

09/08/99

See Sheet 1A for Index of Sheets

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

IREDELL COUNTY

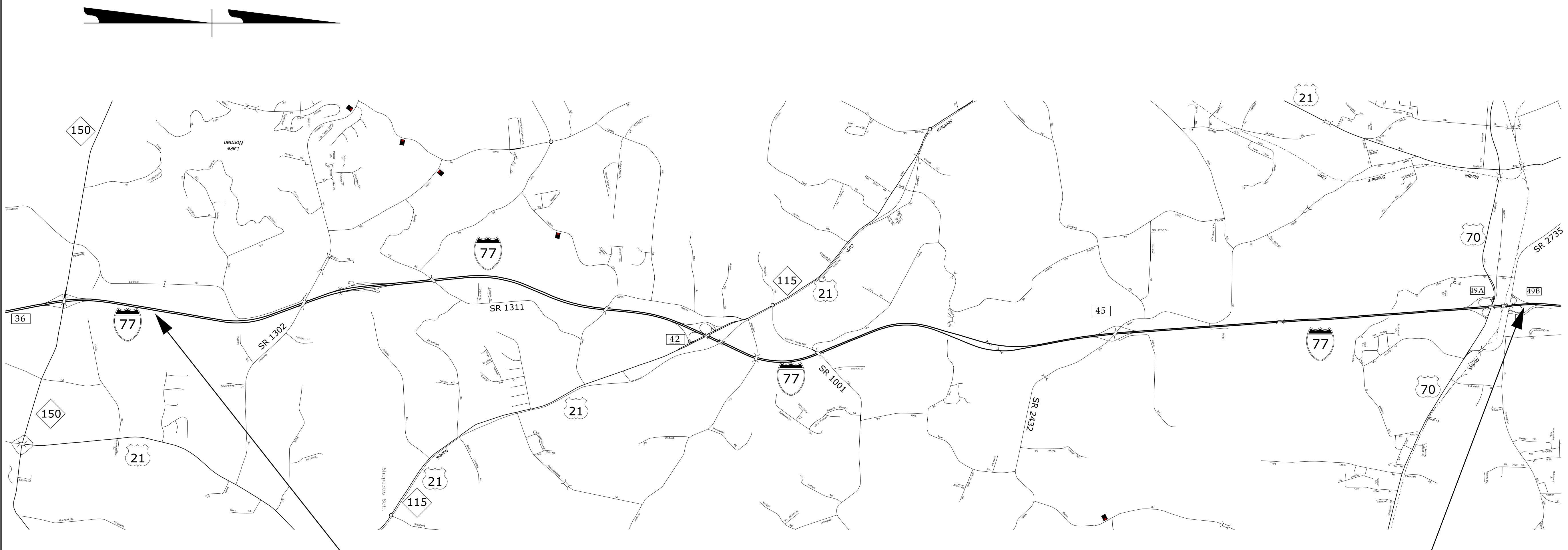
LOCATION: I-77 FROM MM 36.5 to MM 48.85

WORK TYPE: PAVEMENT REHABILITATION.

| | | | |
|-----------------|-----------------------------|-------------|--------------|
| STATE | STATE PROJECT REFERENCE NO. | SHEET NO. | TOTAL SHEETS |
| N.C. | I-5918 | 1 | |
| STATE PROJ. NO. | F.A. PROJ. NO. | DESCRIPTION | |
| 45924.1.2 | | PE | |
| 45924.3.1 | 0077023 | CONST. | |
| | | | |
| | | | |
| | | | |

TIP PROJECT: I-5918

C204948



**BEGIN PROJECT I-5918
MILE MARKER 36.50**

**END PROJECT I-5918
MILE MARKER 48.85**

PROJECT LENGTH

LENGTH ROADWAY TIP PROJECT I-5918 = 12.35 MI

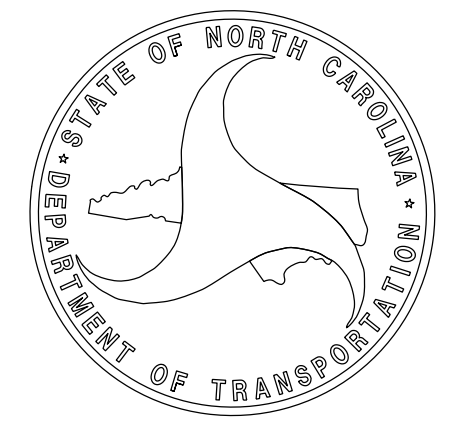
Prepared In the Office of:
DIVISION OF HIGHWAYS
1710 EAST MARION ST., SHELBY NC 28151

2024 STANDARD SPECIFICATIONS

LETTING DATE:
JULY 16, 2024

JASON WILLIS
DIVISION MAINTENANCE ENGINEER

BRYAN K. SOWELL, PE
PROJECT DESIGN ENGINEER



26-APR-2024 07:50
S:\DDC\District 2\Iredell\I-5918 I-77 MM 36 to 49\Types-Maps-Details\I-5918 Final CADD Files\I-5918_Rdy_Tsh_maps.dgn
\$\$\$\$\$USERNAME\$\$\$\$\$

EFF. 01-01-2024

GENERAL NOTES: 2024 SPECIFICATIONS
EFFECTIVE: 01-01-2024

2024 ROADWAY ENGLISH STANDARD DRAWINGS
The following Roadway Standards as appear in "Roadway Standard Drawings"
Highway Design Branch - N. C. Department of Transportation - Raleigh,
N. C., Dated January, 2024 are applicable to this project and by
reference hereby are considered a part of these plans:

| SHEET NUMBER | SHEET |
|----------------|--|
| 1 | TITLE SHEET |
| 1-A | INDEX OF SHEETS, GENERAL NOTES, AND STANDARD DRAWINGS |
| 1-B | MAPS |
| 2A-1 THRU 2A-5 | PAVEMENT SCHEDULE AND TYPICAL SECTIONS |
| 2B-1 THRU 2B-3 | ROADWAY DETAILS |
| 3B-1 THRU 3B-2 | GUARDRAIL SUMMARY |
| 3B-3 THRU 3B-6 | SUMMARY OF QUANTITIES |

SHOULDER RECONSTRUCTION:

Aggregate Shoulder Borrow will be used for all Shoulder Reconstruction.

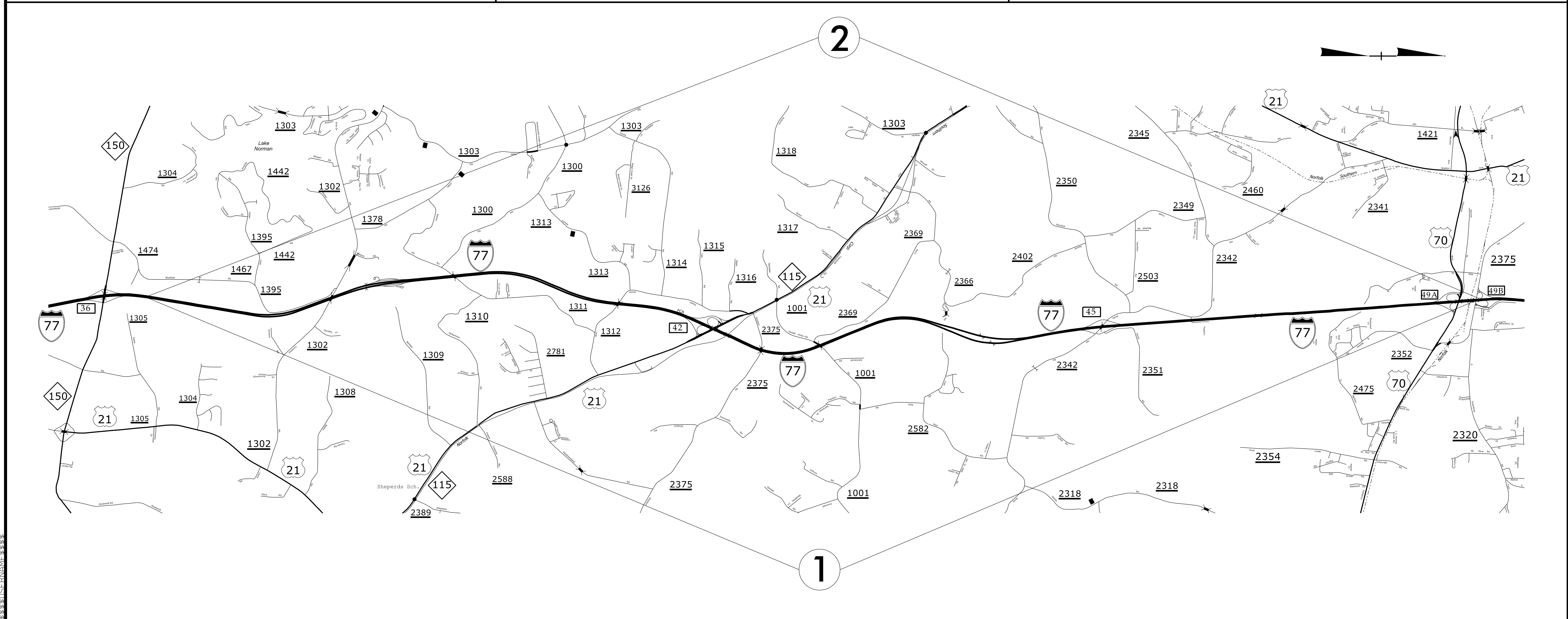
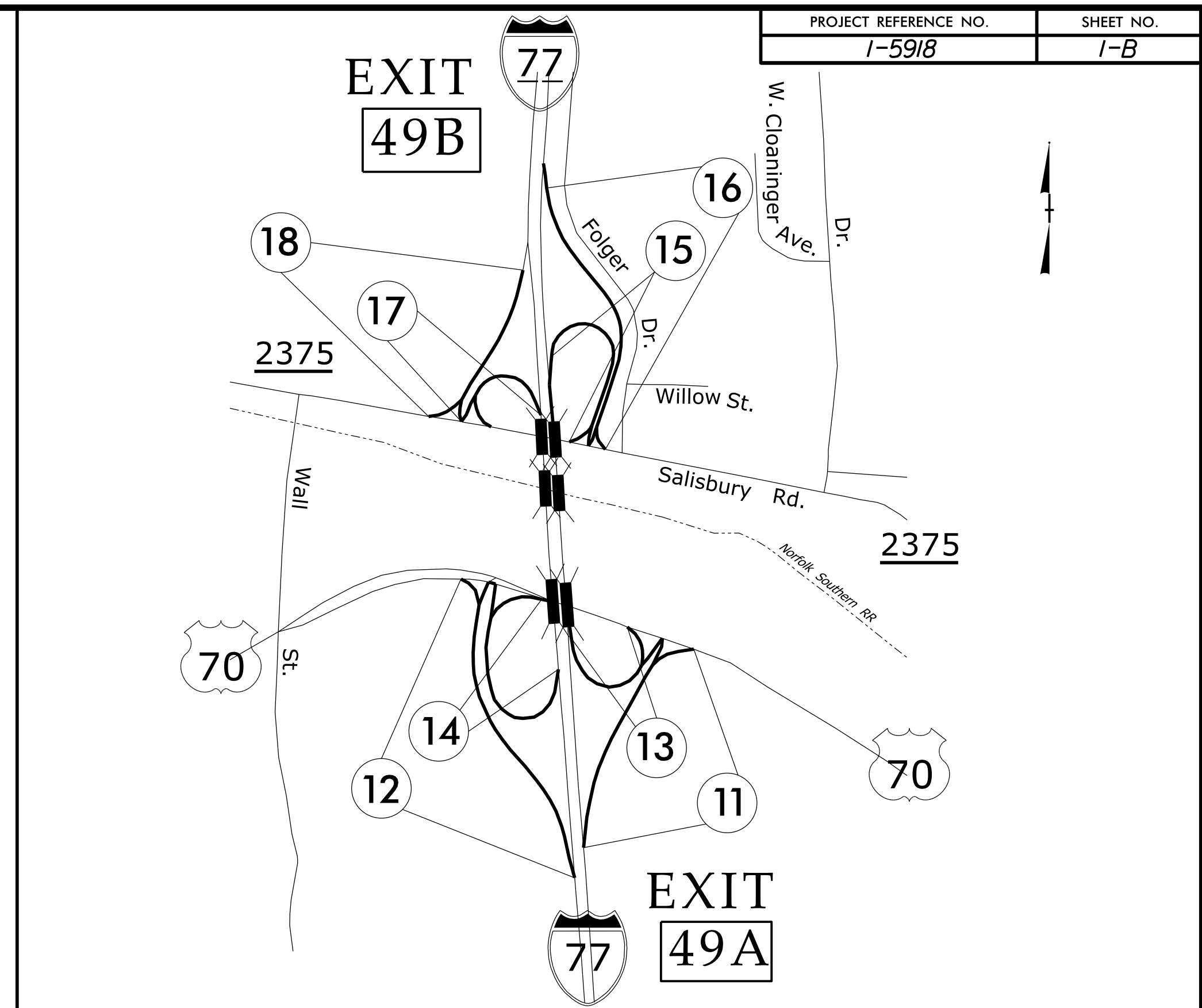
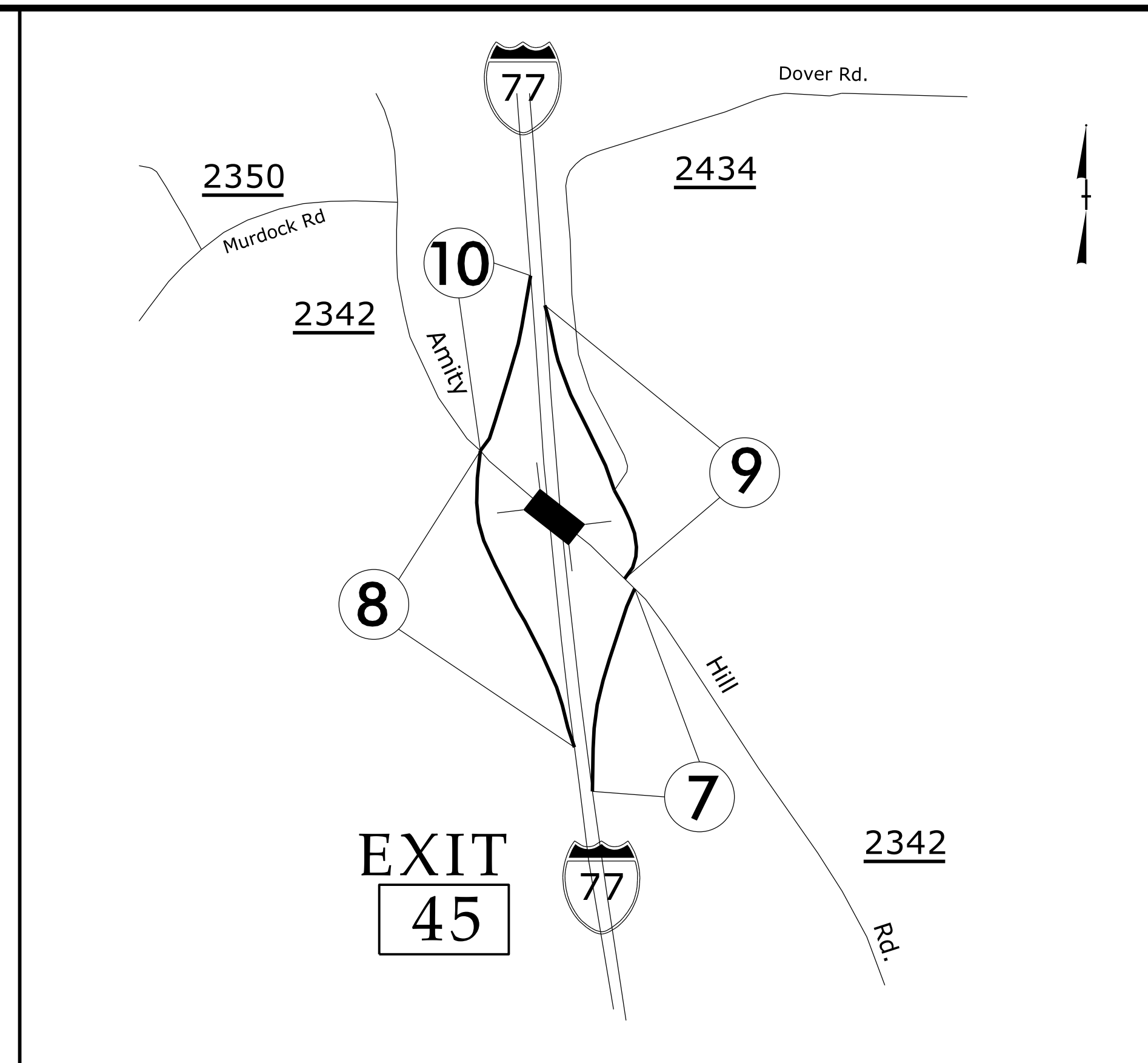
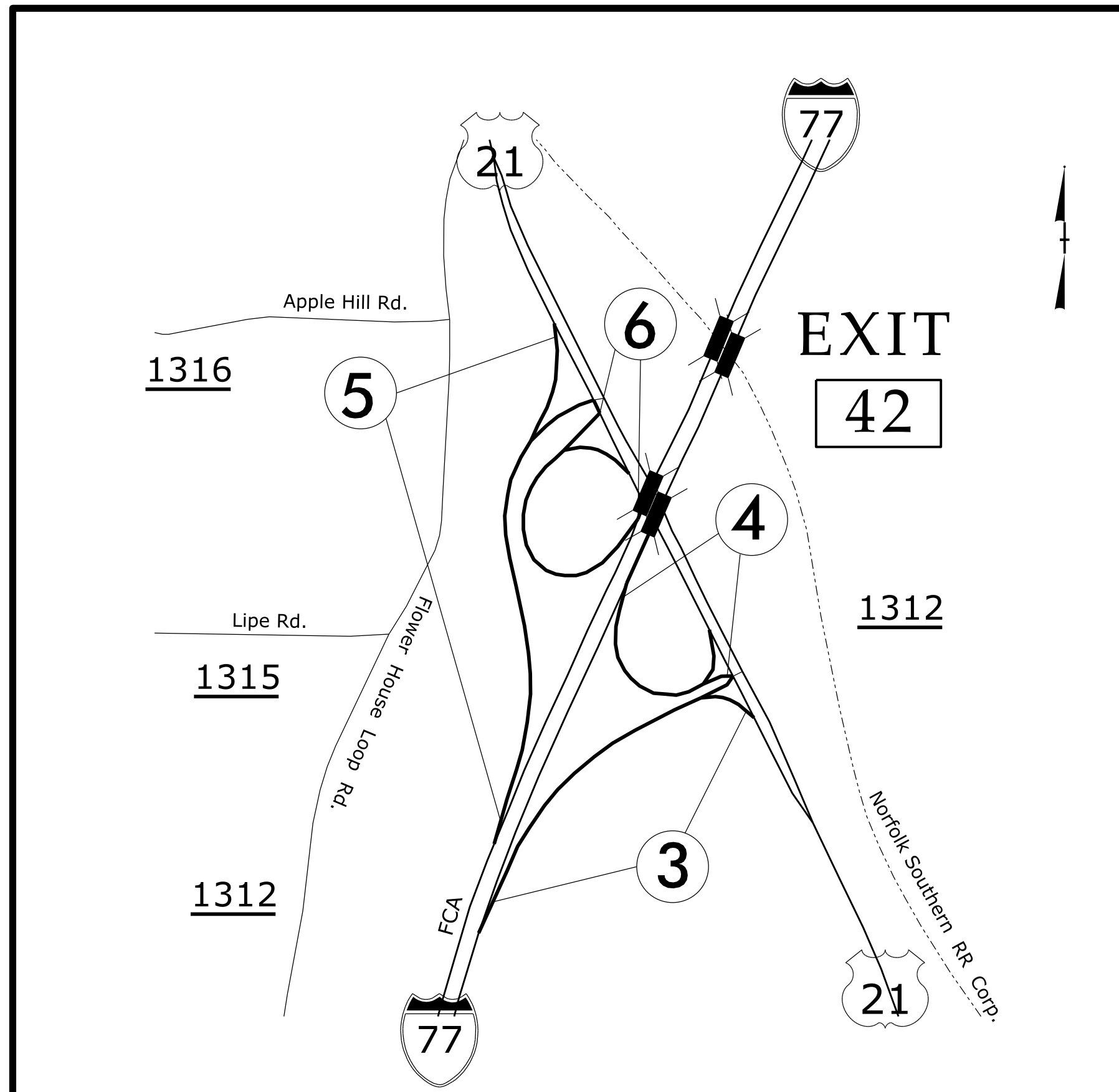
GUARDRAIL:

