

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

DURHAM COUNTY

LOCATION: I-85 FROM THE ORANGE COUNTY LINE TO US 15/US 501
TYPE OF WORK: PAVEMENT REHABILITATION, BRIDGE PRESERVATION, SIGNING, AND SIGNALS

STATE	STATE PROJECT REFERENCE NO.	SHEET NO.	TOTAL SHEETS
N.C.	I-5941	1	
STATE PROJ. NO.	F.A. PROJ. NO.	DESCRIPTION	
45884.1.1	N/A	P.E.	
45884.3.1	4588401	CONST.	

**DOCUMENT NOT CONSIDERED FINAL
UNLESS ALL SIGNATURES COMPLETED**

2024 ROADWAY ENGLISH STANDARD DRAWINGS EFF. 01-16-2024
REV.

The following Roadway Standards as appear in "Roadway Standard Drawings" Contracts Standards and Development Unit - N. C. Department of Transportation - Raleigh, N. C., Dated January 16, 2024 are applicable to this project and by reference hereby are considered a part of these plans:

- | STD.NO. | TITLE |
|---|--|
| DIVISION 5 - SUBGRADE, BASES AND SHOULDERS | |
| 560.01 | Method of Shoulder Construction - Method I |
| 560.02 | Method of Shoulder Construction - Method II |
| DIVISION 6 - ASPHALT BASES AND PAVEMENTS | |
| 665.01 | Asphalt Shoulder Milled Rumble Strips |
| 665.02 | Limits for Asphalt Shoulder Milled Rumble Strips |
| DIVISION 8 - INCIDENTALS | |
| 840.66 | Drainage Structure Steps |
| 846.01 | Concrete Curb, Gutter and Curb & Gutter |
| 846.02 | Drop Inlet Installation in Expressway Gutter |
| 846.04 | Drop Inlet Installation in Shoulder Berm Gutter |
| 848.01 | Concrete Sidewalk |
| 848.06 | Curb Ramp |
| 850.01 | Concrete Paved Ditches |
| 852.01 | Concrete Islands |
| 862.01 | Guardrail Placement |
| 862.02 | Guardrail Installation |
| 862.03 | Structure Anchor Units |
| 862.04 | Anchoring End of Guardrail |
| 866.01 | Chain Link Fence |
| 866.05 | Glare Screen |
| 876.01 | Rip Rap in Channels and Ditches |
| 876.02 | Guide for Rip Rap at Pipe Outlets |
| DIVISION 12 - PAVEMENT MARKINGS, MARKERS AND DELINEATION | |
| 1205.01 | Pavement Markings |
| 1205.02 | Pavement Markings |
| 1205.03 | Pavement Markings |
| 1205.05 | Pavement Markings |
| 1205.06 | Pavement Markings |
| 1205.07 | Pavement Markings |
| 1205.08 | Pavement Markings |
| 1205.13 | Pavement Markings |
| 1250.01 | Raised Pavement Markers |
| 1261.01 | Guardrail and Barrier Delineators |
| 1261.02 | Guardrail & Barrier Delineators |
| 1262.01 | Guardrail End Delineation |
| DIVISION 16 - EROSION CONTROL AND ROADSIDE DEVELOPMENT | |
| 1605.01 | Temporary Silt Fence |
| 1607.01 | Gravel Construction Entrance |
| 1622.01 | Guide for Temporary Berms and Slope Drain |
| 1631.01 | Matting Installation |
| 1632.03 | Rock Inlet Sediment Trap Type C |
| 1633.01 | Temporary Rock Silt Check Type A |
| 1633.02 | Temporary Rock Silt Check Type B |
| 1635.02 | Rock Pipe Inlet Sediment Trap Type B |
| 1636.01 | Wattle Check |

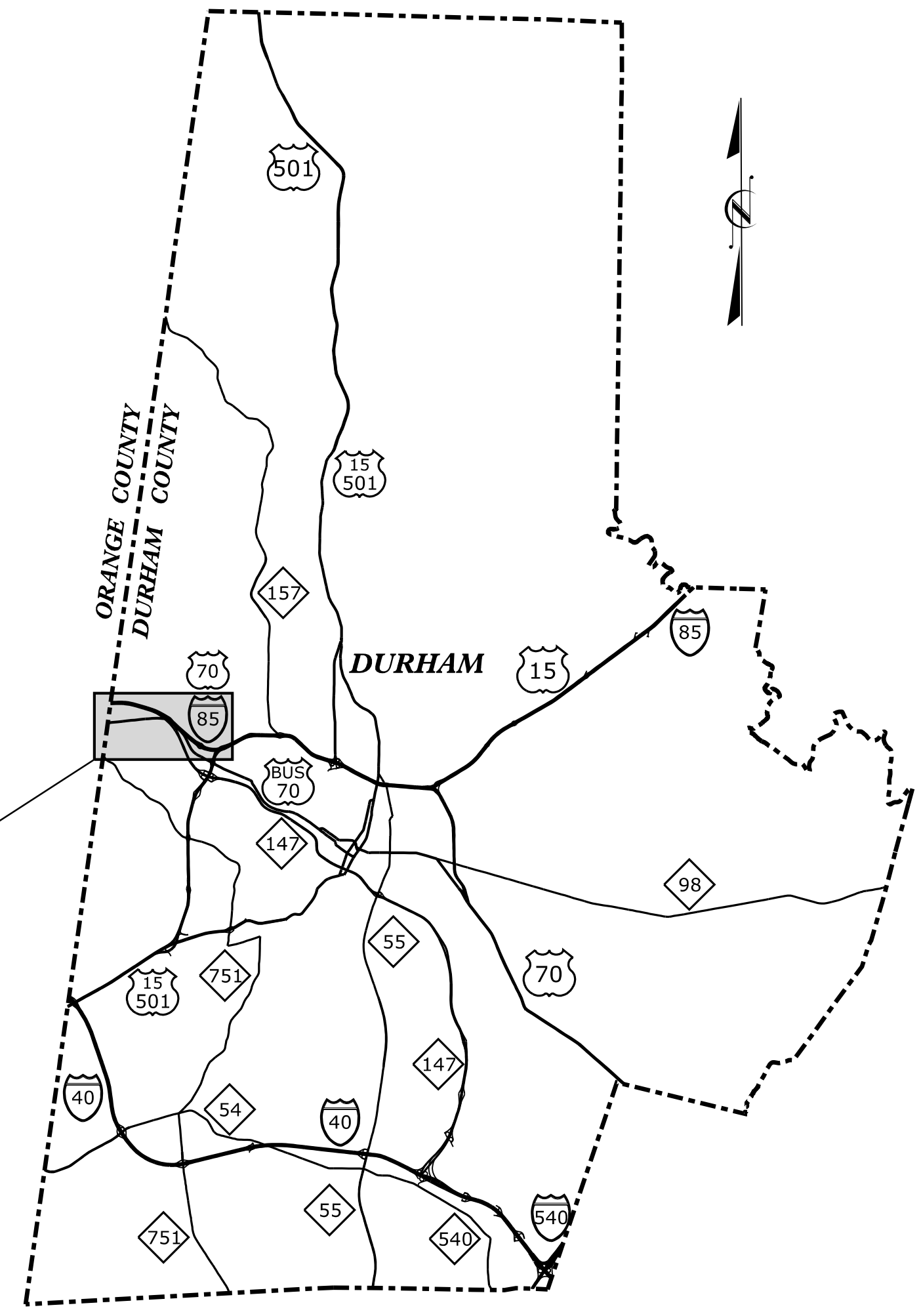
TIP PROJECT: I-5941

CONTRACT: C204885

INDEX OF SHEETS

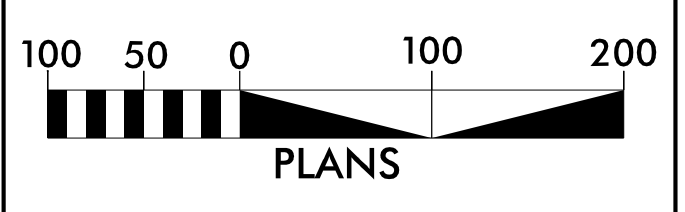
SHEET NO.	DESCRIPTION
1	TITLE SHEET
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**PROJECT LOCATION
(SEE SHEET 1A FOR
MAP DETAILS)**



VICINITY MAP (N.T.S)

GRAPHIC SCALES



DESIGN DATA

2024 ADT = 76,800
T = 9%
POSTED SPEED = 65 MPH
FUNC CLASS = INTERSTATE

PROJECT LENGTH

LENGTH OF ROADWAY TIP PROJECT I-5941 = 2.400 MILES
SEE STRUCTURE PLANS FOR STRUCTURE LENGTH

Prepared for NCDOT in the Office of:



WGI (C-4434)
5640 Dillard Drive, Suite 200
Cary, North Carolina 27518
919-852-0488 www.wginc.com

