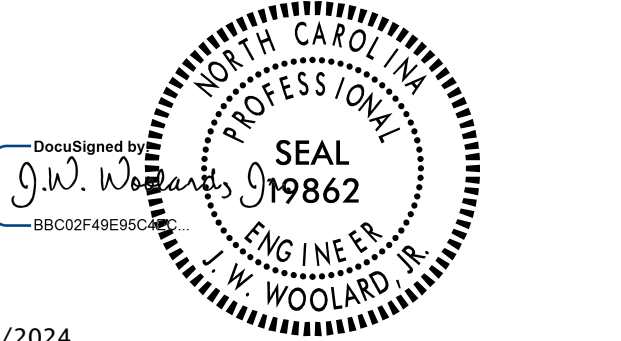
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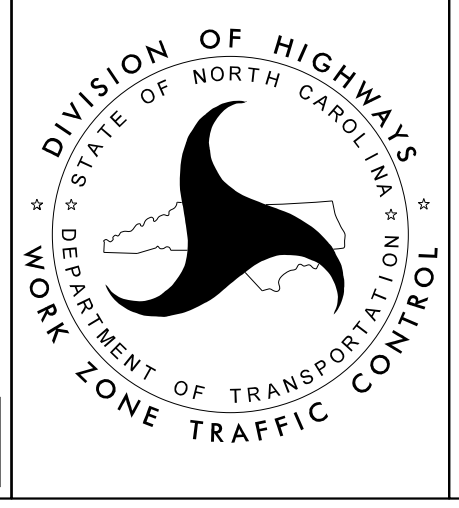
**-L- NEW HOPE CHURCH ROAD
 PHASE II
 STEP 4**



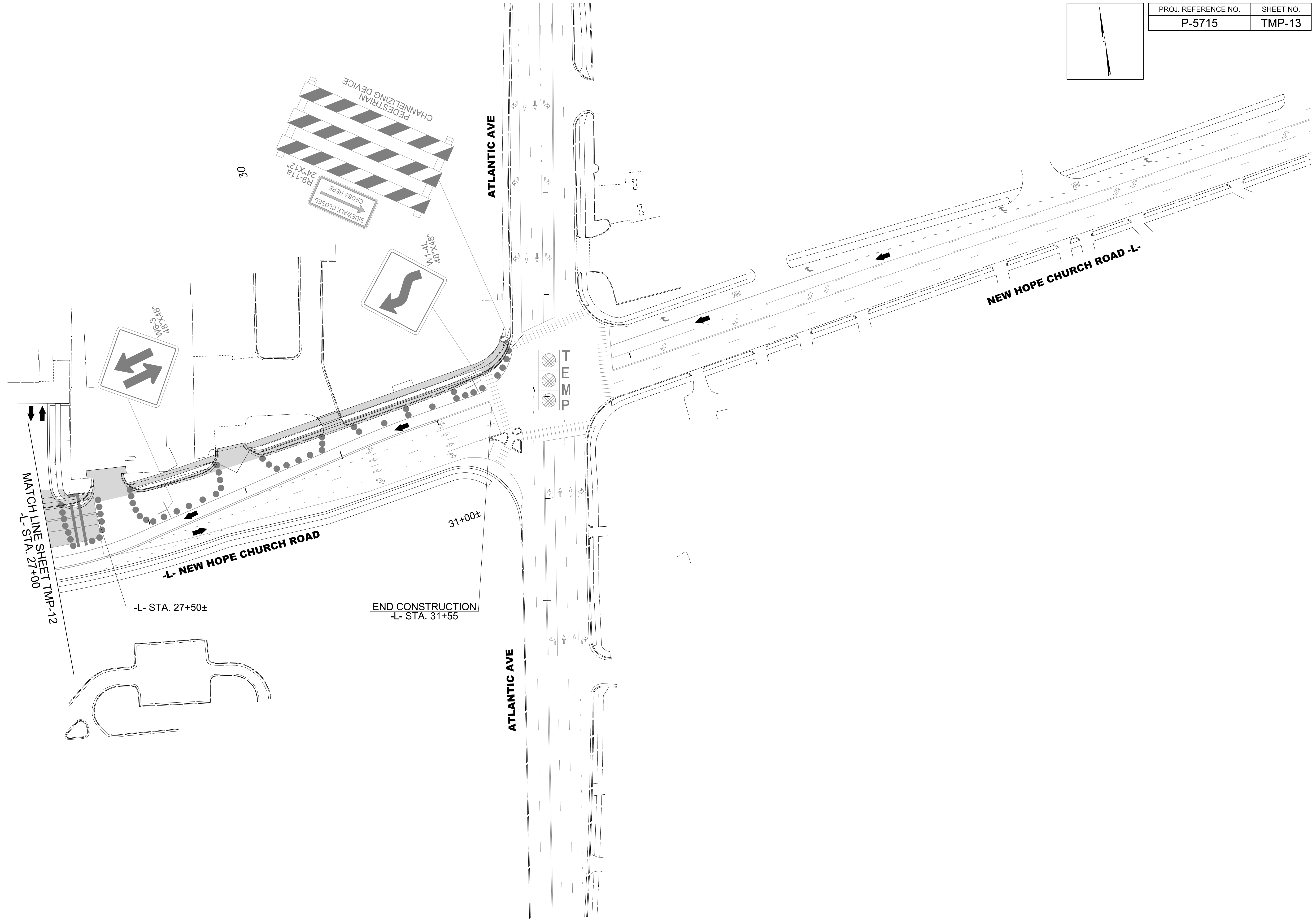
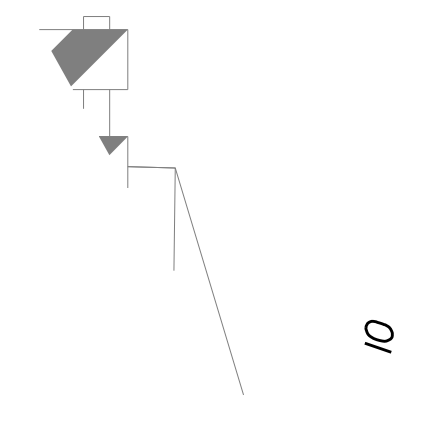
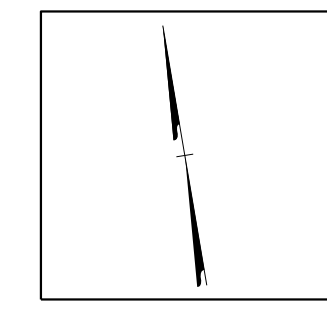
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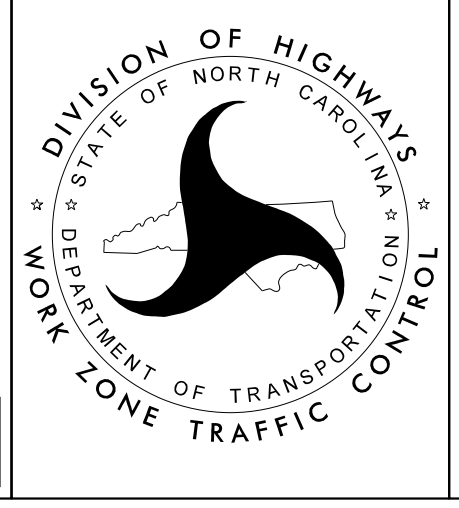
**-L- NEW HOPE CHURCH ROAD
 PHASE II
 STEP 4**