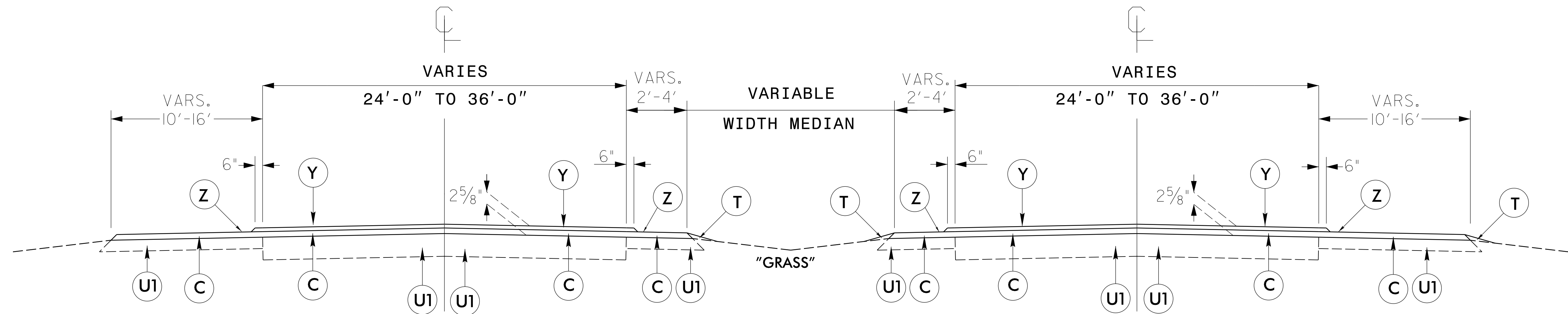
- * Retain all existing approach end guardrail end units unless damaged or otherwise directed by the Engineer.
- * Replace all damaged trailing end guardrail end units (CAT-1).
- * Remove and reset all substandard guardrail on this project in accordance with Section 862 of the Standard Drawings.
- * The following pay items have been included for maintenance of existing guardrail during the project:
 - Guardrail End Unit, Type TL-3
 - Guardrail End Unit, Type Cat-1
 - Steel Beam Guardrail
 - Triple Corrugated Steel Beam Guardrail
 - Guardrail Anchor Unit, Type B-83
 - Steel Beam Guardrail W-TR Transition Section
 - Additional Guardrail Posts

| STD.NO. | TITLE |
|--|---|
| DIVISION 5 - SUBGRADE, BASES AND SHOULDERS | |
| 560.01 | Method Of Shoulder Construction - Method I |
| 560.02 | Method Of Shoulder Construction - Method II |
| DIVISION 6 - ASPHALT BASES AND PAVEMENTS | |
| 665.01 | Asphalt Shoulders - Milled Rumble Strips |
| 665.02 | Limits for Asphalt Shoulder Milled Rumble Strips |
| DIVISION 8 - INCIDENTALS | |
| 862.01 | Guardrail Placement |
| 862.02 | Guardrail Installation |
| 862.03 | Structure Anchor Units |
| 862.04 | Anchoring End of Guardrail - B-77 and B-83 Anchor Units |
| DIVISION 11 - WORK ZONE TRAFFIC CONTROL | |
| 1101.02 | Temporary Lane Closures |
| 1101.11 | Traffic Control Design Tables |
| 1110.01 | Stationary Work Zone Signs |
| 1110.02 | Portable Work Zone Signs |
| 1115.01 | Flashing Arrow Boards |
| 1130.01 | Drums |
| 1135.01 | Cones |
| 1165.01 | Truck Mounted Attenuator |
| 1180.01 | Skinny Drums |
| 1205.01 | Pavement Markings |
| 1205.02 | Pavement Markings |
| 1205.03 | Pavement Markings |
| 1205.04 | Pavement Markings |
| 1205.05 | Pavement Markings |
| 1205.06 | Pavement Markings |
| 1205.08 | Pavement Markings |
| 1205.09 | Pavement Markings |
| 1205.13 | Pavement Markings |
| 1250.01 | Raised Pavement Markers |

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| | |
|-----------------------|-----------|
| PROJECT REFERENCE NO. | SHEET NO. |
| I-5918 | I-B |



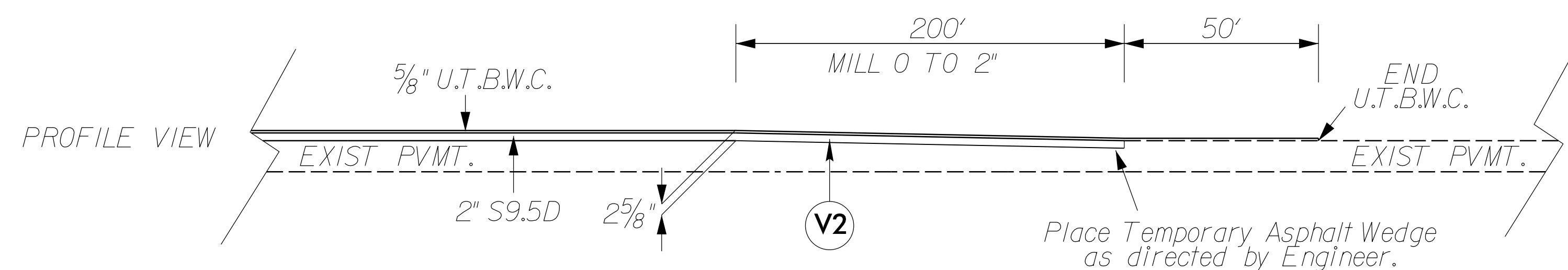


TYPICAL SECTION NO. 1

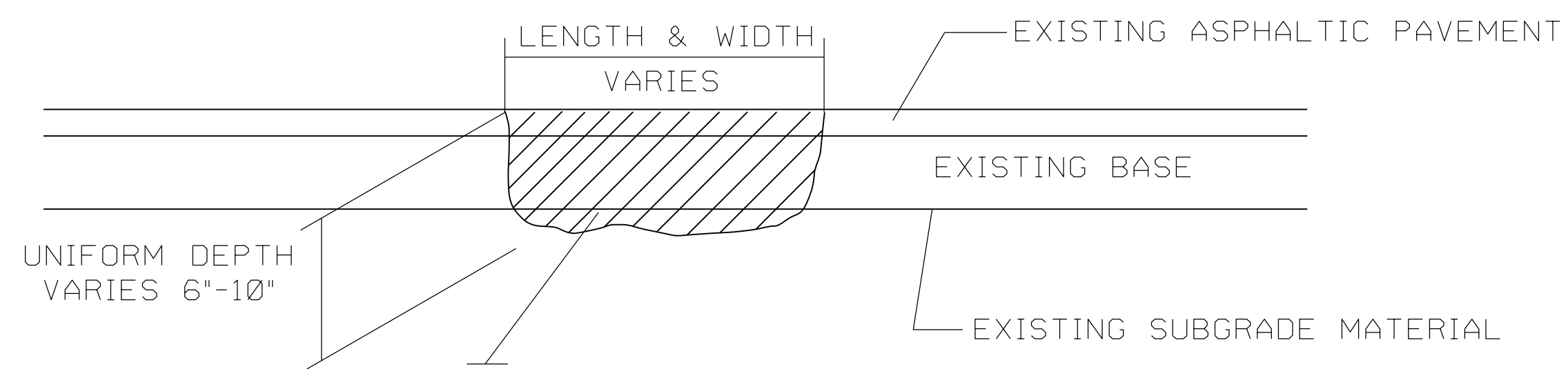
MAP 1 - FROM MM 36.5 TO MM 45.3
 MAP 2 - FROM MM 36.5 TO MM 45.3

*NOTES FOR ALL TYPICALS:

-SEE BRIDGE APPROACH AND DEPARTURE DETAIL FOR PAVEMENT TRANSITIONS AT BRIDGE UNDERPASSES & OVERPASSES.