2024 STANDARD SPECIFICATIONS

RIGHT OF WAY DATE:
N/A

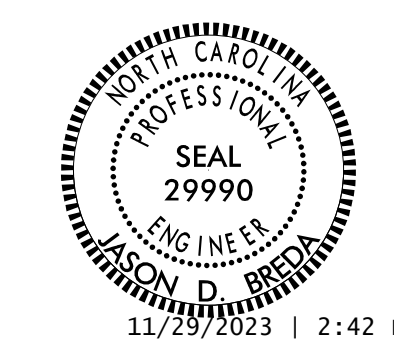
LETTING DATE:
2 /20 /2024

MARK S. GALLO
NCDOT CONTACT

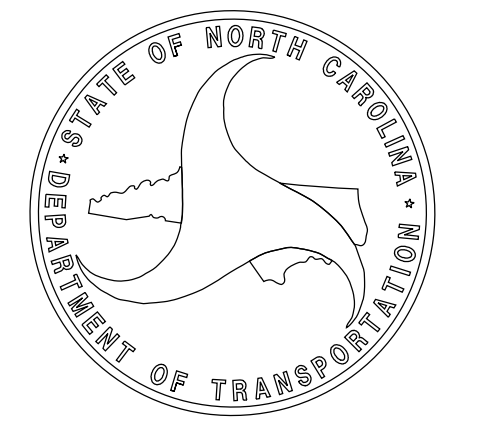
TIM S. HAYES, PE
PROJECT ENGINEER

JASON D. BREDA, PE
PROJECT DESIGN ENGINEER

ROADWAY DESIGN ENGINEER

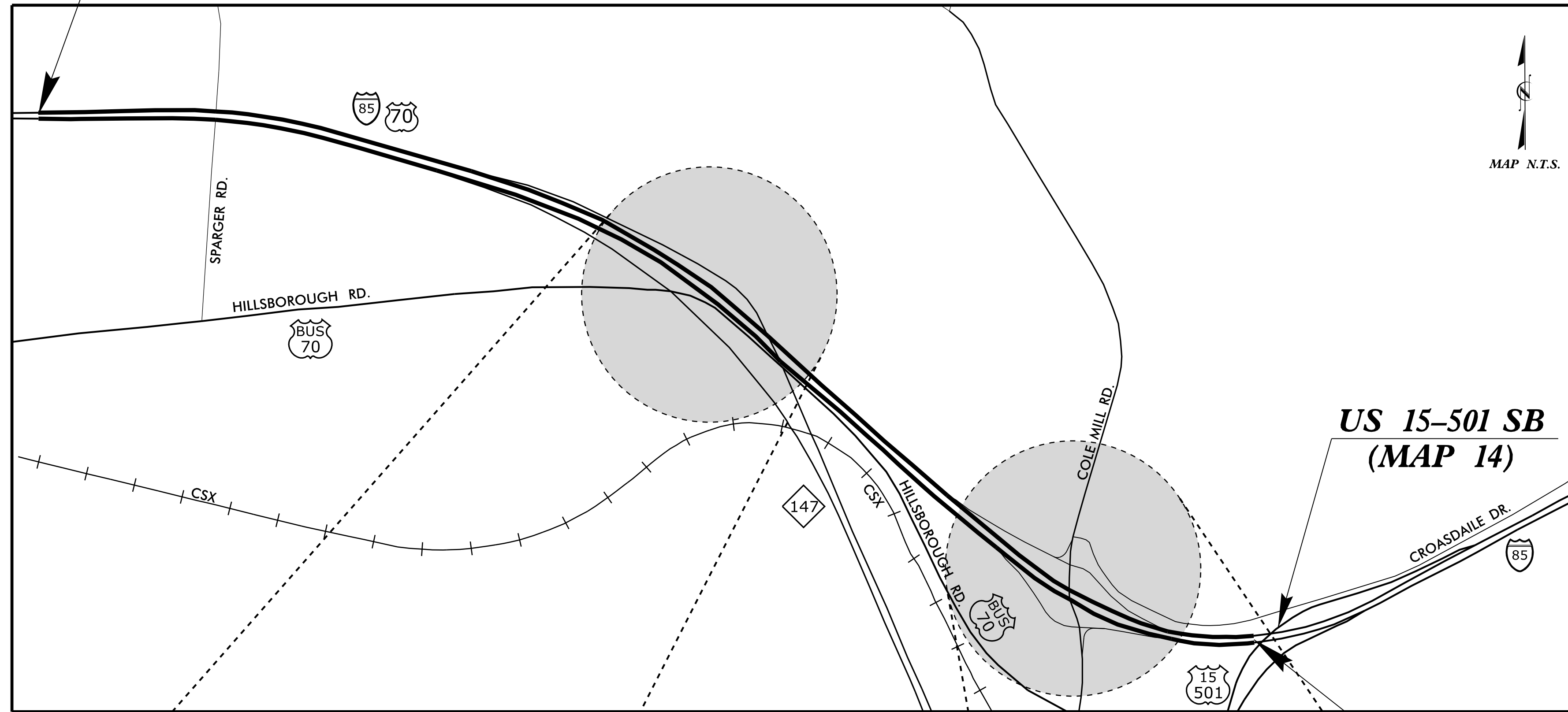


DocuSigned by:
Jason D. Breda P.E.
SIGNATURE



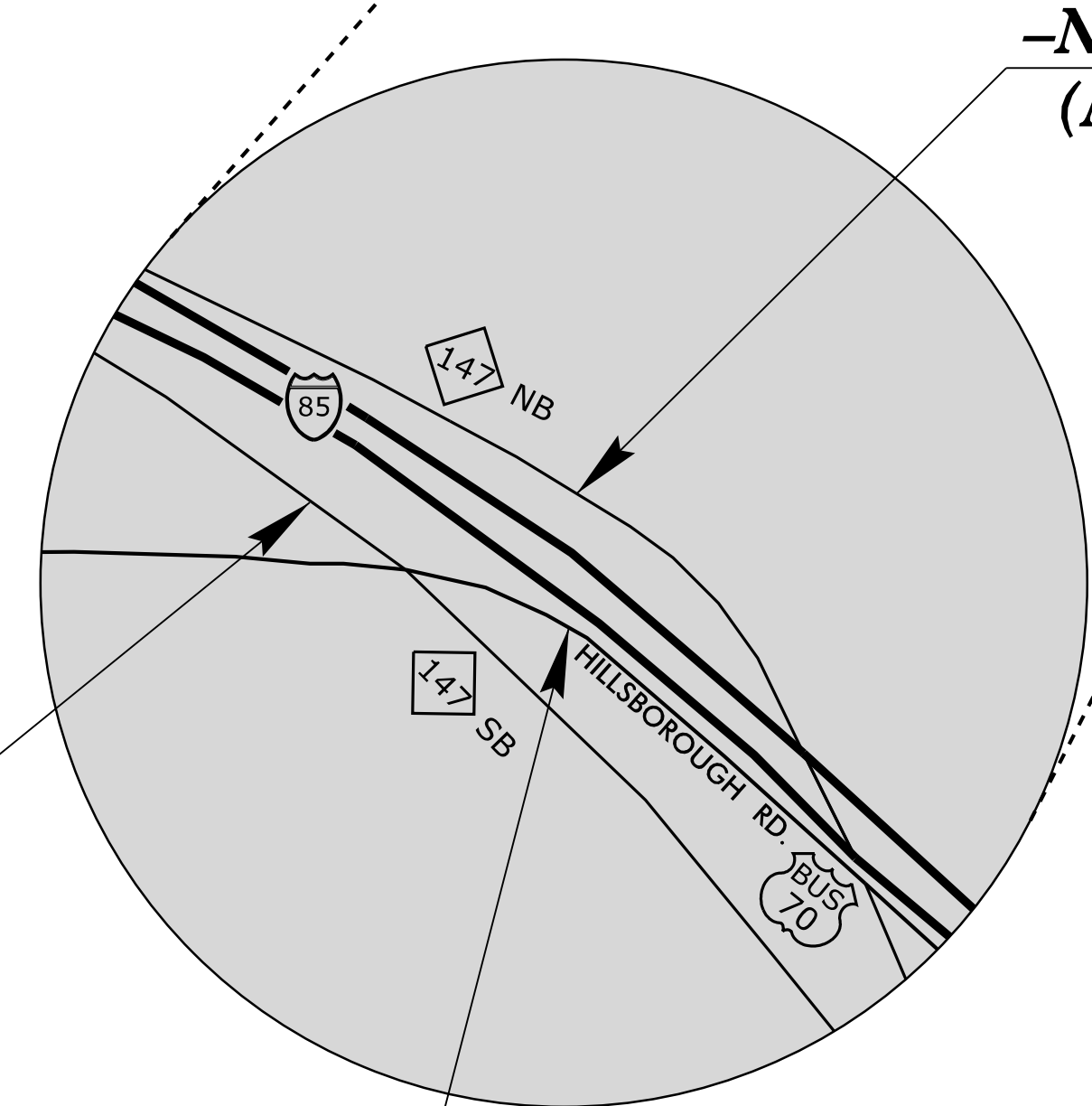
BEGIN TIP PROJECT I-5941
-I85NBL- STA. 102+50 (MAP 1)
-I85SBL- STA. 302+50 (MAP 2)

MAP DETAILS



**US 15-501 SB
(MAP 14)**

END TIP PROJECT I-5941
-I85NBL- STA. 232+07 (MAP 1)
-I85SBL- STA. 432+09 (MAP 2)