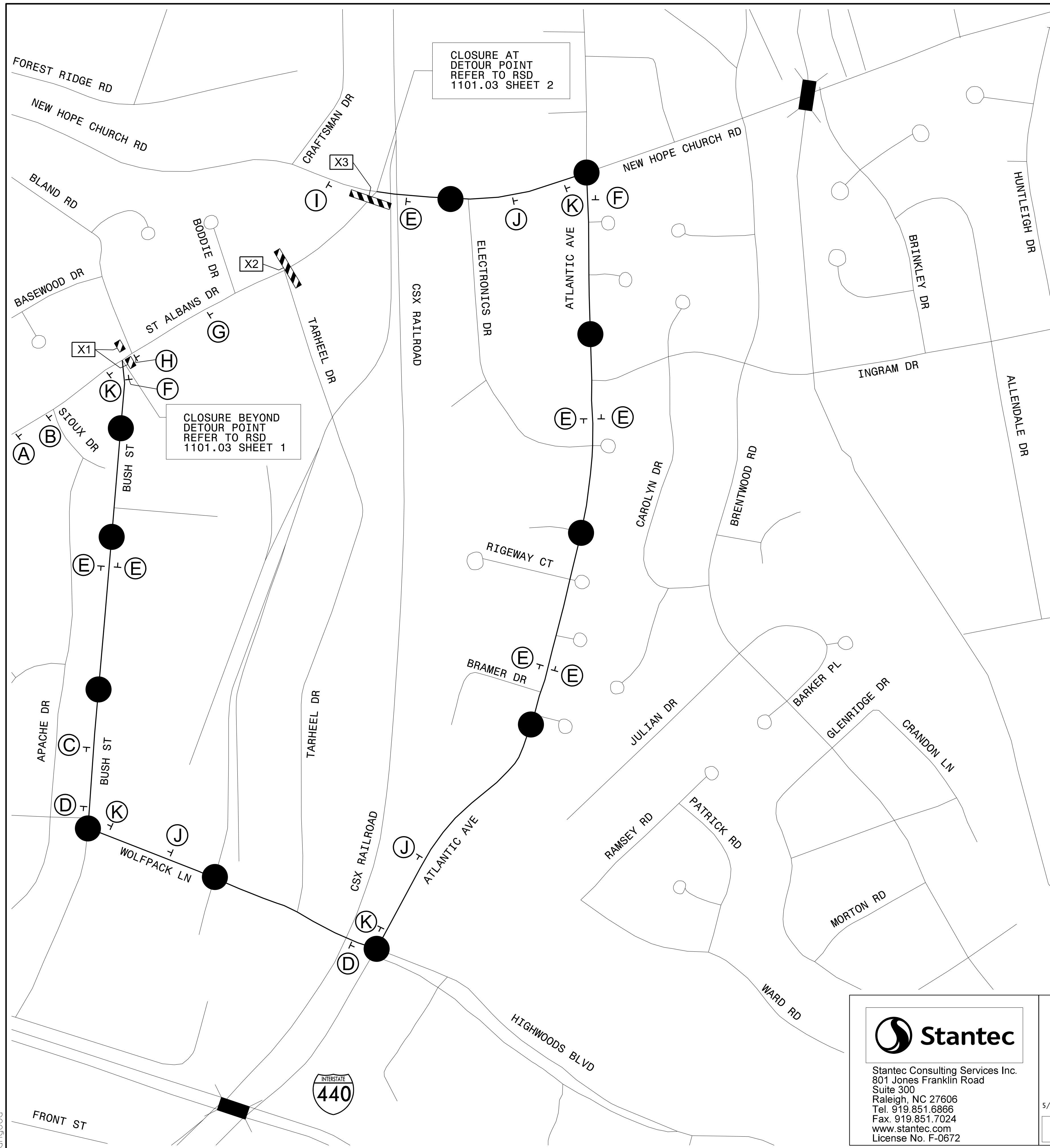
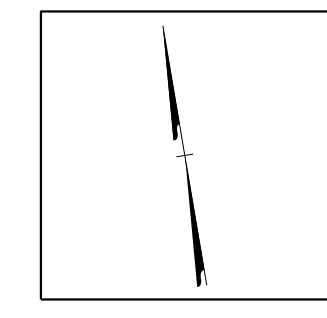
PAINT PAVEMENT MARKING LEGEND	
PA	WHITE EDGELINE (4")
PD	3 FT - 9 FT/SP WHITE MINISKIP (4")
PI	YELLOW DOUBLE CENTER (4")