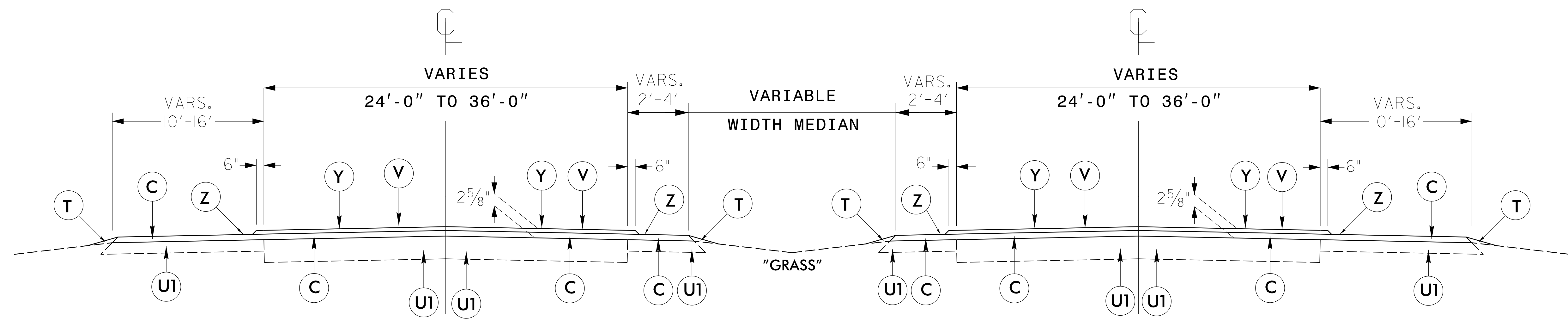
TIE-IN DETAIL AT MM 36.5 NB & SB



PATCHING EXISTING ASPHALT PAVEMENT DETAIL

| PAVEMENT SCHEDULE | |
|-------------------|---|
| C | PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD. |
| C1 | PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D AT AN AVERAGE RATE OF 224 LBS. PER SQ. YARD. |
| C2 | PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YARD. |
| C3 | PROP. APPROX. 3" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YARD IN EACH OF TWO LIFTS. |
| E | PROP. APPROX. 5½" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 627 LBS. PER SQ. YARD |
| E1 | PROP. APPROX. 4" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, WITH FIBER REINFORCEMENT, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YARD |
| T | SHOULDER RECONSTRUCTION (AGGREGATE SHOULDER BORROW) |
| U | EXISTING CONCRETE PAVEMENT. |
| U1 | EXISTING ASPHALT PAVEMENT. |
| V | MILL ASPHALT PAVEMENT APPROX. 5/8" DEPTH |
| V1 | MILL ASPHALT PAVEMENT APPROX 2" DEPTH |
| V2 | MILL ASPHALT PAVEMENT APPROX 0 to 2" DEPTH |
| V3 | MILL ASPHALT PAVEMENT APPROX. 6" TOTAL DEPTH IN TWO 3" PASSES |
| Y | PROP. APPROX. 5/8" ULTRA-THIN BONDED WEARING COURSE, AT AN AVERAGE RATE OF 70 LBS. PER SQ. YD. PER 5/8" DEPTH |
| Z | MILLED RUMBLE STRIPS |

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.



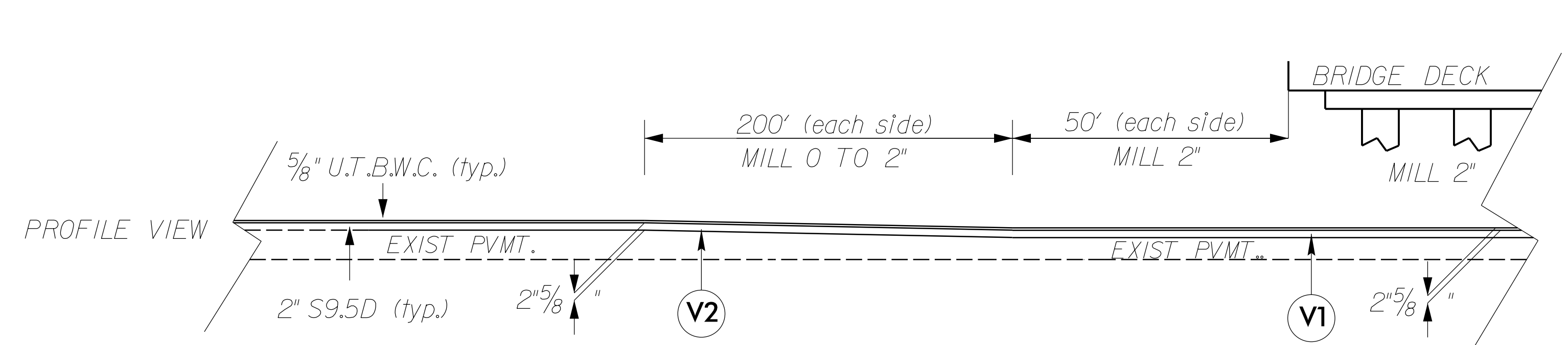
TYPICAL SECTION NO. 2

MAP 1 - FROM MM 45.3 TO MM 48.59
 MAP 2 - FROM MM 45.3 TO MM 48.59

PAVEMENT SCHEDULE

| | |
|----|---|
| C | PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD. |
| C1 | PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D AT AN AVERAGE RATE OF 224 LBS. PER SQ. YARD. |
| C2 | PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YARD. |
| C3 | PROP. APPROX. 3" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YARD IN EACH OF TWO LIFTS. |
| E | PROP. APPROX. 5 1/2" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 627 LBS. PER SQ. YARD |
| E1 | PROP. APPROX. 4" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, WITH FIBER REINFORCEMENT, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YARD |
| T | SHOULDER RECONSTRUCTION (AGGREGATE SHOULDER BORROW) |
| U | EXISTING CONCRETE PAVEMENT. |
| U1 | EXISTING ASPHALT PAVEMENT. |
| V | MILL ASPHALT PAVEMENT APPROX. 5/8" DEPTH |
| V1 | MILL ASPHALT PAVEMENT APPROX 2" DEPTH |
| V2 | MILL ASPHALT PAVEMENT APPROX 0 to 2" DEPTH |
| V3 | MILL ASPHALT PAVEMENT APPROX. 6" TOTAL DEPTH IN TWO 3" PASSES |
| Y | PROP. APPROX. 5/8" ULTRA-THIN BONDED WEARING COURSE, AT AN AVERAGE RATE OF 70 LBS. PER SQ. YD. PER 5/8" DEPTH |
| Z | MILLED RUMBLE STRIPS |

Bridge Underpass Approach And Departure Detail



USE DETAIL AT THE FOLLOWING LOCATIONS IN CONJUNCTION WITH TYPICAL NO. 1 & NO. 2 AT THE FOLLOWING UNDERPASS BRIDGES:

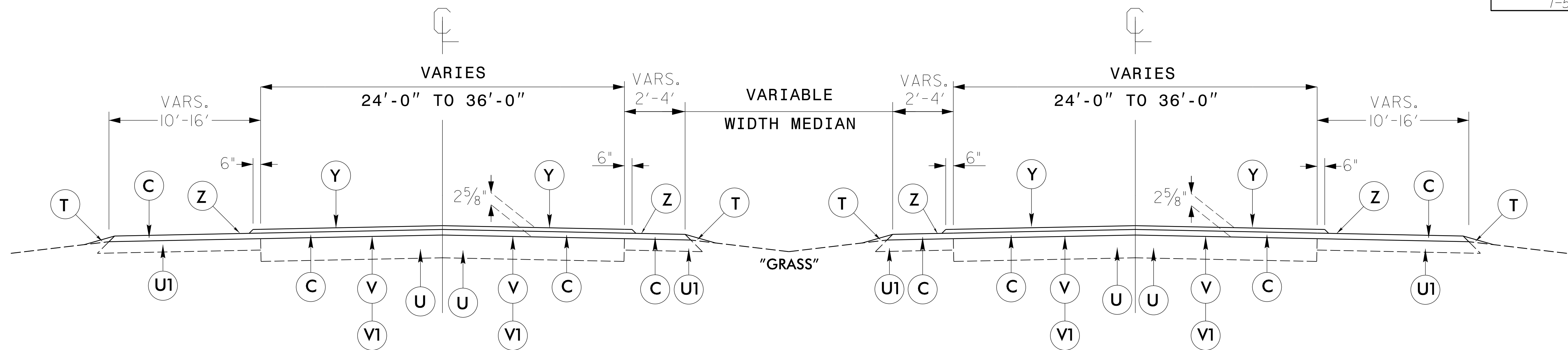
- BRIDGE #080 - SR 1302 (SB LANES ONLY)
- BRIDGE #092 - SR 1312 (SB LANES ONLY)
- BRIDGE #120 - SR 1001 (SB LANES ONLY) ** 5/8" U.T.B.W.C. to be removed prior per Typ. No. 2

****Note:** Existing verticle clearances shall not be reduced at these locations.

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.

5/14/99

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 7 MM 36 to 40X\tops\tops_Details\1-5918_Final\CADD_Files\177_rdy_typicals_041824.dgn



TYPICAL SECTION NO. 3

*NOTES FOR ALL TYPICALS:

-SEE BRIDGE APPROACH AND DEPARTURE DETAIL FOR PAVEMENT TRANSITIONS AT BRIDGE UNDERPASSES & OVERPASSES.

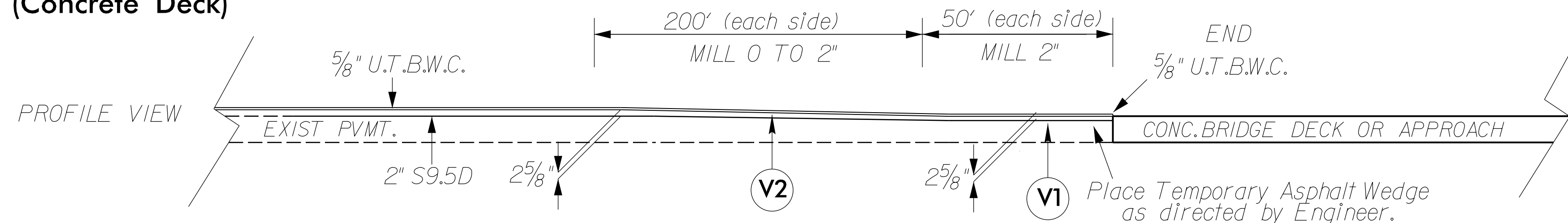
MAP 1 - FROM MM 48.59 TO MM 48.85 (END PROJECT I-3819)
 MAP 2 - FROM MM 48.59 TO MM 48.85 (END PROJECT I-3819)

PAVEMENT SCHEDULE

| | |
|----|---|
| C | PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD. |
| C1 | PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D AT AN AVERAGE RATE OF 224 LBS. PER SQ. YARD. |
| C2 | PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YARD. |
| C3 | PROP. APPROX. 3" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YARD IN EACH OF TWO LIFTS. |
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| E1 | PROP. APPROX. 4" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, WITH FIBER REINFORCEMENT, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YARD |
| T | SHOULDER RECONSTRUCTION (AGGREGATE SHOULDER BORROW) |
| U | EXISTING CONCRETE PAVEMENT. |
| U1 | EXISTING ASPHALT PAVEMENT. |
| V | MILL ASPHALT PAVEMENT APPROX. 5/8" DEPTH |
| V1 | MILL ASPHALT PAVEMENT APPROX 2" DEPTH |
| V2 | MILL ASPHALT PAVEMENT APPROX 0 TO 2" DEPTH |
| V3 | MILL ASPHALT PAVEMENT APPROX. 6" TOTAL DEPTH IN TWO 3" PASSES |
| Y | PROP. APPROX. 5/8" ULTRA-THIN BONDED WEARING COURSE, AT AN AVERAGE RATE OF 70 LBS. PER SQ. YD. PER 5/8" DEPTH |
| Z | MILLED RUMBLE STRIPS |

Bridge Overpass Approach And Departure Details

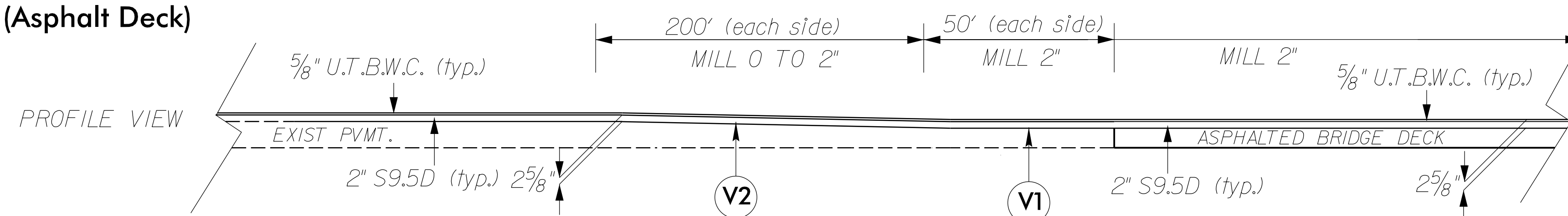
(Concrete Deck)



USE DETAIL AT THE FOLLOWING LOCATIONS IN CONJUNCTION WITH TYPICAL NO. 1 & NO. 2 AT THE FOLLOWING OVERPASS BRIDGES:

- BRIDGE #097 - US 21
- BRIDGE #098 - US 21
- BRIDGE #105 - WEATHERS CRK.
- BRIDGE #106 - WEATHERS CRK.

(Asphalt Deck)



USE DETAIL AT THE FOLLOWING LOCATIONS IN CONJUNCTION WITH TYPICAL NO. 1 & NO. 2 AT THE FOLLOWING OVERPASS BRIDGES:

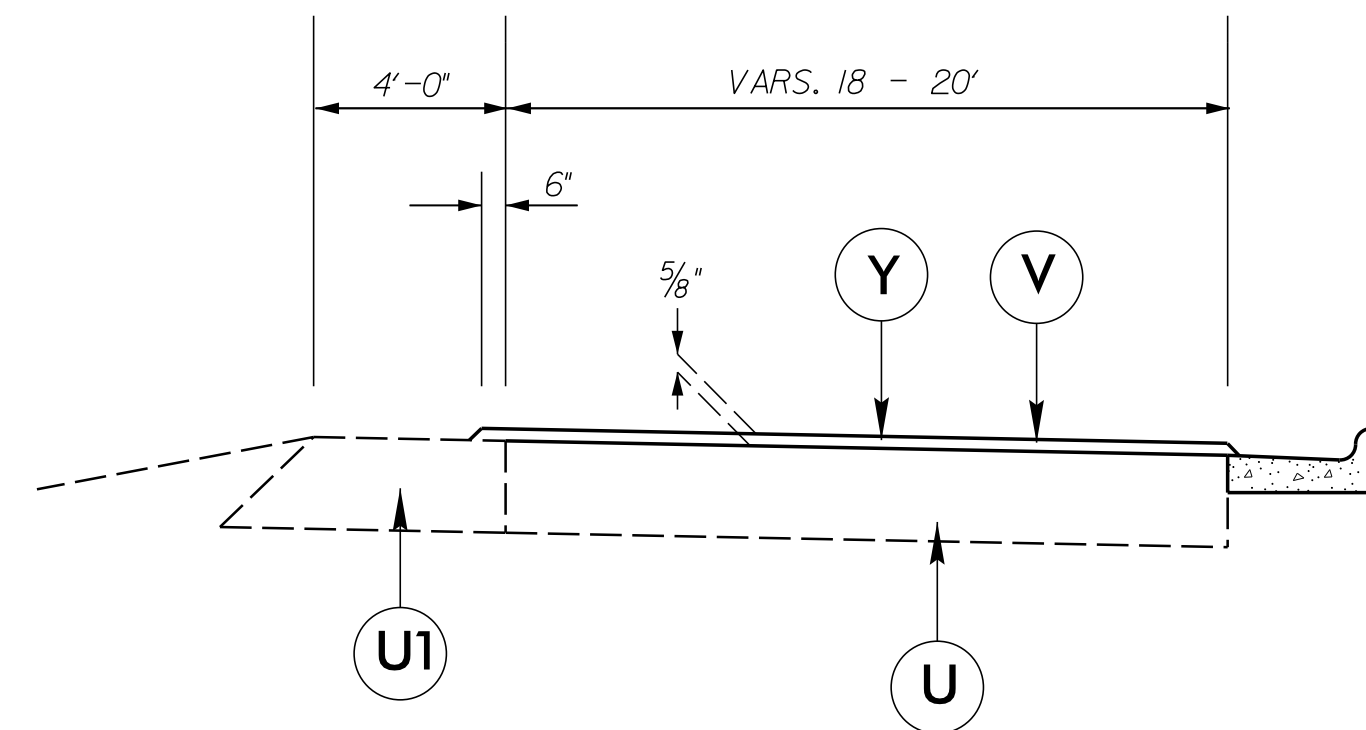
- BRIDGE #128 - THIRD CREEK ** 5/8" U.T.B.W.C. to be removed prior per Typ. No. 2
- BRIDGE #133 - THIRD CREEK ** 5/8" U.T.B.W.C. to be removed prior per Typ. No. 2

**NOTE: BRIDGES #138, #139, #145, #146, #149 & #151 WILL BE MILLED AND REPLACED PER TYPICAL NO. 3

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.

