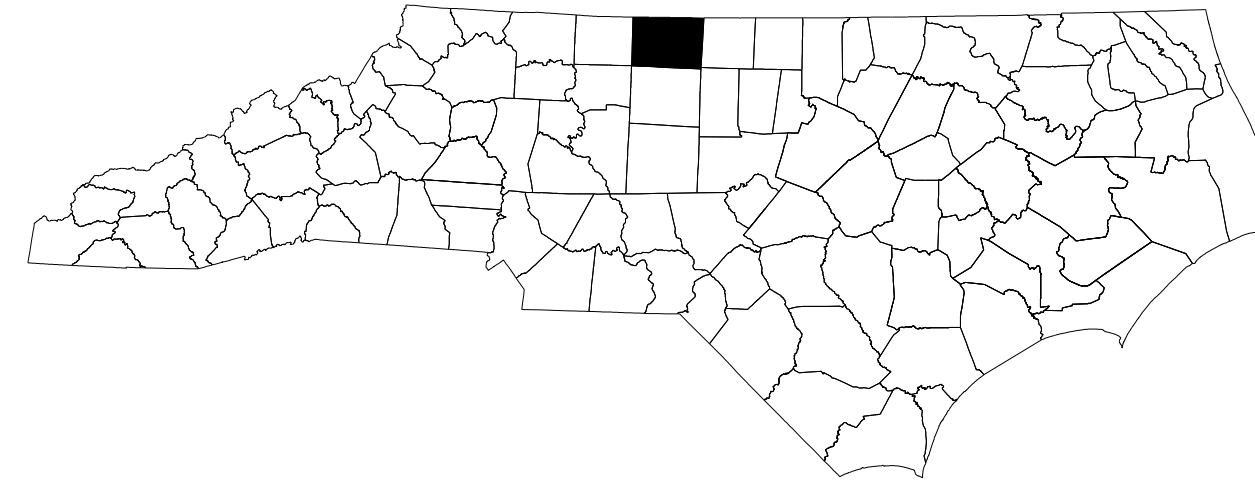


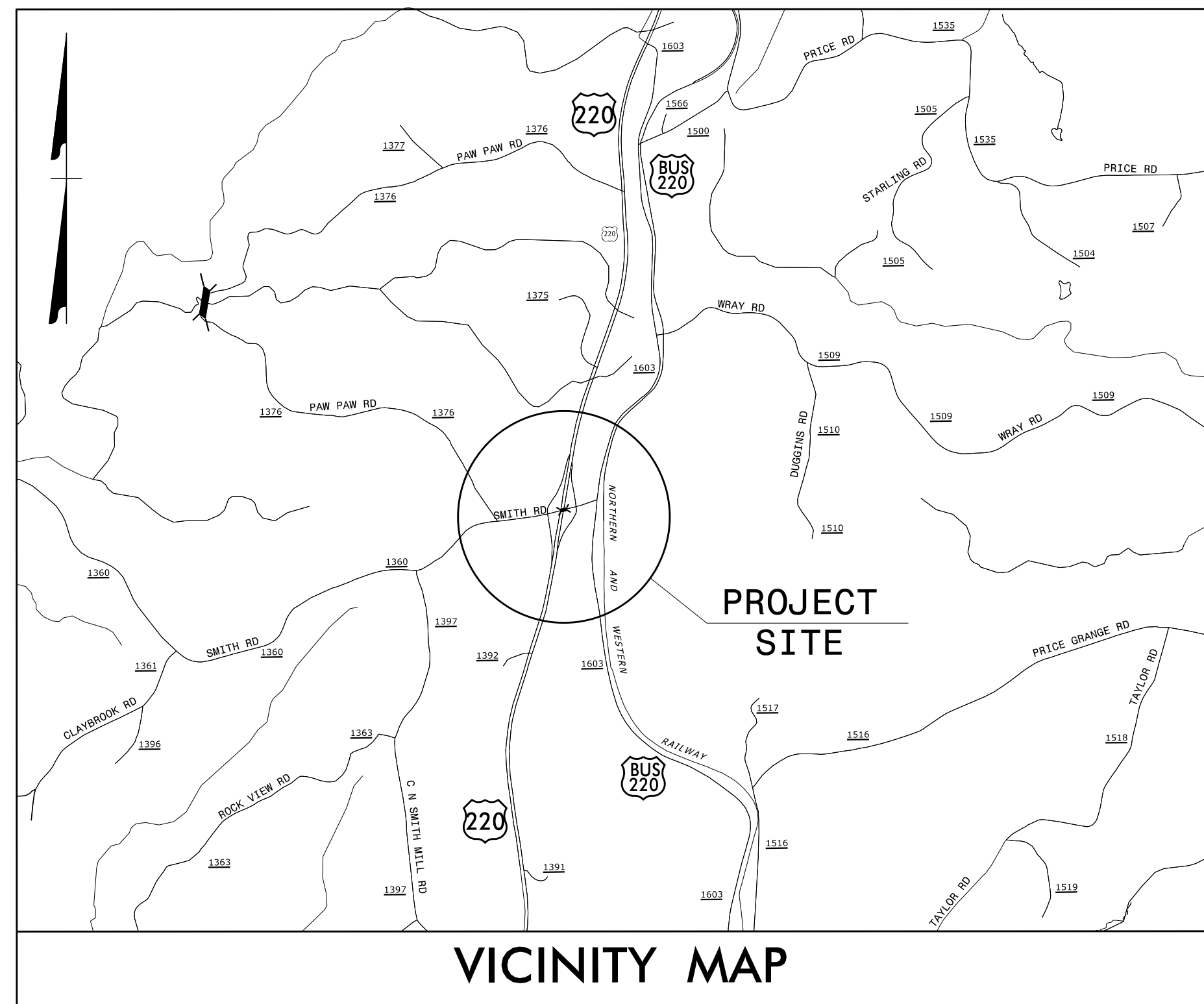
STATE OF NORTH CAROLINA  
DIVISION OF HIGHWAYS

**TRANSPORTATION MANAGEMENT PLAN**

**ROCKINGHAM COUNTY**



**LOCATION: BRIDGE #780170 ON SR 1360 (SMITH RD) OVER US 220**  
**TYPE OF WORK: GRADING, PAVING, DRAINAGE AND STRUCTURE**



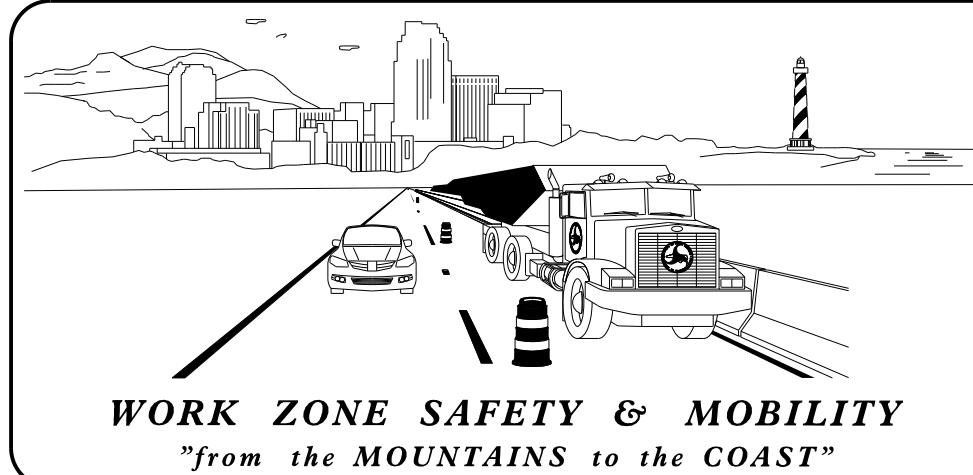
SHEET NO.	TITLE
TMP-1	TITLE SHEET, VICINITY MAP, AND INDEX OF SHEETS
TMP-1A	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, AND LEGEND
TMP-1B	TRANSPORTATION OPERATIONS PLAN: (MANAGEMENT STRATEGIES, GENERAL NOTES, AND LOCAL NOTES)
TMP-2	PORTABLE CONCRETE BARRIER AT TEMPORARY SHORING LOCATIONS
TMP-2A	TEMPORARY SHORING DATA
TMP-2B	SPECIAL SIGN DESIGNS
TMP-3	TEMPORARY TRAFFIC CONTROL PHASING
TMP-4	SMITH ROAD OFFSITE DETOUR
TMP-5	SMITH ROAD & RAMP C OFFSITE DETOUR
TMP-6	SMITH ROAD, RAMP A & RAMP B OFFSITE DETOUR
TMP-7	TRAFFIC CONTROL DETAILS

SHEET NO.  
TMP-1

**BR-0095**

**TIP PROJECT:**

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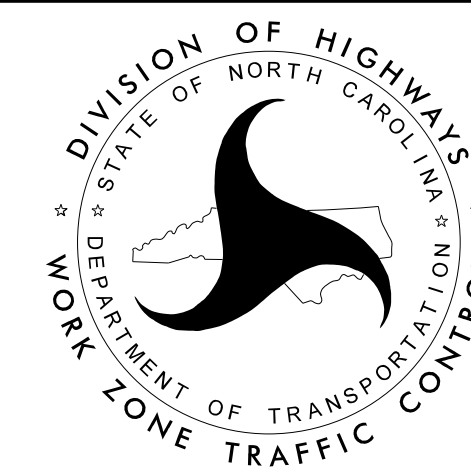
**PLANS PREPARED BY:**

MATTHEW AKLILU

**NCDOT CONTACTS:**

KENNETH C. THORNEWELL, PE  
PROJECT ENGINEER

MICHAEL STEELMAN  
PROJECT DESIGN ENGINEER

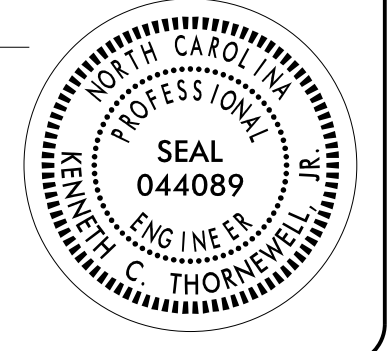


**DOCUMENT NOT CONSIDERED FINAL  
UNLESS ALL SIGNATURES COMPLETED**

APPROVED: Kenneth C. Thornevell, Jr.