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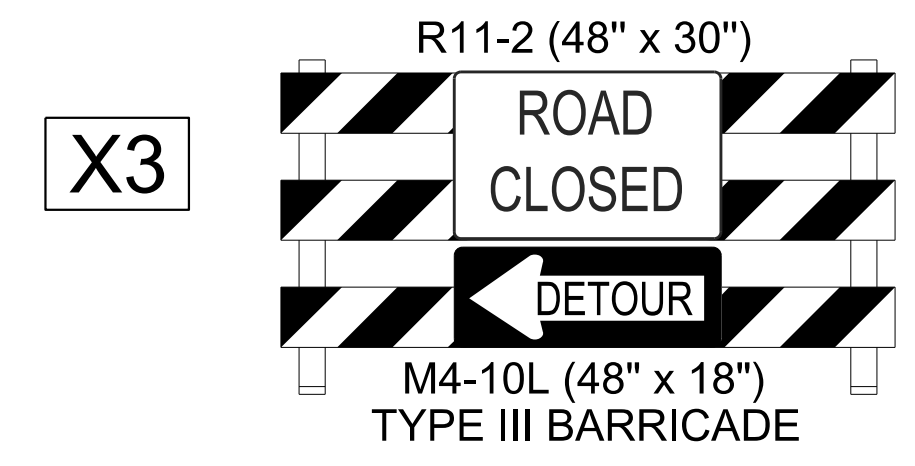
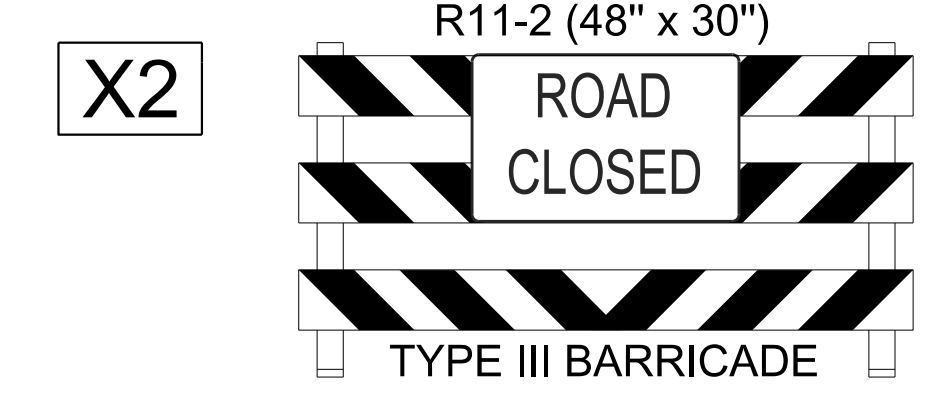
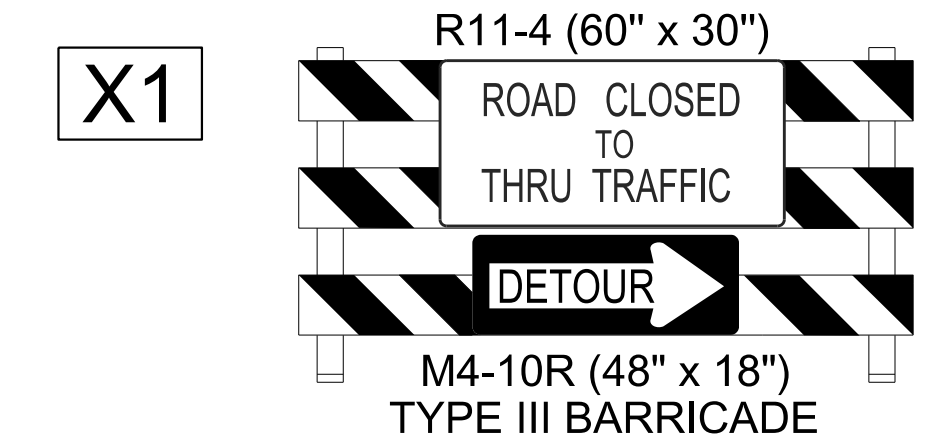