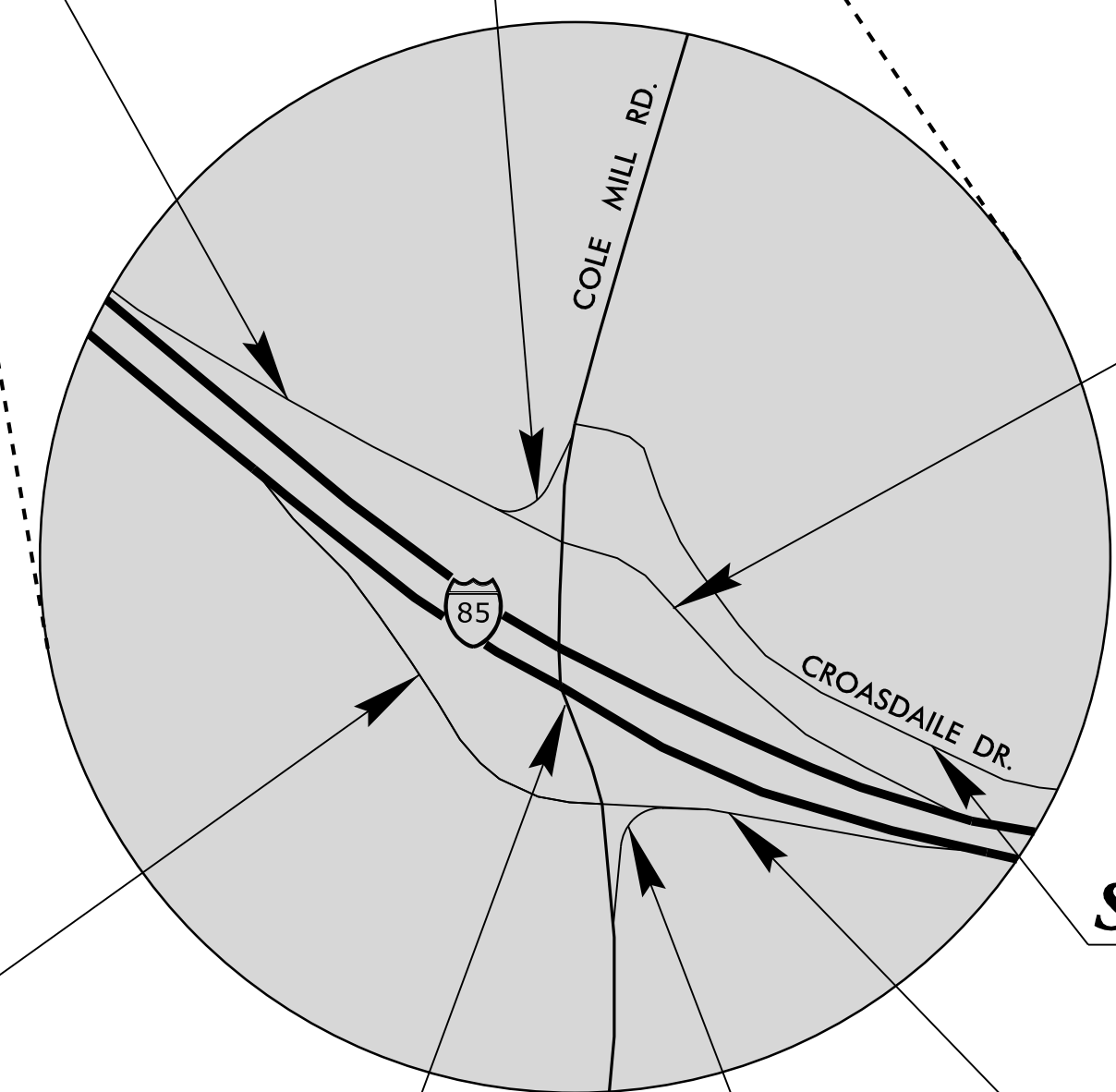
**-NCI47NB-
(MAP 4)**

**-NCI47SB-
(MAP 3)**

**US 70 BUS (HILLSBOROUGH RD)
(MAP 11)**

**-CMRAMPB-
(MAP 6)**

**-CMSPURB-
(MAP 9)**



**-CMRAMPA-
(MAP 5)**

**-CMRAMPC-
(MAP 7)**

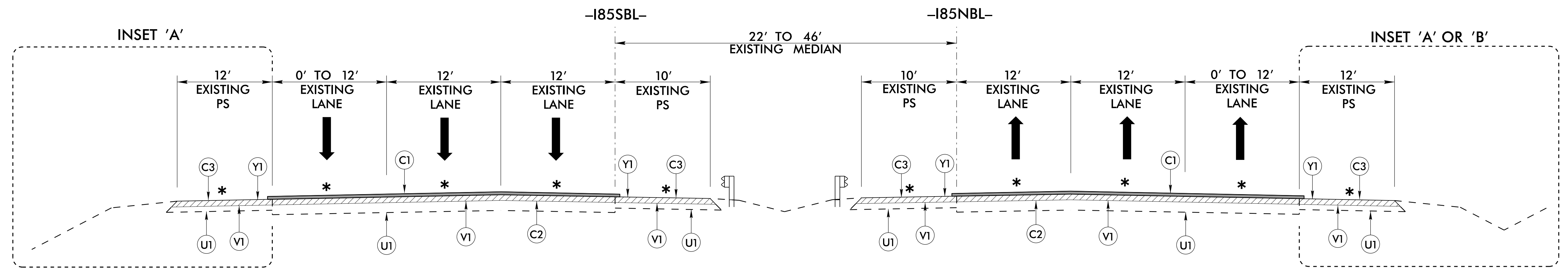
**SR 1401 (COLE MILL RD)
(MAP 12)**

**SR 1493 (CROASDAILE DR)
(MAP 13)**

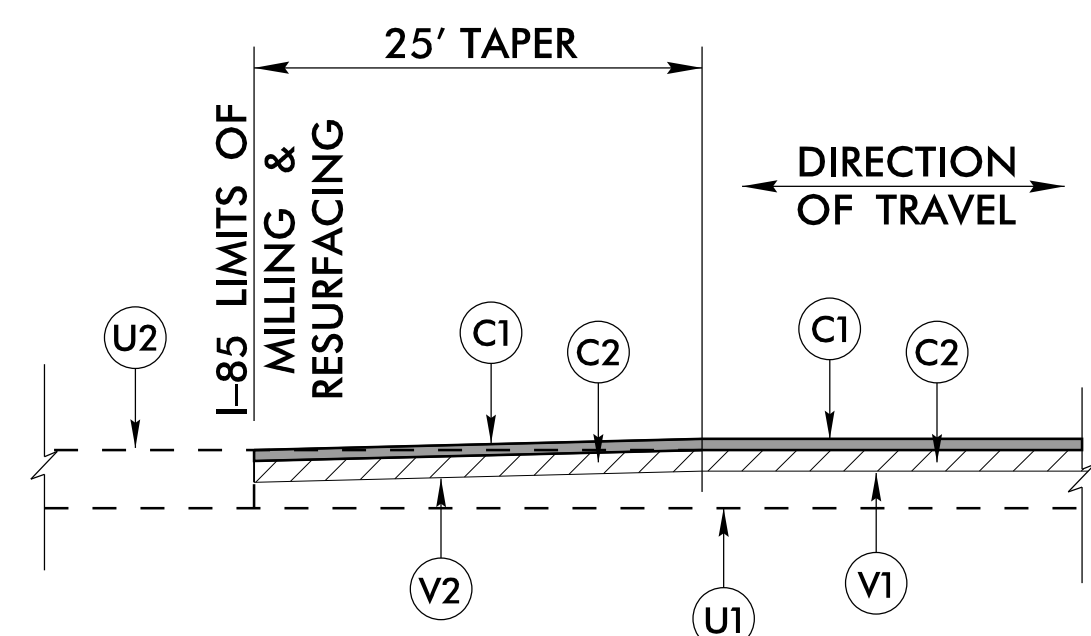
**-CMRAMPD-
(MAP 8)**

**-CMSPURD-
(MAP 10)**

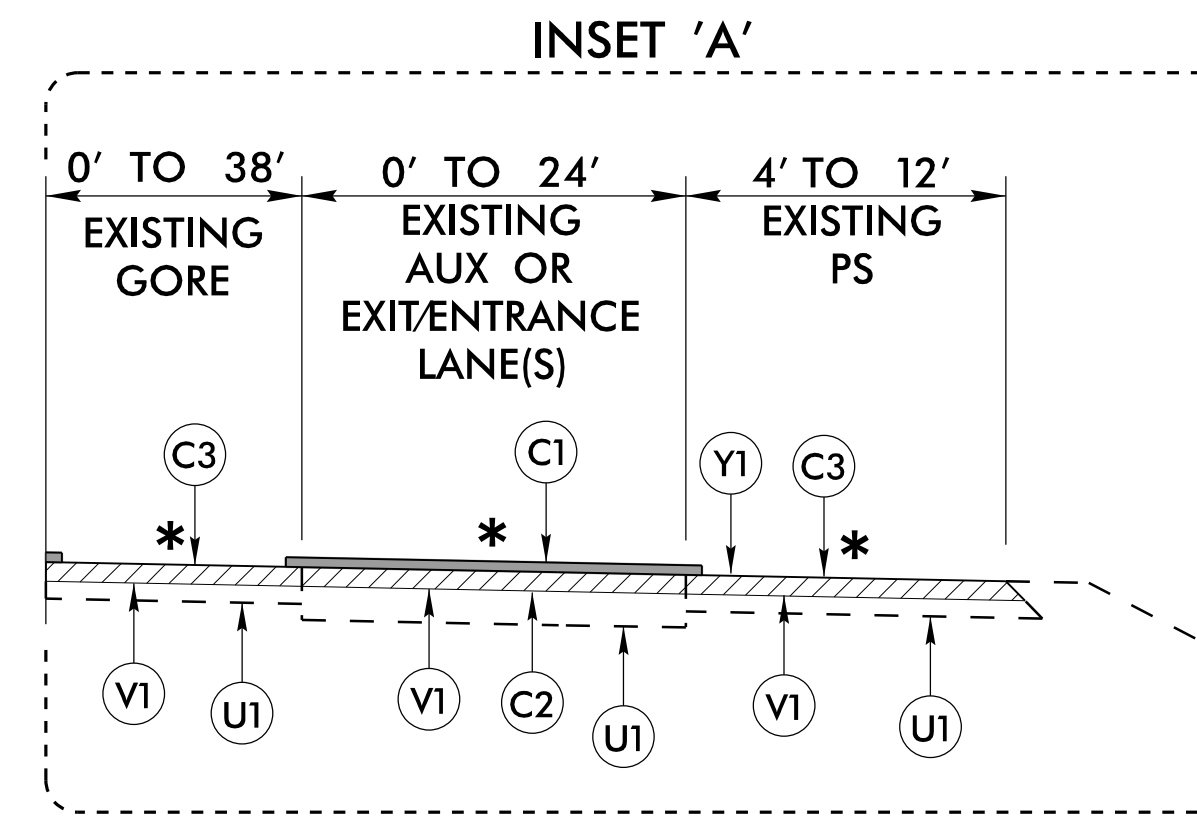
8/17/99
4/25/01
1:1 Roadway/Map/01-I-5941-RDY-MapDetails.dwg



-185NBL- STA. 102+50 TO STA. 212+89 (BEGIN APPROACH SLAB) } (MAP 1)
 -185NBL- STA. 215+76 (END APPROACH SLAB) TO STA. 232+07 }
 -185SBL- STA. 302+50 TO STA. 412+77 (BEGIN APPROACH SLAB) } (MAP 2)
 -185SBL- STA. 415+66 (END APPROACH SLAB) TO STA. 432+09 }

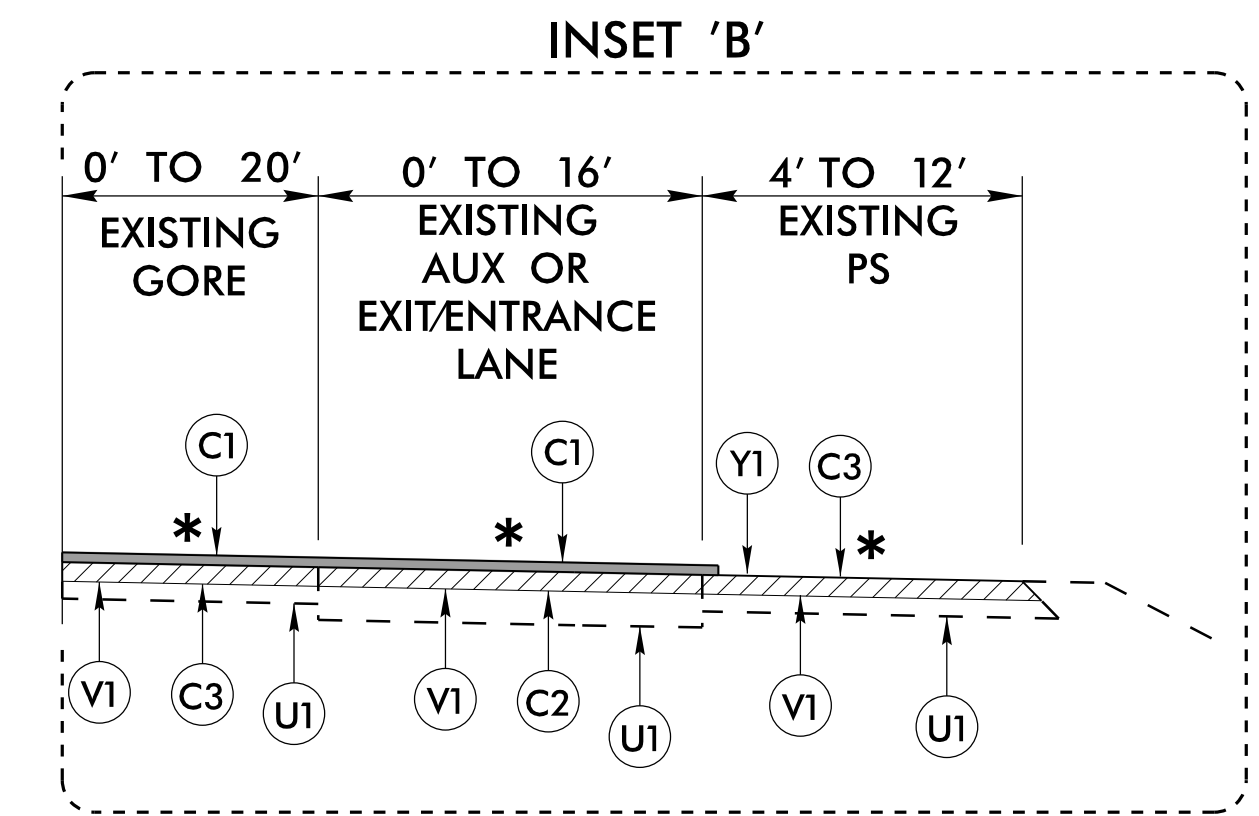


ULTRA-THIN BONDED WEARING COURSE TIE-IN DETAIL
 USE AT I-85 BEGIN/END PROJECT, BRIDGE, AND COLE MILL RD RAMP TIE-INS
 (DO NOT USE AT NC 147 TIE-INS)



USE INSET 'A' AS FOLLOWS:

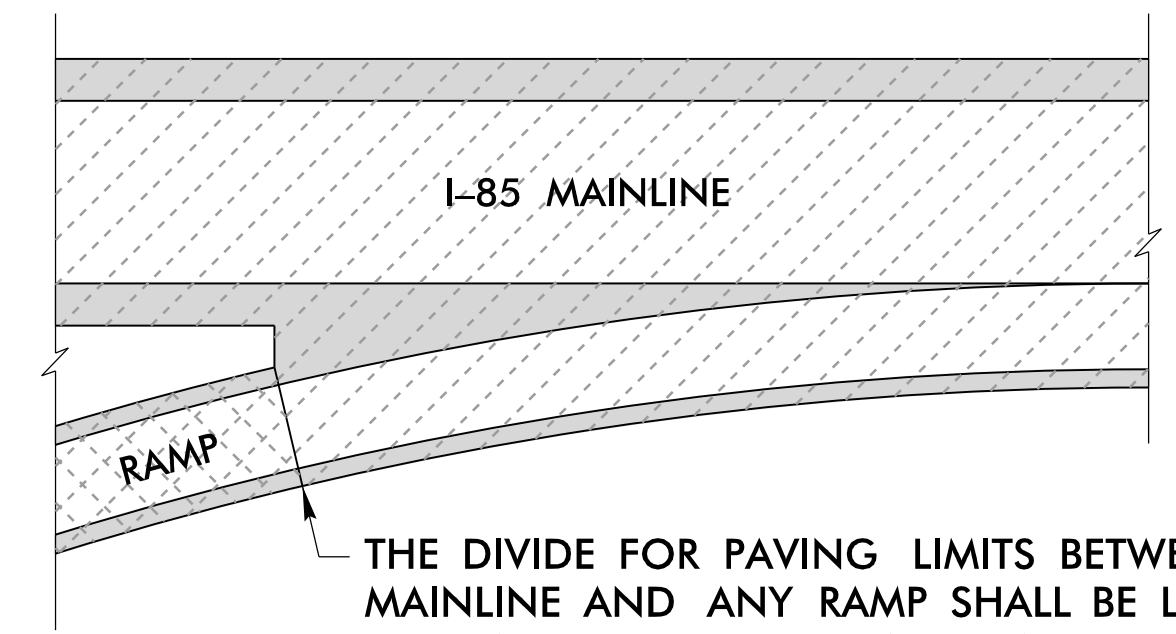
-185NBL- STA. 126+30 TO STA. 151+96 -185SBL- STA. 349+20 TO STA. 364+60
 -185NBL- STA. 207+80 TO STA. 208+85 -185SBL- STA. 389+05 TO STA. 404+26
 -185NBL- STA. 220+48 TO STA. 232+07 -185SBL- STA. 419+54 TO STA. 432+09



