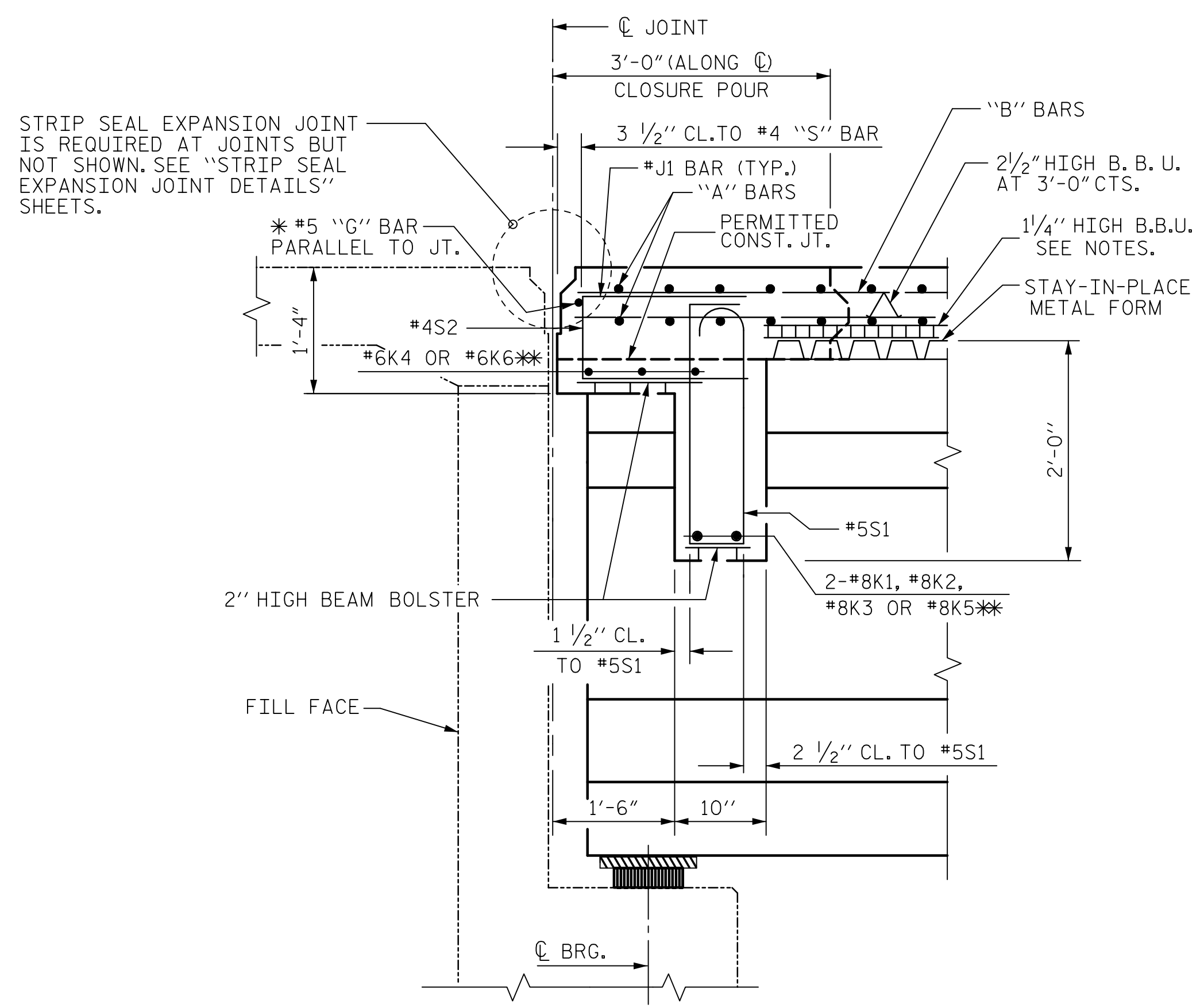


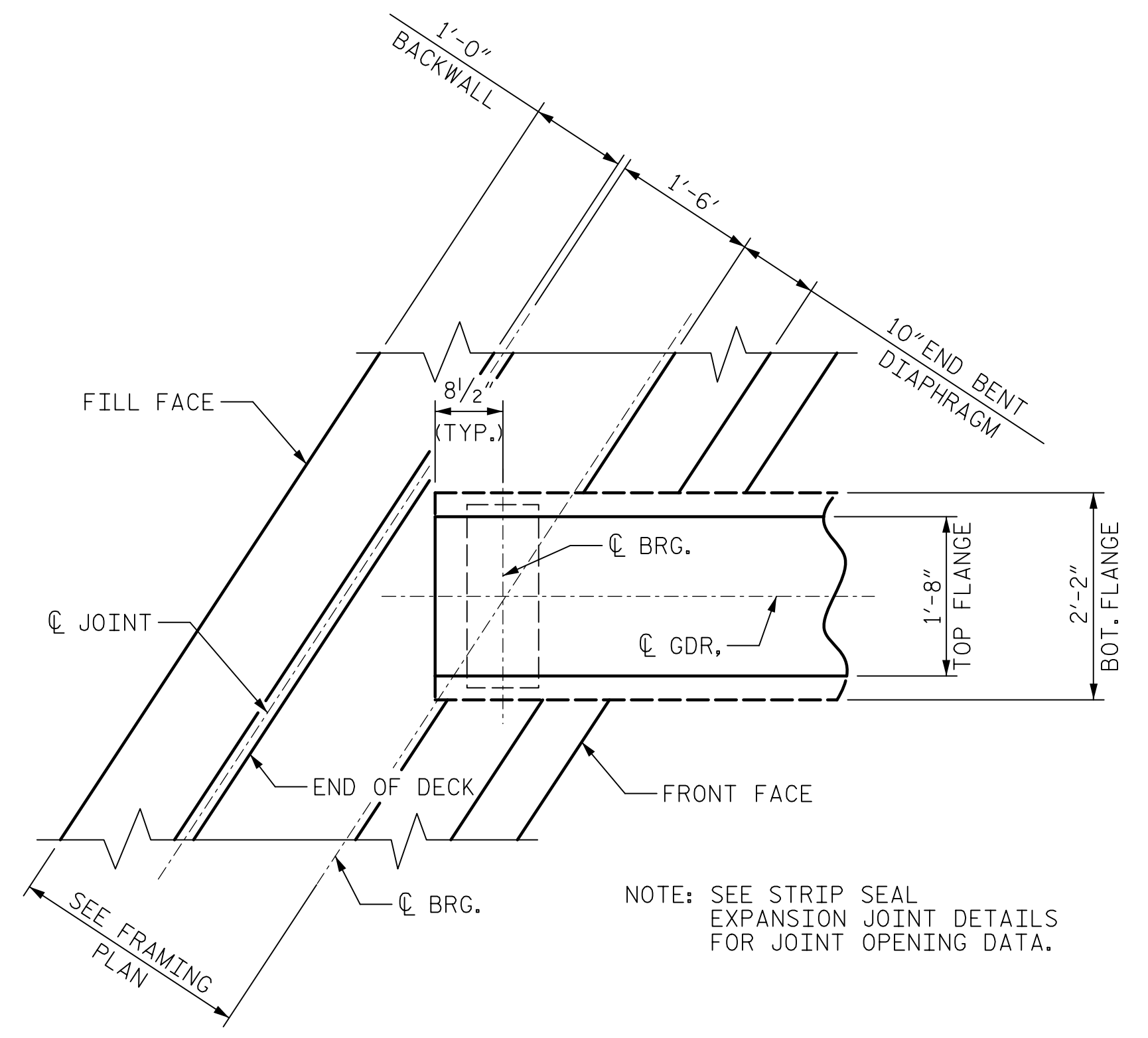
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TIME: 5:08:55 AM