**-L- NEW HOPE CHURCH ROAD
 PHASE II
 STEP 4**



● NEW HOPE CHURCH RD. WEST SIDE TO ST ALBANS DR- ATLANTIC AVE. TO WOLFPACK DR TO BUSH ST.
ST ALBANS DR. TO NEW HOPE CHURCH RD- BUSH ST TO WOLFPACK TO ATLANTIC AVE.

- A** ROAD CLOSED AHEAD W20-3 48" X 48"
- B** DETOUR AHEAD W20-2 48" X 48"
- C** ST ALBANS DR DETOUR M4-8 24"X12" M5-1 21"x15"
- D** ST ALBANS DR DETOUR M4-8 24"X12" M6-1L 21"x15"
- E** ST ALBANS DR DETOUR M4-8 24"X12" M6-3 21"x15"
- F** END DETOUR M4-8a 24"X18"
- G** ROAD CLOSED 500 FT W20-3 48"X48"
- H** ROAD CLOSED 1000 FT W20-3 48"X48"
- I** ROAD CLOSED NEXT RIGHT W20-3 48" X 48" SP-4R 48"X12"
- J** ST ALBANS DR DETOUR M4-8 24"X12" M5-1R 21"x15"
- K** ST ALBANS DR DETOUR M4-8 24"X12" M6-1 21"x15"