DATE: 01/04/2024

SEAL



# ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS SHOWN IN "ROADWAY STANDARD DRAWINGS" - N.C. DEPARTMENT OF TRANSPORTATION - RALEIGH, N.C., DATED JANUARY 2024 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.01	WORK ZONE WARNING SIGNS
1101.02	TEMPORARY LANE CLOSURES
1101.03	TEMPORARY ROAD CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.06	WARNING SIGNS FOR BLASTING ZONES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW BOARDS
1130.01	DRUMS
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGING DEVICES
1160.01	TEMPORARY CRASH CUSHION
1165.01	TRUCK MOUNTED ATTENUATOR
1170.01	PORTABLE CONCRETE BARRIER
1180.01	SKINNY - DRUMS
1205.01	PAVEMENT MARKINGS - LINE TYPES AND OFFSETS
1205.02	PAVEMENT MARKINGS - TWO LANE AND MULTILANE ROADWAYS
1205.03	PAVEMENT MARKINGS - EXITS AND ENTRANCE RAMP
1205.04	PAVEMENT MARKINGS - INTERSECTIONS
1205.05	PAVEMENT MARKINGS - TURN LANES
1205.06	PAVEMENT MARKINGS - LANE DROPS
1205.07	PAVEMENT MARKINGS - PEDESTRIAN CROSSWALKS
1205.08	PAVEMENT MARKINGS - SYMBOLS AND WORD MESSAGES
1205.09	PAVEMENT MARKINGS - PAINTED ISLANDS
1205.10	PAVEMENT MARKINGS - SCHOOL AREAS
1205.11	PAVEMENT MARKINGS - RAILROAD CROSSINGS
1205.12	PAVEMENT MARKINGS - BRIDGES
1205.13	PAVEMENT MARKINGS - LANE REDUCTIONS
1205.14	PAVEMENT MARKINGS - ROUNDABOUTS
1205.15	PAVEMENT MARKINGS - SUPERSTREETS
1250.01	RAISED PAVEMENT MARKERS - INSTALLATION SPACING
1251.01	RAISED PAVEMENT MARKERS - (PERMANENT AND TEMPORARY)
1261.01	GUARDRAIL AND BARRIER DELINEATORS - INSTALLATION SPACING
1261.02	GUARDRAIL AND BARRIER DELINEATORS - TYPES AND MOUNTING
1262.01	GUARDRAIL END DELINEATION
1264.01	OBJECT MARKERS - TYPES
1264.02	OBJECT MARKERS - INSTALLATION

# LEGEND

## GENERAL

- DIRECTION OF TRAFFIC FLOW
- DIRECTION OF PEDESTRIAN TRAFFIC FLOW
- EXIST. PVMT.
- NORTH ARROW
- PROPOSED PVMT.
- TEMP. SHORING (LOCATION PURPOSES ONLY)

WORK AREA

REMOVAL

## SIGNALS

- EXISTING
- PROPOSED
- TEMPORARY
- PORTABLE

## PAVEMENT MARKINGS

- EXISTING LINES
- TEMPORARY LINES

## TRAFFIC CONTROL DEVICES

- BARRICADE (TYPE III)
- CONE
- DRUM
- SKINNY DRUM
- TUBULAR MARKER
- TEMPORARY CRASH CUSHION
- FLASHING ARROW BOARD
- FLAGGER
- LAW ENFORCEMENT
- TRUCK MOUNTED ATTENUATOR (TMA)
- CHANGEABLE MESSAGE SIGN

## TEMPORARY SIGNING

- PORTABLE SIGN
- STATIONARY SIGN
- STATIONARY OR PORTABLE SIGN
- STATIONARY SIGN DUAL MOUNTED

## PAVEMENT MARKERS

- CRYSTAL/CRYSTAL
- CRYSTAL/RED
- YELLOW/YELLOW

## PAVEMENT MARKING SYMBOLS

- PAVEMENT MARKING SYMBOLS

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APPROVED: DATE: 01/04/2024			<p style="text-align: center;"><b>ROADWAY STANDARD DRAWINGS &amp; LEGEND</b></p>
<p><b>DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED</b></p>			

# MANAGEMENT STRATEGIES

THE FOLLOWING LISTED WORK ZONE STRATEGIES ARE RECOMMENDED FOR INCLUSION WITHIN THIS TRANSPORTATION MANAGEMENT PLAN (TMP).

**RECOMMENDED STRATEGIES:**

- TRAFFIC MANAGEMENT STRATEGIES:**  
 FULL ROADWAY CLOSURES  
 LANE SHIFTS OR CLOSURES  
 SHOULDER CLOSURES  
 ONE-LANE, TWO WAY OPERATION (FLAGGING)  
 RAMP CLOSURES / RELOCATION  
 NIGHT WORK  
 WEEKEND WORK  
 WORK HOUR RESTRICTIONS FOR PEAK TRAVEL  
 OFF-SITE DETOURS / USE OF ALTERNATIVE ROUTES

## GENERAL NOTES / LOCAL NOTES

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS, AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS OR RESULT IN DUPLICATE OR UNDESIRED OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING, OR REMOVAL OF DEVICES AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT EXCEPT WHEN OTHERWISE NOTED IN THE PLAN OR DIRECTED BY THE ENGINEER.

**TIME RESTRICTIONS**

A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
US 220	MONDAY - FRIDAY 6AM - 9AM & 4PM - 7PM

B) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS:

ROAD NAME
US 220

**HOLIDAY**

- FOR ANY UNEXPECTED OCCURRENCE THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
- FOR NEW YEAR'S, BETWEEN THE HOURS OF 6:00 A.M. DECEMBER 31st TO 7:00 P.M. JANUARY 2ND. IF NEW YEAR'S DAY IS ON A FRIDAY, SATURDAY, SUNDAY, OR MONDAY THEN UNTIL 7:00 P.M. THE FOLLOWING TUESDAY.
- FOR EASTER, BETWEEN THE HOURS OF 6:00 A.M. THURSDAY AND 7:00 P.M. MONDAY.
- FOR MEMORIAL DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY TO 7:00 P.M. TUESDAY.
- FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 6:00 A.M. THE DAY BEFORE INDEPENDENCE DAY AND 7:00 P.M. THE DAY AFTER INDEPENDENCE DAY.

IF INDEPENDENCE DAY IS ON A FRIDAY, SATURDAY, SUNDAY OR MONDAY THEN BETWEEN THE HOURS OF 6:00 A.M. THE THURSDAY BEFORE INDEPENDENCE DAY AND 7:00 P.M. THE TUESDAY AFTER INDEPENDENCE DAY.

- FOR LABOR DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY AND 7:00 P.M. TUESDAY.
- FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 6:00 A.M. TUESDAY TO 7:00 P.M. MONDAY.
- FOR CHRISTMAS, BETWEEN THE HOURS OF 6:00 A.M. THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 7:00 P.M. THE FOLLOWING TUESDAY AFTER THE WEEK OF CHRISTMAS.

C) DO NOT CLOSE ROADS AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
US 220	MONDAY - SUNDAY 5AM - 10PM

D) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE HAULING OPERATION IS PROTECTED BY BARRIER OR GUARDRAIL OR AS DIRECTED BY THE ENGINEER.

**LANE AND SHOULDER CLOSURE REQUIREMENTS**

- REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED OR AS DIRECTED BY THE ENGINEER.
- WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 15 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR A LANE CLOSURE IS INSTALLED.
- WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

H) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS, OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.

I) DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY, RAMP, OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.

**TRAFFIC PATTERN ALTERATIONS**

J) NOTIFY THE ENGINEER THIRTY (30) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

**SIGNING**

- INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 40 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.
- PROVIDE SIGNING AND DEVICES REQUIRED TO CLOSE THE ROAD ACCORDING TO THE ROADWAY STANDARD DRAWINGS AND TRAFFIC CONTROL PLANS.

PROVIDE SIGNING REQUIRED FOR THE OFF-SITE DETOUR ROUTE AS SHOWN IN THE TRAFFIC CONTROL PLANS.

M) COVER OR REMOVE ALL SIGNS AND DEVICES REQUIRED TO CLOSE THE ROAD WHEN ROAD CLOSURE IS NOT IN OPERATION.

COVER OR REMOVE ALL SIGNS REQUIRED FOR THE OFF-SITE DETOUR WHEN THE DETOUR IS NOT IN OPERATION.

N) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

O) COVER EXISTING TRAILBLAZING SIGNS IF THEY ARE IN CONFLICT WITH THE DETOUR ROUTE.

**TRAFFIC BARRIER**

P) INSTALL TEMPORARY BARRIER ACCORDING TO THE TRANSPORTATION MANAGEMENT PLANS A MAXIMUM OF TWO (2) WEEKS PRIOR TO BEGINNING WORK IN ANY LOCATION. ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION PROCEED IN A CONTINUOUS MANNER TO COMPLETE THE PROPOSED WORK IN THAT LOCATION UNLESS OTHERWISE STATED IN THE TRANSPORTATION MANAGEMENT PLANS OR AS DIRECTED BY THE ENGINEER.

DO NOT PLACE BARRIER DIRECTLY ON ANY SURFACE OTHER THAN ASPHALT OR CONCRETE.

ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION AND NO WORK IS PERFORMED BEHIND THE TEMPORARY BARRIER FOR A PERIOD LONGER THAN TWO (2) MONTHS, REMOVE / RESET TEMPORARY BARRIER AT NO COST TO THE DEPARTMENT UNLESS OTHERWISE STATED IN THE TRANSPORTATION MANAGEMENT PLANS, TEMPORARY BARRIER IS PROTECTING A HAZARD, OR AS DIRECTED BY THE ENGINEER.

INSTALL TEMPORARY BARRIER WITH THE TRAFFIC FLOW BEGINNING WITH THE UPSTREAM SIDE OF TRAFFIC. REMOVE TEMPORARY BARRIER AGAINST THE TRAFFIC FLOW BEGINNING WITH THE DOWNSTREAM SIDE OF TRAFFIC.

INSTALL AND SPACE DRUMS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH) TO CLOSE OR KEEP THE SECTION OF THE ROADWAY CLOSED UNTIL THE TEMPORARY BARRIER CAN BE PLACED OR AFTER THE TEMPORARY BARRIER IS REMOVED.

Q) PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER AT ALL TIMES DURING THE INSTALLATION AND REMOVAL OF THE BARRIER BY EITHER A TRUCK MOUNTED ATTENUATOR (MAXIMUM 72 HOURS) OR A TEMPORARY CRASH CUSHION.

PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER FROM ONCOMING TRAFFIC AT ALL TIMES BY A TEMPORARY CRASH CUSHION UNLESS THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER IS OFFSET FROM ONCOMING TRAFFIC AS FOLLOWS OR AS SHOWN IN THE PLANS: (SEE ALSO 1101.05)

POSTED SPEED LIMIT	MINIMUM OFFSET
40 OR LESS	15 FT
45 - 50	20 FT
55	25 FT
60 MPH or HIGHER	30 FT

**TRAFFIC CONTROL DEVICES**

R) WHEN LANE CLOSURES ARE NOT IN EFFECT SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER IN FEET THAN TWICE THE POSTED SPEED LIMIT (MPH) EXCEPT, 10 FT ON-CENTER IN RADIUS, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY. REFER TO STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES SECTIONS 1130 (DRUMS), 1135 (CONES) AND 1180 (SKINNY DRUMS) FOR ADDITIONAL REQUIREMENTS.

S) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY.

T) USE SEQUENTIAL FLASHING WARNING LIGHTS WITH LANE CLOSURE MERGE TAPERS IN ACCORDANCE WITH SECTION 1140 IN THE STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES.

**PAVEMENT MARKINGS AND MARKERS**

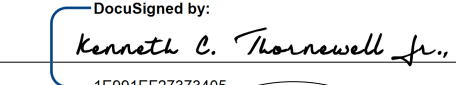
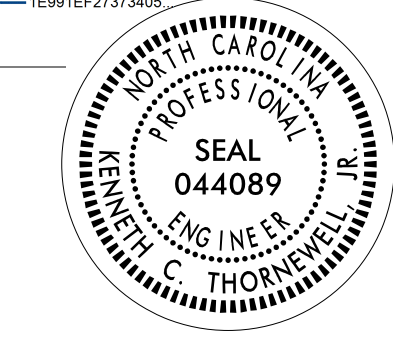
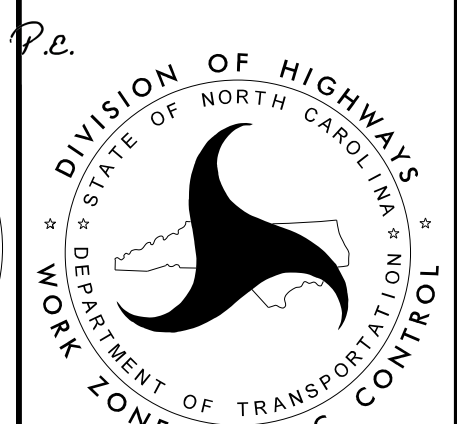
U) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.

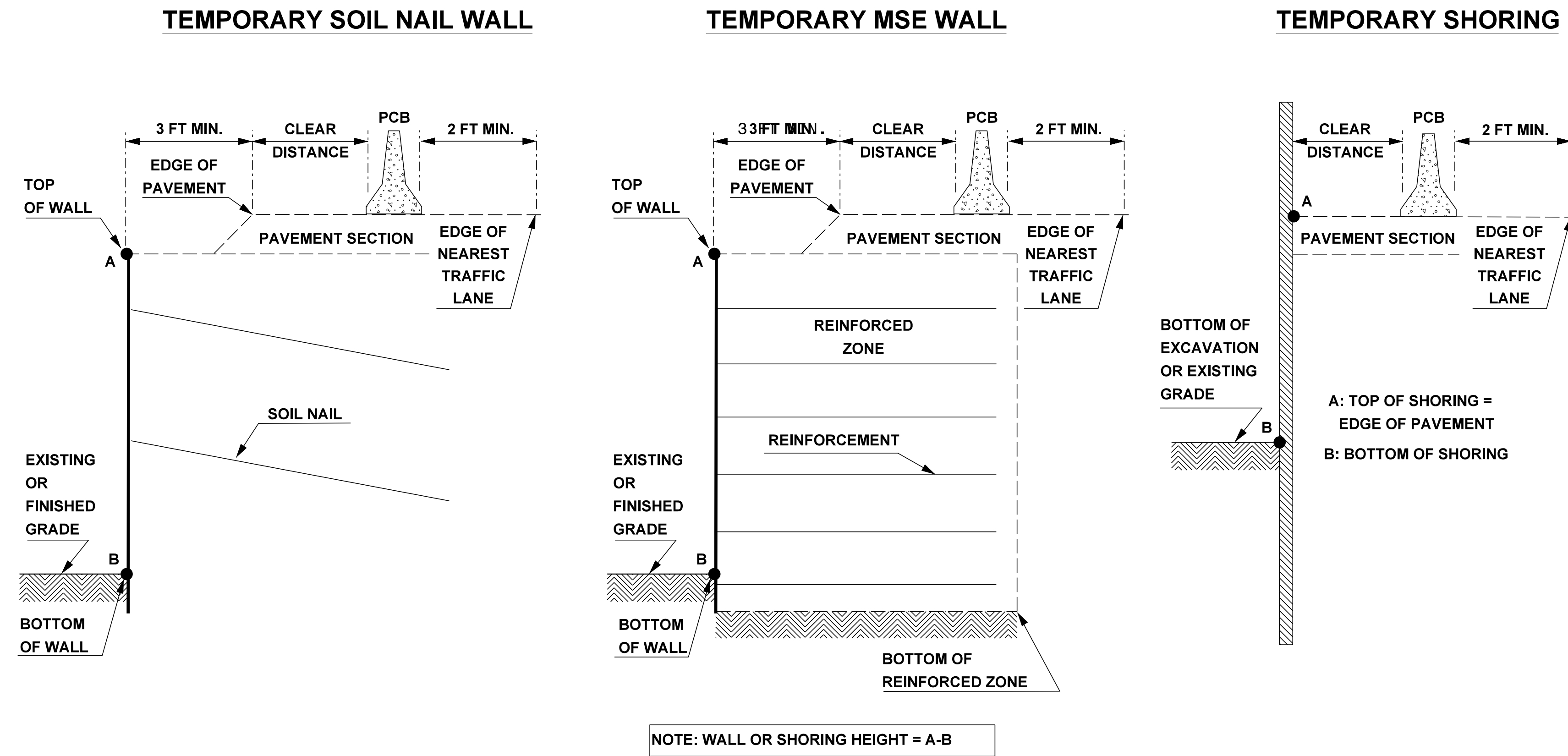
V) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS BY THE END OF EACH DAY'S OPERATION.

**MISCELLANEOUS**

W) LAW ENFORCEMENT MAY BE USED TO MAINTAIN TRAFFIC THROUGH THE WORK AREA AND/OR INTERSECTIONS AS DIRECTED BY THE ENGINEER.

PROJ. REFERENCE NO. BR-0095	SHEET NO. TMP-1B
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APPROVED:  DATE: 01/04/2024 		<p style="text-align: center;"><b>TRANSPORTATION OPERATIONS PLAN</b></p>
<p><b>DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED</b></p>		



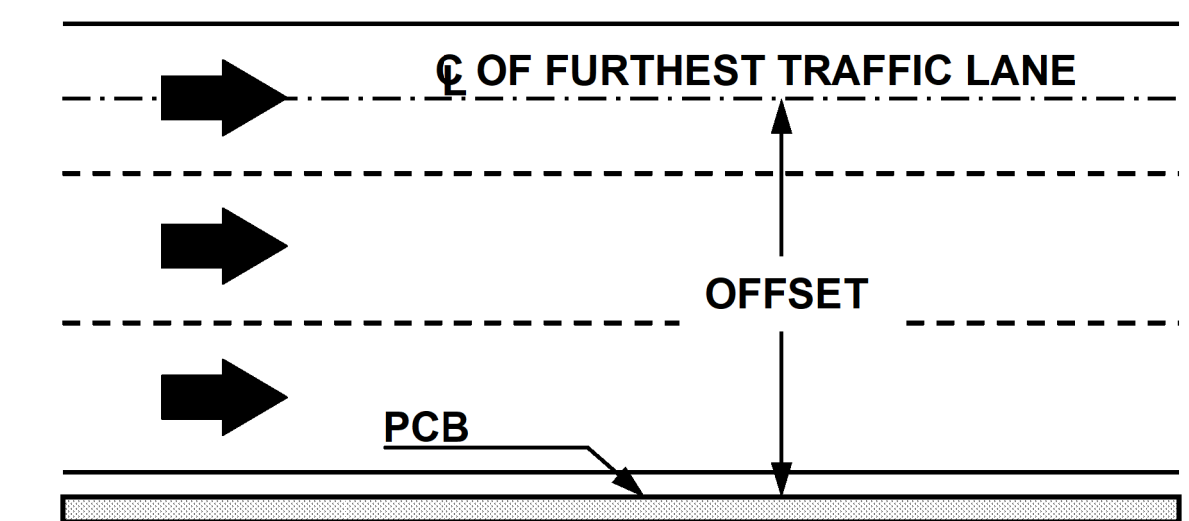
### NOTES

- REFER TO THE TRAFFIC CONTROL PLANS FOR TEMPORARY SHORING LOCATIONS AND NOTES.
- REFER TO THE "TEMPORARY SHORING" STANDARD PROVISION FOR INFORMATION ABOUT TEMPORARY SHORING AND PORTABLE CONCRETE BARRIER (PCB).
- PCB IS REQUIRED IF TEMPORARY SHORING/WALL IS LOCATED WITHIN THE CLEAR ZONE IN ACCORDANCE WITH THE AASHTO ROADSIDE DESIGN GUIDE. DO NOT PLACE BARRIER DIRECTLY ON ANY SURFACE OTHER THAN ASPHALT OR CONCRETE. (CONTACT NCDOT PAVEMENT MANAGEMENT FOR APPLICABLE PAVEMENT DESIGN).
- BASED ON THE CLEAR DISTANCE, OFFSET, DESIGN SPEED AND PAVEMENT TYPE, CHOOSE AN UNANCHORED OR ANCHORED PCB FROM THE TABLE SHOWN IN FIGURE B. CLEAR DISTANCE IS DEFINED AS SHOWN IN FIGURE A AND OFFSET IS DEFINED AS SHOWN IN FIGURE B.
- AT THE CONTRACTOR'S OPTION OR IF THE MINIMUM REQUIRED CLEAR DISTANCE IS NOT AVAILABLE, SET PCB NEXT TO AND UP AGAINST THE TRAFFIC SIDE OF THE TEMPORARY SHORING/WALLS EXCEPT FOR BARRIER ABOVE TEMPORARY WALLS. PCB WITH THE MINIMUM REQUIRED CLEAR DISTANCE IS REQUIRED ABOVE TEMPORARY WALLS.
- USE NCDOT PORTABLE CONCRETE BARRIER (PCB) IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1170.01 AND SECTION 1170 OF THE STANDARD SPECIFICATIONS.
- SET PCB WITH A MINIMUM HORIZONTAL DISTANCE OF 2 FT BETWEEN THE FRONT FACE OF THE BARRIER AND THE EDGE OF THE NEAREST TRAFFIC LANE AS SHOWN IN FIGURE A UNLESS OTHERWISE SHOWN IN THE PLANS OR APPROVED BY THE ENGINEER.
- FOR PCB ABOVE AND BEHIND TEMPORARY WALLS, PROVIDE A MINIMUM DISTANCE OF 3 FT BETWEEN THE EDGE OF PAVEMENT AND THE WALL FACE AS SHOWN IN FIGURE A. IF THIS MINIMUM REQUIRED DISTANCE IS NOT AVAILABLE, CONTACT THE ENGINEER.
- TABLE SHOWN IN FIGURE B IS BASED ON NCDOT RESEARCH PROJECT NO. 2005-010 WITH VEHICLE TYPE USED FOR NCHRP 350 CRASH TESTS.

