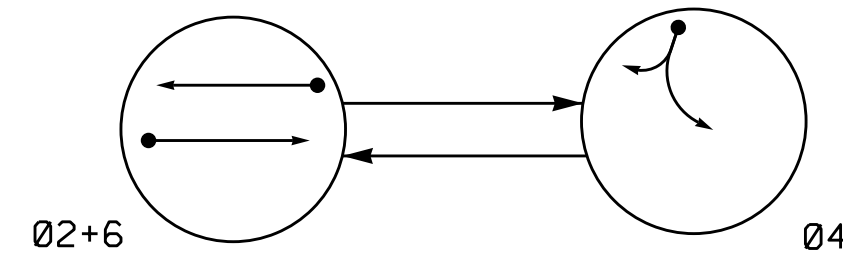


PHASING DIAGRAM



PHASING DIAGRAM DETECTION LEGEND

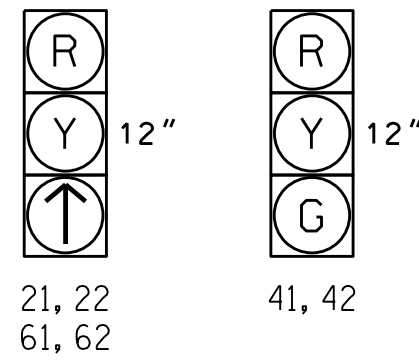
- → DETECTED MOVEMENT
- → UNDETECTED MOVEMENT (OVERLAP)
- UNSIGNALIZED MOVEMENT
- ⚡ PEDESTRIAN MOVEMENT

TABLE OF OPERATION

SIGNAL FACE	PHASE		
	02+6	04	FLASH
21, 22	↑	R	Y
41, 42	R	G	R
61, 62	↑	R	Y

SIGNAL FACE I.D.

All Heads L.E.D.



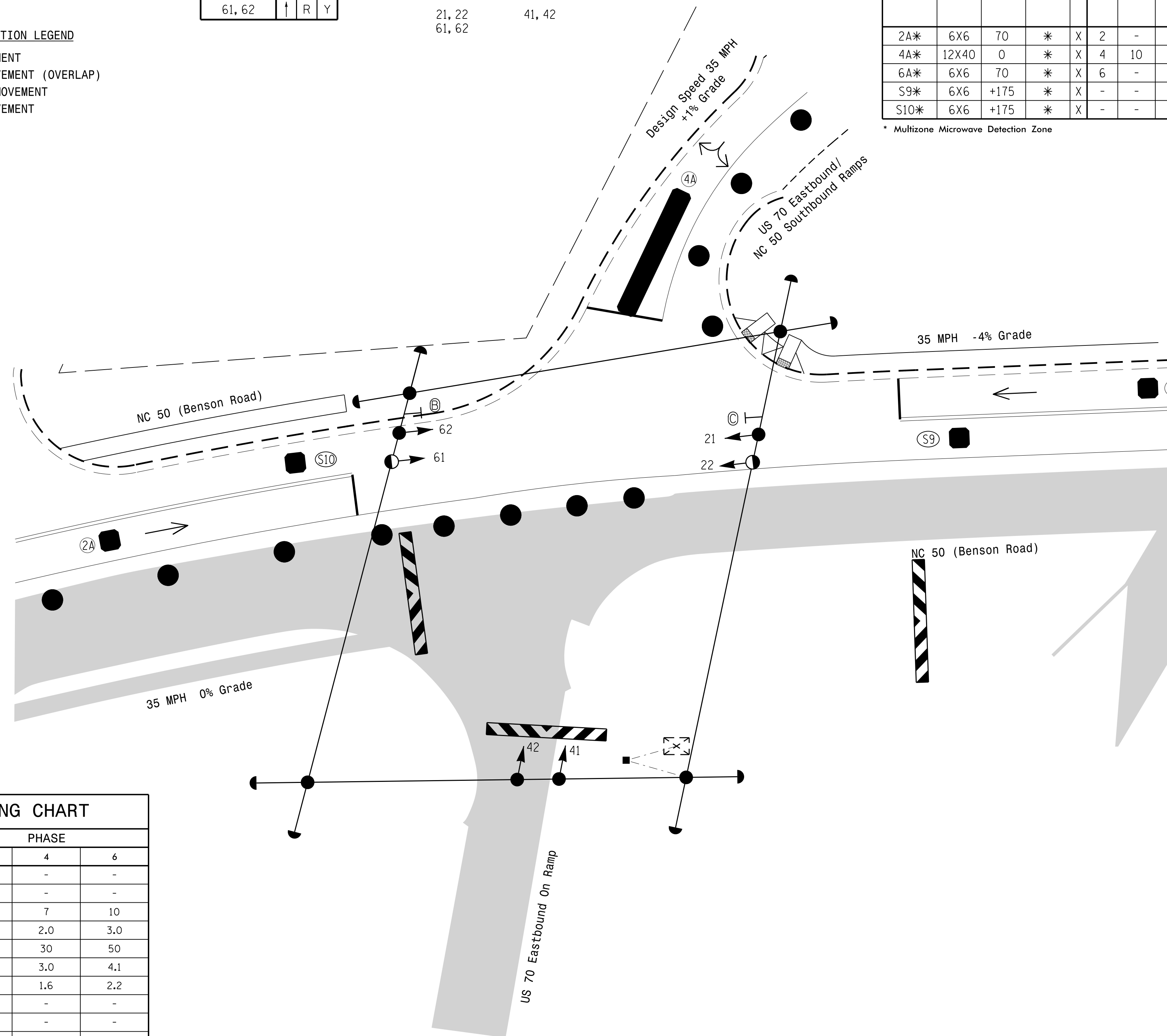
MAXTIME DETECTOR INSTALLATION CHART											
DETECTOR						PROGRAMMING					
ZONE	SIZE (FT)	DISTANCE FROM STOPBAR (FT)	TURNS	NEW LOOP	CALL PHASE	DELAY TIME	EXTEND TIME	EXTEND	ADDED INITIAL	CALL	DELAY DURING GREEN
2A*	6X6	70	*	X	2	-	-	X	-	X	-
4A*	12X40	0	*	X	4	10	-	X	-	X	-
6A*	6X6	70	*	X	6	-	-	X	-	X	-
S9*	6X6	+175	*	X	-	-	-	-	-	-	-
S10*	6X6	+175	*	X	-	-	-	-	-	-	-