USE INSET 'B' AS FOLLOWS:
 -185NBL- STA. 201+40 TO STA. 207+80

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 5/8" ULTRA-THIN BONDED WEARING COURSE, AT AN AVERAGE RATE OF 70 LBS. PER SQ. YD.
C2	PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
C3	PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
E1	PROP. APPROX. 4.5" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 513 LBS. PER SQ. YD.
T1	EARTH MATERIAL
U1	EXISTING ASPHALT PAVEMENT
U2	EXISTING OR PROP. PAVEMENT OR BRIDGE APPROACH SLAB
V1	MILLING EXISTING ASPHALT, 2" DEPTH
V2	MILLING EXISTING ASPHALT, VAR. DEPTH (2" TO 2 5/8")
Y1	RUMBLE STRIPS

I-85 MAINLINE MAP QUANTITIES
 RAMP MAP QUANTITIES



TYPICAL DIVIDE FOR PAVING LIMITS DETAIL
 USE AT ALL I-85 MAINLINE RAMP CONNECTIONS

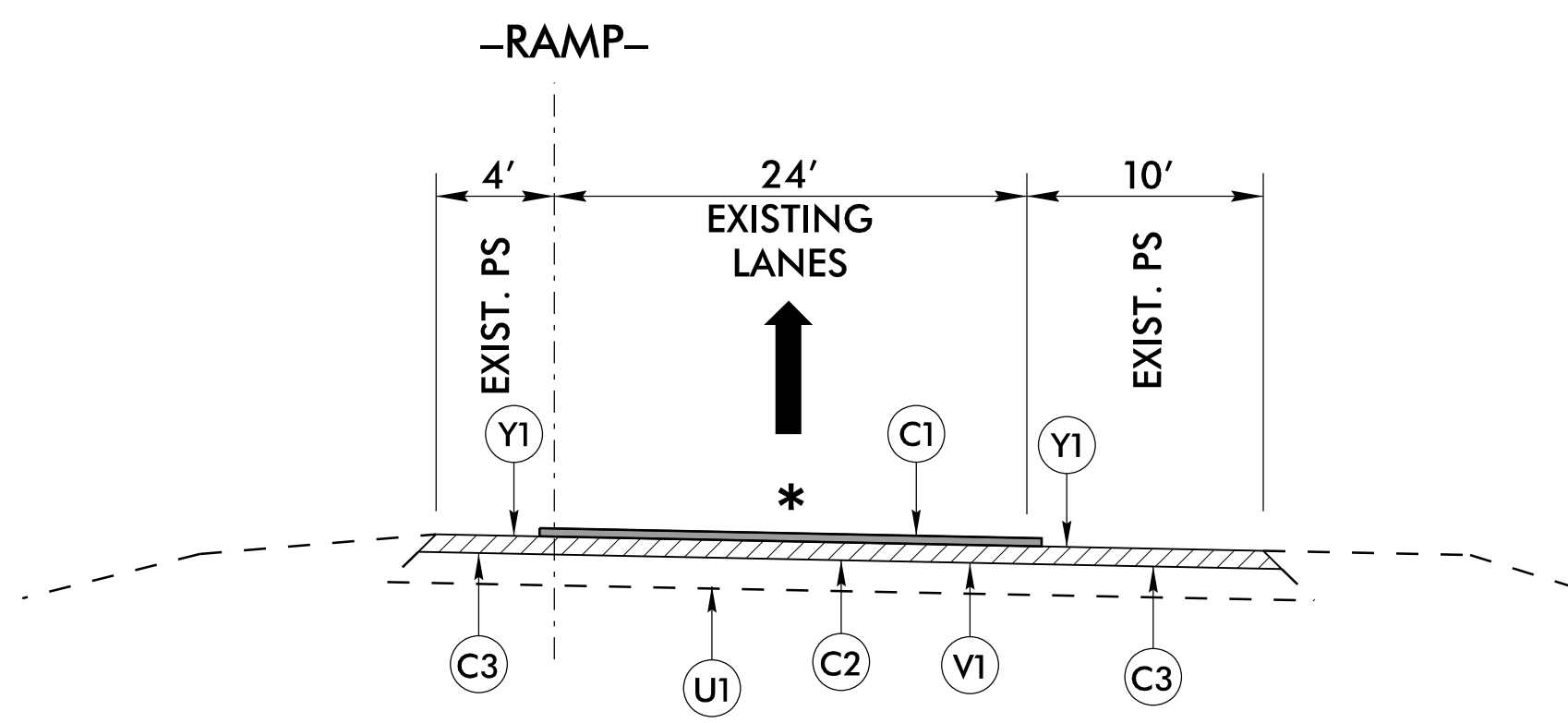
NOTES

- TYPICAL SECTION STATIONS (STA.) SHOWN ARE APPROXIMATE AND MAY ADJUST AS DETERMINED BY THE ENGINEER.
- MILLED I-85 TRAVEL LANES AND PAVED SHOULDERS SHALL BE RESURFACED WITH ASPHALT CONCRETE SURFACE COURSE BY THE END OF EACH WORK DAY.
- ULTRA-THIN BONDED WEARING COURSE SHALL BE EXTENDED 6" OUTSIDE OF TRAVEL LANES.
- PAVED SHOULDER WIDTHS SHOWN ARE WIDER AT GUARDRAIL, CONC. BARRIER, CONC. GUTTER, AND BRIDGE LOCATIONS. MILL AND RESURFACE THE ENTIRE PAVED SHOULDER WIDTH AT THESE LOCATIONS.
- NEW GUARDRAIL UNIFORM OFFSETS FROM TRAVEL LANES SHALL MATCH EXISTING, EXCEPT AS DETERMINED BY THE ENGINEER.

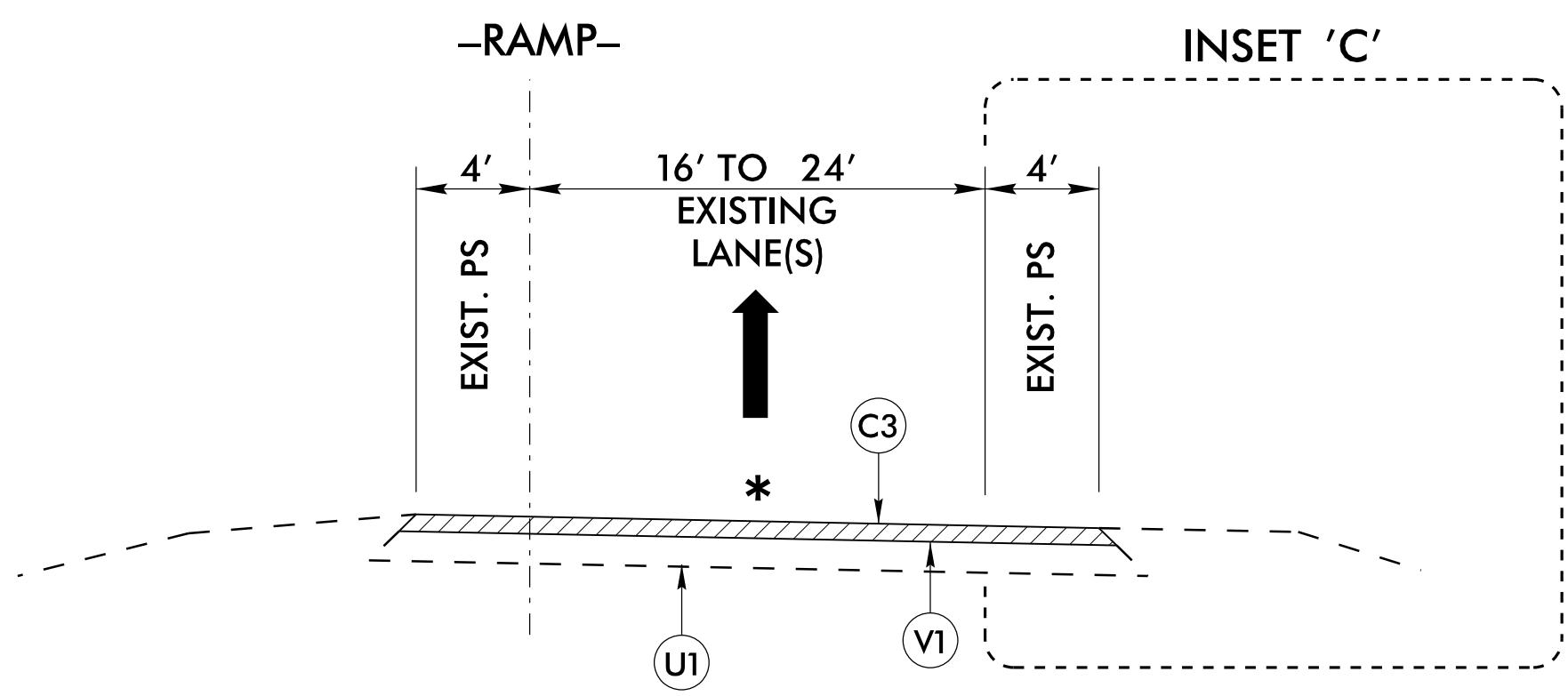
* MATCH EXIST. CROSS SLOPES

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4/25/01
J:\Roadway\I-85\I-594I\RDY.tup.dgn

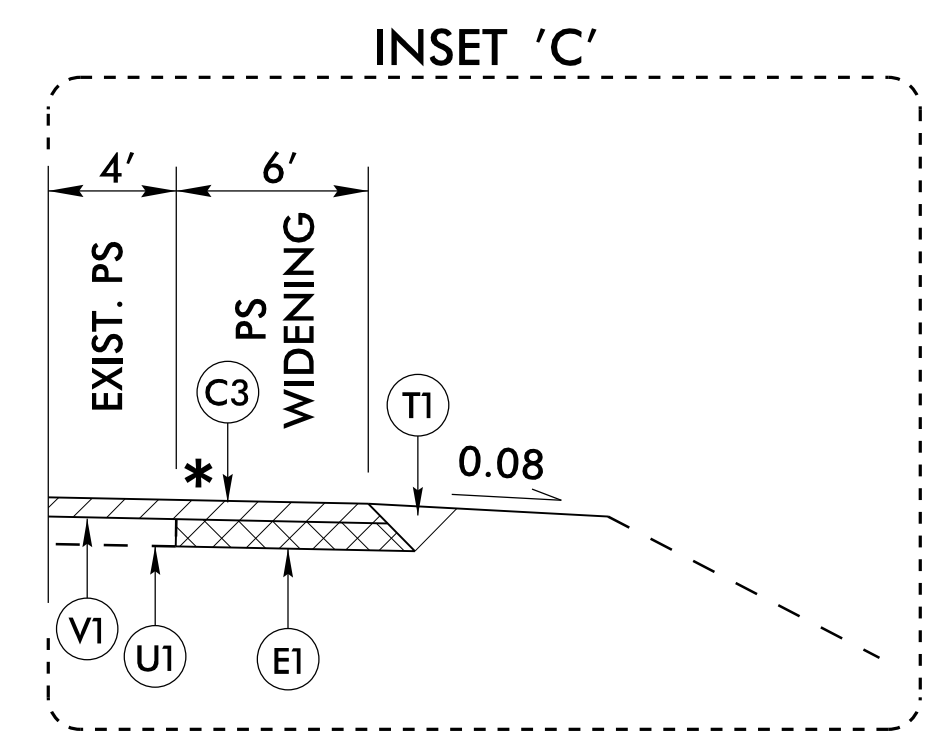
NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.