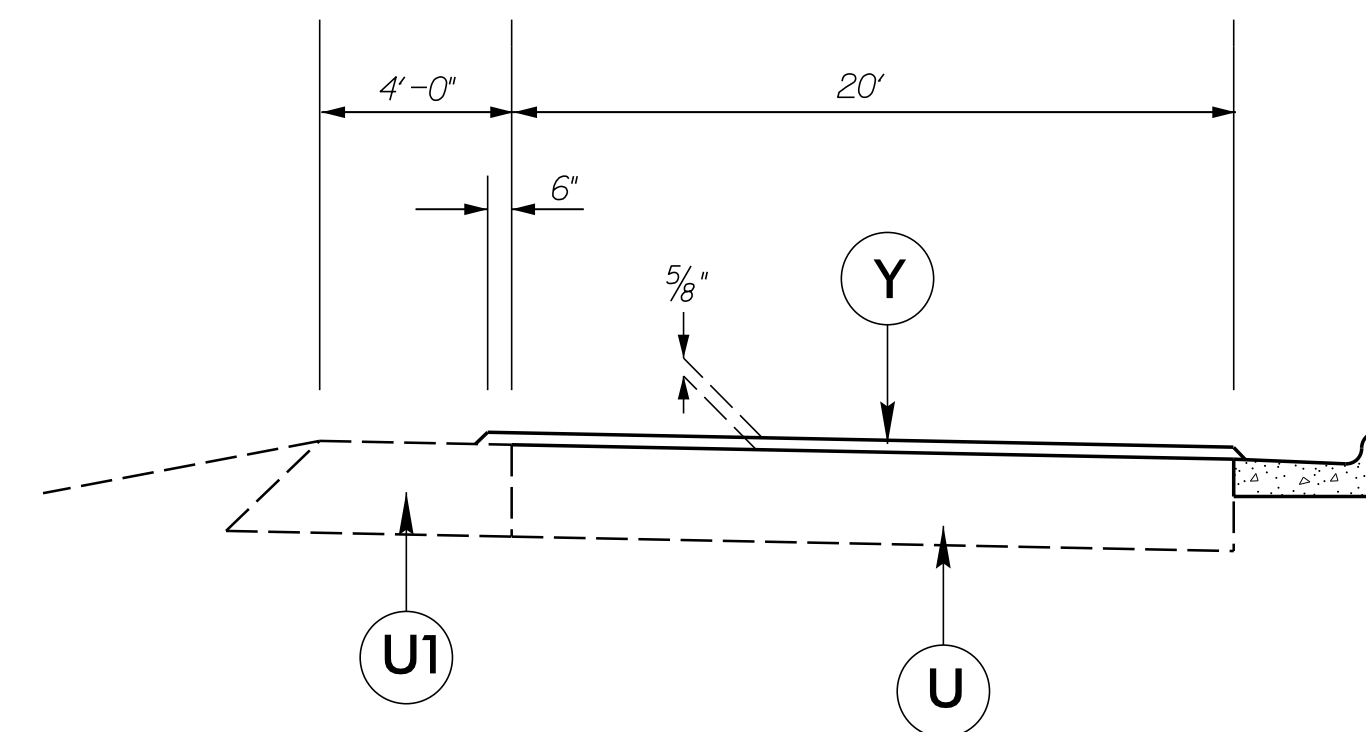
| PAVEMENT SCHEDULE | |
|-------------------|---|
| C | PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD. |
| C1 | PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D AT AN AVERAGE RATE OF 224 LBS. PER SQ. YARD. |
| C2 | PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YARD. |
| C3 | PROP. APPROX. 3" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YARD IN EACH OF TWO LIFTS. |
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| E1 | PROP. APPROX. 4" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, WITH FIBER REINFORCEMENT, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YARD |
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| U | EXISTING CONCRETE PAVEMENT. |
| U1 | EXISTING ASPHALT PAVEMENT. |
| V | MILL ASPHALT PAVEMENT APPROX. 5/8" DEPTH |
| V1 | MILL ASPHALT PAVEMENT APPROX 2" DEPTH |
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| V3 | MILL ASPHALT PAVEMENT APPROX. 6" TOTAL DEPTH IN TWO 3" PASSES |
| Y | PROP. APPROX. 5/8" ULTRA-THIN BONDED WEARING COURSE, AT AN AVERAGE RATE OF 70 LBS. PER SQ. YD. PER 5/8" DEPTH |
| Z | MILLED RUMBLE STRIPS |

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.



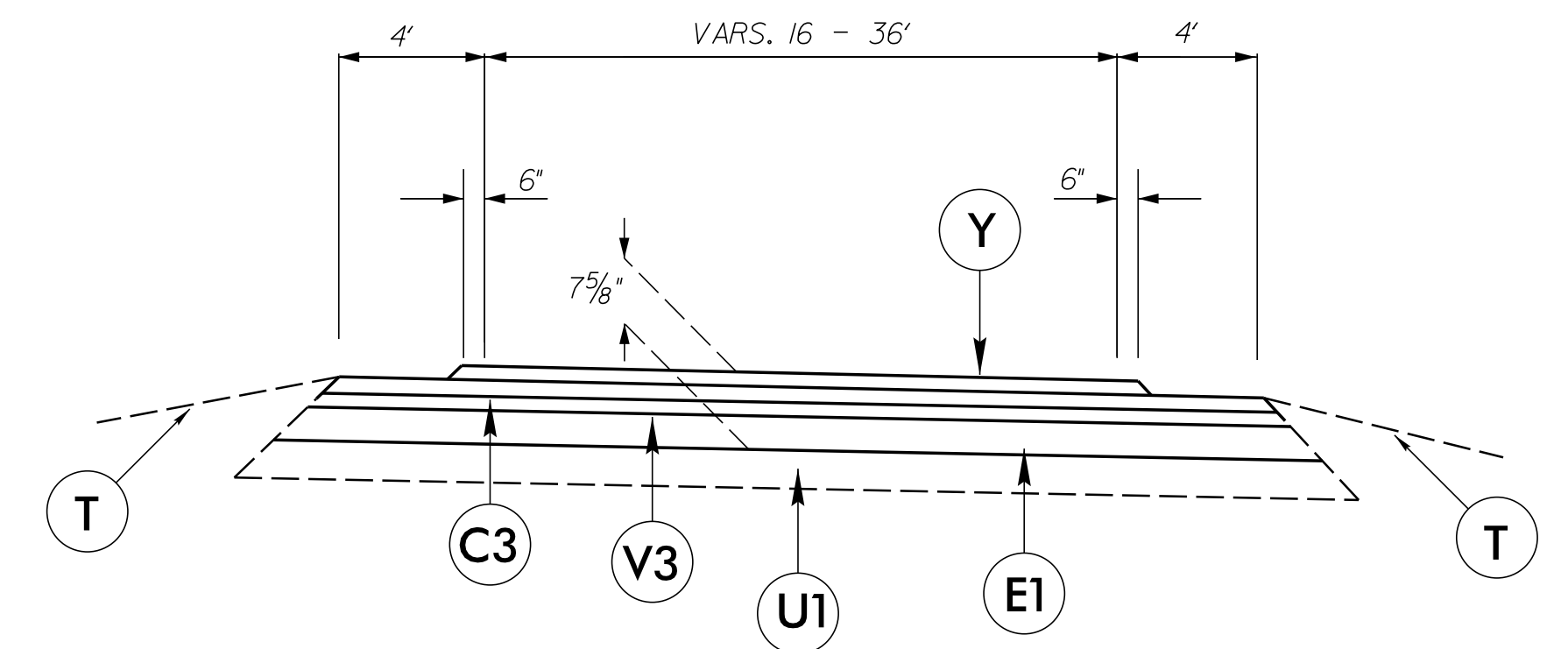
TYPICAL SECTION NO. 9

MAP 13 - EXIT 49A NB ON-LOOP
 MAP 15 - EXIT 49B NB OFF-LOOP (0+00 TO 4+25)
 MAP 17 - EXIT 49B SB ON-LOOP
 MAP 18 - EXIT 49B SB OFF-RAMP