* Multizone Microwave Detection Zone

2 Phase Fully Actuated (US 70 (Garner Signal System A)) Signal System #: D05-10_Garner

NOTES

- Refer to "Roadway Standard Drawings NCDOT" dated January 2024 and "Standard Specifications for Roads and Structures" dated January 2024.
- Do not program signal for late night flashing operation unless otherwise directed by the Engineer.
- Reposition existing signs and signal heads numbered 21, 22, 41, 42, 61, and 62.
- Set all detector units to presence mode.
- This intersection uses multi-zone microwave detection. Install detectors according to the manufacturer's instructions to achieve the desired detection.
- Maximum times shown in timing chart are for free-run operation only. Coordinated signal system timing values supersede these values.



MAXTIME TIMING CHART

FEATURE	PHASE		
	2	4	6
Walk *	-	-	-
Ped Clear	-	-	-
Min Green *	10	7	10
Passage *	3.0	2.0	3.0
Max I *	50	30	50
Yellow Change	3.8	3.0	4.1
Red Clear	1.8	1.6	2.2
Added Initial *	-	-	-
Maximum Initial *	-	-	-
Time Before Reduction *	-	-	-
Time To Reduce *	-	-	-
Minimum Gap	-	-	-
Advance Walk	-	-	-
Non Lock Detector	-	X	-
Vehicle Recall	MIN RECALL	-	MIN RECALL
Dual Entry	-	-	-

* These values may be field adjusted. Do not adjust Min Green and Extension times for phases 2 and 6 lower than what is shown. Min Green for all other phases should not be lower than 4 seconds.

LEGEND

- | PROPOSED | EXISTING |
|--|-----------------------------------|
| ○ → Traffic Signal Head | ● → Traffic Signal Head |
| ○ → Modified Signal Head | N/A |
| ○ → Pedestrian Signal Head With Push Button & Sign | N/A |
| ○ → Signal Pole with Guy | ● → Signal Pole with Guy |
| ○ → Signal Pole with Sidewalk Guy | ● → Signal Pole with Sidewalk Guy |
| □ → Inductive Loop Detector | □ → Inductive Loop Detector |
| □ → Controller & Cabinet | □ → Controller & Cabinet |
| □ → Junction Box | □ → Junction Box |
| --- 2-in Underground Conduit | --- 2-in Underground Conduit |
| N/A → Right of Way | --- Right of Way |
| → Directional Arrow | → Directional Arrow |
| ● → Construction Zone Drums | ● → Construction Zone Drums |
| ■ → Construction Zone | ■ → Construction Zone |
| ■ → Microwave Detection Zone | ■ → Microwave Detection Zone |
| Ⓟ → No Right Turn Sign (R3-1) | Ⓟ → No Right Turn Sign (R3-1) |
| Ⓠ → No Left Turn Sign (R3-2) | Ⓠ → No Left Turn Sign (R3-2) |

Signal Upgrade - Temporary Design 2 (TMP Phase III)

	<p>NC 50 (Benson Road) at US 70 Eastbound/ NC 50 Southbound Ramps</p>		<p>SEAL NORTH CAROLINA PROFESSIONAL ENGINEER ROBERT J. LIZEMA 026486</p>				
	<p>Division 5 Wake County Garner</p>	<p>PLAN DATE: May 2023 REVIEWED BY:</p>		<p>PREPARED BY: J.A. Lohr REVIEWED BY:</p>			
<p>750 N. Greenfield Pkwy, Garner, NC 27529</p>	<p>SCALE 0 20 1" = 20'</p>	<p>REVISIONS</p> <table border="1"> <tr> <th>INIT.</th> <th>DATE</th> </tr> <tr> <td> </td> <td> </td> </tr> </table>	INIT.	DATE			<p>DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED</p> <p>05/17/2023</p> <p>SIG. INVENTORY NO. 05-081012</p>
INIT.	DATE						