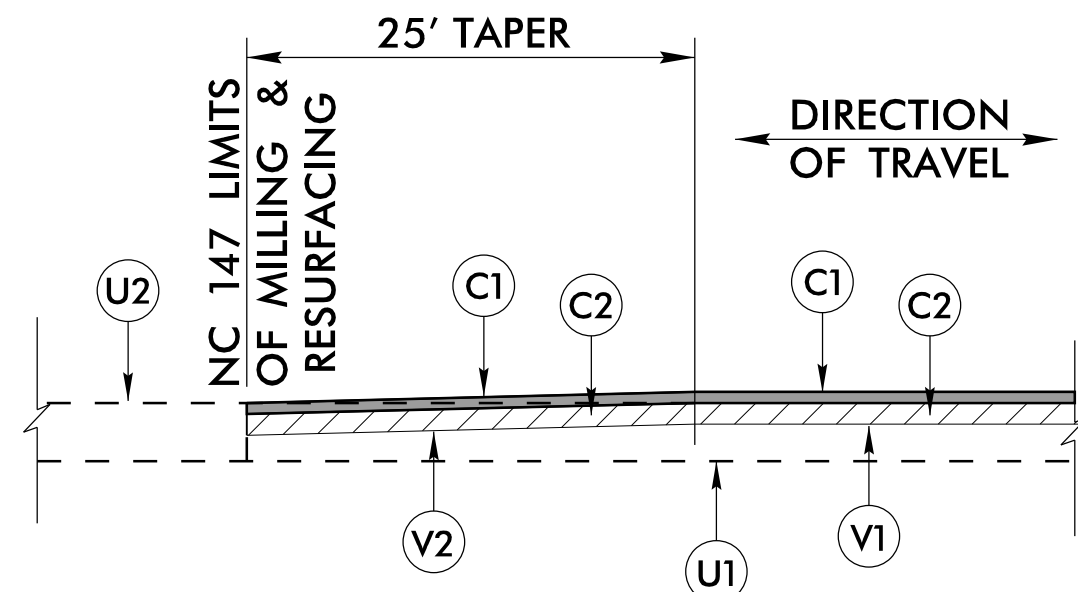
TYPICAL SECTION NO. 2
 USE TYPICAL SECTION NO. 2 AS FOLLOWS:
 -NC147SB- STA. 10+00 TO STA. 22+75 (MAP 3)
 -NC147NB- STA. 12+21 TO STA. 21+85 (MAP 4)



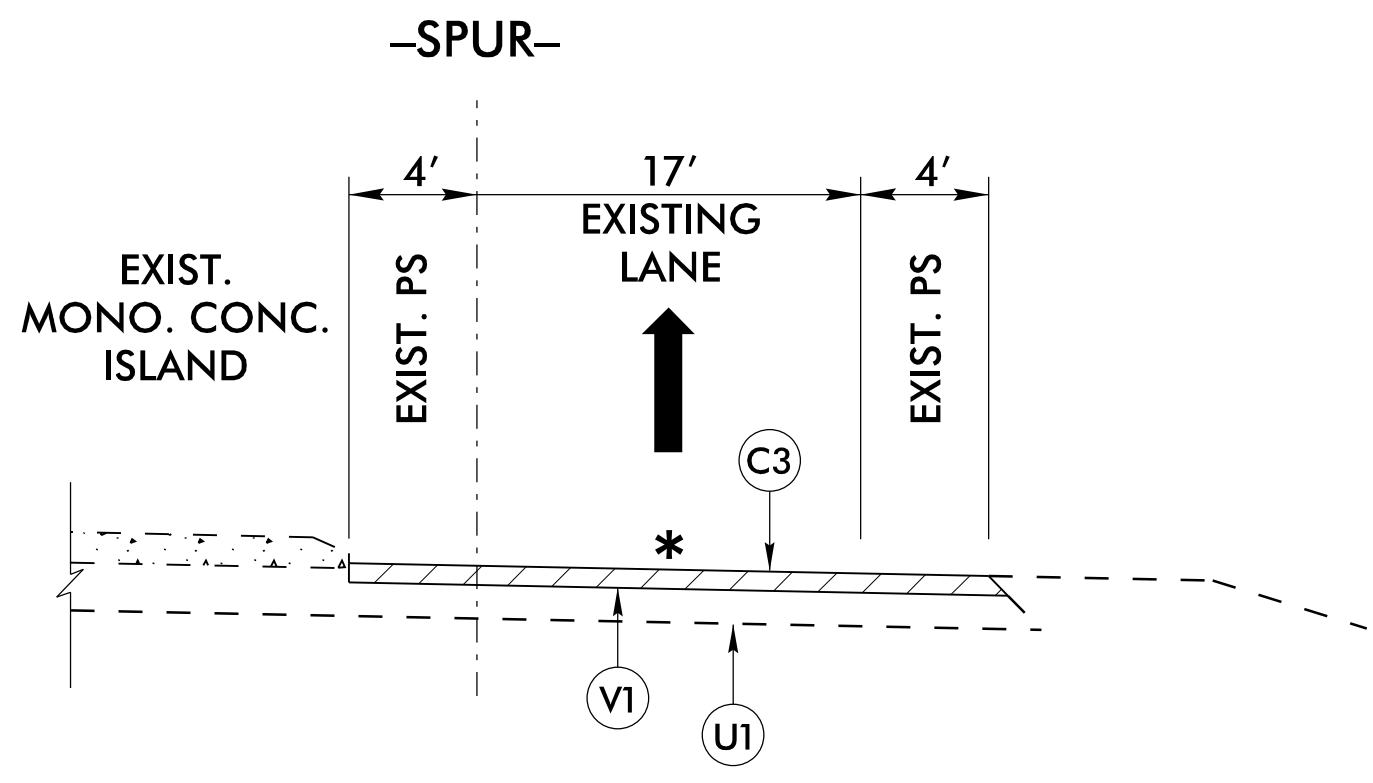
TYPICAL SECTION NO. 3
 USE TYPICAL SECTION NO. 3 AS FOLLOWS:
 -CMRAMPA- STA. 10+00 TO STA. 16+29 (MAP 5)
 -CMRAMPB- STA. 10+45 TO STA. 18+13 (MAP 6)
 -CMRAMPC- STA. 10+00 TO STA. 17+31 (MAP 7)
 -CMRAMPD- STA. 10+37 TO STA. 14+77 (MAP 8)



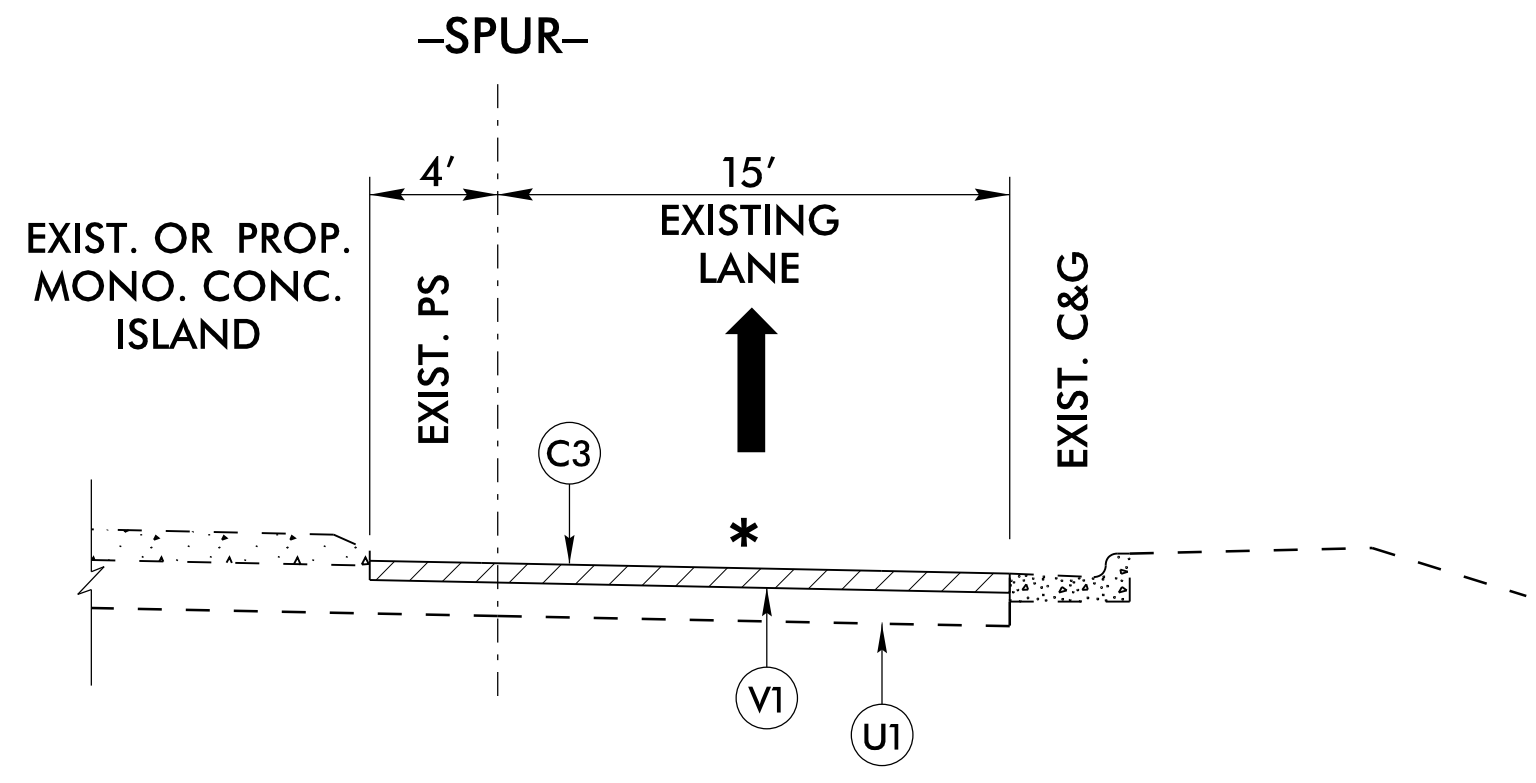
USE INSET 'C' AS FOLLOWS:
 -CMRAMPB- STA. 11+61 TO STA. 15+50 (RT)
 -CMRAMPD- STA. 11+22 TO STA. 14+75 (RT)



ULTRA-THIN BONDED WEARING COURSE TIE-IN DETAIL
 USE AT NC 147 BRIDGE TIE-INS



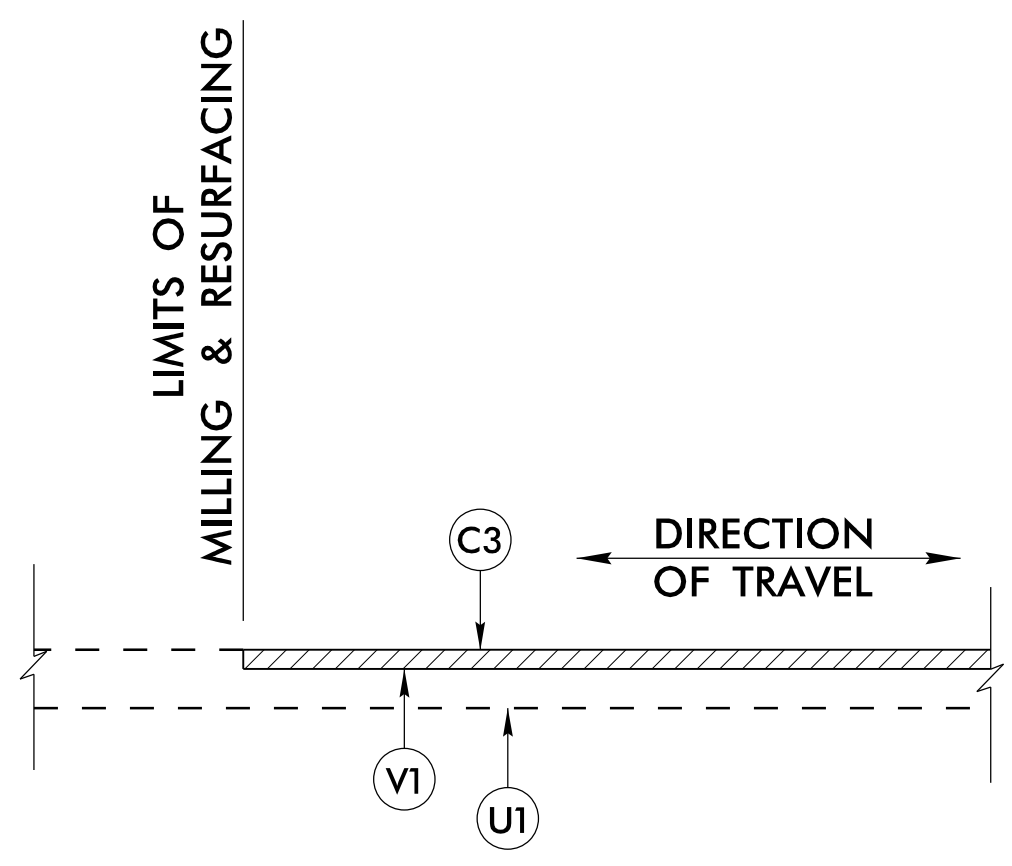
TYPICAL SECTION NO. 4
 USE TYPICAL SECTION NO. 4 AS FOLLOWS:
 -CMSPURB- STA. 10+29 TO STA. 11+30 (MAP 9)