### MINIMUM REQUIRED CLEAR DISTANCE, inches

Barrier Type	Pavement Type	Offset * ft	Design Speed, mph					
			<30	31-40	41-50	51-60	61-70	71-80
Unanchored PCB	Asphalt	<8	24	26	29	32	36	40
		8-14	26	28	31	35	38	42
		14-20	27	29	34	36	39	43
		20-26	28	31	35	38	40	44
		26-32	29	32	36	39	42	45
		32-38	30	34	38	41	43	46
		38-44	31	34	41	43	45	48
	Concrete	44-50	31	35	41	43	46	49
		50-56	32	36	42	44	47	50
		>56	32	36	42	45	47	51
		<8	17	18	21	22	25	26
		8-14	19	20	23	25	26	29
		14-20	22	22	24	26	28	31
		20-26	23	24	26	27	30	34
Anchored PCB	Asphalt	26-32	24	25	27	28	32	35
		32-38	24	26	27	30	33	36
		38-44	25	26	28	30	34	37
		44-50	26	26	28	32	35	37
		50-56	26	26	28	32	35	38
		>56	26	27	29	32	36	38
		All Offsets	24 for All Design Speeds					
Anchored PCB	Concrete (including bridge approach slabs)	All Offsets	12 for All Design Speeds					

\* See Figure Below



APPROVED: <i>Kenneth C. Thornevell Jr.</i> DATE: 01/04/2024 		PORTABLE CONCRETE BARRIER AT TEMPORARY SHORING LOCATIONS
<b>DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED</b>		

THE CONTRACTOR IS ADVISED THAT THE TEMPORARY SHORING SHOWN ON SHEET NO. TMP-7 OF THE TRANSPORTATION MANAGEMENT PLANS FOR BRIDGE CONSTRUCTION WILL INTERFERE WITH TWO EXISTING RCPs. THE CONTRACTOR SHOULD DESIGN THE TEMPORARY SHORING TO AVOID DAMAGE TO THESE TWO RCPs AND THE CONNECTING BOX.

Temporary Shoring No. 1 Notes on Plans

FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.

DESIGN TEMPORARY SHORING FROM STATION 14+18 +/- -Y-, 10.5 FT. LT. TO STATION 14+82 +/- -Y-, 10.5 FT. LT., FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:

UNIT WEIGHT OF SOIL ABOVE WATER TABLE,  $\gamma = 120$  PCF  
 UNIT WEIGHT OF SOIL BELOW WATER TABLE,  $\gamma' = 60$  PCF  
 FRICTION ANGLE,  $\phi = 30$   
 COHESION,  $c = 0$  PSF  
 GROUNDWATER ELEVATION = 955 FT

BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.

LIMITED SUBSURFACE INFORMATION IS AVAILABLE IN THE VICINITY OF TEMPORARY SHORING FROM STATION 14+18 +/- -Y-, 10.5 FT. LT. TO STATION 14+82 +/- -Y-, 10.5 FT. LT. THE INFORMATION PROVIDED FOR TEMPORARY SHORING DESIGN WAS ASSUMED AND MAY NOT BE APPLICABLE TO THE ACTUAL SITE CONDITIONS ENCOUNTERED DURING CONSTRUCTION.

AT THE CONTRACTOR\*S OPTION, USE STANDARD TEMPORARY SHORING FOR TEMPORARY SHORING FROM STATION 14+18 +/- -Y-, 10.5 FT. LT. TO STATION 14+82 +/- -Y-, 10.5 FT. LT. SEE GEOTECHNICAL STANDARD DETAIL 1801.01 FOR STANDARD TEMPORARY SHORING.

DRIVEN PILING FOR TEMPORARY SHORING FROM STATION 14+18 +/- -Y-, 10.5 FT. LT. TO STATION 14+82 +/- -Y-, 10.5 FT. LT. MAY NOT PENETRATE BELOW ELEVATION 942.5 FT. +/- . DUE TO OBSTRUCTIONS, VERY DENSE OR HARD SOIL, BOULDERS OR WEATHERED OR HARD ROCK.

Temporary Shoring No. 2 Notes on Plans

FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.

DESIGN TEMPORARY SHORING FROM STATION 14+18 +/- -Y-, 9.5 FT. RT. TO STATION 14+82 +/- -Y-, 9.5 FT. RT., FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:

UNIT WEIGHT OF SOIL ABOVE WATER TABLE,  $\gamma = 120$  PCF  
 UNIT WEIGHT OF SOIL BELOW WATER TABLE,  $\gamma' = 60$  PCF  
 FRICTION ANGLE,  $\phi = 30$   
 COHESION,  $c = 0$  PSF  
 GROUNDWATER ELEVATION = 955 FT

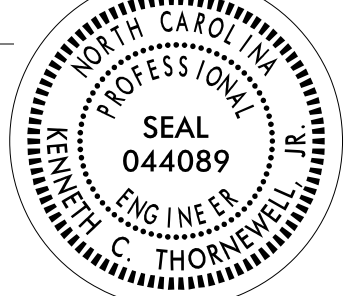

BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.

LIMITED SUBSURFACE INFORMATION IS AVAILABLE IN THE VICINITY OF TEMPORARY SHORING FROM STATION 14+18 +/- -Y-, 9.5 FT. RT. TO STATION 14+82 +/- -Y-, 9.5 FT. RT. THE INFORMATION PROVIDED FOR TEMPORARY SHORING DESIGN WAS ASSUMED AND MAY NOT BE APPLICABLE TO THE ACTUAL SITE CONDITIONS ENCOUNTERED DURING CONSTRUCTION.

AT THE CONTRACTOR\*S OPTION, USE STANDARD TEMPORARY SHORING FOR TEMPORARY SHORING FROM STATION 14+18 +/- -Y-, 9.5 FT. RT. TO STATION 14+82 +/- -Y-, 9.5 FT. RT. SEE GEOTECHNICAL STANDARD DETAIL 1801.01 FOR STANDARD TEMPORARY SHORING.

DRIVEN PILING FOR TEMPORARY SHORING FROM STATION 14+18 +/- -Y-, 9.5 FT. RT. TO STATION 14+82 +/- -Y-, 9.5 FT. RT. MAY NOT PENETRATE BELOW ELEVATION 942.5 FT. +/- . DUE TO OBSTRUCTIONS, VERY DENSE OR HARD SOIL, BOULDERS OR WEATHERED OR HARD ROCK.

THE TEMPORARY SHORING NOTES SHOWN ON THIS SHEET WERE PROVIDED THROUGH A SEALED DOCUMENT FROM THE GEOTECHNICAL ENGINEERING UNIT. THE DOCUMENT WAS SUBMITTED TO THE WZTC SECTION ON (8/22/2023) AND SEALED BY A PROFESSIONAL ENGINEER, (DAVID L. TEAGUE), LICENSE # (027869).

<p>APPROVED: _____  <small>Digitally signed by</small>  <i>Keneth L. Thornwell, Jr., P.E.</i>  <small>16091672737405</small></p> <p>DATE: 03/13/2024</p>  <p><b>DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED</b></p>		<p>TEMPORARY SHORING DATA</p>
--	---	-------------------------------

SIGN NUMBER: SP23250	BACKG COLOR: Fluorescent Orange	DESIGN BY: J.Navarrete	CHECKED BY:	Oct 02, 2023
TYPE: D	COPY COLOR: Black	PROJECT ID:	LOCATION:	DIV: 7
QUANTITY: 1				

SYMBOL	X	Y	WID	HT

USE NOTES:

- Legend and border(except those that are colored black) shall be direct applied Grade C sheeting.
- Background shall be Grade C reflective sheeting.

Spacing Factor is 1 unless specified otherwise

LETTER POSITIONS

Letter spacings are to start of next letter

	U	S	2	2	O	T	R	A	F	F	I	C		Series/Size			
	5,6	5,1	4,1	6	5,3	5,3	4,3	6	4,6	4,6	6	4,6	4,6	2,3	4,1	5,6	D 2000
		F	O	L	L	O	W	S	M	I	T	H					D 2000
	10,3	4,5	5,6	4,6	4,5	5	5,3	6	5,1	6,1	1,9	4,6	4,1	10,3		57,4	
		R	O	A	D	D	E	T	O	U	R						D 2000
	11,3	5	5	6	4,1	6	5,4	4,2	4,5	5,6	5,5	4,1	11,3			55,4	

BR0095\_sign\_designs 3

NORTH CAROLINA D.O.T. SIGN DETAIL

SIGN NUMR:	BACKG. COLOR: White	DESIGN BY:	CHK BY:	STD #:
TYPE: F Ground	COPY COLOR: Black	PROJECT ID:	DIV:	DATE:
QUANTITY: 1				

SYMBOL	X	Y	WID	HT
u arrow	3.7	3.5	12.7	13.9

USE NOTES:

- Legend and border shall be direct applied Non-reflective sheeting.
- Background shall be Grade C reflective sheeting.

Spacing Factor is 1 unless specified otherwise

Arrow Details

ARROW DIMENSIONS (INCHES)	F	G	H	M	N	P	Q	R
	2.625	2.625	8.659	5.25	3	5.25	0.375	0.5

Spacing Factor is 1 unless specified otherwise

FILENAME: GSENG

SIGN NUMBER: SP23218	BACKG COLOR: Fluorescent Orange	DESIGN BY: J.Navarrete	CHECKED BY:	Aug 31, 2023
TYPE: D	COPY COLOR: Black	PROJECT ID:	LOCATION:	DIV: 7
QUANTITY: 1				

SYMBOL	X	Y	WID	HT

USE NOTES:

- Legend and border(except those that are colored black) shall be direct applied Grade C sheeting.
- Background shall be Grade C reflective sheeting.