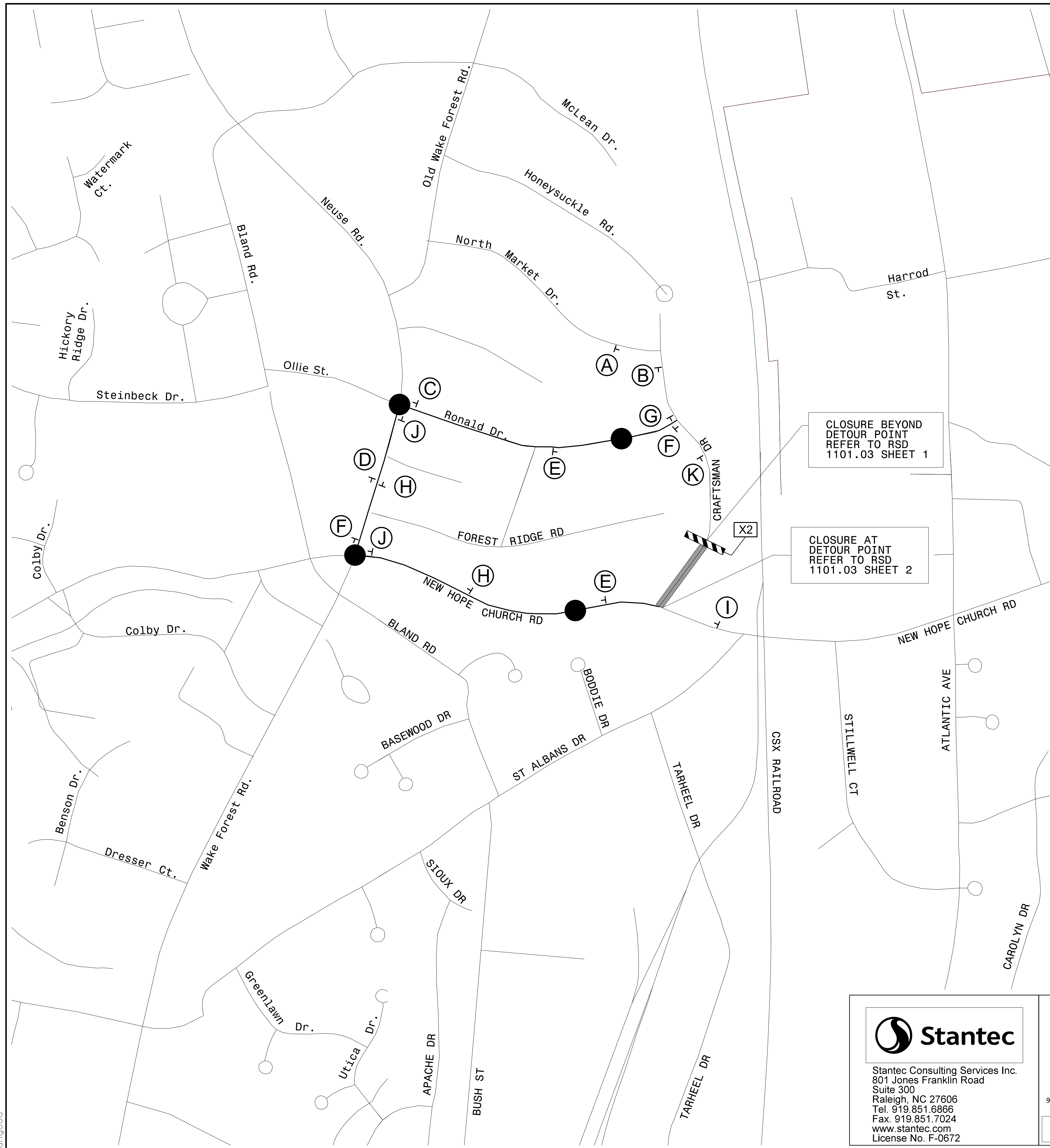
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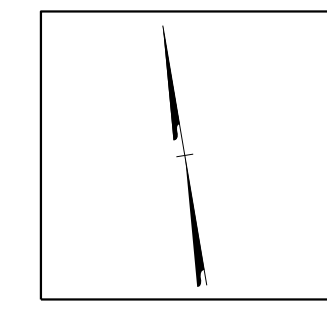
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DEPARTMENT OF TRANSPORTATION
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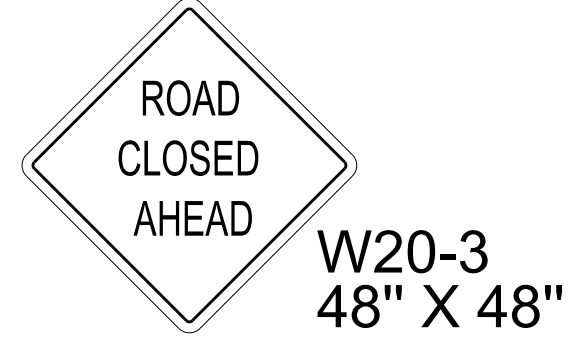
-Y2- ST ALBANS DRIVE
DETOUR ROUTE



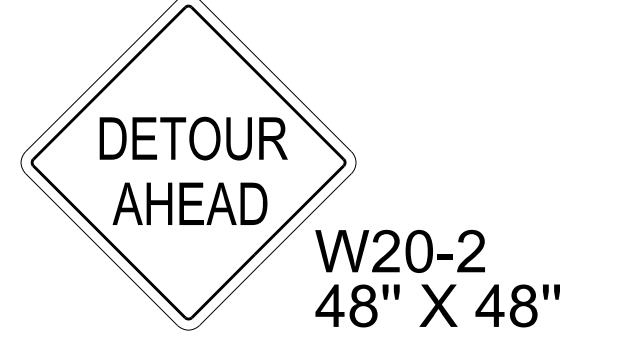
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 NEW HOPE CHURCH RD. TO NEUSE RD. TO RONALD DR. TO CRAFTSMAN DR.