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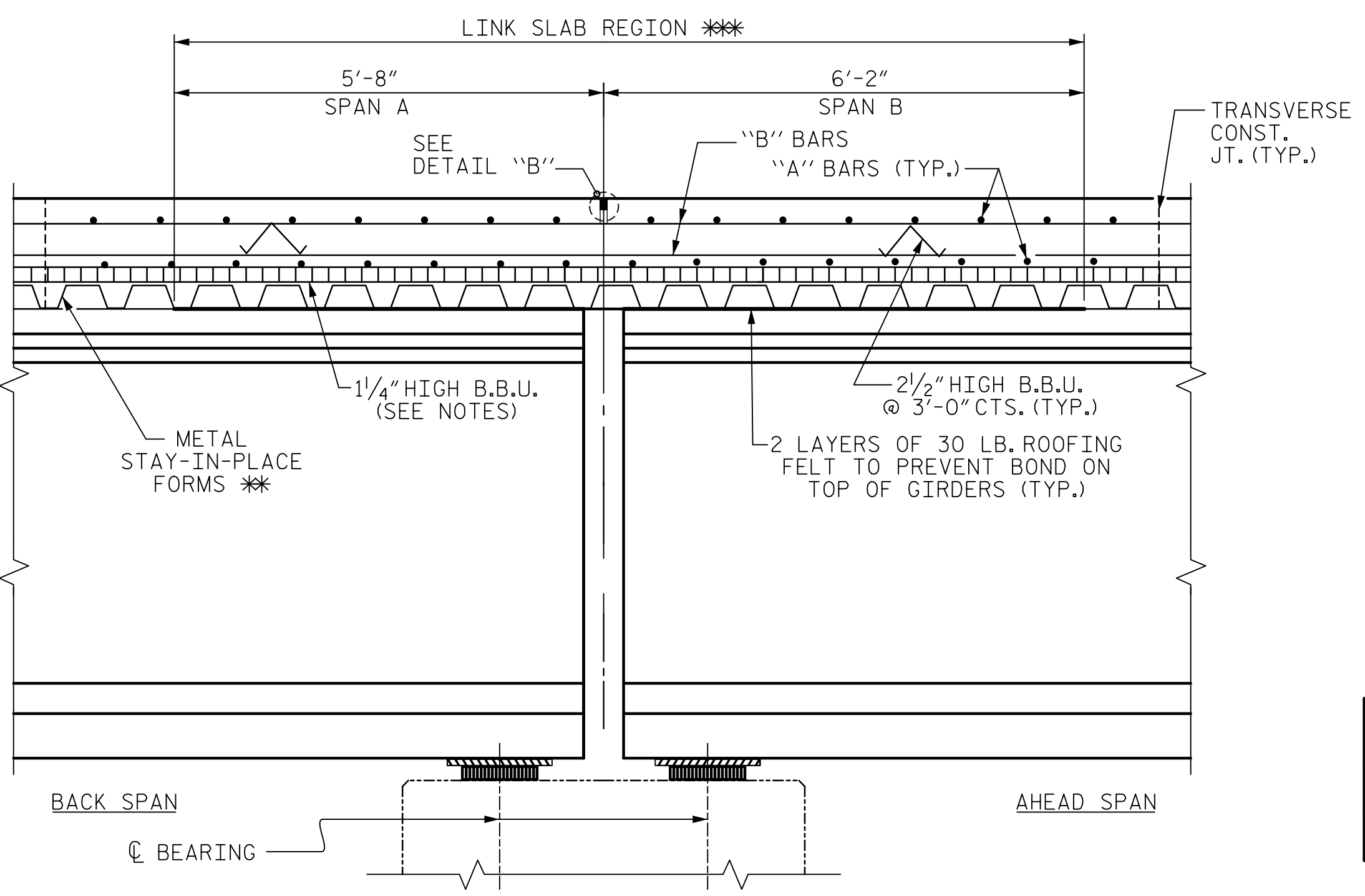