Spacing Factor is 1 unless specified otherwise

LETTER POSITIONS

Letter spacings are to start of next letter

	S	M	I	T	H		Series/Size
	7,1	5,1	6,1	1,9	4,6	4,1	7,1
		R	O	A	D		
	7,9	5	5	6	4,1	7,9	20,1

BR0095\_sign\_designs 3

NORTH CAROLINA D.O.T. SIGN DETAIL

APPROVED: <i>Rene B. Roach, PE</i>			<p>SPECIAL SIGN DESIGNS</p>
DATE: 01/05/2024			
<p>DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED</p>			

I:\4\2024  
 P:\TIP\Projects-B\BR0095\TrafficControl\CP\BR-0095-TC-TMP-2B.dgn  
 User:mackitlu

## TEMPORARY TRAFFIC CONTROL PHASING

BEFORE CONSTRUCTION BEGINS:

- INSTALL ADVANCE WARNING SIGNS IN ACCORDANCE WITH RSD 1101.01.
- IN ORDER TO ADEQUATELY REROUTE SCHOOL BUSES, ROCKINGHAM COUNTY SCHOOLS TRANSPORTATION WILL BE CONTACTED AT 336-634-3275 AT LEAST 3 MONTHS PRIOR TO CONSTRUCTION. ROCKINGHAM COUNTY'S EMERGENCY SERVICES DIRECTOR SHOULD BE CONTACTED AT 336-634-3000 AT LEAST 3 MONTHS PRIOR TO CONSTRUCTION TO MAKE NECESSARY TEMPORARY REASSIGNMENTS TO PRIMARY RESPONSE UNITS.
- MAINTENANCE WILL INVESTIGATE WHETHER THE RUT AT THE TOP OF THE MARTINSVILLE LOOP/US 220 BUS MEDIAN CROSSOVER REQUIRES REPAIR AND WILL IMPLEMENT CORRECTIVE ACTION PRIOR TO THE DETOUR BEING PUT IN PLACE.

PHASE I

- STEP 1 - INSTALL SIGNS AND DEVICES & COVER INDICATED SIGNS TO CLOSE AND DETOUR SMITH ROAD TRAFFIC OFFSITE PER RSD 1101.03, SHEET 2, TMP-4 & TMP-7. USE DRUMS & SKINNY DRUMS TO MAINTAIN AT LEAST 12' LANES WHERE THE RAMPS MEET SMITH ROAD IN THE CONFIGURATION SHOWN ON TMP-7.
- STEP 2 - USING RSD 1101.02, SHEET 4 AS NEEDED, DEMO THE EXISTING BRIDGE SUPERSTRUCTURE. FOR OPERATIONS THAT POSE A HAZARD TO US 220 TRAFFIC, USE RSD 1101.03, SHEET 7 TO CLOSE THE AFFECTED DIRECTION OF US 220 AND DETOUR TRAFFIC VIA THE RAMPS.
- STEP 3 - USING RSD 1101.02, SHEET 4 AS NEEDED, DEMO THE EXISTING BRIDGE END BENTS (END BENTS #1 & #2 IN THE EXISTING STRUCTURE PLANS) AND EXISTING SLOPE PROTECTION. DEMO THE OUTSIDE BENTS(BENTS #1 & #3 IN THE EXISTING STRUCTURE PLANS). MAINTAIN EXISTING GUARDRAIL AND JERSEY BARRIER PROTECTING THE OUTSIDE BENTS.
- STEP 4 - AWAY FROM TRAFFIC, BEGIN CONSTRUCTION ON THE PROPOSED BRIDGE END BENTS #1 & #2, PROPOSED SLOPE PROTECTION, AND THE SMITH ROAD BRIDGE APPROACHES, BETWEEN -L- 14+25 AND -L- 19+50. THE EXISTING GUARDRAIL AND JERSEY BARRIER PROTECTING THE OUTSIDE BENTS MAINTAINED IN STEP 3 CAN BE REMOVED AFTER COMPLETION OF THE FINAL ROADWAY CONFIGURATION UNDER THE BRIDGE.
- STEP 5 - USING RSD 1101.02, SHEET 4 & TMP-7, INSTALL PORTABLE CONCRETE BARRIER ON US 220. REMOVE EXISTING GUARDRAIL AND INSTALL TEMPORARY CAT-1 AND GREU-TL3 GUARDRAIL ANCHORS (SEE ROADWAY QUANTITIES).
- STEP 6 - BEHIND BARRIER, INSTALL TEMPORARY SHORING (PER TMP-7) AND REMOVE THE EXISTING BRIDGE MIDDLE BENT FOOTING (CORRESPONDING TO BENT #2 IN THE EXISTING STRUCTURE PLANS), RETAINING ANY EXCAVATED HYDRAULICS ITEMS, THEN CONSTRUCT THE PROPOSED BRIDGE BENT #1 AND REMOVE TEMPORARY SHORING.
  - COMPLETE CONSTRUCTION OF PROPOSED BRIDGE END BENTS #1 & #2 AND PROPOSED SLOPE PROTECTION.
- STEP 7 - USE RSD 1101.03, SHEET 7 TO CLOSE THE AFFECTED DIRECTION OF US 220 AND DETOUR TRAFFIC VIA THE RAMPS, AND THEN INSTALL PROPOSED BRIDGE GIRDERS AWAY FROM TRAFFIC. USING RSD 1101.02, SHEET 4 AS NEEDED, COMPLETE PROPOSED BRIDGE SUPERSTRUCTURE WORK.
- STEP 8 - USING LANE CLOSURES, AS NECESSARY, RETURN MEDIAN GUARDRAIL TO ITS ORIGINAL CONFIGURATION AND REMOVE THE MEDIAN PORTABLE CONCRETE BARRIER IN EACH DIRECTION OF US 220. AT NO TIME DURING THE AFOREMENTIONED WORK SHALL THE MEDIAN BRIDGE PIERS BE EXPOSED TO TRAFFIC UNLESS A LANE CLOSURE IS IN PLACE WITH TRAFFIC PLACED IN THE TRAVEL LANE FURTHEST FROM THE WORK.

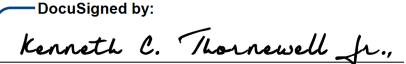
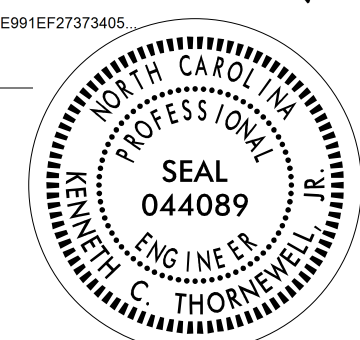

PHASE II

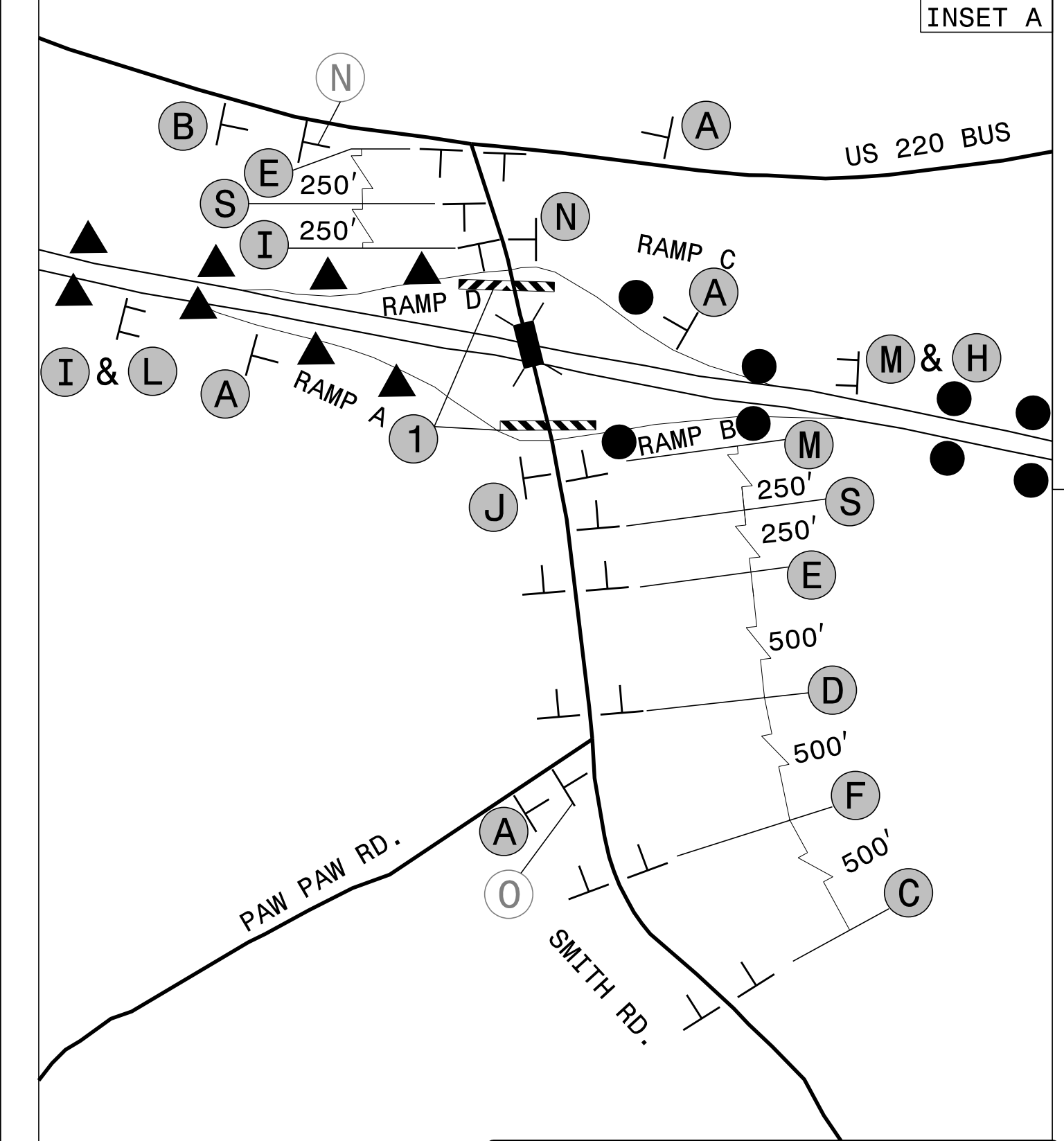
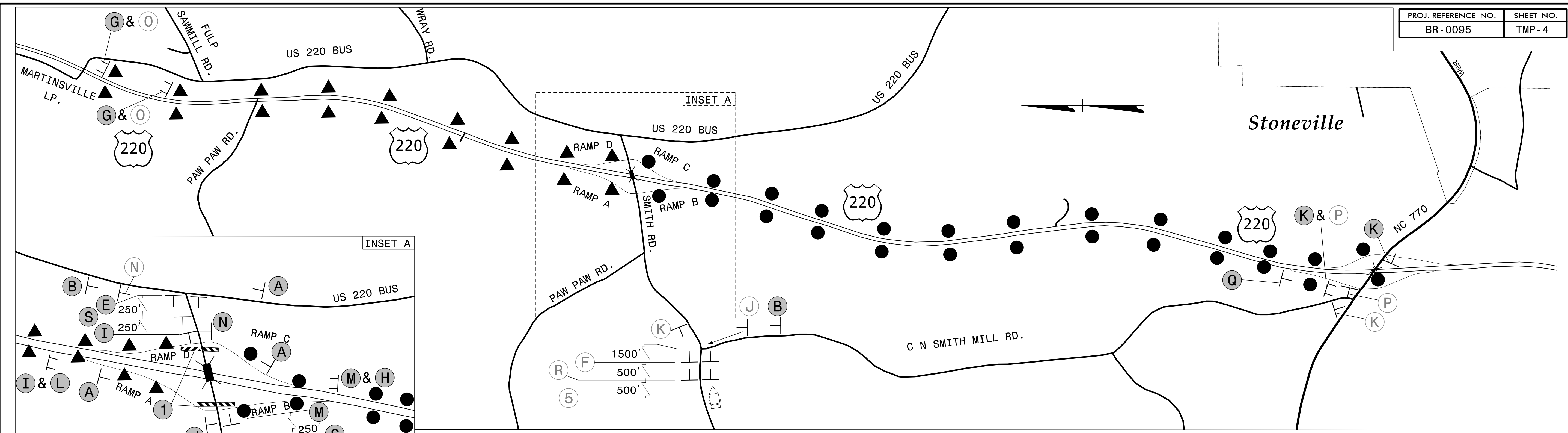
NOTE: PHASE II, STEP 1 MAY BE COMPLETED AT ANY POINT IN THIS PHASE PRIOR TO PHASE II, STEP 7, AT WHICH POINT IT MUST BE COMPLETE.