TYPICAL SECTION NO. 5
 USE TYPICAL SECTION NO. 5 AS FOLLOWS:
 -CMSPURD- STA. 10+29 TO STA. 11+85 (MAP 10)

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 5/8" ULTRA-THIN BONDED WEARING COURSE, AT AN AVERAGE RATE OF 70 LBS. PER SQ. YD.
C2	PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
C3	PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
E1	PROP. APPROX. 4.5" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 513 LBS. PER SQ. YD.
T1	EARTH MATERIAL
U1	EXISTING ASPHALT PAVEMENT
U2	EXISTING PAVEMENT OR BRIDGE APPROACH SLAB
V1	MILLING EXISTING ASPHALT, 2" DEPTH
V2	MILLING EXISTING ASPHALT, VAR. DEPTH (2" TO 2 5/8")
Y1	RUMBLE STRIPS

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.



SURFACE COURSE TIE-IN DETAIL
 USE AT ALL COLE MILL RD RAMP AND SPUR TIE-INS

NOTES

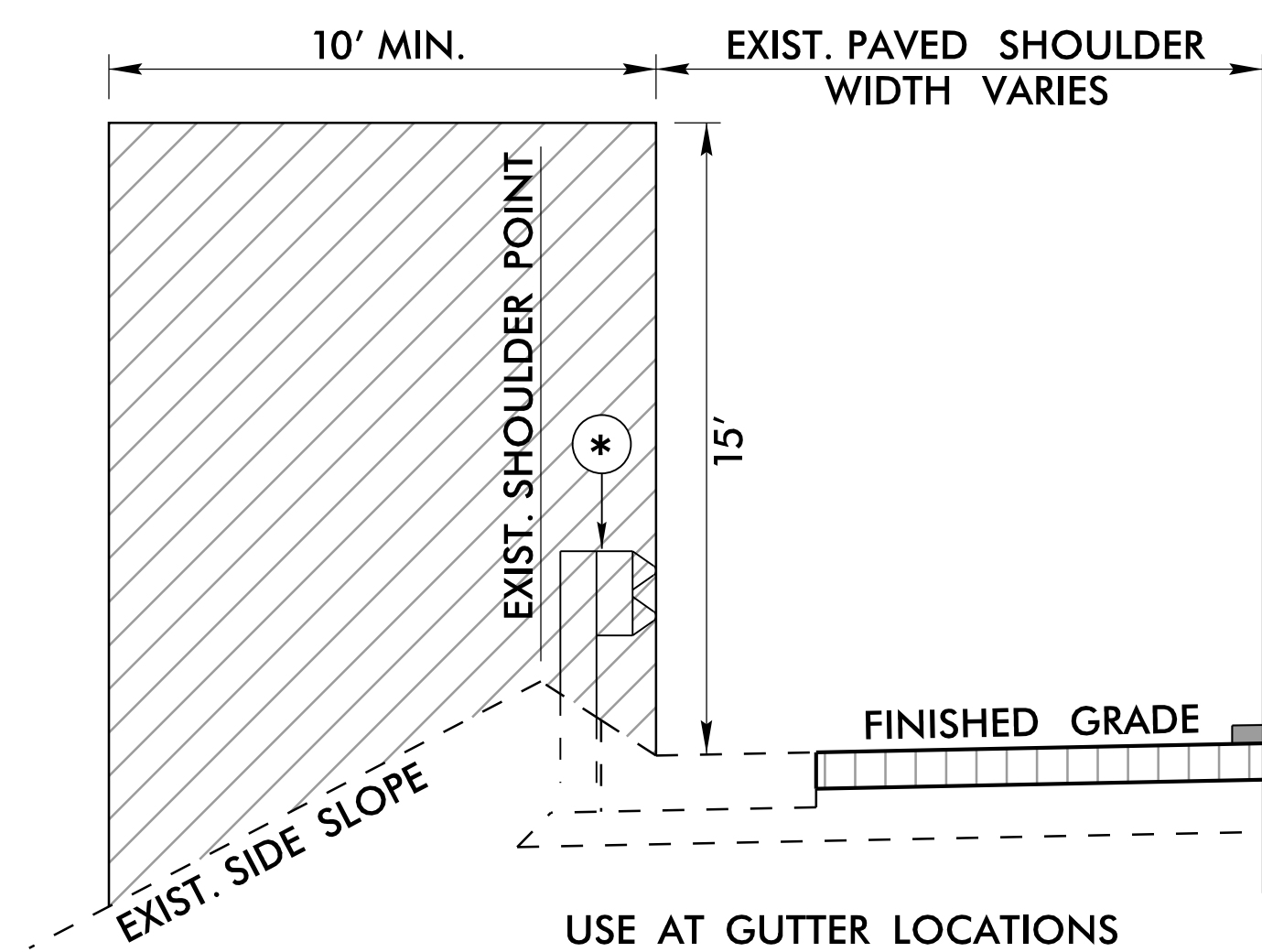
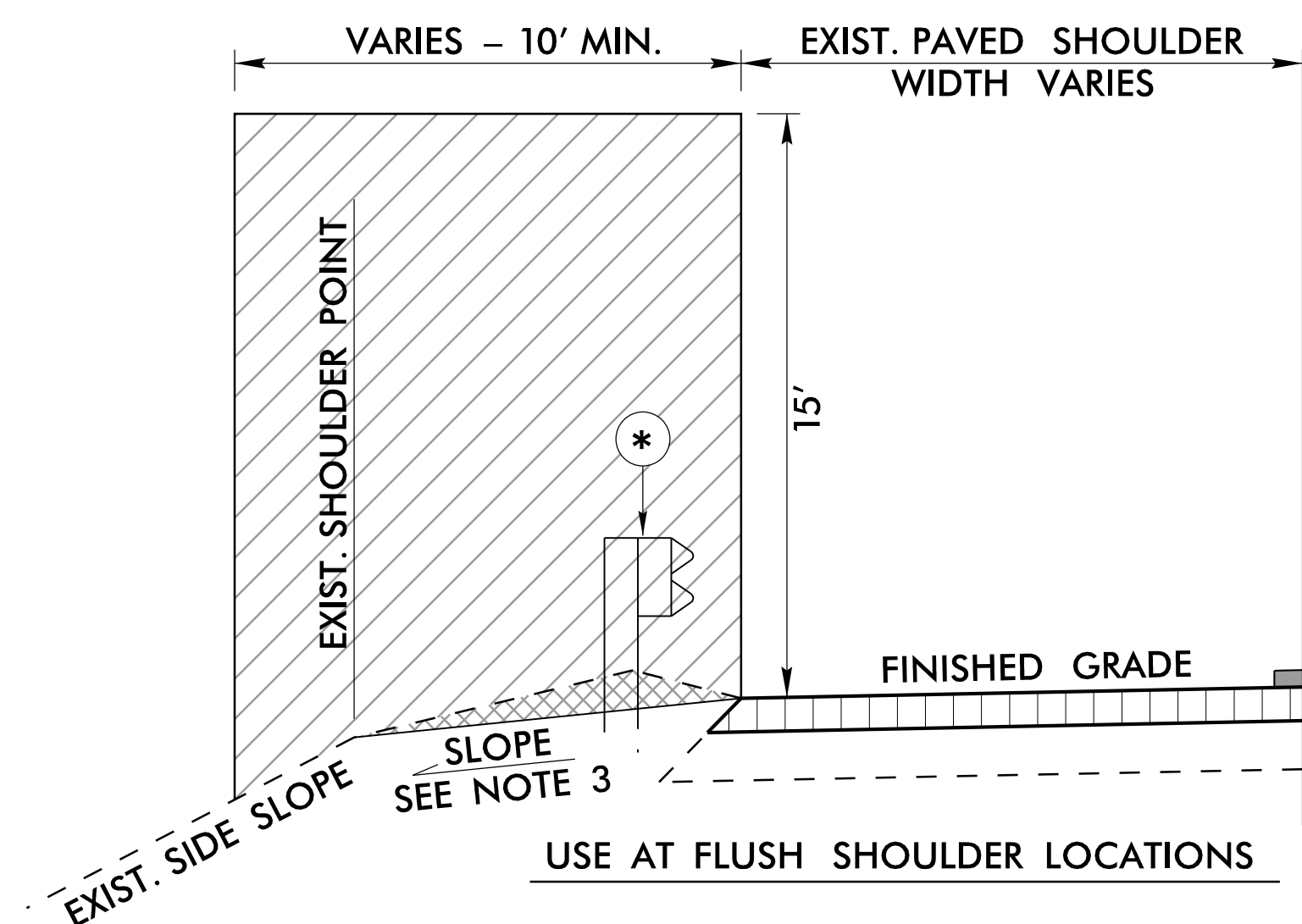
1. TYPICAL SECTION STATIONS (STA.) SHOWN ARE APPROXIMATE AND MAY BE ADJUSTED AS DETERMINED BY THE ENGINEER.
2. ALL MILLED RAMP AND SPUR TRAVEL LANES AND SHOULDERS SHALL BE RESURFACED WITH ASPHALT CONCRETE SURFACE COURSE BY THE END OF EACH WORK DAY.
3. ULTRA-THIN BONDED WEARING COURSE SHALL BE EXTENDED 6" OUTSIDE OF TRAVEL LANES.
4. PAVED SHOULDER WIDTHS SHOWN ARE WIDER AT GUARDRAIL, CONC. BARRIER, CONC. GUTTER, AND BRIDGE LOCATIONS. MILL AND RESURFACE THE ENTIRE PAVED SHOULDER WIDTH AT THESE LOCATIONS.
5. NEW GUARDRAIL UNIFORM OFFSETS FROM TRAVEL LANES SHALL MATCH EXISTING, EXCEPT AS DETERMINED BY THE ENGINEER.

* MATCH EXIST. CROSS SLOPES

6/2/99
 4/25/01
 J.A. Boudreau, P.E., 1326
 J.A. Boudreau, P.E., 1-5941, RDY, tnp.dan

GUARDRAIL REPLACEMENT, CLEARING, AND SHOULDER RECONSTRUCTION – DETAIL A

N.T.S.