SECTION THRU END BENT DIAPHRAGMS

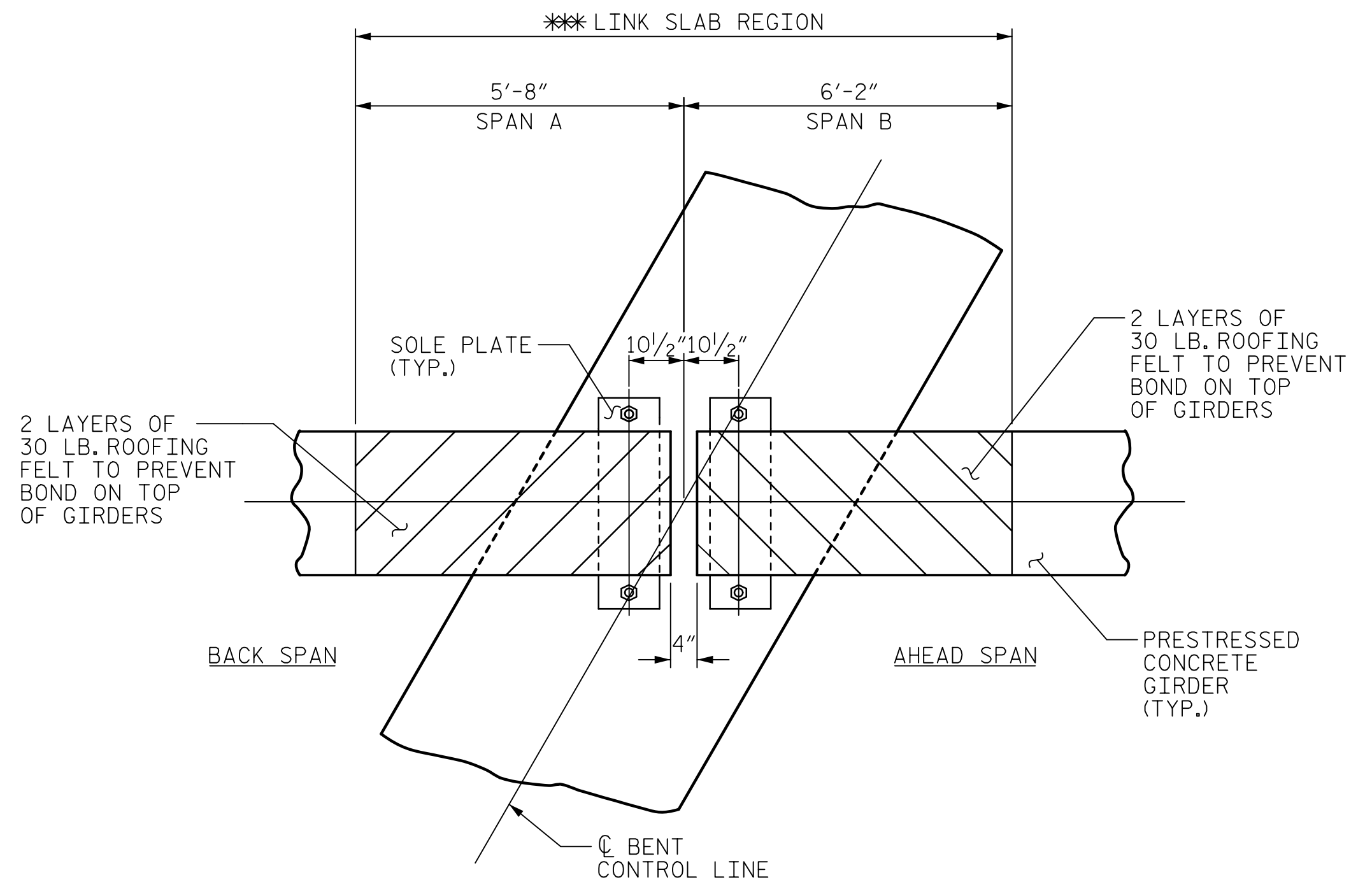
* #5 "G" BAR MAY BE SHIFTED SLIGHTLY AS NECESSARY TO CLEAR REINFORCING STEEL AND STIRRUPS.
** #8K5 AND #6K6 IN CLOSURE POUR