TYPICAL SECTION NO. 10

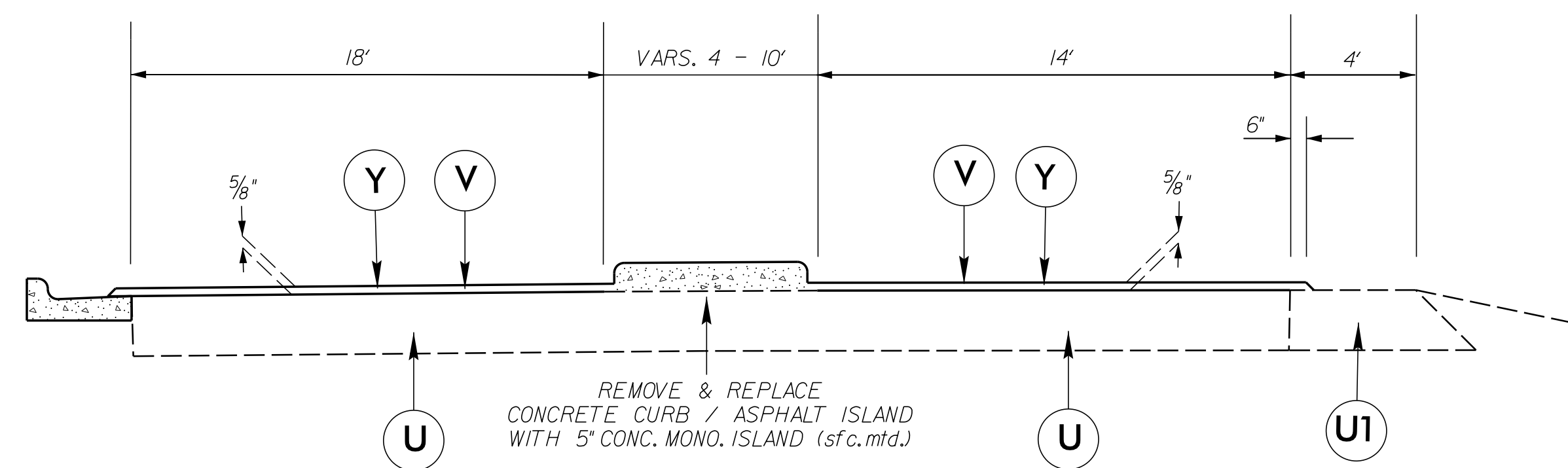
MAP 14 - EXIT 49A SB OFF-LOOP



** Install pavement interlayer prior to placing base course or as directed.

TYPICAL SECTION NO. 12

MAP 5 - EXIT 42 SB ON-RAMP

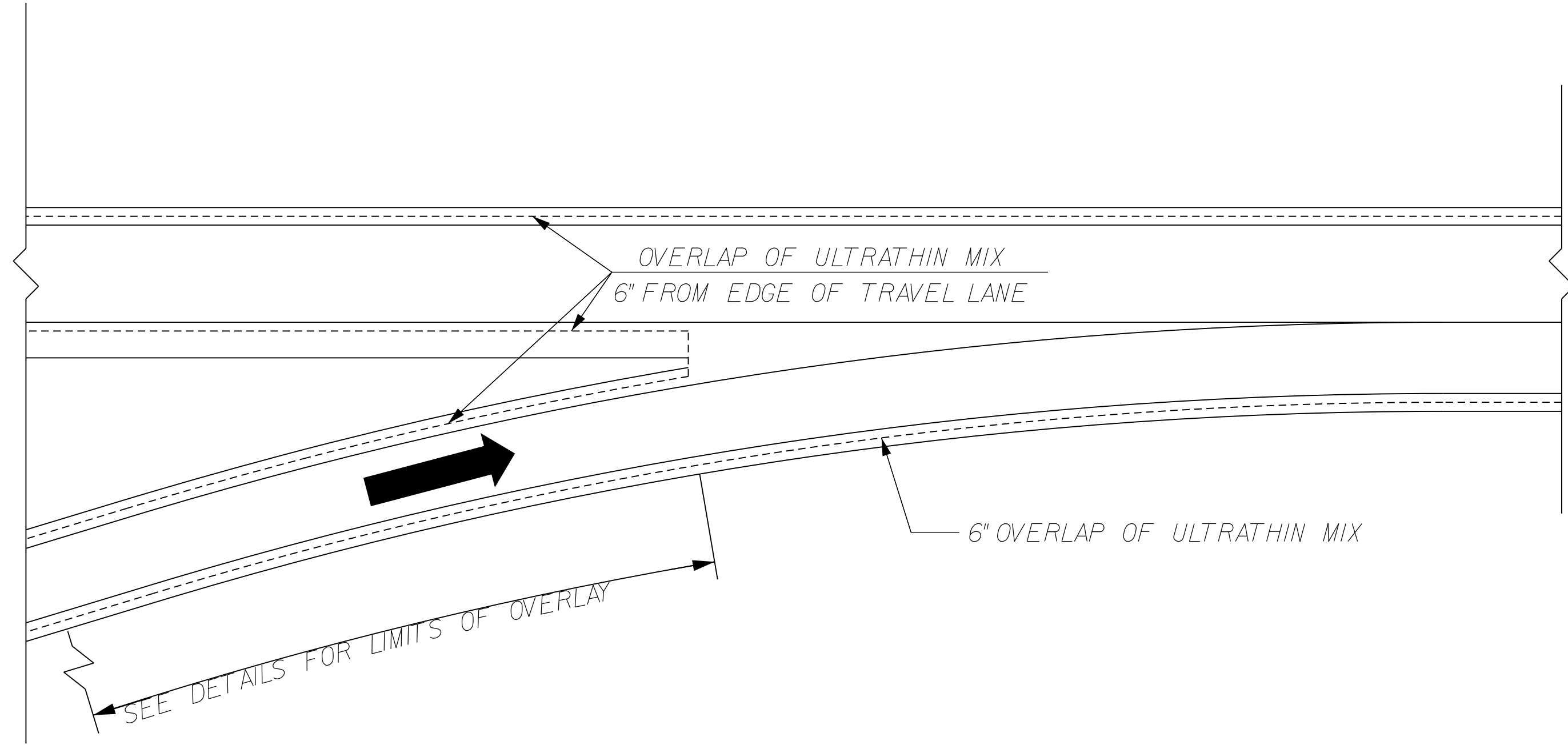
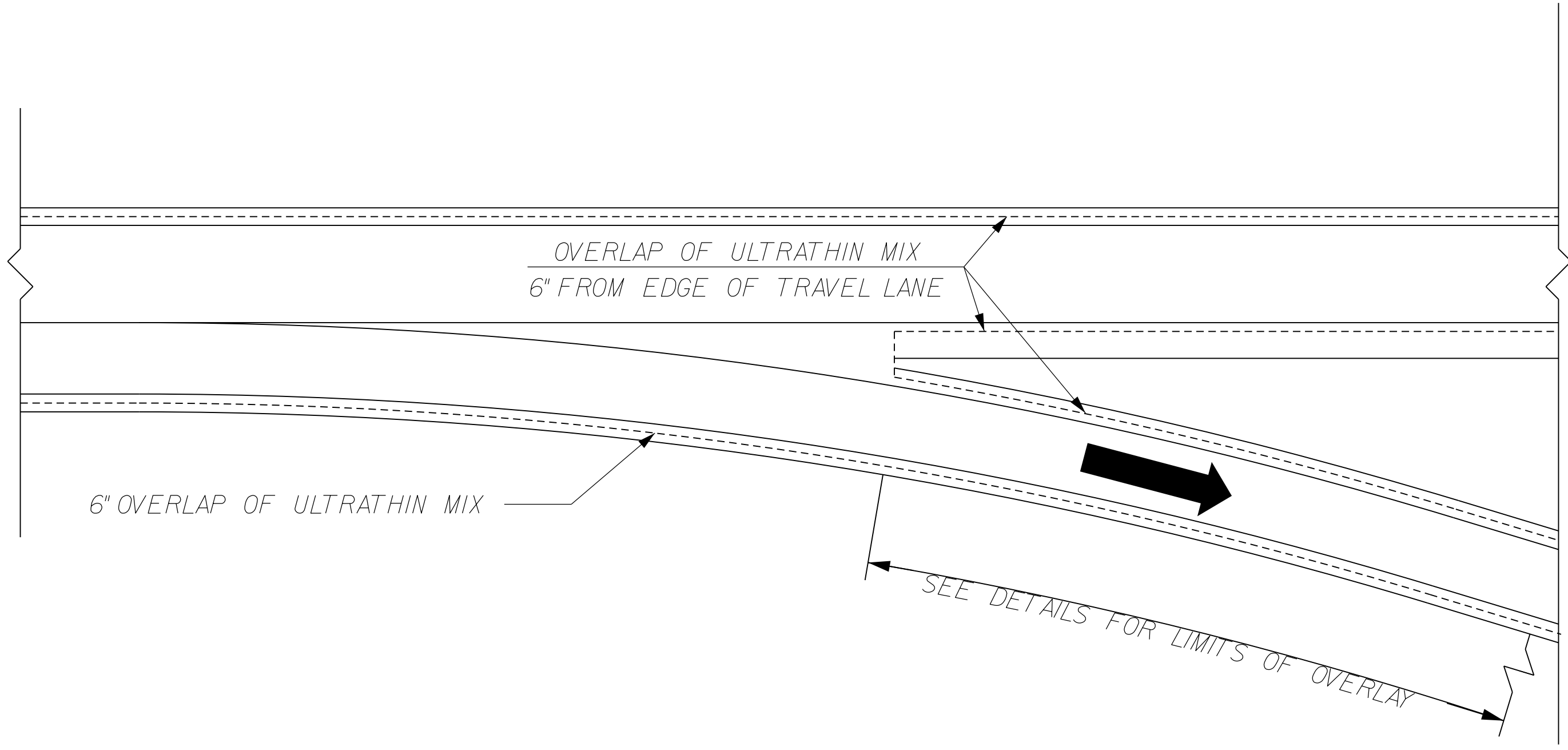
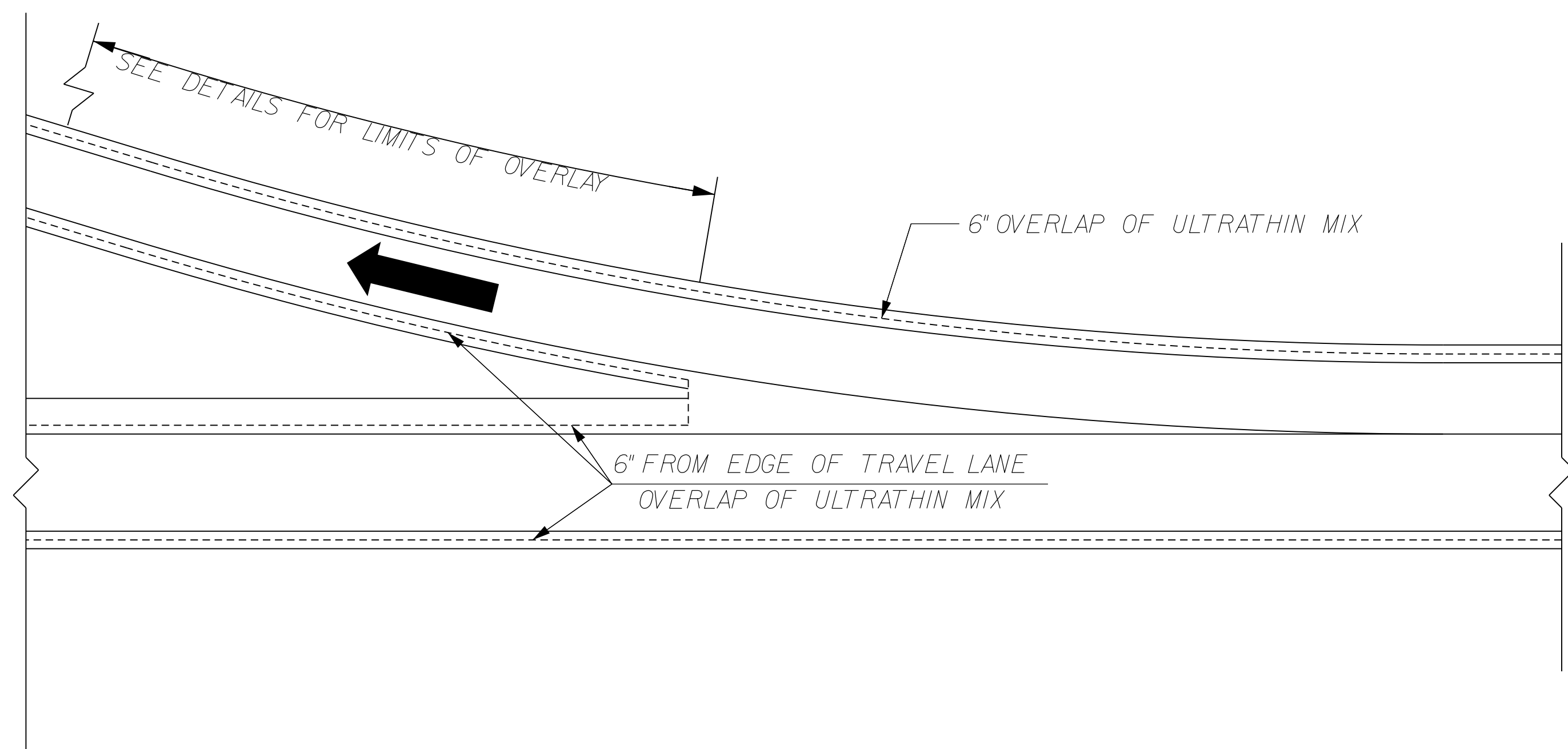
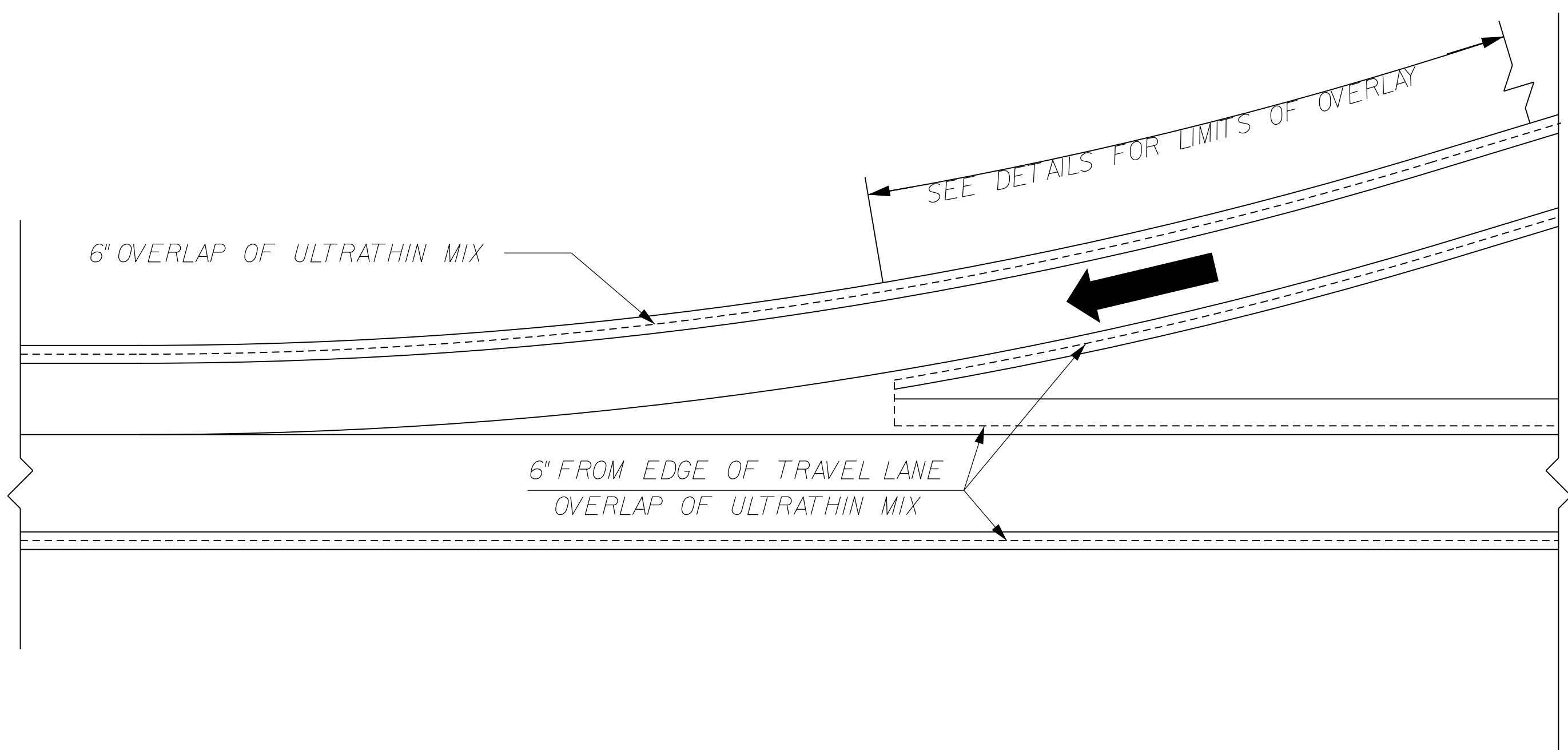


TYPICAL SECTION NO. 11

MAP 15 - EXIT 49B NB OFF-LOOP (STA 4+25 TO 8+75)

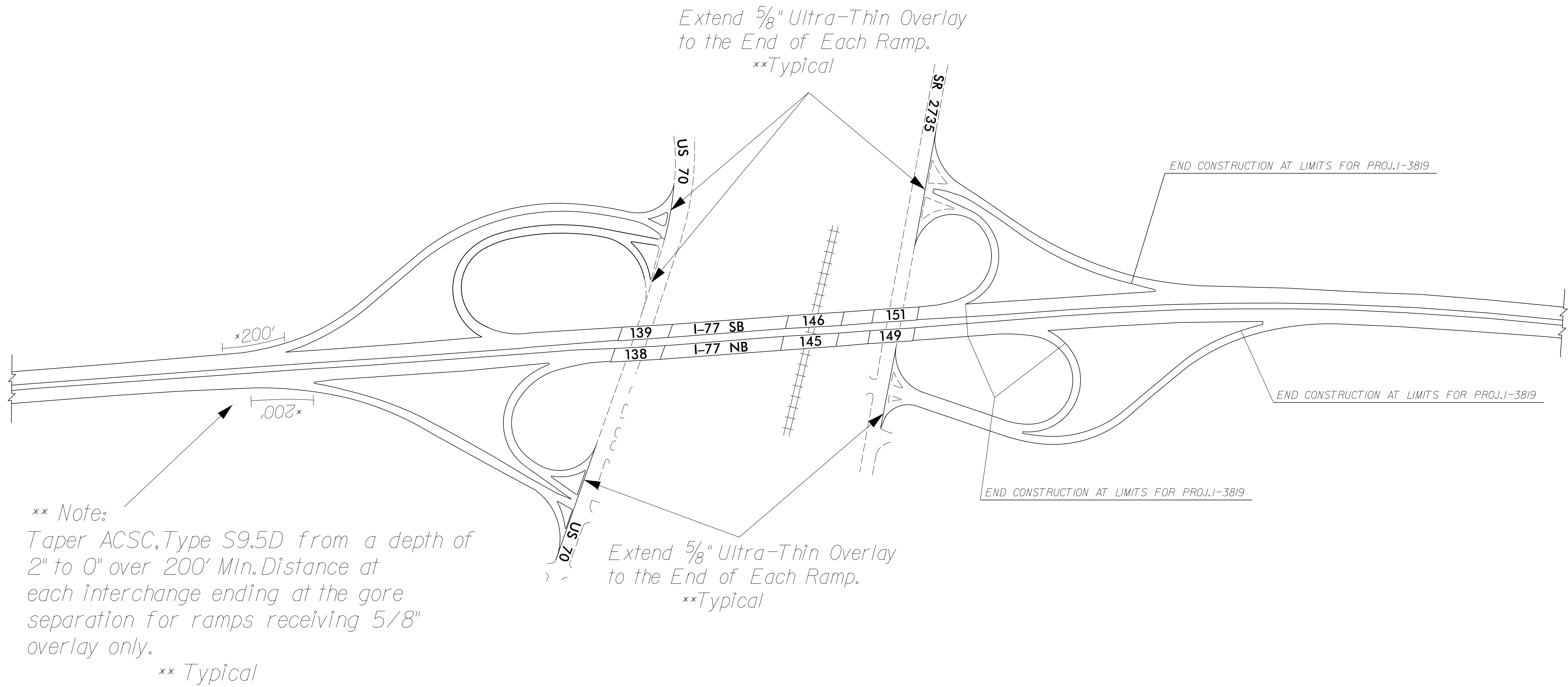
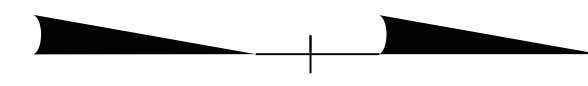
MAP 16 - EXIT 49B NB ON-RAMP (STA. 0+00 TO 4+50)

5/14/99
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 05-APP-0024.dwg
 49\Types_Maps_Details\177_rdy_typical.dgn