- * REMOVE AND REPLACE EXISTING GUARDRAIL
- REMOVE ALL VEGETATION (SEE NOTE 2)
- REMOVE EXCESS EARTH MATERIAL (SEE NOTE 3)

USE DETAIL A AS FOLLOWS:

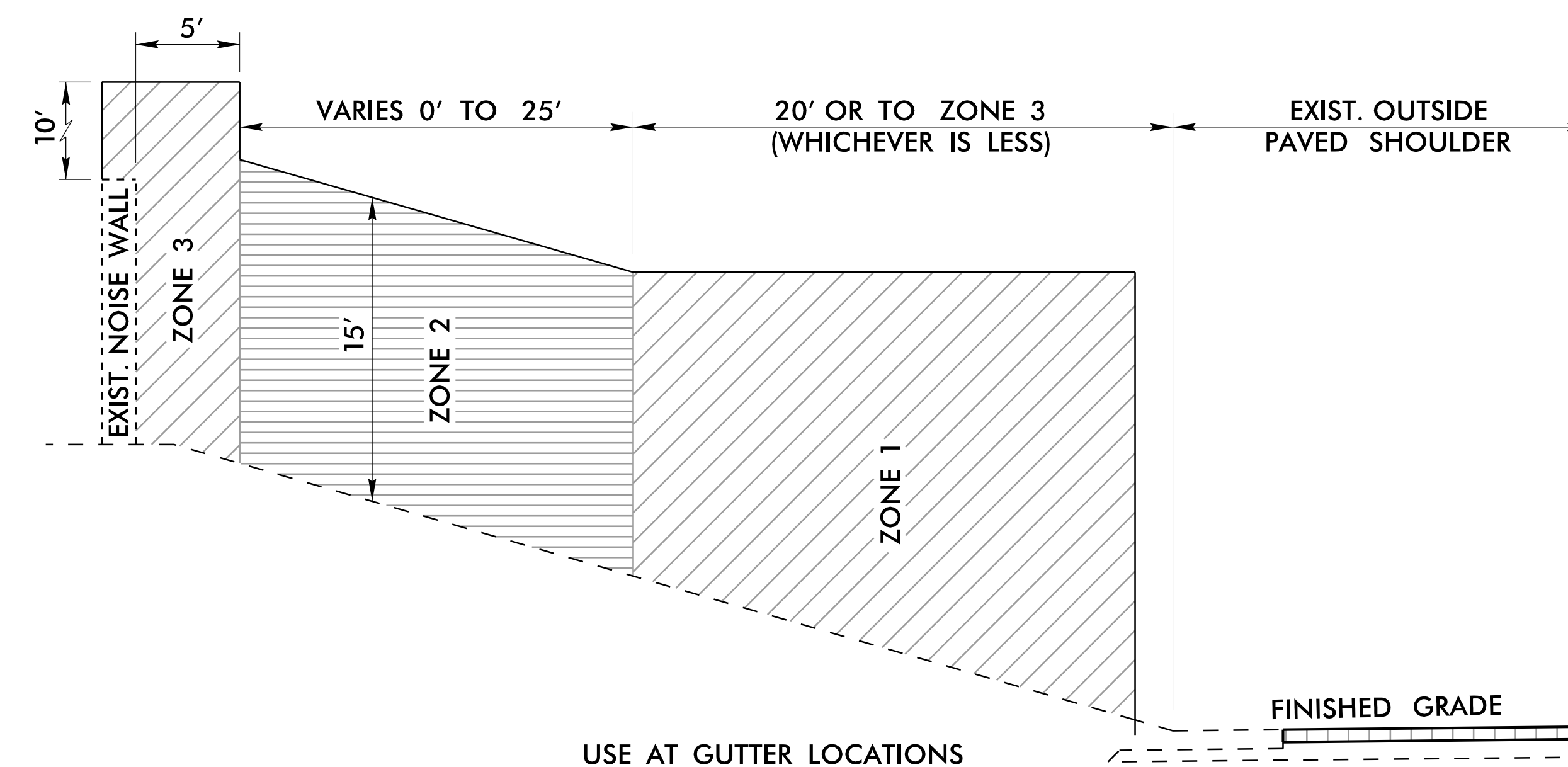
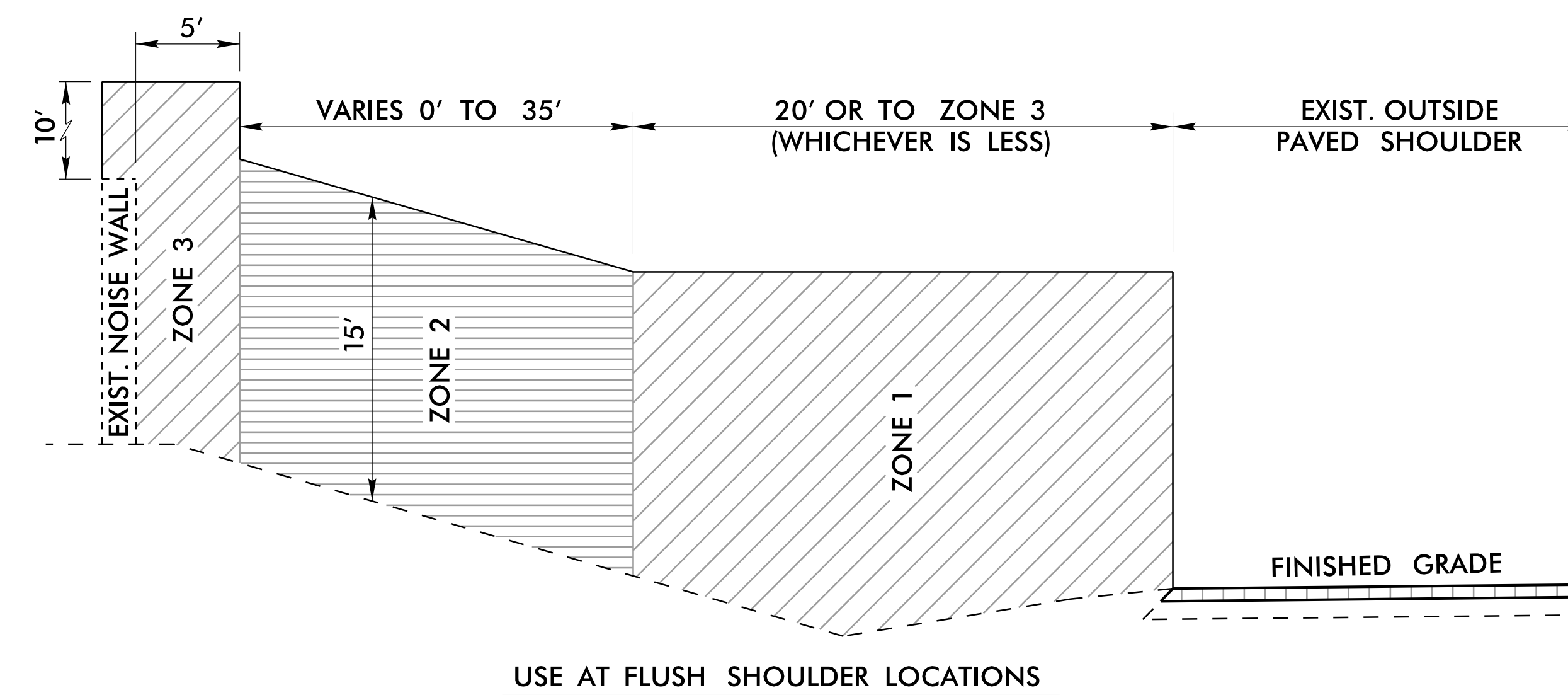
ALL GUARDRAIL REPLACEMENT LOCATIONS INCLUDING MEDIAN AND OUTSIDE SHOULDERS (SEE GUARDRAIL SUMMARY SHEET).

NOTES – DETAIL A

- MATCH EXISTING OFFSETS FROM TRAVEL LANES TO FACE OF GUARDRAIL, EXCEPT AS DETERMINED BY THE ENGINEER. MATCH AND RESTORE EXISTING MEDIAN GUARDRAIL OPENINGS.
- REMOVE ALL EXISTING VEGETATION INCLUDING TREES, SHRUBS, AND OVERHANGING LIMBS WITHIN THE REMOVAL LIMITS. FILL, COMPACT, AND GRADE SMOOTH ANY GROUND VOIDS WITH EXCESS EARTH MATERIAL FROM RAMP WIDENING AREAS SHOWN IN THE TYPICAL SECTIONS. NO PAYMENT WILL BE MADE FOR THIS WORK AND WILL BE INCIDENTAL TO "CLEARING AND GRUBBING". ANY ADDITIONAL EARTH MATERIAL REQUIRED TO FILL GROUND VOIDS WILL BE PAID FOR UNDER "BORROW EXCAVATION".
- GRADE EARTH SHOULDER SLOPES TO DRAIN IN ACCORDANCE WITH STANDARD DRAWING NO. 560.01 AND 560.02. THIS WORK WILL BE PAID FOR UNDER "SHOULDER RECONSTRUCTION".
- RESTORE DISTURBED SIDE SLOPES AND DITCHES TO EXISTING GRADE. REPLACE ANY CONCRETE ITEM OR RIP RAP DITCH LINING THAT IS DAMAGED DURING CONSTRUCTION. NO PAYMENT WILL BE MADE FOR THIS WORK AND WILL BE INCIDENTAL TO "CLEARING AND GRUBBING".
- INSTALL EROSION CONTROL DEVICES PER THE EROSION CONTROL PLANS.

TYPICAL VEGETATION REMOVAL AT NOISE WALLS – DETAIL B

N.T.S.



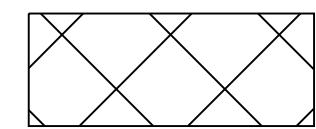
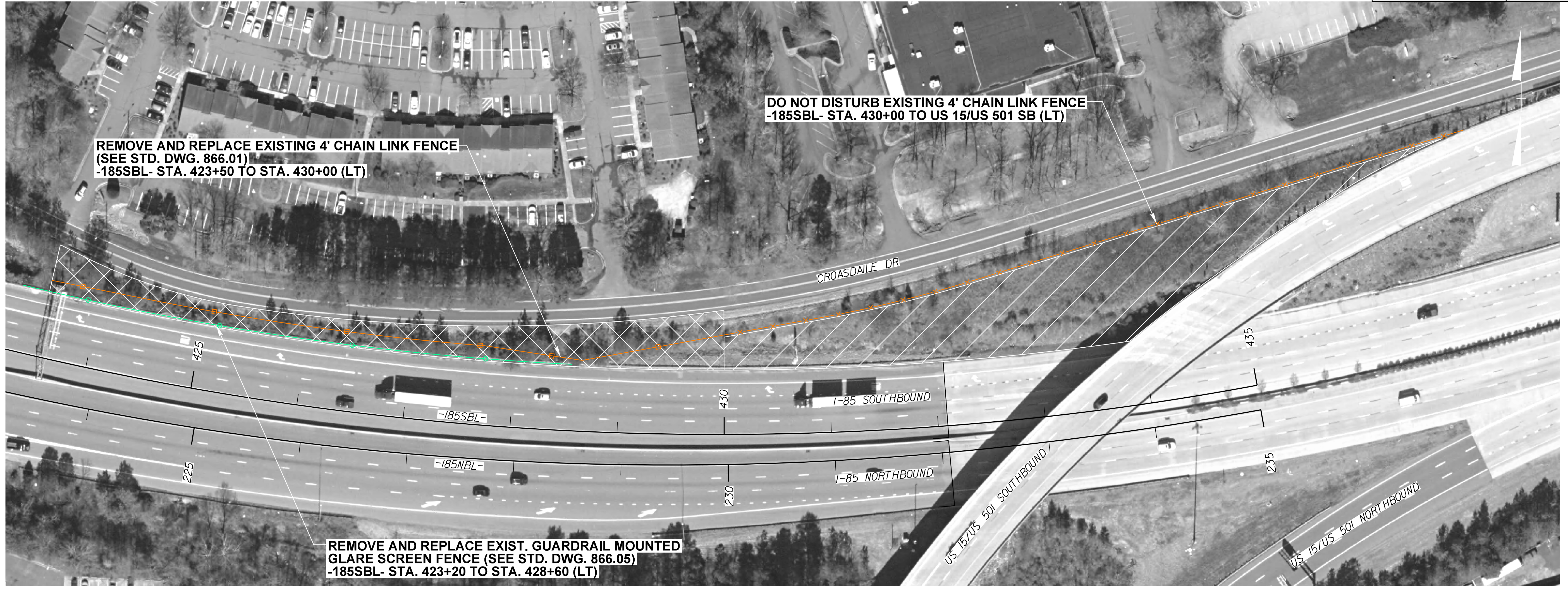
- REMOVE ALL VEGETATION – ZONES 1 AND 3 (SEE NOTE 1)
- REMOVE LIMBS ONLY – ZONE 2 (SEE NOTE 2)

