

* #4 "B" BARS MAY BE ADJUSTED
LATERALLY TO TIE WITH
BRIDGE RAIL REINFORCING.

NOTES:

PROVIDE 1 1/4" HIGH BEAM BOLSTERS UPPER AT 4'-0" CTS. ATOP THE METAL STAY-IN-PLACE FORMS TO SUPPORT THE BOTTOM MAT OF "A" BARS. WHEN USING REMOVABLE FORMS, PROVIDE CONTINUOUS HIGH CHAIRS FOR METAL DECK (C.H.C.M.) @ 4'-0" CTS. WITH A HEIGHT TO SUPPORT THE BOTTOM MAT OF "A" BARS A CLEAR DISTANCE OF 2 1/2" ABOVE THE TOP OF THE REMOVABLE FORM.

LONGITUDINAL STEEL MAY BE SHIFTED SLIGHTLY, AS NECESSARY, TO AVOID INTERFERENCE WITH STIRRUPS IN PRESTRESSED CONCRETE GIRDERS.

#5 "G" BAR MAY BE SHIFTED SLIGHTLY, AS NECESSARY, TO CLEAR REINFORCING STEEL AND STIRRUPS.

THE JOINT IN THE DECK SHALL BE SAWED PRIOR TO CASTING OF SIDEWALK.

PREVIOUSLY CAST CONCRETE IN A CONTINUOUS UNIT SHALL HAVE ATTAINED A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI BEFORE ADDITIONAL CONCRETE IS CAST IN THE UNIT.

CLASSIC CONCRETE BRIDGE RAIL IN A CONTINUOUS UNIT SHALL NOT BE CAST UNTIL ALL SLAB CONCRETE IN THE UNIT HAS BEEN CAST AND HAS REACHED A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI.

FOR CLASSIC CONCRETE BRIDGE RAIL REINFORCING AND DETAILS, SEE "CLASSIC CONCRETE BRIDGE RAIL" SHEETS.

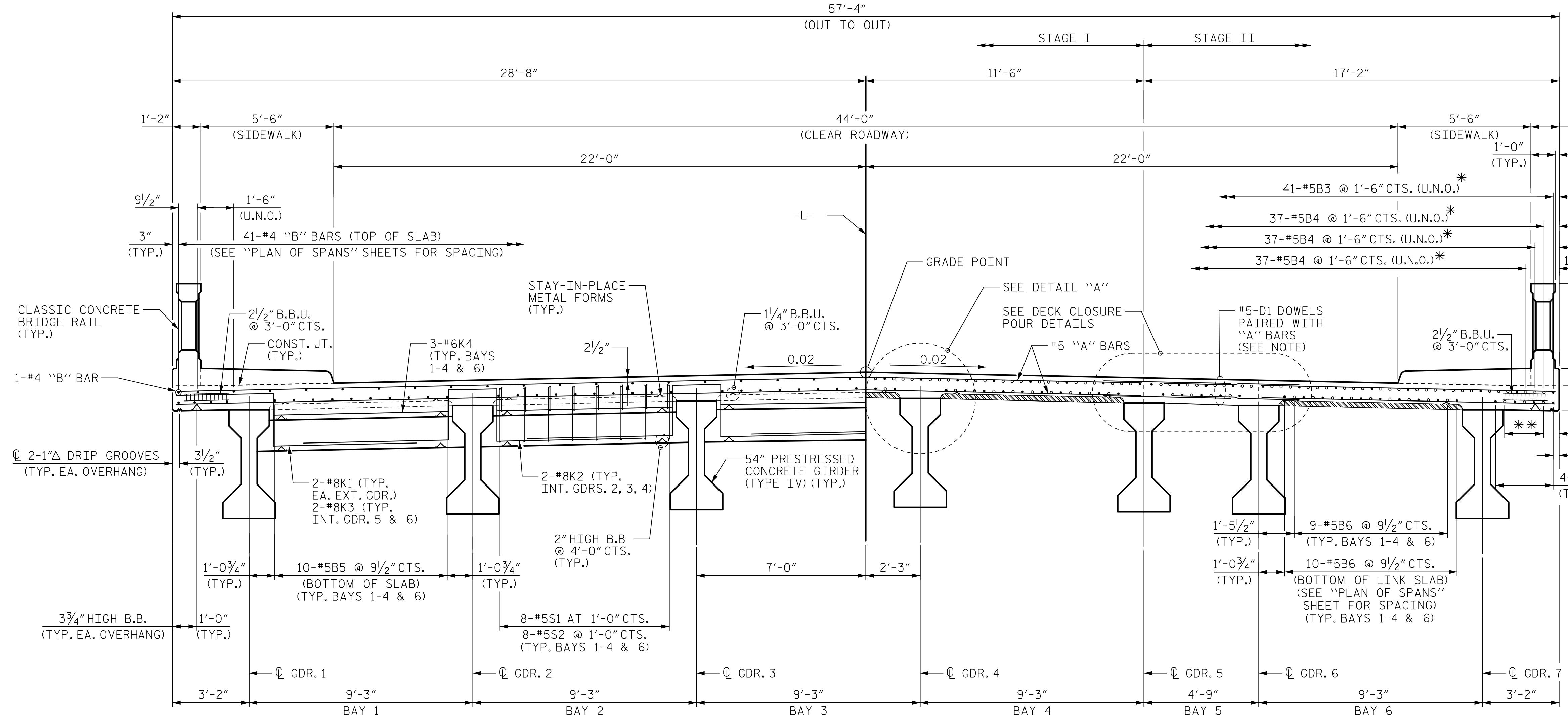
DOWELS SHALL BE PLACED IN THE SAME HORIZONTAL PLANE AS THE TOP AND BOTTOM SLAB REINFORCING STEEL.