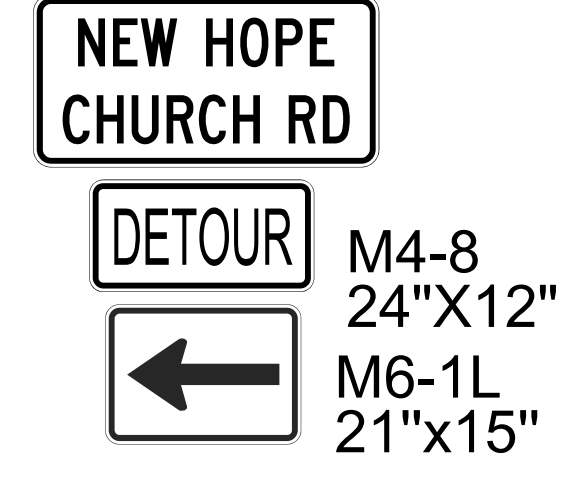
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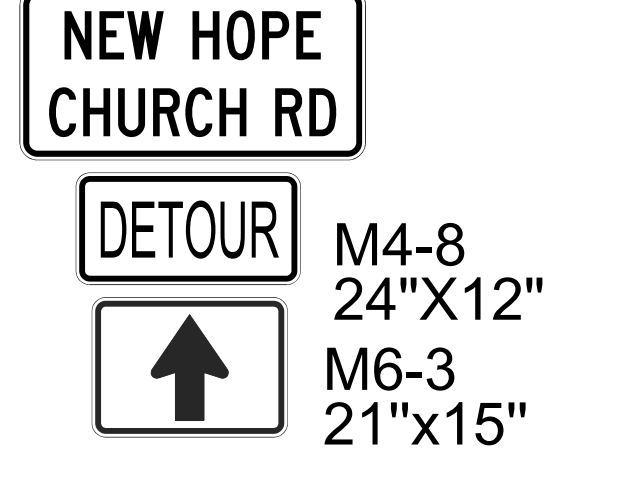
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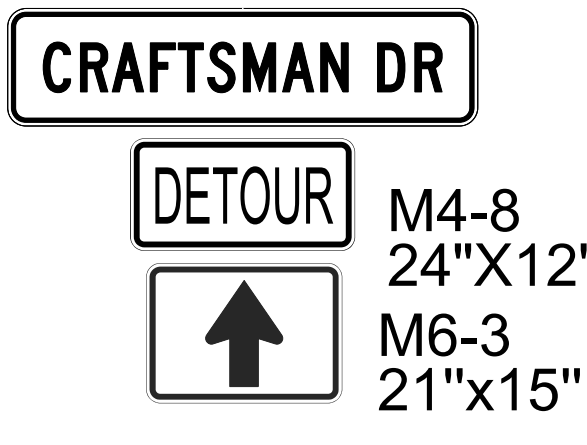
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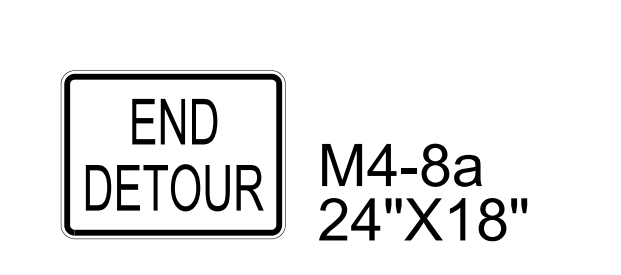
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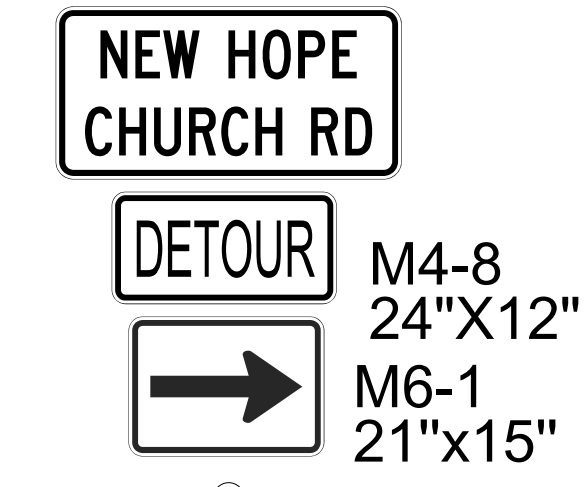