- STEP 1 - COMPLETE CONSTRUCTION OF THE BRIDGE AND APPROACHES, BETWEEN -L- 14+25 AND -L- 19+50, INCLUDING FINAL SURFACE COURSE AND PAVEMENT MARKINGS. SMITH ROAD WILL REMAIN CLOSED AT THE COMPLETION OF THIS STEP.
 

COMPLETE THE WORK OF PHASE II, STEPS 2 THRU 4 IN A CONTINUOUS MANNER BEGINNING FRIDAY AT 5:00 AM TO THE FOLLOWING MONDAY AT 5:00 AM. (SEE INTERMEDIATE CONTRACT TIMES AND LIQUIDATED DAMAGES)
- STEP 2 - WITH SMITH ROAD STILL CLOSED, COVER/UNCOVER SIGNS AND INSTALL DEVICES FOR THE SMITH ROAD & RAMP C OFFSITE DETOUR (PER TMP-5 & RSD 1101.02, SHEET 12) TO CLOSE AND DETOUR RAMP C TRAFFIC OFFSITE.
- STEP 3 - USING FLAGGERS AND DEVICES TO MAINTAIN AT LEAST A 10' LANE FOR THE OPEN PORTION OF SMITH ROAD AND RAMP D TRAFFIC, COMPLETE CONSTRUCTION OF -RPC-, -RPD- AND -L- 19+50 TO -L- 21+50, INCLUDING PROPOSED GUARDRAIL, FINAL SURFACE COURSE AND PAVEMENT MARKINGS. REFER TO RSD 1101.02, SHEET 1.
- STEP 4 - COVER/UNCOVER SIGNS AND INSTALL DEVICES AS SHOWN ON TMP-4 TO OPEN RAMP C & D TO TRAFFIC AND KEEP SMITH ROAD CLOSED. USE DRUMS & SKINNY DRUMS TO MAINTAIN AT LEAST 12' LANES WHERE RAMP C & RAMP D MEET SMITH ROAD IN A CONFIGURATION LIKE THAT SHOWN ON TMP-7.
 

COMPLETE THE WORK OF PHASE II, STEPS 5 THRU 7 IN A CONTINUOUS MANNER BEGINNING FRIDAY AT 5:00 AM TO THE FOLLOWING TUESDAY AT 5:00 AM. (SEE INTERMEDIATE CONTRACT TIMES AND LIQUIDATED DAMAGES)
- STEP 5 - WITH SMITH ROAD STILL CLOSED, COVER/UNCOVER SIGNS AND INSTALL DEVICES FOR THE SMITH ROAD, RAMP A & RAMP B OFFSITE DETOUR (PER TMP-6, RSD 1101.02, SHEET 12 & RSD 1101.03, SHEET 1) TO CLOSE AND DETOUR RAMP A & RAMP B TRAFFIC OFFSITE.
- STEP 6 - AWAY FROM TRAFFIC, COMPLETE CONSTRUCTION OF -RPA-, -RPB- AND -L- 12+25 TO -L- 14+25, INCLUDING PROPOSED GUARDRAIL, FINAL SURFACE COURSE AND PAVEMENT MARKINGS.
- STEP 7 - REMOVE DEVICES AND COVER/REMOVE ALL OFFSITE DETOUR SIGNS (INCLUDING W4-4aP SIGNS MOUNTED BELOW YIELD SIGNS AT SMITH ROAD RAMP B & RAMP D) TO OPEN SMITH ROAD, RAMP A & RAMP B TO TRAFFIC. AT THE COMPLETION OF THIS STEP, ALL ROADS WILL BE OPEN TO TRAFFIC IN THE FINAL PATTERN.
- STEP 8 - REMOVE ALL TEMPORARY TRAFFIC CONTROL SIGNS AND DEVICES.

APPROVED:  DATE: 01/04/2024			<h3 style="margin: 0;">TEMPORARY TRAFFIC CONTROL PHASING</h3>
<b>DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED</b>			



**OFFSITE DETOUR ROUTE LEGEND**

- EB SMITH RD. ROUTE
- ▲▲▲ WB SMITH RD. ROUTE

**SIGN LEGEND\***

- (X) UNCOVERED SIGNS
- (X) COVERED/NOT INSTALLED SIGNS (USED FOR A DIFFERENT STEP)
- \*ALL SIGNS FOR ALL DETOURS THROUGHOUT THE LIFE OF THE PROJECT ARE SHOWN

<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>	<b>G</b>	<b>H</b>	<b>I</b>
<b>J</b>	<b>K</b>	<b>L</b>	<b>M</b>	<b>N</b>	<b>O</b>	<b>P</b>	<b>Q</b>	<b>R</b>

<b>1</b>		<b>2</b>		<b>S</b>	
	TYPE III BARRICADE(S)		TYPE III BARRICADE		

<b>3</b>		<b>4</b>		<b>5</b>	
	TYPE III BARRICADE		TYPE III BARRICADE(S)		SHORT TERM (SEE TMP-6)

APPROVED:

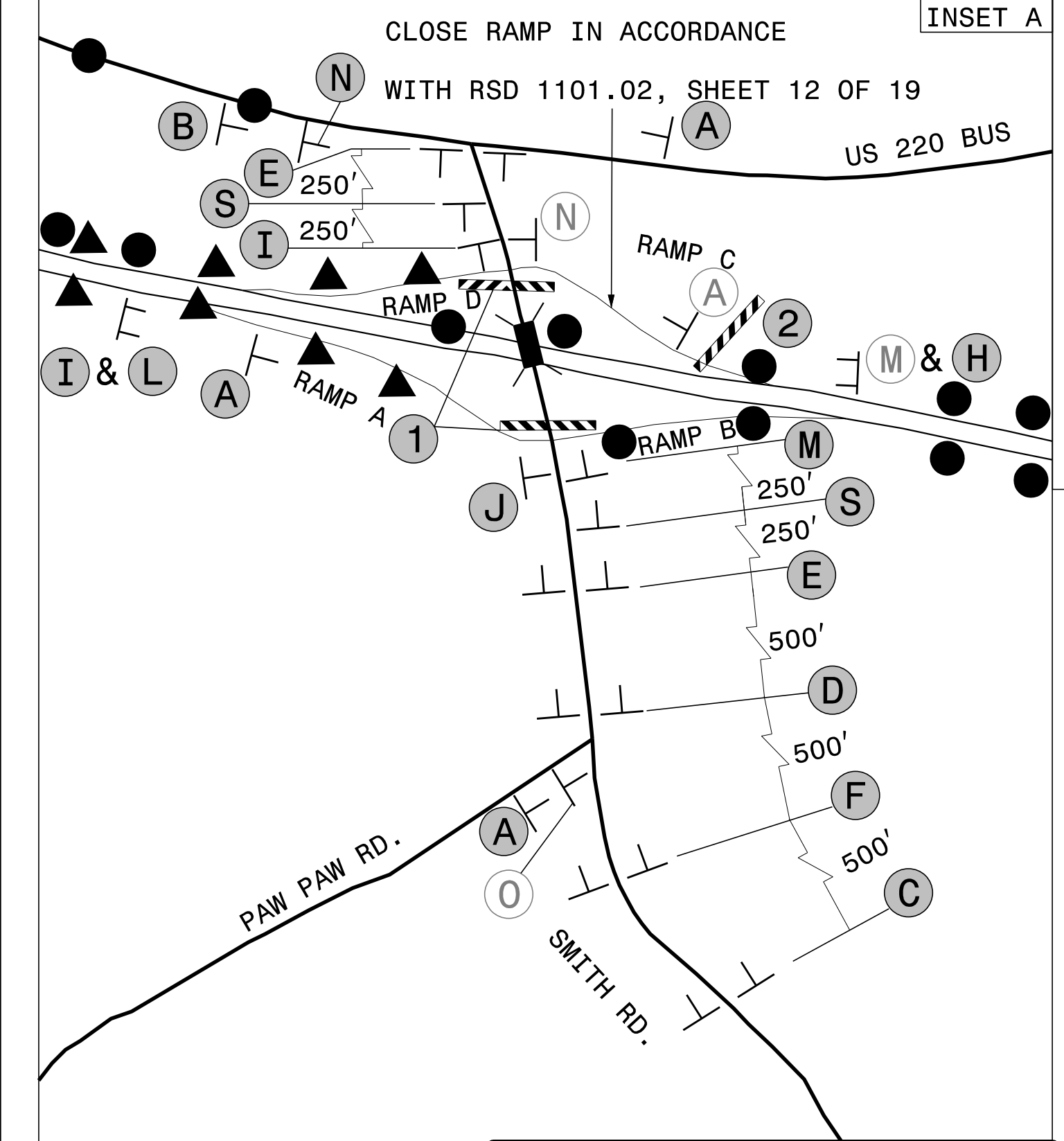
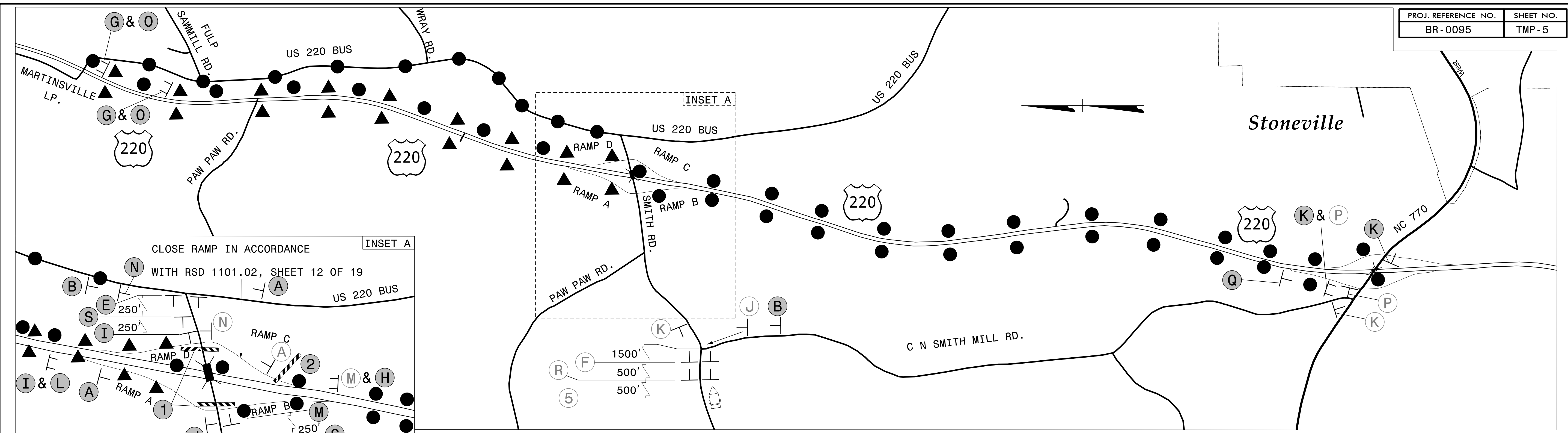
DATE: 01/04/2024