SEE CONSTRUCTION SEQUENCE SHEETS FOR LOCATION OF TEMPORARY PORTABLE CONCRETE BARRIER (ANCHORED).

FOR SIDEWALK REINFORCING AND DETAILS, SEE "SIDEWALK DETAILS" SHEET.

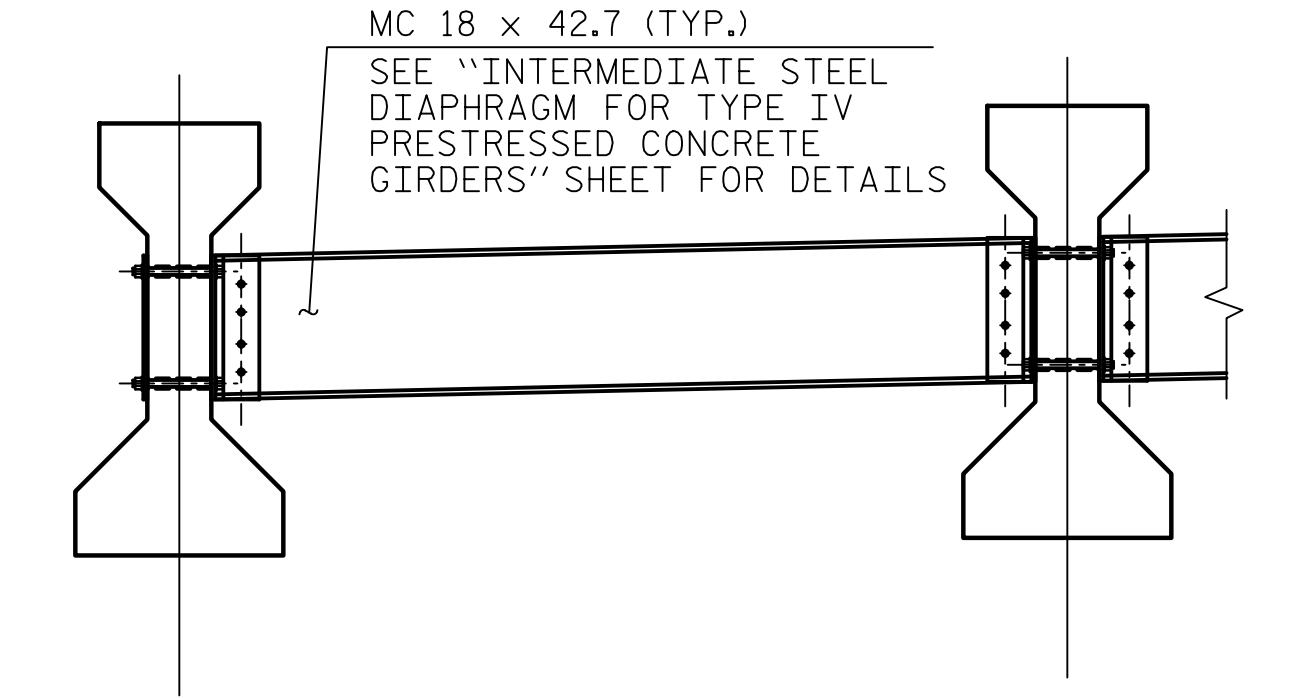
* SEE "PLAN OF SPANS" SHEETS FOR BAR SPACING AND LOCATION

** 3-#5B6 @ 9/2" CTS. SPACED BTWN. #5B5



TYPICAL HALF SECTION
(SHOWING END BENT DIAPHRAGMS)

TYPICAL HALF SECTION
(SHOWING LINK SLABS AT BENTS)