USE DETAIL B AS FOLLOWS:

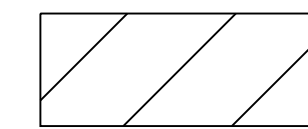
-I85NBL- STA. 120+00 TO STA. 134+50 (RT)
-I85SBL- STA. 326+00 TO STA. 352+00 (LT)

NOTES – DETAIL B

- REMOVE ALL EXISTING VEGETATION INCLUDING TREES, SHRUBS, AND OVERHANGING LIMBS WITHIN THE REMOVAL LIMITS. FILL, COMPACT, AND GRADE SMOOTH ANY GROUND VOIDS WITH EXCESS EARTH MATERIAL FROM RAMP WIDENING AREAS SHOWN IN THE TYPICAL SECTIONS. NO PAYMENT WILL BE MADE FOR THIS WORK AND WILL BE INCIDENTAL TO "CLEARING AND GRUBBING". ANY ADDITIONAL EARTH MATERIAL REQUIRED TO FILL GROUND VOIDS WILL BE PAID FOR UNDER "BORROW EXCAVATION".
- REMOVE LIMBS FROM EXISTING TREES ONLY, FROM EXISTING GROUND TO A HEIGHT OF 15' ABOVE GROUND. THIS WORK WILL BE PAID FOR UNDER "TREE TRIMMING".
- WHERE EXISTING GUARDRAIL IS REPLACED IN FRONT OF NOISE WALLS, GRADE FLUSH SHOULDERS PER DETAIL A.
- RESTORE DISTURBED SIDE SLOPES AND DITCHES TO EXISTING GRADE. REPLACE ANY CONCRETE ITEM OR RIP RAP DITCH LINING THAT IS DAMAGED DURING CONSTRUCTION. NO PAYMENT WILL BE MADE FOR THIS WORK AND WILL BE INCIDENTAL TO "CLEARING AND GRUBBING" AND "TREE TRIMMING".
- INSTALL EROSION CONTROL DEVICES PER THE EROSION CONTROL PLANS.



ZONE 4 VEGETATION REMOVAL LIMITS
-185SBL- STA. 423+50 TO STA. 430+00 (LT)



ZONE 5 VEGETATION REMOVAL LIMITS
-185SBL- STA. 430+00 TO US 15/US 501 SB (LT)

ZONE 4 NOTES

1. REMOVE ALL EXISTING VEGETATION INCLUDING TREES, SHRUBS, AND OVERHANGING LIMBS BETWEEN THE I-85 SOUTHBOUND EDGE OF PAVEMENT AND THE CROASDAILE DRIVE EDGE OF PAVEMENT. FILL, COMPACT, AND GRADE SMOOTH ANY GROUND VOIDS WITH EXCESS EARTH MATERIAL FROM RAMP WIDENING AREAS SHOWN IN THE TYPICAL SECTIONS. THIS WORK WILL BE PAID FOR UNDER "CLEARING AND GRUBBING". ANY ADDITIONAL EARTH MATERIAL REQUIRED TO FILL GROUND VOIDS WILL BE PAID FOR UNDER "BORROW EXCAVATION".
2. REMOVE AND REPLACE EXISTING I-85 SOUTHBOUND GUARDRAIL PER DETAIL A. DO NOT DISTURB EXISTING GUARDRAIL ALONG CROASDAILE DRIVE.
3. RESTORE DISTURBED SIDE SLOPES AND DITCHES TO EXISTING GRADE. REPLACE ANY CONCRETE ITEM OR RIP RAP DITCH LINING THAT IS DAMAGED DURING CONSTRUCTION. NO PAYMENT WILL BE MADE FOR THIS WORK AND IS CONSIDERED INCIDENTAL TO "CLEARING AND GRUBBING".
4. INSTALL EROSION CONTROL DEVICES PER THE EROSION CONTROL PLANS.
5. WORK IN ZONE 4 IS INCLUDED IN MAP 2 PAY ITEMS.

ZONE 5 NOTES

1. REMOVE ALL EXISTING VEGETATION INCLUDING TREES, SHRUBS, AND OVERHANGING LIMBS BETWEEN THE I-85 SOUTHBOUND EDGE OF PAVEMENT AND THE EXISTING CHAIN LINK FENCE (CONTROLLED ACCESS FENCE). FILL, COMPACT, AND GRADE SMOOTH ANY GROUND VOIDS WITH EXCESS EARTH MATERIAL FROM RAMP WIDENING AREAS SHOWN IN THE TYPICAL SECTIONS. THIS WORK WILL BE PAID FOR UNDER "CLEARING AND GRUBBING". ANY ADDITIONAL EARTH MATERIAL REQUIRED TO FILL GROUND VOIDS WILL BE PAID FOR UNDER "BORROW EXCAVATION".
2. REMOVE AND REPLACE EXISTING I-85 SOUTHBOUND GUARDRAIL PER DETAIL A. DO NOT DISTURB EXISTING GUARDRAIL ALONG CROASDAILE DRIVE OR ALONG US 15/US 501.
3. RESTORE DISTURBED SIDE SLOPES AND DITCHES TO EXISTING GRADE. REPLACE ANY CONCRETE ITEM OR RIP RAP DITCH LINING THAT IS DAMAGED DURING CONSTRUCTION. NO PAYMENT WILL BE MADE FOR THIS WORK AND IS CONSIDERED INCIDENTAL TO "CLEARING AND GRUBBING".
4. INSTALL EROSION CONTROL DEVICES PER THE EROSION CONTROL PLANS.
5. WORK IN ZONE 5 IS INCLUDED IN MAP 2 PAY ITEMS.

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS
GUARDRAIL SUMMARY

"N" = DISTANCE FROM EDGE OF LANE TO FACE OF GUARDRAIL
TOTAL SHOULDER WIDTH = DISTANCE FROM EDGE OF TRAVEL LANE TO SHOULDER BREAK POINT
FLARE LENGTH = DISTANCE FROM LAST SECTION OF PARALLEL GUARDRAIL TO END OF GUARDRAIL
W = TOTAL WIDTH OF FLARE FROM BEGINNING OF TAPER TO END OF GUARDRAIL

SURVEY LINE	BEG. STA.	END STA.	LOCATION	LENGTH			WARRANT POINT		"N" DIST. FROM E.O.L.	TOTAL SHOULDER WIDTH	FLARE LENGTH		W		ANCHORS					IMPACT ATTENUATOR TL-2 TL-3	CABLE GUIDERAIL RESET	REMOVE EXISTING GUARDRAIL	REMOVE & STOCKPILE EXISTING GUARDRAIL	REMARKS							
				STRAIGHT	SHOP CURVED	DOUBLE FACED	APPROACH END	TRAILING END			APPROACH END	TRAILING END	APPROACH END	TRAILING END	GREU TL-3	CAT-1	TYPE III	Type B-77	Type B-83												
-I85NBL-	102+50.00	116+07.00	LT (MED)			1357.0																					1357		TRANSITION TO EXIST GR		
-I85NBL-	116+07.00	135+76.00	LT (MED)	1969.00																							1969				
-I85NBL-	136+06.00	191+09.00	LT (MED)	5503.00																							5503				
-I85NBL-	191+36.00	213+05.00	LT (MED)	2169.00																							2169				
-I85NBL-	215+46.00	217+81.00	LT (MED)	235.00																							235				
-I85SBL-	316+14.00	366+94.00	RT (MED)	5080.00																							5080				
-I85SBL-	367+24.00	407+80.00	RT (MED)	4056.00																							4056				
-I85SBL-	408+10.00	413+08.00	RT (MED)	498.00																							498				
-I85SBL-	415+42.00	418+00.00	RT (MED)	258.00																							258				
-I85NBL-	102+50.00	103+72.00	RT	122.00																							122		TRANSITION TO EXIST GR		
-I85NBL-	103+77.00	111+90.00	RT	813.00																							813				
-I85NBL-	122+21.00	134+62.00	RT	1241.00																							1241				
-I85NBL-	189+26.00	192+48.00	RT			322.00																					322				
-I85NBL-	202+40.00	203+69.00	RT	129.00																							129				
-I85NBL-	211+40.00	213+64.00	RT	224.00																							224				
-I85NBL-	223+49.00	226+53.00	RT	304.00																							304				
-I85NBL-	229+30.00	234+23.00	RT	493.00																							493				
-I85SBL-	305+09.00	312+59.00	LT	750.00																							750				
-I85SBL-	323+28.00	333+36.00	LT	1008.00																							1008				
-I85SBL-	338+84.00	343+86.00	LT	502.00																							502				
-I85SBL-	373+92.00	375+31.00	LT	139.00																							139				
-I85SBL-	376+01.00	378+16.00	LT	215.00																							215				
-I85SBL-	392+53.00	397+97.00	LT	544.00																							544				
-I85SBL-	415+03.00	417+32.00	LT	229.00																							229				
-I85SBL-	419+60.00	432+34.00	LT	1274.00																							1274				
-NC147NB-	11+81.00	14+48.00	LT	267.00																							267				
-NC147SB-	20+06.00	23+02.00	LT	296.00																							296				
-NC147SB-	19+74.00	22+52.00	RT	278.00																							278				
-CMRAMPA-	10+00.00	14+92.00	RT	492.00																							492				
SUBTOTAL				29088.00		1679.00																									
LESS ANCHOR DEDUCTIONS																															
GREU TL-3 19@50.00'						-950																									
CAT-1 18@6.25'						-112.5																									
TYPE III 2@18.75'						-37.5																									
B-77 1@22.875'						-22.875																									
B-83 12@25.0'						-250																									
						1 ON EACH SIDE OF DBL FACED (DEDUCT 1 ONLY)																									
PROJECT TOTAL				27715.13		1654.00																									
SAY				27717		1654																									
ADDITIONAL GUARDRAIL POSTS = 10 EA.																															
THE FOLLOWING EXISTING BARRIER LOCATIONS TO REMAIN SHALL INCLUDE INSTALLATION OF NEW DELINEATORS IN ACCORDANCE WITH STANDARD DRAWINGS 1261.01 AND 1261.02.																															
-I85NBL-	168+38.00	189+26.00	RT																											EXIST CONC BARRIER	
-I85NBL-	213+64.00	215+86.00	RT																											EXIST CONC BRIDGE RAIL	
-I85NBL-	213+05.00	215+46.00	LT (MED)																											EXIST CONC BRIDGE RAIL	
-I85NBL-	217+81.00	232+07.00	LT (MED)																											EXIST CONC BARRIER	
-I85SBL-	375+31.00	376+01.00	LT																											EXIST CONC BARRIER	
-I85SBL-	412+64.00	415+03.00	LT																											EXIST CONC BRIDGE RAIL	
-I85SBL-	413+08.00	415+42.00	RT (MED)																											EXIST CONC BRIDGE RAIL	
-I85SBL-	418+08.00	432+09.00	RT (MED)																											EXIST CONC BARRIER	

NOTES:

1. NO LOCATION SURVEY HAS BEEN PERFORMED ON THIS PROJECT. ALIGNMENTS SHOWN ARE INTENDED TO PROVIDE APPROXIMATE LENGTHS AND LOCATIONS ONLY.
2. PLAN STATIONS ARE APPROXIMATE AND MAY REQUIRE FIELD ADJUSTMENT AS DETERMINED BY THE ENGINEER.
3. REMOVE AND REPLACE ALL GUARDRAIL AND GUARDRAIL ANCHORAGES. SEE GUARDRAIL SUMMARY SHEET FOR LOCATIONS.



REVISIONS

8/17/99

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