PLAN VIEW OF END BENT DIAPHRAGM



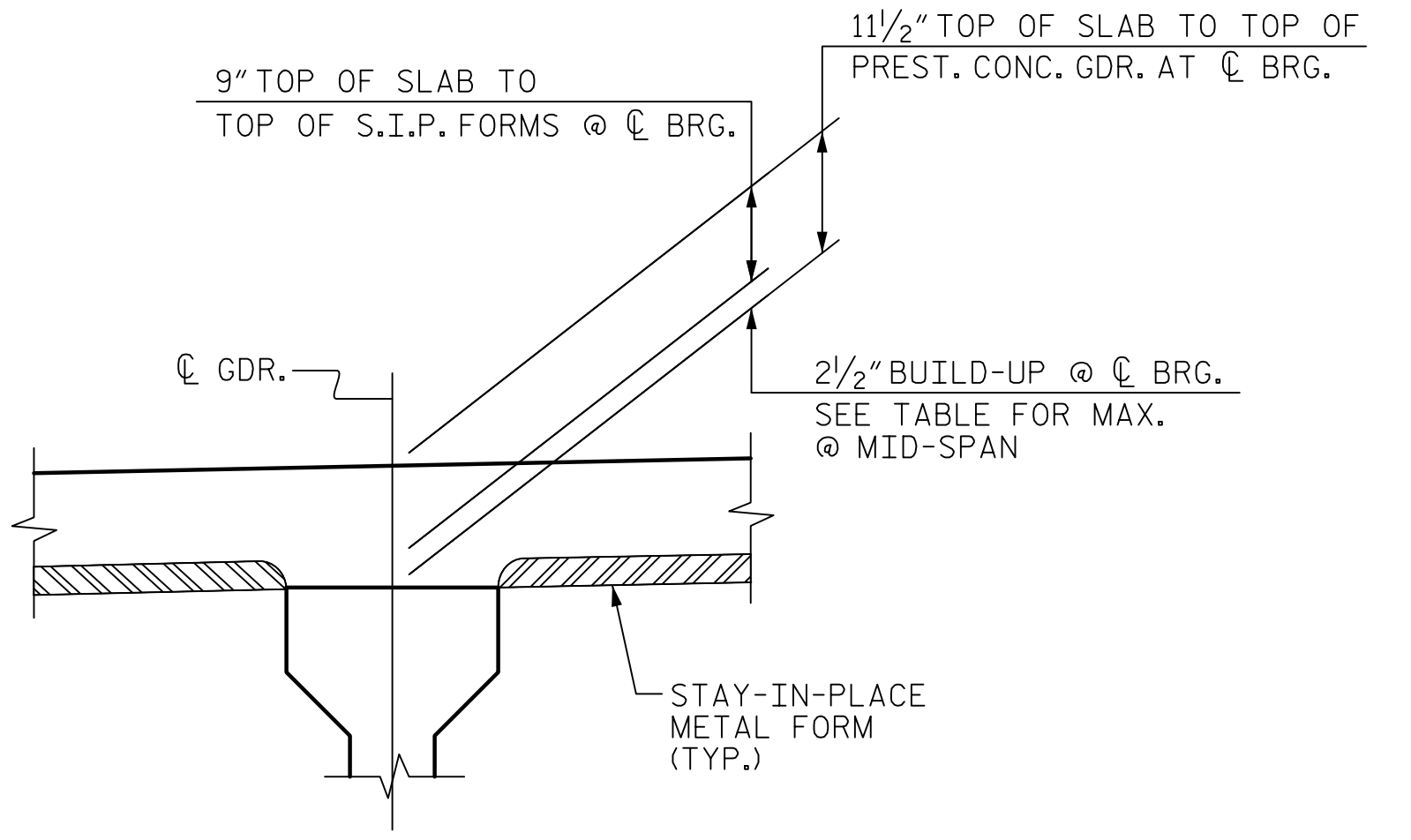
SECTION B-B @ LINK SLAB
SECTION SHOWN ALONG GIRDER



***: THE TOP OF GIRDER IN THE REGION OF THE LINK SLAB SHALL BE SMOOTH (NOT RAKED) AND FREE OF STIRRUPS, ANCHOR STUDS, DECK FORMWORK ATTACHMENTS, AND OVERHANG FALSEWORK/FORMWORK ATTACHMENTS.

METAL STAY-IN-PLACE FORMS SHALL NOT BE WELDED TO THE GIRDER FLANGES IN THE REGION OF THE LINK SLAB.

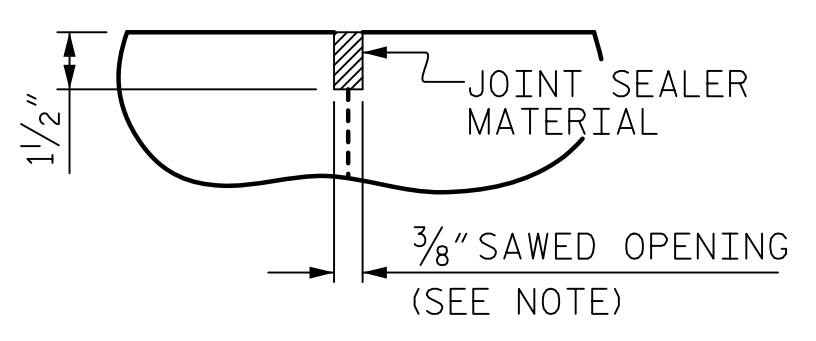
PLAN @ BENT



SPAN	MAX. MID-SPAN BUILD-UP (INCHES) *	CONTROLLING GIRDER
A	1 ¹⁵ / ₁₆ "	2, 3 & 4
B	2 ³ / ₈ "	2, 3 & 4

* BASED ON PREDICTED FINAL CAMBER AND THEORETICAL GRADE LINE ELEVATIONS

DETAIL "A"



A 1 1/2" DEEP, 3/8" WIDE CONTRACTION JOINT AT BENT CONTROL LINE SHALL BE SAWN WITHIN 24 HOURS OF POURING THE DECK. THE JOINT SHALL BE FILLED WITH JOINT SEALER MATERIAL. THE JOINT SEALER MATERIAL SHALL CONFORM TO THE REQUIREMENTS OF SECTION 1028-3 OF THE STANDARD SPECIFICATIONS.

DETAIL "B"

PROJECT NO. B-4654
WAKE COUNTY
STATION: 22+71.80 -L-

SHEET 2 OF 2



STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH
SUPERSTRUCTURE
TYPICAL SECTION DETAILS

DRAWN BY : K. MUENCH DATE : 05/2022
CHECKED BY : J.C. MORRISON DATE : 05/2022
DESIGNED BY : D. RITACCO DATE : 05/2022
DESIGN CHECKED BY : J.C. MORRISON DATE : 05/2022

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S-08
1			3			TOTAL SHEETS
2			4			49