EXTERIOR GIRDER INTERIOR GIRDER

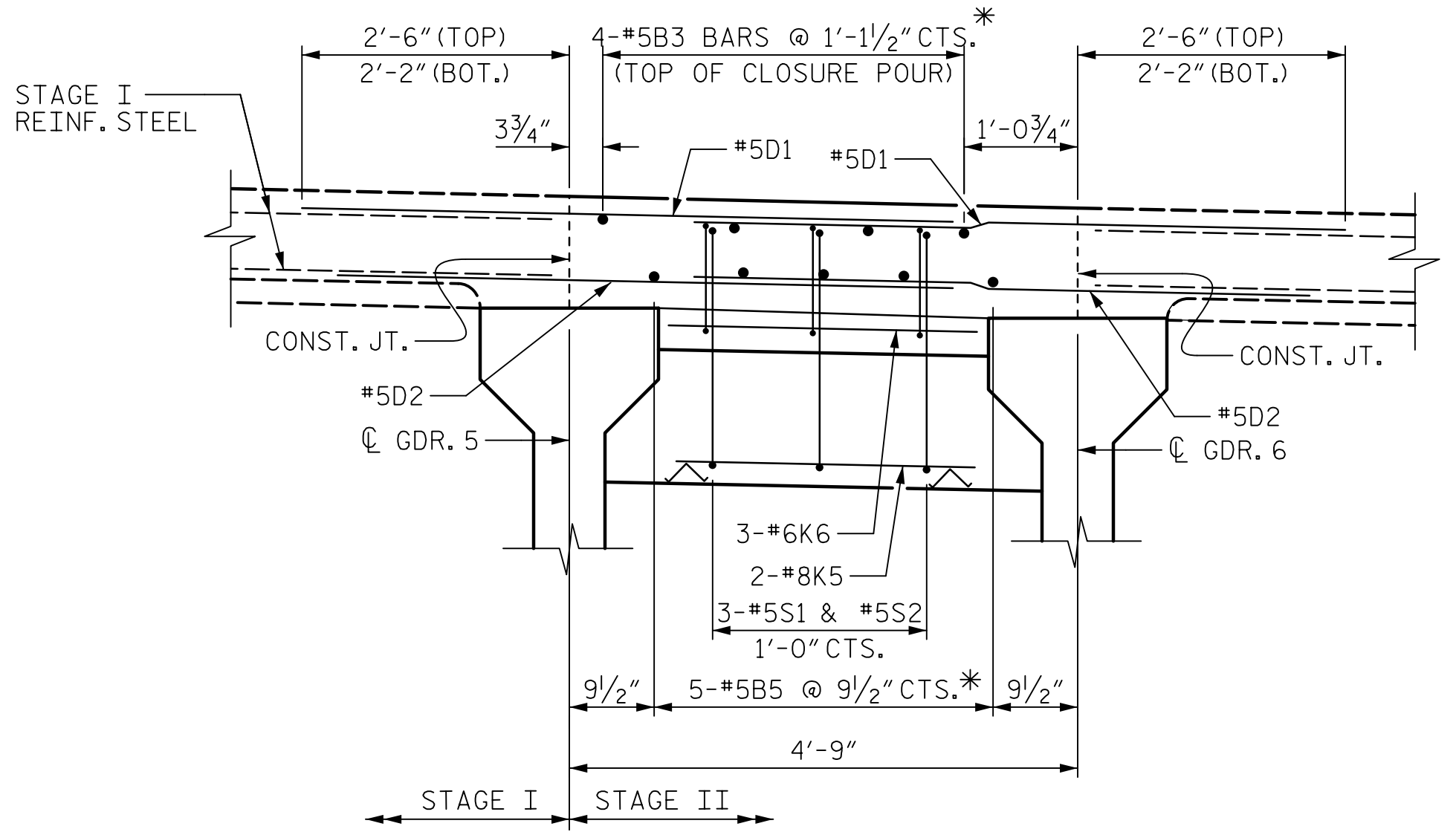
PART TYPICAL SECTION
(SHOWING INTERMEDIATE STEEL DIAPHRAGMS)