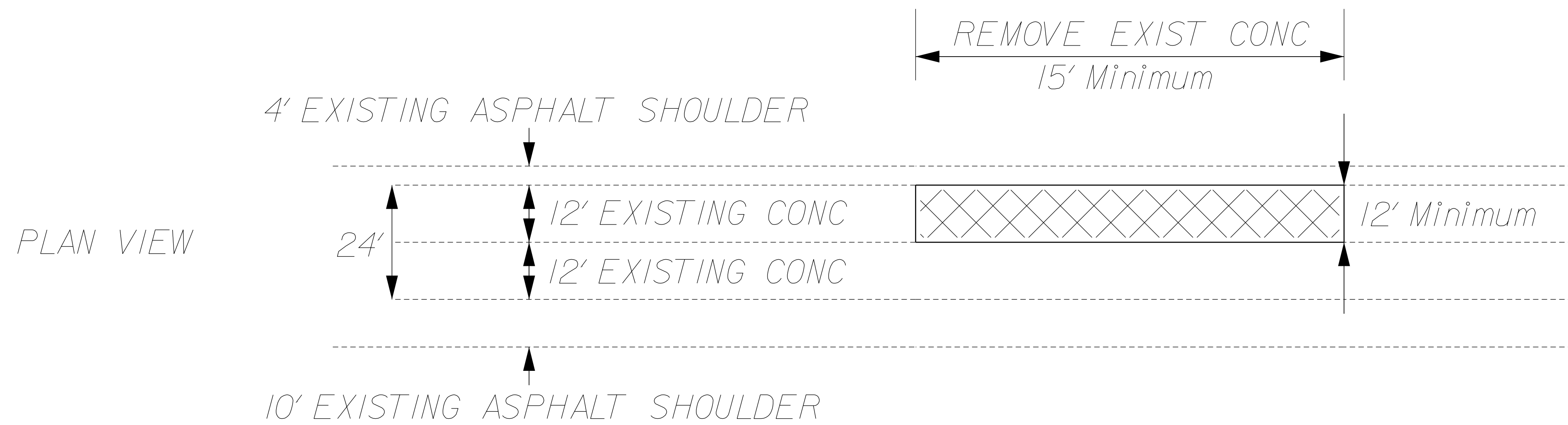
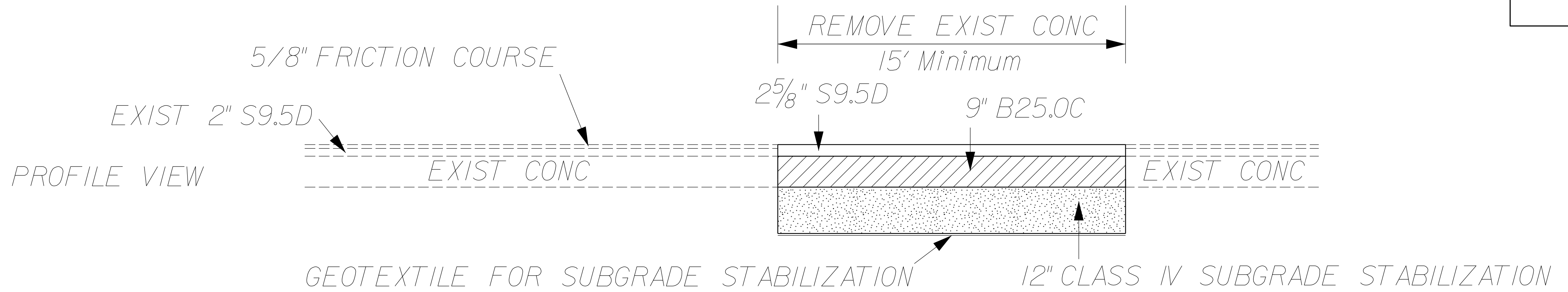
GORE AND RAMP RESURFACING DETAIL

USE DETAIL IN CONJUNCTION WITH TYPICAL SECTIONS



END CONSTRUCTION DETAIL

| | |
|-------------------------|---------------------|
| PROJECT REFERENCE NO. | SHEET NO. |
| 1-5918 | 2B-3 |
| RW SHEET NO. | |
| ROADWAY DESIGN ENGINEER | HYDRAULICS ENGINEER |



Concrete Slab Removal and Asphalt Replacement Detail

** Existing Concrete Pavement extends from approx. MM 45.0 N. to MM 48.85 (end project).

Notes:

1. Breaking & Removal of Concrete Slabs will be paid as "Removal of Existing Concrete Pavement Slabs (SY)"
2. Asphalt placement will be paid for as "Patching Existing Pavement (Ton)."

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 \$\$\$\$ USER: JREBELL \$\$\$\$

| | |
|-------------|-----------|
| PROJECT NO. | SHEET NO. |
| I-5916 | 3B-3 |

SUMMARY OF QUANTITIES

| PROJECT NO | COUNTY | MAP NO | ROUTE | DESCRIPTION | TYP NO | LANES | LANE TYPE | FINAL SURFACE TESTING REQUIRED | WARM MIX ASPHALT REQUIRED | LENGTH | WIDTH | BEGIN MP | END MP | 017000000-E | 024100000-E | 025500000-E | 109950000-E | 109970000-E | 111200000-E | 122000000-E | 124500000-E | 129700000-E | | | 130800000-E | 133000000-E | 149100000-E | 152300000-E | 152420000-E | 157500000-E | 157700000-E | 170400000-E | 183914000-E | |
|-------------------------------------|---------|--------|-------------------------------------|--|--------|-------|-----------|--------------------------------|---------------------------|--------|--------------|----------|-----------------|---------------------------------|--|---------------------------|------------------|---------------------------------|---------------------------------------|-----------------------|-------------------------|--------------|----------------|---------------|------------------|--------------------|---------------------|-----------------------|---|------------------------------|---------------------------------|----------------------------|----------------------------------|------------------|
| | | | | | | | | | | | | | | REMOVAL OF EXST CONC PVMT SLABS | REMOVAL OF EXISTING ASPHALT / CONC. ISLAND | AGGREGATE SHOULDER BORROW | SHALLOW UNDERCUT | CLASS IV SUBGRADE STABILIZATION | GEOTEXTILE FOR SUBGRADE STABILIZATION | INCIDENTAL STONE BASE | SHOULDER RECONSTRUCTION | 5/8" MILLING | 2" MILLING | 3" MILLING | 0" TO 2" MILLING | INCIDENTAL MILLING | BASE COURSE, B25.0C | SURFACE COURSE, S9.5C | ASPHALT CONC SURFACE COURSE, TYPE S9.5D | ASPHALT BINDER FOR PLANT MIX | POLYMER MODIFIED ASPHALT BINDER | PATCHING EXISTING PAVEMENT | ULTRA-THIN BONDED WEARING COURSE | |
| | | | | | | | | | | | MI | FT | SY | SY | TON | CY | TON | SY | TON | SMI | SY | SY | SY | SY | SY | TON | TON | TON | TON | TON | TON | TON | | |
| 45924.3.1 | Iredell | 1 | I-77 N | FROM MM 36.5 TO MM 48.85 | 1,2,3 | 2,3 | MD | NO | NO | 12.35 | 38 | 36.5 | 48.85 | 1,000.00 | | 4,100 | 250.00 | 750 | 1,000.00 | 500 | 24.70 | 55,000 | 6,500 | 7,750 | | | | | 34,266 | | 2,365 | 1,000 | 7,256.00 | |
| TOTAL FOR MAP NO. 1 | | | | | | | | | | | 12.35 | | 1,000.00 | | 4,100 | 250.00 | 750 | 1,000.00 | 500 | 24.70 | 55,000 | 6,500 | 7,750 | | | | | 34,266 | | 2,365 | 1,000 | 7,256.00 | | |
| 45924.3.1 | Iredell | 2 | I-77 S | FROM MM 36.5 TO MM 48.85 | 1,2,3 | 2,3 | MD | NO | NO | 12.35 | 38 | 36.5 | 48.85 | 1,000.00 | | 4,100 | 250.00 | 750 | 1,000.00 | 500 | 24.70 | 55,000 | 7,500 | 13,750 | | | | | 34,282 | | 2,366 | 1,000 | 7,260.67 | |
| TOTAL FOR MAP NO. 2 | | | | | | | | | | | 12.35 | | 1,000.00 | | 4,100 | 250.00 | 750 | 1,000.00 | 500 | 24.70 | 55,000 | 7,500 | 13,750 | | | | | 34,282 | | 2,366 | 1,000 | 7,260.67 | | |
| 45924.3.1 | Iredell | 3 | EXIT 42 - I-77 NB OFF RAMP | FROM I-77 NB TO US 21 | 4 | 1 | | NO | NO | 0.25 | 26 | 0 | 0.25 | | | 100 | | | | | 0.36 | | 3,500 | | | | | | 618 | | 47 | 50 | 144.00 | |
| TOTAL FOR MAP NO. 3 | | | | | | | | | | | 0.25 | | | | 100 | | | | | | 0.36 | | 3,500 | | | | | | 618 | | 47 | 50 | 144.00 | |
| 45924.3.1 | Iredell | 4 | EXIT 42 - I-77 NB ON LOOP | FROM US 21 TO I-77 NB | 5 | 1 | | NO | NO | 0.15 | 20 | 0 | 0.15 | | | 15 | | | | | 0.09 | | 3,500 | | | | | | 367 | | 30 | | 104.00 | |
| TOTAL FOR MAP NO. 4 | | | | | | | | | | | 0.15 | | | | 15 | | | | | | 0.09 | | 3,500 | | | | | | 367 | | 30 | | 104.00 | |
| 45924.3.1 | Iredell | 5 | EXIT 42 - I-77 SB ON RAMP | FROM US 21 TO I-77 SB | 12 | 1 | | NO | NO | 0.38 | 24 | 0 | 0.38 | | | 180 | | | | | 0.74 | | | 9,000 | | | 1,479 | 1,195 | 80 | 75 | | 193.00 | | |
| TOTAL FOR MAP NO. 5 | | | | | | | | | | | 0.38 | | | | 180 | | | | | | 0.74 | | | 9,000 | | | 1,479 | 1,195 | 80 | 75 | | 193.00 | | |
| 45924.3.1 | Iredell | 6 | EXIT 42 - I-77 SB OFF LOOP | FROM I-77 SB TO US 21 | 5 | 1 | | NO | NO | 0.22 | 20 | 0 | 0.22 | | | 100 | | | | | 0.26 | | 4,000 | | 250 | | | | 518 | | 45 | | 161.00 | |
| TOTAL FOR MAP NO. 6 | | | | | | | | | | | 0.22 | | | | 100 | | | | | | 0.26 | | 4,000 | | 250 | | | | 518 | | 45 | | 161.00 | |
| 45924.3.1 | Iredell | 7 | EXIT 45 - I-77 NB OFF RAMP | FROM I-77 NB TO AMITY HILL RD (SR 2342) | 6 | 1 | | NO | NO | 0.16 | 24 | 0 | 0.16 | | | | | | | | | | | | 250 | | | | 4 | | | 74.00 | | |
| TOTAL FOR MAP NO. 7 | | | | | | | | | | | 0.16 | | | | | | | | | | | | | 250 | | | | | | 4 | | 74.00 | | |
| 45924.3.1 | Iredell | 8 | EXIT 45 - I-77 SB ON RAMP | FROM AMITY HILL RD (SR 2342) TO I-77 SB | 7 | 1 | | NO | NO | 0.25 | 24 | 0 | 0.25 | | | 150 | | | | | 0.50 | | | | 250 | 400 | 135 | | 26 | 6 | | 111.00 | | |
| TOTAL FOR MAP NO. 8 | | | | | | | | | | | 0.25 | | | | 150 | | | | | | 0.50 | | | 250 | 400 | 135 | | | 26 | 6 | | 111.00 | | |
| 45924.3.1 | Iredell | 9 | EXIT 45 I-77 NB ON RAMP / DOVER RD. | FROM AMITY HILL RD (SR 2342) TO I-77 NB | 8 | 1 | | NO | NO | 0.23 | 30 | 0 | 0.23 | | | | | | | | | 5,131 | | 250 | | | | | 7 | | | 132.00 | | |
| TOTAL FOR MAP NO. 9 | | | | | | | | | | | 0.23 | | | | | | | | | | | 5,131 | | 250 | | | | | | 7 | | | 132.00 | |
| 45924.3.1 | Iredell | 10 | EXIT 45 - I-77 SB OFF RAMP | FROM I-77 SB TO AMITY HILL RD (SR 2342) | 8 | 1 | | NO | NO | 0.16 | 30 | 0 | 0.16 | | | | | | | | | | 1,200 | | 250 | | | | 4 | | | 81.00 | | |
| TOTAL FOR MAP NO. 10 | | | | | | | | | | | 0.16 | | | | | | | | | | | 1,200 | | 250 | | | | | | 4 | | | 81.00 | |
| 45924.3.1 | Iredell | 11 | EXIT 49A - I-77 NB OFF RAMP | FROM I-77 NB TO US 70 | 8 | 1 | | NO | NO | 0.22 | 24 | 0 | 0.22 | | | | | | | | | 3,668 | | | | | | | 6 | | | 110.00 | | |
| TOTAL FOR MAP NO. 11 | | | | | | | | | | | 0.22 | | | | | | | | | | | 3,668 | | | | | | | | 6 | | | 110.00 | |
| 45924.3.1 | Iredell | 12 | EXIT 49A - I-77 SB ON RAMP | FROM US 70 TO I-77 SB | 8 | 1 | | NO | NO | 0.3 | 24 | 0 | 0.3 | | | | | | | | | 4,824 | | | | | | | 7 | | | 144.00 | | |
| TOTAL FOR MAP NO. 12 | | | | | | | | | | | 0.3 | | | | | | | | | | | 4,824 | | | | | | | | 7 | | | 144.00 | |
| 45924.3.1 | Iredell | 13 | EXIT 49A - I-77 NB ON LOOP | FROM US 70 TO I-77 NB | 9 | 1 | | NO | NO | 0.14 | 24 | 0 | 0.14 | | | | | | | | | 2,305 | | | | | | | 4 | | | 69.00 | | |
| TOTAL FOR MAP NO. 13 | | | | | | | | | | | 0.14 | | | | | | | | | | | 2,305 | | | | | | | | 4 | | | 69.00 | |
| 45924.3.1 | Iredell | 14 | EXIT 49A - I-77 SB OFF LOOP | FROM I-77 SB TO US 70 | 10 | 1 | | NO | NO | 0.21 | 26 | 0 | 0.21 | | | | | | | | | | | | | | | | 5 | | | 104.00 | | |
| TOTAL FOR MAP NO. 14 | | | | | | | | | | | 0.21 | | | | | | | | | | | | | | | | | | | | 5 | | | 104.00 |
| 45924.3.1 | Iredell | 15 | EXIT 49B - I-77 NB OFF LOOP | FROM I-77 NB TO SALISBURY RD (SR 2735) | 9,11 | 1 | | NO | NO | 0.17 | 22 | 0 | 0.17 | | | 225.00 | | | | | | 2,294 | | | | | | | 4 | | | 80.00 | | |
| TOTAL FOR MAP NO. 15 | | | | | | | | | | | 0.17 | | | | 225.00 | | | | | | | 2,294 | | | | | | | | 4 | | | 80.00 | |
| 45924.3.1 | Iredell | 16 | EXIT 49B - I-77 NB ON RAMP | FROM SALISBURY RD (SR 2735) TO I-77 NB | 8,11 | 1 | | NO | NO | 0.26 | 24 | 0 | 0.26 | | | | | | | | | 3,861 | | | | | | | 7 | | | 135.00 | | |
| TOTAL FOR MAP NO. 16 | | | | | | | | | | | 0.26 | | | | | | | | | | | 3,861 | | | | | | | | 7 | | | 135.00 | |
| 45924.3.1 | Iredell | 17 | EXIT 49B - I-77 SB ON LOOP | FROM SALISBURY RD (SR2375) TO I-77 SB | 9 | 1 | | NO | NO | 0.13 | 22 | 0 | 0.13 | | | | | | | | | 2,011 | | | | | | | 4 | | | 70.00 | | |
| TOTAL FOR MAP NO. 17 | | | | | | | | | | | 0.13 | | | | | | | | | | | 2,011 | | | | | | | | 4 | | | 70.00 | |
| 45924.3.1 | Iredell | 18 | EXIT 49B - I-77 SB OFF RAMP | FROM I-3819 END CONSTRUCTION TO SALISBURY RD (SR 2375) | 9 | 1 | | NO | NO | 0.13 | 22 | 0 | 0.13 | | | | | | | | | 2,011 | | | | | | | 4 | | | 70.00 | | |
| TOTAL FOR MAP NO. 18 | | | | | | | | | | | 0.13 | | | | | | | | | | | 2,011 | | | | | | | | 4 | | | 70.00 | |
| TOTAL FOR PROJ NO. 45924.3.1 | | | | | | | | | | | 28.06 | | | | 2,000.00 | 225.00 | 8,745 | 500.00 | 1,500 | 2,000.00 | 1,000 | 51.35 | 137,305 | 25,000 | 9,000 | 21,500 | 1,250 | 1,879 | 135 | 71,246 | 26 | 4,995 | 2,125 | 16,298.67 |
| GRAND TOTAL | | | | | | | | | | | 28.06 | | | | 2,000.00 | 225.00 | 8,745 | 500.00 | 1,500 | 2,000.00 | 1,000 | 51.35 | 137,305 | 25,000 | 9,000 | 21,500 | 1,250 | 1,879 | 135 | 71,246 | 26 | 4,995 | 2,125 | 16,298.67 |