**SMITH RD. OFFSITE DETOUR DETAILS**

**DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED**

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**OFFSITE DETOUR ROUTE LEGEND**

- ● ● EB SMITH RD. ROUTE
- ▲ ▲ ▲ WB SMITH RD. ROUTE

**SIGN LEGEND\***

- (X) UNCOVERED SIGNS
- (X) COVERED/NOT INSTALLED SIGNS (USED FOR A DIFFERENT STEP)
- \*ALL SIGNS FOR ALL DETOURS THROUGHOUT THE LIFE OF THE PROJECT ARE SHOWN

<td> <td> <td> <td> <td> </td></td></td></td></td>	<td> <td> <td> <td> </td></td></td></td>	<td> <td> <td> </td></td></td>	<td> <td> </td></td>	<td> </td>							
<td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td>											
<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>	<b>G</b>	<b>H</b>	<b>I</b>			
<td> <td> <td> <td> <td> </td></td></td></td></td>	<td> <td> <td> <td> </td></td></td></td>	<td> <td> <td> </td></td></td>	<td> <td> </td></td>	<td> </td>							
<b>J</b>	<b>K</b>	<b>L</b>	<b>M</b>	<b>N</b>	<b>O</b>	<b>P</b>	<b>Q</b>	<b>R</b>	<td> <p><b>1</b>  TYPE III BARRICADE(S)</p> <p><b>2</b>  TYPE III BARRICADE</p> <p><b>3</b>  TYPE III BARRICADE</p> <p><b>4</b>  TYPE III BARRICADE(S)</p> <p><b>5</b>  TYPE III BARRICADE(S)</p> <p><b>S</b> </p> </td>	<p><b>1</b>  TYPE III BARRICADE(S)</p> <p><b>2</b>  TYPE III BARRICADE</p> <p><b>3</b>  TYPE III BARRICADE</p> <p><b>4</b>  TYPE III BARRICADE(S)</p> <p><b>5</b>  TYPE III BARRICADE(S)</p> <p><b>S</b> </p>	

MESSAGE NO. 1  
US 220 RAMP CLOSED

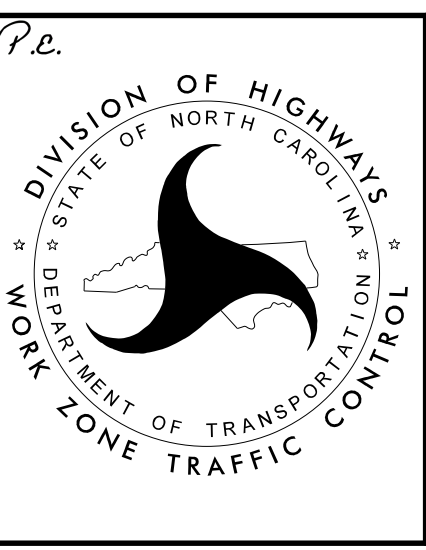
MESSAGE NO. 2  
FOLLOW SMITH RD DETOUR

CHANGEABLE MESSAGE SIGN  
SHORT TERM  
(SEE TMP-6)

APPROVED:

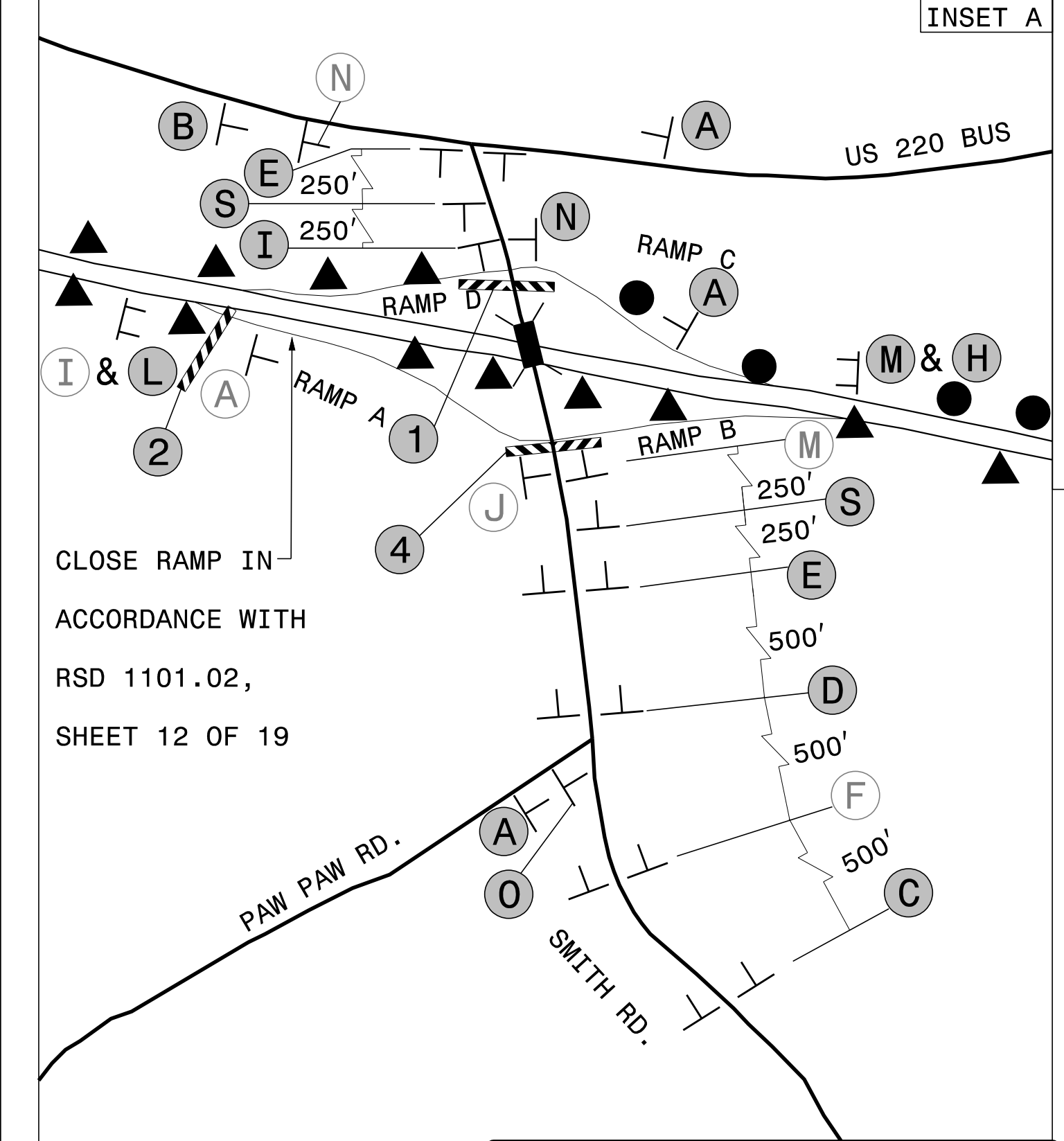
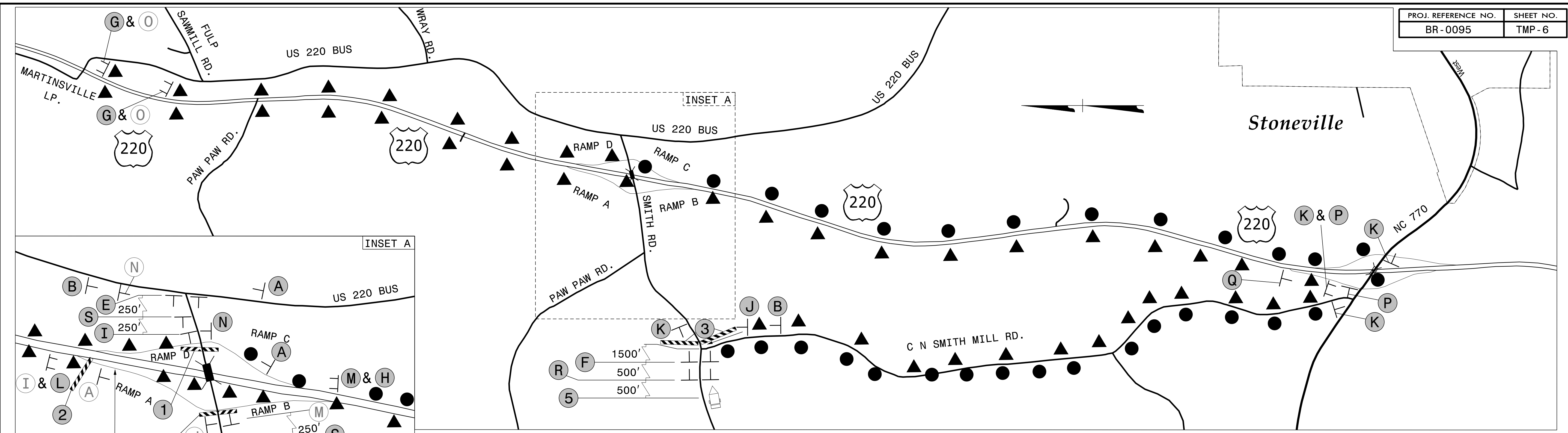
DATE: 01/04/2024

**DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED**



**SMITH RD. & RAMP C OFFSITE DETOUR DETAILS**

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**OFFSITE DETOUR ROUTE LEGEND**

- ● ● EB SMITH RD. ROUTE
- ▲ ▲ ▲ WB SMITH RD. ROUTE

**SIGN LEGEND\***

- (X) UNCOVERED SIGNS
- (X) COVERED/NOT INSTALLED SIGNS (USED FOR A DIFFERENT STEP)
- \*ALL SIGNS FOR ALL DETOURS THROUGHOUT THE LIFE OF THE PROJECT ARE SHOWN


**1** TYPE III BARRICADE(S)

**2** TYPE III BARRICADE

**3** TYPE III BARRICADE

**4** TYPE III BARRICADE(S)

**5** TYPE III BARRICADE(S)

**MESSAGE NO. 1**  
US 220 RAMP CLOSED

**MESSAGE NO. 2**  
FOLLOW SMITH RD DETOUR

CHANGEABLE MESSAGE SIGN SHORT TERM

APPROVED: P.E.  
DATE: 01/04/2024

**SMITH RD. RAMP A & RAMP B OFFSITE DETOUR DETAILS**

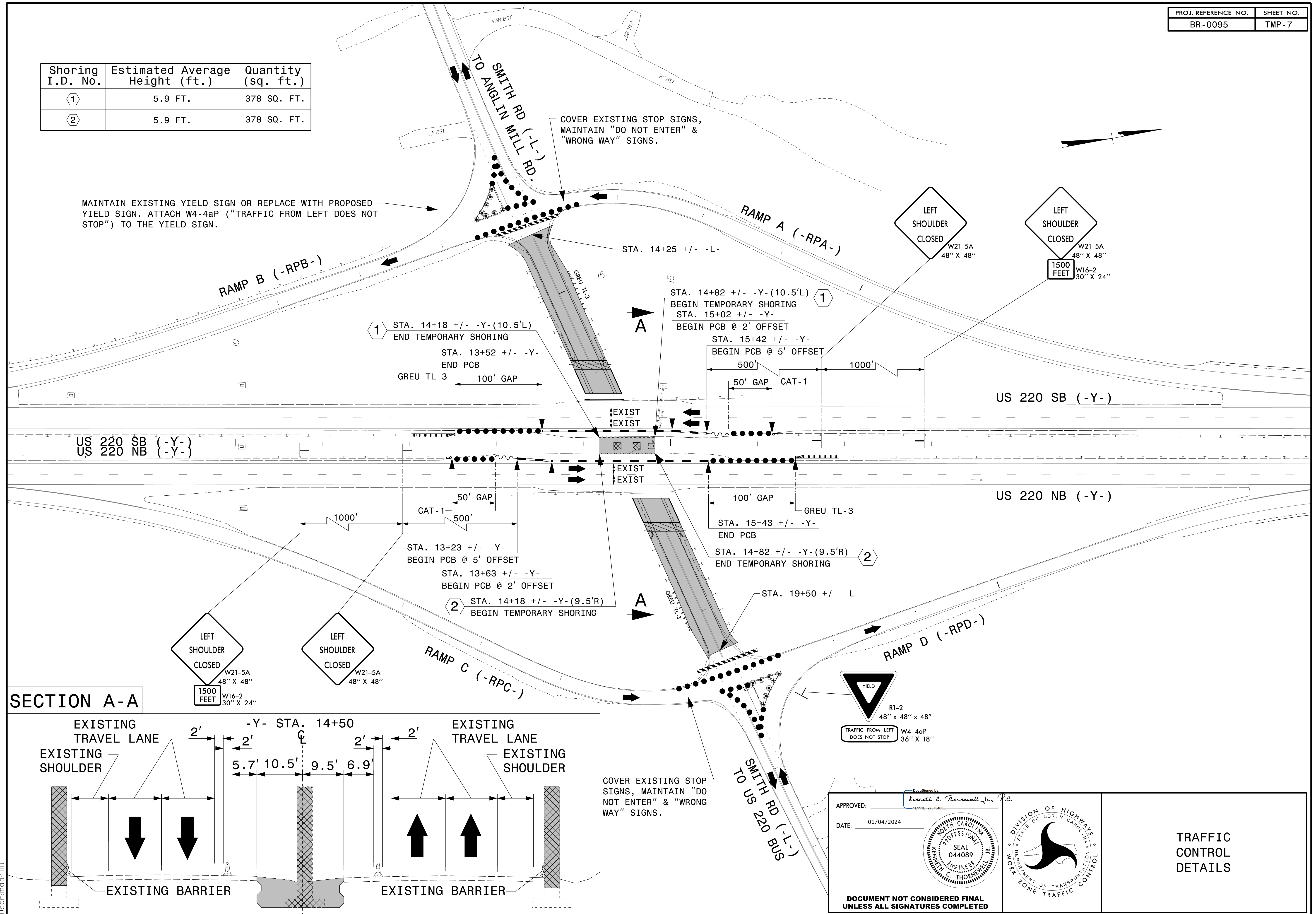
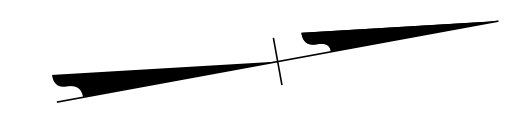
DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

1/4/2024 P:\TIP\Projects\BR-0095\TrafficControl\CPN\BR-0095\_TC\_TMP\_6.dgn User:mackfilu

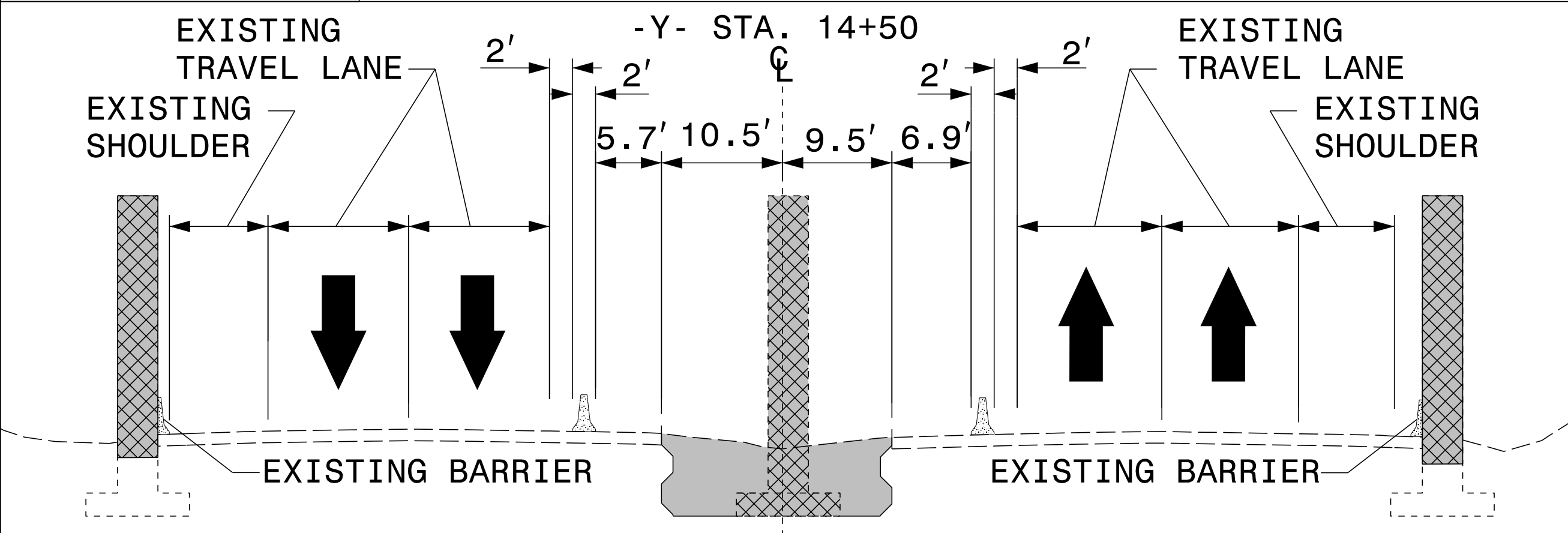
Shoring I.D. No.	Estimated Average Height (ft.)	Quantity (sq. ft.)
①	5.9 FT.	378 SQ. FT.
②	5.9 FT.	378 SQ. FT.

MAINTAIN EXISTING YIELD SIGN OR REPLACE WITH PROPOSED YIELD SIGN. ATTACH W4-4aP ("TRAFFIC FROM LEFT DOES NOT STOP") TO THE YIELD SIGN.

COVER EXISTING STOP SIGNS, MAINTAIN "DO NOT ENTER" & "WRONG WAY" SIGNS.



**SECTION A-A**



COVER EXISTING STOP SIGNS, MAINTAIN "DO NOT ENTER" & "WRONG WAY" SIGNS.

APPROVED: *Kenneth C. Thornwell Jr., P.E.*  
 DATE: 01/04/2024

SEAL 044089  
 ENGINEER  
 C. THORNWELL

DOCUMENT NOT CONSIDERED FINAL  
 UNLESS ALL SIGNATURES COMPLETED



TRAFFIC CONTROL DETAILS

1/4/2024  
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 User:smackillu