PROJECT NO. B-4654

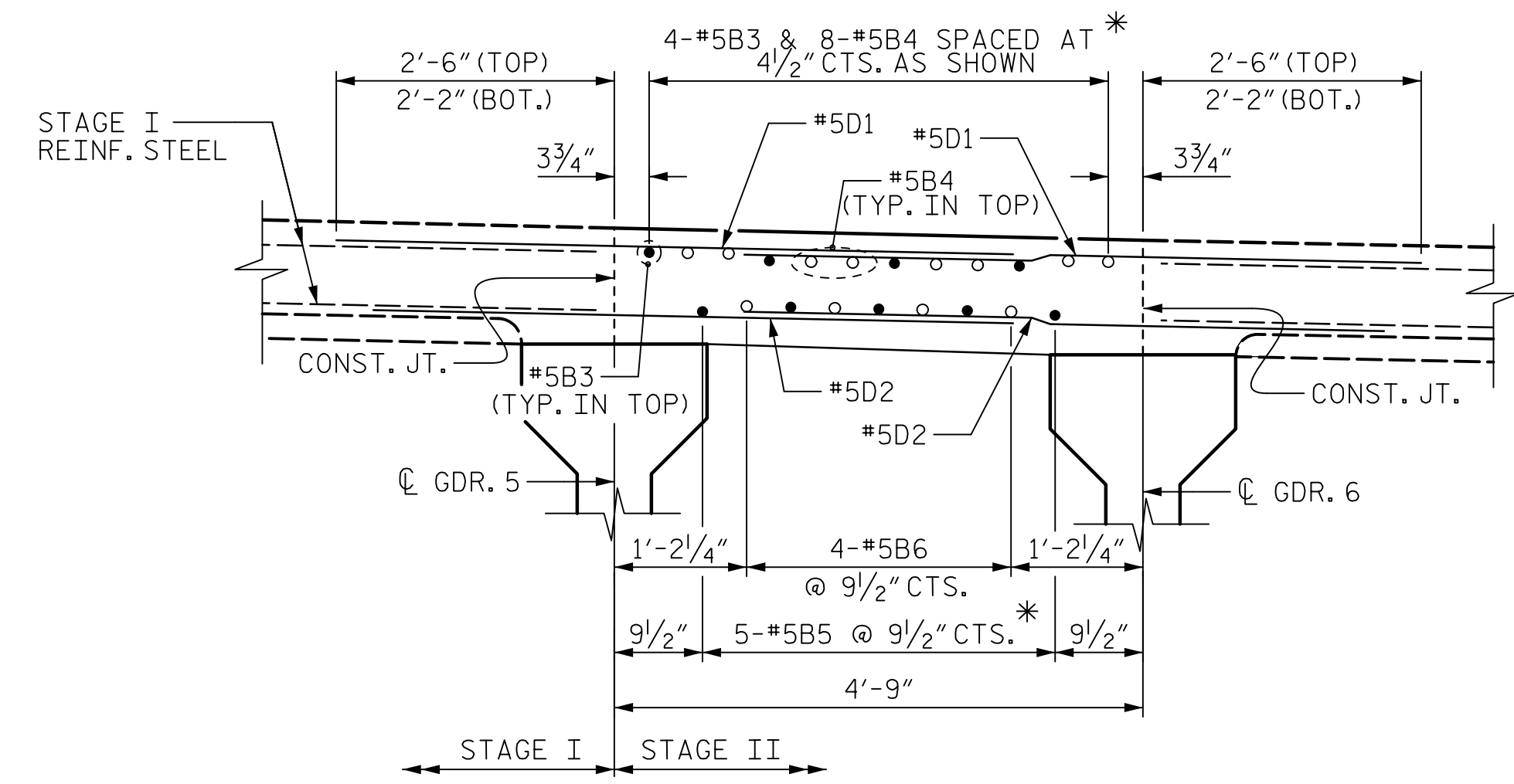
WAKE COUNTY

STATION: 22+71.80 -L-

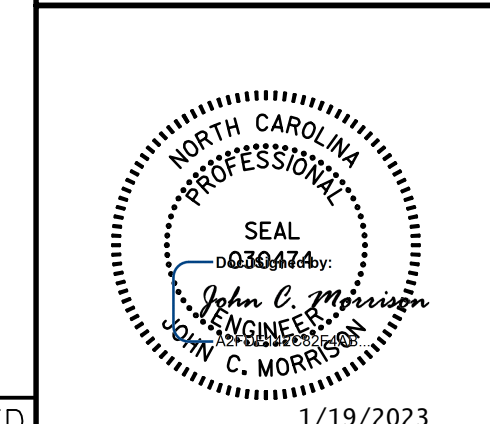
SHEET 1 OF 2



DECK CLOSURE POUR DETAIL @ END BENT



DECK CLOSURE POUR DETAIL @ BENT



STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH
SUPERSTRUCTURE
TYPICAL SECTION

REVISIONS					
NO.	BY:	DATE:	NO.	BY:	DATE:
1			3		
2			4		

SHEET NO. S-07
TOTAL SHEETS 49

DRAWN BY : K. MUENCH DATE : 05/2022
CHECKED BY : J.C. MORRISON DATE : 05/2022
DESIGNED BY : D. RITACCO DATE : 05/2022
DESIGN CHECKED BY : J.C. MORRISON DATE : 05/2022

DOCUMENT NOT CONSIDERED
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DATE: 1/19/2023 TIME: 5:05:45 AM
USER: jmorris@ncdot.gov PROJECT: USRA13\Legacy\Projects\60436195-B-465A\00_Kier\K910\CAD\YO_MCDOT_TIP_Structures\04_Drawing\400_13_S107_B4654_SML1.dgn