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| PROJECT NO. | SHEET NO. |
| I-5918 | 3B-4 |

SUMMARY OF QUANTITIES

| PROJECT NO | COUNTY | MAP NO | ROUTE | DESCRIPTION | TYP NO | LANES | LANE TYPE | FINAL SURFACE TESTING REQUIRED | WARM MIX ASPHALT REQUIRED | LENGTH | WIDTH | BEGIN MP | END MP | 184000000-E | 189100000-E | 262700000-E | 264700000-E | 303000000-E | 307500000-E | 313500000-N | 315000000-N | 321000000-N | 321500000-N | 328700000-N | 336000000-E | 343500000-N | 525500000-N | 732400000-N | 744400000-E | 745610000-E | | | | | | | | |
|-------------------------------------|---------|--------|-------------------------------------|--|--------|-------|-----------|--------------------------------|---------------------------|--------|-------|----------|--------|----------------------|---------------------|--------------------------|------------------------------|----------------------|--|---|----------------------------|---------------------------------|---------------------------------|--------------------------------|---------------------------|--------------------------------------|-------------------|--------------------------|-----------------------|----------------------|-------------|---------------|-------------------|--------------|-------------|----------|------------|------------|
| | | | | | | | | | | | | | | MILLED RUMBLE STRIPS | PAVEMENT INTERLAYER | 4" CONCRETE ISLAND COVER | 5" MONO ISLAND SURFACE MOUNT | STEEL BEAM GUARDRAIL | TRIPLE CORRUGATED STEEL BEAM GUARDRAIL | W-TR STEEL BEAM GUARDRAIL TRANSITION SECTIONS | ADDITIONAL GUARDRAIL POSTS | GUARDRAIL END UNITS, TYPE CAT-1 | GUARDRAIL ANCHOR UNIT, TYPE III | GUARDRAIL END UNITS, TYPE TL-3 | REMOVE EXISTING GUARDRAIL | REMOVE & RESET EXST. GDRAIL END UNIT | PORTABLE LIGHTING | JUNCTION BOX (STD. SIZE) | INDUCTIVE LOOP SAWCUT | LEAD-IN CABLE (14-2) | | | | | | | | |
| | | | | | | | | | | MI | FT | | | LF | SY | SY | SY | LF | LF | EA | EA | EA | EA | EA | EA | EA | EA | EA | EA | EA | EA | | | | | | | |
| 45924.3.1 | Iredell | 1 | I-77 N | FROM MM 36.5 TO MM 48.85 | 1,2,3 | 2, 3 | MD | NO | NO | 12.35 | 38 | 36.5 | 48.85 | 130,416 | | | | 84,298.00 | 250.00 | 2.00 | 50.00 | 38.00 | 1.00 | 100.00 | 84,298.00 | 5.00 | * | | | | | | | | | | | |
| TOTAL FOR MAP NO. 1 | | | | | | | | | | | | | | 130,416 | | | | 84,298.00 | 250.00 | 2.00 | 50.00 | 38.00 | 1.00 | 100.00 | 84,298.00 | 5.00 | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 2 | I-77 S | FROM MM 36.5 TO MM 48.85 | 1,2,3 | 2, 3 | MD | NO | NO | 12.35 | 38 | 36.5 | 48.85 | 130,416 | | | | 83,039.00 | 250.00 | 2.00 | 50.00 | 37.00 | 1.00 | 100.00 | 83,039.00 | 5.00 | * | | | | | | | | | | | |
| TOTAL FOR MAP NO. 2 | | | | | | | | | | | | | | 130,416 | | | | 83,039.00 | 250.00 | 2.00 | 50.00 | 37.00 | 1.00 | 100.00 | 83,039.00 | 5.00 | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 3 | EXIT 42 - I-77 NB OFF RAMP | FROM I-77 NB TO US 21 | 4 | 1 | | NO | NO | 0.25 | 26 | 0 | 0.25 | | | 450 | | | | | | | | | | | * | 1 | 264 | 200 | | | | | | | | |
| TOTAL FOR MAP NO. 3 | | | | | | | | | | | | | | 0.25 | | | | 450 | | | | | | | | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 4 | EXIT 42 - I-77 NB ON LOOP | FROM US 21 TO I-77 NB | 5 | 1 | | NO | NO | 0.15 | 20 | 0 | 0.15 | | | | | | | | | | | | | | * | | | | | | | | | | | |
| TOTAL FOR MAP NO. 4 | | | | | | | | | | | | | | 0.15 | | | | | | | | | | | | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 5 | EXIT 42 - I-77 SB ON RAMP | FROM US 21 TO I-77 SB | 12 | 1 | | NO | NO | 0.38 | 24 | 0 | 0.38 | | 4,500.00 | | | | | | | | | | | | * | | | | | | | | | | | |
| TOTAL FOR MAP NO. 5 | | | | | | | | | | | | | | 0.38 | | | | 4,500.00 | | | | | | | | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 6 | EXIT 42 - I-77 SB OFF LOOP | FROM I-77 SB TO US 21 | 5 | 1 | | NO | NO | 0.22 | 20 | 0 | 0.22 | | | | | | | | | | | | | | * | 1 | 264 | 200 | | | | | | | | |
| TOTAL FOR MAP NO. 6 | | | | | | | | | | | | | | 0.22 | | | | | | | | | | | | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 7 | EXIT 45 - I-77 NB OFF RAMP | FROM I-77 NB TO AMITY HILL RD (SR 2342) | 6 | 1 | | NO | NO | 0.16 | 24 | 0 | 0.16 | | | | | | | | | | | | | | * | | | | | | | | | | | |
| TOTAL FOR MAP NO. 7 | | | | | | | | | | | | | | 0.16 | | | | | | | | | | | | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 8 | EXIT 45 - I77 SB ON RAMP | FROM AMITY HILL RD (SR 2342) TO I-77 SB | 7 | 1 | | NO | NO | 0.25 | 24 | 0 | 0.25 | | | | | | | | | | | | | | * | | | | | | | | | | | |
| TOTAL FOR MAP NO. 8 | | | | | | | | | | | | | | 0.25 | | | | | | | | | | | | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 9 | EXIT 45 I-77 NB ON RAMP / DOVER RD. | FROM AMITY HILL RD (SR 2342) TO I-77NB | 8 | 1 | | NO | NO | 0.23 | 30 | 0 | 0.23 | | | | | | | | | | | | | | * | | | | | | | | | | | |
| TOTAL FOR MAP NO. 9 | | | | | | | | | | | | | | 0.23 | | | | | | | | | | | | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 10 | EXIT 45 - I-77 SB OFF RAMP | FROM I-77 SB TO AMITY HILL RD (SR 2342) | 8 | 1 | | NO | NO | 0.16 | 30 | 0 | 0.16 | | | | | | | | | | | | | | * | | | | | | | | | | | |
| TOTAL FOR MAP NO. 10 | | | | | | | | | | | | | | 0.16 | | | | | | | | | | | | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 11 | EXIT 49A - I-77 NB OFF RAMP | FROM I-77 NB TO US 70 | 8 | 1 | | NO | NO | 0.22 | 24 | 0 | 0.22 | | | | | | | | | | | | | | * | | | | | | | | | | | |
| TOTAL FOR MAP NO. 11 | | | | | | | | | | | | | | 0.22 | | | | | | | | | | | | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 12 | EXIT 49A - I-77 SB ON RAMP | FROM US 70 TO I-77 SB | 8 | 1 | | NO | NO | 0.3 | 24 | 0 | 0.3 | | | | | | | | | | | | | | * | | | | | | | | | | | |
| TOTAL FOR MAP NO. 12 | | | | | | | | | | | | | | 0.3 | | | | | | | | | | | | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 13 | EXIT 49A - I-77 NB ON LOOP | FROM US 70 TO I-77 NB | 9 | 1 | | NO | NO | 0.14 | 24 | 0 | 0.14 | | | | | | | | | | | | | | * | | | | | | | | | | | |
| TOTAL FOR MAP NO. 13 | | | | | | | | | | | | | | 0.14 | | | | | | | | | | | | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 14 | EXIT 49A - I-77 SB OFF LOOP | FROM I-77 SB TO US 70 | 10 | 1 | | NO | NO | 0.21 | 26 | 0 | 0.21 | | | | | | | | | | | | | | * | | | | | | | | | | | |
| TOTAL FOR MAP NO. 14 | | | | | | | | | | | | | | 0.21 | | | | | | | | | | | | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 15 | EXIT 49B - I-77 NB OFF LOOP | FROM I-77 NB TO SALISBURY RD (SR 2735) | 9,11 | 1 | | NO | NO | 0.17 | 22 | 0 | 0.17 | | | 225 | | | | | | | | | | | * | | | | | | | | | | | |
| TOTAL FOR MAP NO. 15 | | | | | | | | | | | | | | 0.17 | | | | | | | 225 | | | | | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 16 | EXIT 49B - I-77 NB ON RAMP | FROM SALISBURY RD (SR 2735) TO I-77 NB | 8,11 | 1 | | NO | NO | 0.26 | 24 | 0 | 0.26 | | | | | | | | | | | | | | * | | | | | | | | | | | |
| TOTAL FOR MAP NO. 16 | | | | | | | | | | | | | | 0.26 | | | | | | | | | | | | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 17 | EXIT 49B - I-77 SB ON LOOP | FROM SALISBURY RD (SR2375) TO I-77 SB | 9 | 1 | | NO | NO | 0.13 | 22 | 0 | 0.13 | | | | | | | | | | | | | | * | | | | | | | | | | | |
| TOTAL FOR MAP NO. 17 | | | | | | | | | | | | | | 0.13 | | | | | | | | | | | | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 18 | EXIT 49B - I-77 SB OFF RAMP | FROM I-3819 END CONSTRUCTION TO SALISBURY RD (SR 2375) | 9 | 1 | | NO | NO | 0.13 | 22 | 0 | 0.13 | | | | | | | | | | | | | | * | | | | | | | | | | | |
| TOTAL FOR MAP NO. 18 | | | | | | | | | | | | | | 0.13 | | | | | | | | | | | | | | | | | | | | | | | | |
| TOTAL FOR PROJ NO. 45924.3.1 | | | | | | | | | | | | | | 28.06 | | | | | | | | 260,832 | 4,500.00 | 450 | 225 | 167,337.00 | 500.00 | 4.00 | 100.00 | 75.00 | 2.00 | 200.00 | 167,337.00 | 10.00 | 1.00 | 2 | 528 | 400 |
| GRAND TOTAL | | | | | | | | | | | | | | 28.06 | | | | | | | | 260,832 | 4,500.00 | 450 | 225 | 167,337.00 | 500.00 | 4.00 | 100.00 | 75.00 | 2.00 | 200.00 | 167,337.00 | 10.00 | 1.00 | 2 | 528 | 400 |

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| PROJECT NO. | SHEET NO. |
| I-5918 | 3B-5 |