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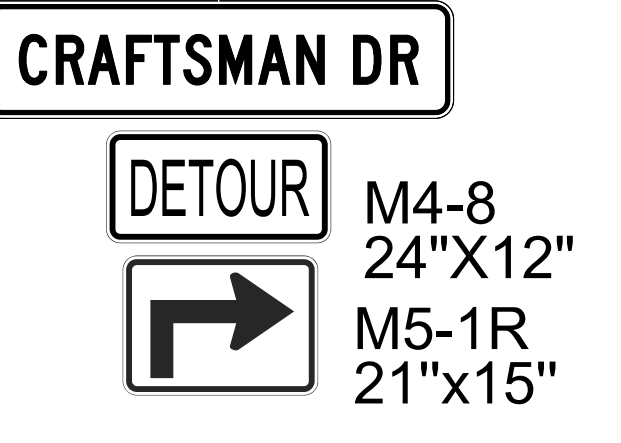
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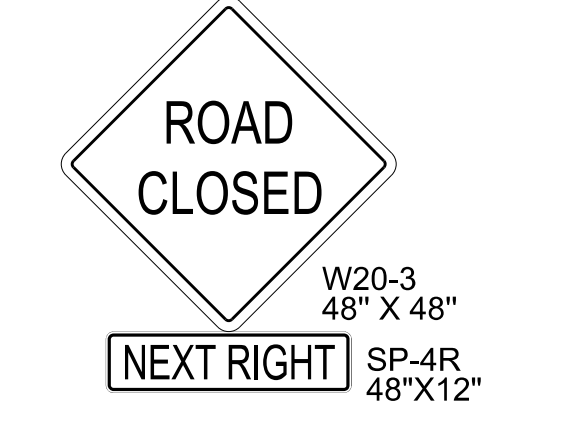
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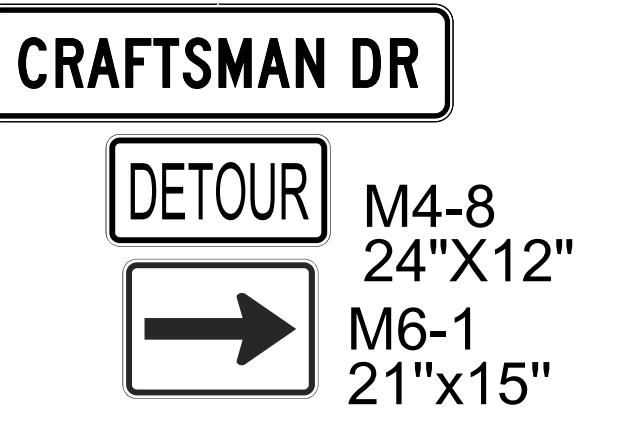
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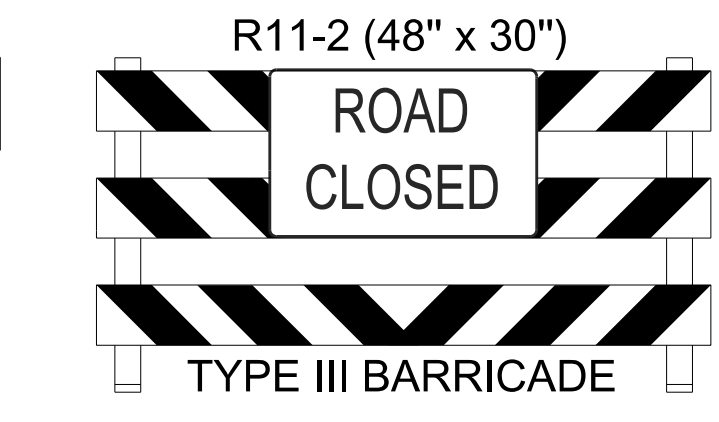
(I)



(J)



X2



(K)



CLOSURE BEYOND
 DETOUR POINT
 REFER TO RSD
 1101.03 SHEET 1

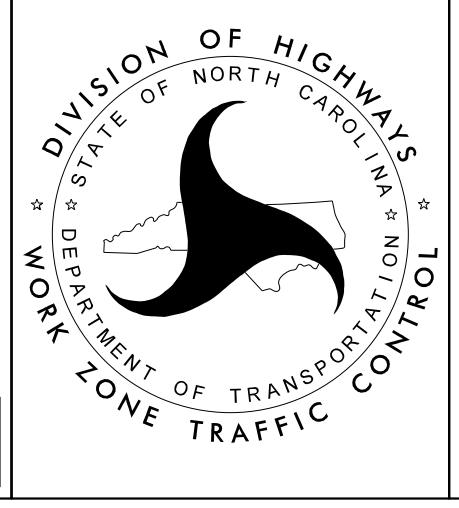
CLOSURE AT
 DETOUR POINT
 REFER TO RSD
 1101.03 SHEET 2

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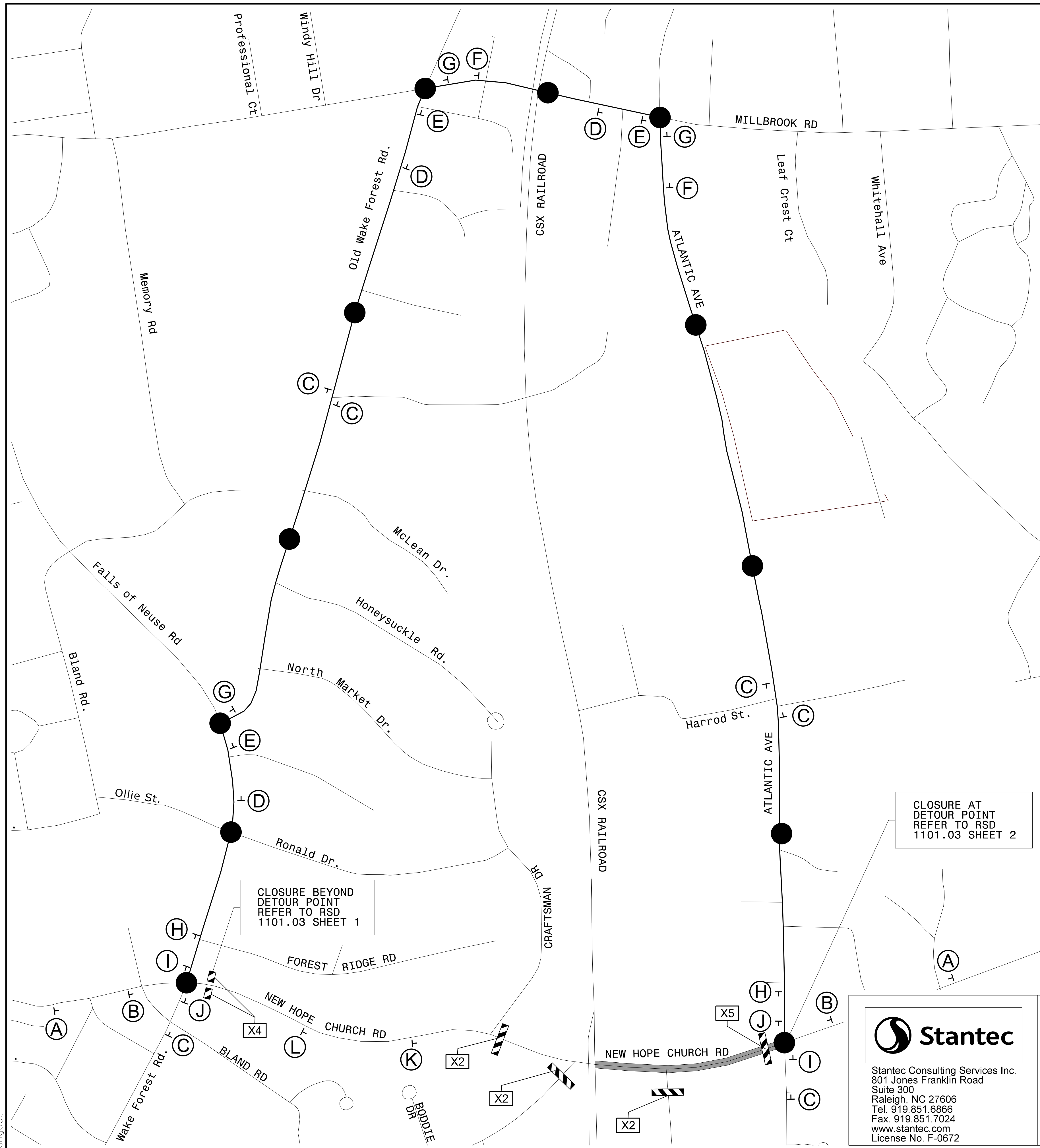
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**-Y1- CRAFTSMAN DRIVE
 DETOUR ROUTE**

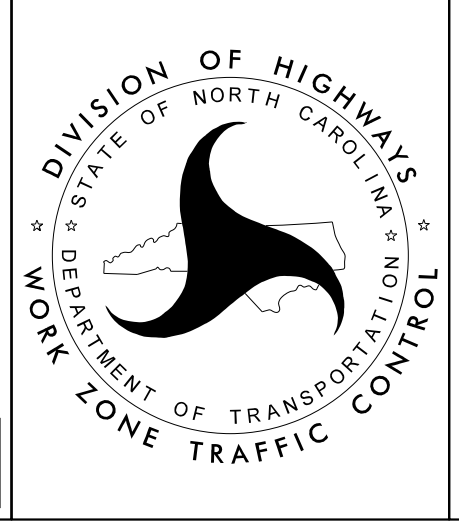
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PROJ. REFERENCE NO. P-5715		SHEET NO. TMP-16	
NEUSE RD. TO WAKE FOREST RD. TO MILLBROOK RD. TO ATLANTIC AVE. ATLANTIC AVE. TO MILLBROOK RD. TO WAKE FOREST RD. TO NEUSE RD.			
(A) ROAD CLOSED AHEAD W20-3 48" X 48"	(B) DETOUR AHEAD W20-2 48" X 48"	(C) NEW HOPE CHURCH RD DETOUR M4-8 24"x12" M6-3 21"x15" 	
(D) NEW HOPE CHURCH RD DETOUR M4-8 24"x12" M5-1R 21"x15" 	(E) NEW HOPE CHURCH RD DETOUR M4-8 24"x12" M6-1 21"x15" 	(F) NEW HOPE CHURCH RD DETOUR M4-8 24"x12" M5-1 21"x15" 	
(G) NEW HOPE CHURCH RD DETOUR M4-8 24"x12" M6-1L 21"x15" 	(H) END DETOUR M4-8a 24"x18"	(I) ROAD CLOSED W20-3 48" X 48" NEXT LEFT SP-4L 48"x12"	
(J) ROAD CLOSED W20-3 48" X 48" NEXT RIGHT SP-4R 48"x12"	(K) ROAD CLOSED 500 FT W20-3 48"x48"	(L) ROAD CLOSED 1000 FT W20-3 48"x48"	
X2 R11-2 (48" x 30") ROAD CLOSED TYPE III BARRICADE	X4 R11-4 (60" x 30") ROAD CLOSED TO THRU TRAFFIC DETOUR M4-10L (48" x 18") TYPE III BARRICADE		
X5 R11-2 (48" x 30") ROAD CLOSED DETOUR M4-10R (48" x 18") TYPE III BARRICADE			

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 ENGINEER
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-L- NEW HOPE CHURCH ROAD
 DETOUR ROUTE