THERMOPLASTIC AND PAINT QUANTITIES

| PROJECT NO | COUNTY | MAP NO | ROUTE | DESCRIPTION | TYP NO | LANES | LANE TYPE | LENGTH | WIDTH | BEGIN MP | END MP | 4709000000-E | | 4720000000-E | | 4725000000-E | | 4815000000-E | | 4825000000-E | | 4835000000-E | | 4845000000-N | | | 4847040000-E | | | 4847080000-E | | 4855000000-E | | 4870000000-E | | 4875000000-N | | 4905100000-N | | | |
|-------------------------------------|---------|--------|-------------------------------------|--|--------|-------|-----------|--------------|-------|----------|--------|-------------------------|----------------------|-------------------------|----------------------|----------------------|-----------------------|----------------------------|----------------|-----------------|-----------------|-------------------|----------------|----------------|--|---|---|-----------------|------------------|--|--------------------------------|--------------|--|--------------|------------|--------------|--|--------------|--|--|--|
| | | | | | | | | | | | | 24" X 90 M WHITE THERMO | THERMO MSG ONLY 90 M | THERMO MERGE ARROW 90 M | THERMO LT ARROW 90 M | THERMO RT ARROW 90 M | THERMO STR ARROW 90 M | THERMO STR & LT ARROW 90 M | 6" WHITE PAINT | 12" WHITE PAINT | 24" WHITE PAINT | PAINT MERGE ARROW | PAINT LT ARROW | PAINT RT ARROW | POLYUREA PAVEMENT MARKING LINES (6", 30 MILS) (HIGHLY REFLECTIVE ELEMENTS) WHITE | POLYUREA PAVEMENT MARKING LINES (6", 30 MILS) (HIGHLY REFLECTIVE ELEMENTS) YELLOW | POLYUREA PAVEMENT MARKING LINES (12", 30 MILS) (HIGHLY REFLECTIVE ELEMENTS) WHITE | 6" LINE REMOVAL | 24" LINE REMOVAL | REMOVAL OF PAVEMENT MARKING SYMBOLS & CHARACTERS | NON-CAST SNOW PLOWABLE MARKERS | | | | | | | | | | |
| MI | FT | LF | EA | EA | EA | EA | EA | EA | EA | EA | EA | EA | LF | LF | LF | EA | EA | EA | LF | LF | LF | LF | LF | EA | EA | | | | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 1 | I-77 N | FROM MM 36.5 TO MM 48.85 | 1,2,3 | 2,3 | MD | 12.35 | 38 | 36.5 | 48.85 | | | 11 | | | | 150,000 | 2,600 | | 11 | | | 82,000 | 68,000 | 2,700 | | | | | | | | | | 985 | | | | | |
| TOTAL FOR MAP NO. 1 | | | | | | | | 12.35 | | | | | | | 11 | | | | 150,000 | 2,600 | | 11 | | | 82,000 | 68,000 | 2,700 | | | | | | | | 985 | | | | | | |
| 45924.3.1 | Iredell | 2 | I-77 S | FROM MM 36.5 TO MM 48.85 | 1,2,3 | 2,3 | MD | 12.35 | 38 | 36.5 | 48.85 | | | 9 | | | | 150,000 | 2,900 | | 9 | | | 82,000 | 68,000 | 2,900 | | | | | | | | | | 985 | | | | | |
| TOTAL FOR MAP NO. 2 | | | | | | | | 12.35 | | | | | | | 9 | | | | 150,000 | 2,900 | | 9 | | | 82,000 | 68,000 | 2,900 | | | | | | | | | 985 | | | | | |
| 45924.3.1 | Iredell | 3 | EXIT 42 - I-77 NB OFF RAMP | FROM I-77 NB TO US 21 | 4 | 1 | | 0.25 | 26 | 0 | 0.25 | 26 | | | 6 | 3 | | | | | | | 1 | 1 | 1,400 | 1,200 | | | | | | | | | | | | | | | |
| TOTAL FOR MAP NO. 3 | | | | | | | | 0.25 | | | | 26 | | | 6 | 3 | | | | | | | | | 1 | 1 | 1,400 | 1,200 | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 4 | EXIT 42 - I-77 NB ON LOOP | FROM US 21 TO I-77 NB | 5 | 1 | | 0.15 | 20 | 0 | 0.15 | | | | | | | | | | | | | | 750 | 750 | | | | | | | | | | | | | | | |
| TOTAL FOR MAP NO. 4 | | | | | | | | 0.15 | | | | | | | | | | | | | | | | | | | 750 | 750 | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 5 | EXIT 42 - I-77 SB ON RAMP | FROM US 21 TO I-77 SB | 12 | 1 | | 0.38 | 24 | 0 | 0.38 | | | | | | | | | | | | | | 1,975 | 750 | | | | | | | | | | | | | | | |
| TOTAL FOR MAP NO. 5 | | | | | | | | 0.38 | | | | | | | | | | | | | | | | | | | 1,975 | 750 | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 6 | EXIT 42 - I-77 SB OFF LOOP | FROM I-77 SB TO US 21 | 5 | 1 | | 0.22 | 20 | 0 | 0.22 | 25 | | | | 2 | 2 | | | | | | | | 1,000 | 1,000 | | | | | | | | | | | | | | | |
| TOTAL FOR MAP NO. 6 | | | | | | | | 0.22 | | | | 25 | | | | | 2 | 2 | | | | | | | | | 1,000 | 1,000 | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 7 | EXIT 45 - I-77 NB OFF RAMP | FROM I-77 NB TO AMITY HILL RD (SR 2342) | 6 | 1 | | 0.16 | 24 | 0 | 0.16 | 38 | 4 | | | 2 | | | | | | | | 625 | 625 | | 1,250 | 38 | | 6 | | | | | | | | | | | |
| TOTAL FOR MAP NO. 7 | | | | | | | | 0.16 | | | | 38 | 4 | | | | 2 | | | | | | | | | | 625 | 625 | | 1,250 | 38 | 6 | | | | | | | | | |
| 45924.3.1 | Iredell | 8 | EXIT 45 - I-77 SB ON RAMP | FROM AMITY HILL RD (SR 2342) TO I-77 SB | 7 | 1 | | 0.25 | 24 | 0 | 0.25 | | | 2 | | | | | | | | | | 1,075 | 1,075 | | | | | | | | | | | | | | | | |
| TOTAL FOR MAP NO. 8 | | | | | | | | 0.25 | | | | | | | 2 | | | | | | | | | | | | 1,075 | 1,075 | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 9 | EXIT 45 I-77 NB ON RAMP / DOVER RD. | FROM AMITY HILL RD (SR 2342) TO I-77 NB | 8 | 1 | | 0.23 | 30 | 0 | 0.23 | 42 | | | 3 | | | | | | | | | 1,675 | 1,675 | | | | | | | | | | | | | | | | |
| TOTAL FOR MAP NO. 9 | | | | | | | | 0.23 | | | | 42 | | | 3 | | | | | | | | | | | | 1,675 | 1,675 | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 10 | EXIT 45 - I-77 SB OFF RAMP | FROM I-77 SB TO AMITY HILL RD (SR 2342) | 8 | 1 | | 0.16 | 30 | 0 | 0.16 | 42 | | | | | | | | | | | | 600 | 600 | | 750 | 40 | | 1 | | | | | | | | | | | |
| TOTAL FOR MAP NO. 10 | | | | | | | | 0.16 | | | | 42 | | | | | | | | | | | | | | | 600 | 600 | | 750 | 40 | 1 | | | | | | | | | |
| 45924.3.1 | Iredell | 11 | EXIT 49A - I-77 NB OFF RAMP | FROM I-77 NB TO US 70 | 8 | 1 | | 0.22 | 24 | 0 | 0.22 | 36 | | | | 2 | | | | | | | | 1,050 | 1,050 | | | | | | | | | | | | | | | | |
| TOTAL FOR MAP NO. 11 | | | | | | | | 0.22 | | | | 36 | | | | | 2 | | | | | | | | | | 1,050 | 1,050 | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 12 | EXIT 49A - I-77 SB ON RAMP | FROM US 70 TO I-77 SB | 8 | 1 | | 0.3 | 24 | 0 | 0.3 | | | 3 | | | | | | | | | | 1,475 | 1,475 | | | | | | | | | | | | | | | | |
| TOTAL FOR MAP NO. 12 | | | | | | | | 0.3 | | | | | | | 3 | | | | | | | | | | | | 1,475 | 1,475 | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 13 | EXIT 49A - I-77 NB ON LOOP | FROM US 70 TO I-77 NB | 9 | 1 | | 0.14 | 24 | 0 | 0.14 | | | | | | | | | | | | | 700 | 700 | | | | | | | | | | | | | | | | |
| TOTAL FOR MAP NO. 13 | | | | | | | | 0.14 | | | | | | | | | | | | | | | | | | | 700 | 700 | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 14 | EXIT 49A - I-77 SB OFF LOOP | FROM I-77 SB TO US 70 | 10 | 1 | | 0.21 | 26 | 0 | 0.21 | 30 | | | | 2 | | | | | | | | 1,075 | 1,075 | | 2,150 | | | | | | | | | | | | | | |
| TOTAL FOR MAP NO. 14 | | | | | | | | 0.21 | | | | 30 | | | | | 2 | | | | | | | | | | 1,075 | 1,075 | | 2,150 | | | | | | | | | | | |
| 45924.3.1 | Iredell | 15 | EXIT 49B - I-77 NB OFF LOOP | FROM I-77 NB TO SALISBURY RD (SR 2735) | 9,11 | 1 | | 0.17 | 22 | 0 | 0.17 | 36 | | | | 1 | | 1 | | | | | | 925 | 925 | | | | | | | | | | | | | | | | |
| TOTAL FOR MAP NO. 15 | | | | | | | | 0.17 | | | | 36 | | | | | 1 | | 1 | | | | | | | | 925 | 925 | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 16 | EXIT 49B - I-77 NB ON RAMP | FROM SALISBURY RD (SR 2735) TO I-77 NB | 8,11 | 1 | | 0.26 | 24 | 0 | 0.26 | | | 3 | | | | | | | | | | 1,500 | 1,500 | | | | | | | | | | | | | | | | |
| TOTAL FOR MAP NO. 16 | | | | | | | | 0.26 | | | | | | | 3 | | | | | | | | | | | | 1,500 | 1,500 | | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 17 | EXIT 49B - I-77 SB ON LOOP | FROM SALISBURY RD (SR 2735) TO I-77 SB | 9 | 1 | | 0.13 | 22 | 0 | 0.13 | | | | | | | | | | | | | 650 | 650 | | | | | | | | | | | | | | | | |
| TOTAL FOR MAP NO. 17 | | | | | | | | 0.13 | | | | | | | | | | | | | | | | | | | | 650 | 650 | | | | | | | | | | | | |
| 45924.3.1 | Iredell | 18 | EXIT 49B - I-77 SB OFF RAMP | FROM I-3819 END CONSTRUCTION TO SALISBURY RD (SR 2735) | 9 | 1 | | 0.13 | 22 | 0 | 0.13 | 32 | | | 1 | 2 | | | | | | | | 900 | 900 | | | | | | | | | | | | | | | | |
| TOTAL FOR MAP NO. 18 | | | | | | | | 0.13 | | | | 32 | | | 1 | 2 | | | | | | | | | | | | 900 | 900 | | | | | | | | | | | | |
| TOTAL FOR PROJ NO. 45924.3.1 | | | | | | | | 28.06 | | | | 307 | 4 | 31 | 7 | 14 | 2 | 1 | 300,000 | 5,500 | 26 | 20 | 1 | 1 | 181,375 | 151,950 | 5,600 | 4,150 | 78 | 7 | 1,970 | | | | | | | | | | |
| GRAND TOTAL | | | | | | | | 28.06 | | | | 307 | 4 | 31 | 7 | 14 | 2 | 1 | 300,000 | 5,500 | 26 | 20 | 1 | 1 | 181,375 | 151,950 | 5,600 | 4,150 | 78 | 7 | 1,970 | | | | | | | | | | |

| | |
|-------------|-----------|
| PROJECT NO. | SHEET NO. |
| I-5918 | 3B-6 |

I-5918 - TEMPORARY TRAFFIC CONTROL QUANTITIES

| PROJECT NO | COUNTY | MAP NO | ROUTE | DESCRIPTION | 4600000000-N | | | | | 4400000000-E | 4423000000-N | 4424000000-N | 4434000000-N | 4510000000-N |
|------------------------------------|---------|--------|-------------------------------------|--|---------------------|---------------------|---------------------|---------------------------|-------------------------------|----------------------------|-------------------------------------|-----------------------------|------------------------------------|-----------------|
| | | | | | SINGLE LANE CLOSURE | DOUBLE LANE CLOSURE | RAMP/ LOOP CLOSURES | RAMP/LOOP TRAFFIC CONTROL | CONNECTED LANE CLOSURE SYSTEM | STATIONARY WORK ZONE SIGNS | WORK ZONE DIGITAL SPEED LIMIT SIGNS | WORK ZONE PRESENCE LIGHTING | SEQUENTIAL FLASHING WARNING LIGHTS | LAW ENFORCEMENT |
| | | | | | EA | EA | EA | EA | EA | SF | EA | EA | EA | HR |
| 45924.3.1 | IREDELL | 1 | I-77 N | FROM MM 36.5 TO MM 48.85 | 95 | 5 | | | 1 | 232 | 5 | 14 | 12 | 2358 |
| 45924.3.1 | IREDELL | 2 | I-77 S | FROM MM 36.5 TO MM 48.85 | 100 | | | | 1 | 232 | 5 | 14 | 12 | 2358 |
| 45924.3.1 | IREDELL | 3 | EXIT 42 - I-77 NB OFF RAMP | FROM I-77 NB TO US 21 | | | 1 | | | | | | | |
| 45924.3.1 | IREDELL | 4 | EXIT 42 - I-77 NB ON LOOP | FROM US 21 TO I-77 NB | | | 1 | | | | | | | |
| 45924.3.1 | IREDELL | 5 | EXIT 42 - I-77 SB ON RAMP | FROM US 21 TO I-77 SB | | | 5 | | | | | | | |
| 45924.3.1 | IREDELL | 6 | EXIT 42 - I-77 SB OFF LOOP | FROM I-77 SB TO US 21 | | | 1 | | | | | | | |
| 45924.3.1 | IREDELL | 7 | EXIT 45 - I-77 NB OFF RAMP | FROM I-77 NB TO AMITY HILL RD (SR 2342) | | | 1 | | | | | | | |
| 45924.3.1 | IREDELL | 8 | EXIT 45 - I-77 SB ON RAMP | FROM AMITY HILL RD (SR 2342) TO I77 SB | | | 1 | | | | | | | |
| 45924.3.1 | IREDELL | 9 | EXIT 45 I-77 NB ON RAMP / DOVER RD. | FROM AMITY HILL RD (SR 2342) TO I77NB | | | 1 | | | | | | | |
| 45924.3.1 | IREDELL | 10 | EXIT 45 - I-77 SB OFF RAMP | FROM I-77 SB TO AMITY HILL RD (SR 2342) | | | 1 | | | | | | | |
| 45924.3.1 | IREDELL | 11 | EXIT 49A - I-77 NB OFF RAMP | FROM I-77 NB TO US 70 | | | 1 | | | | | | | |
| 45924.3.1 | IREDELL | 12 | EXIT 49A - I-77 SB ON RAMP | FROM US 70 TO I-77 SB | | | 1 | | | | | | | |
| 45924.3.1 | IREDELL | 13 | EXIT 49A - I-77 NB ON LOOP | FROM US 70 TO I-77 NB | | | 1 | | | | | | | |
| 45924.3.1 | IREDELL | 14 | EXIT 49A - I-77 SB OFF LOOP | FROM I-77 SB TO US 70 | | | 1 | | | | | | | |
| 45924.3.1 | IREDELL | 15 | EXIT 49B - I-77 NB OFF LOOP | FROM I-77 NB TO SALISBURY RD (SR 2735) | | | 1 | 2 | | | | | | |
| 45924.3.1 | IREDELL | 16 | EXIT 49B - I-77 NB ON RAMP | FROM SALISBURY RD (SR 2735) TO I77 NB | | | 1 | | | | | | | |
| 45924.3.1 | IREDELL | 17 | EXIT 49B - I-77 SB ON LOOP | FROM SALISBURY RD (SR2375) TO I77 SB | | | 1 | | | | | | | |
| 45924.3.1 | IREDELL | 18 | EXIT 49B - I-77 SB OFF RAMP | FROM I-3819 END CONSTRUCTION TO SALISBURY RD (SR 2375) | | | 1 | | | | | | | |
| TOTAL FOR PROJECT 45924.3.1 | | | | | 195 | 5 | 20 | 2 | 2 | 464 | 10 | 28 | 24 | 4716 |