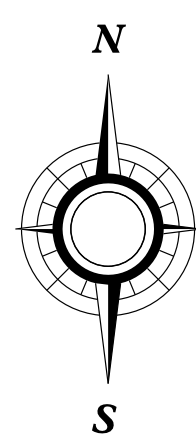
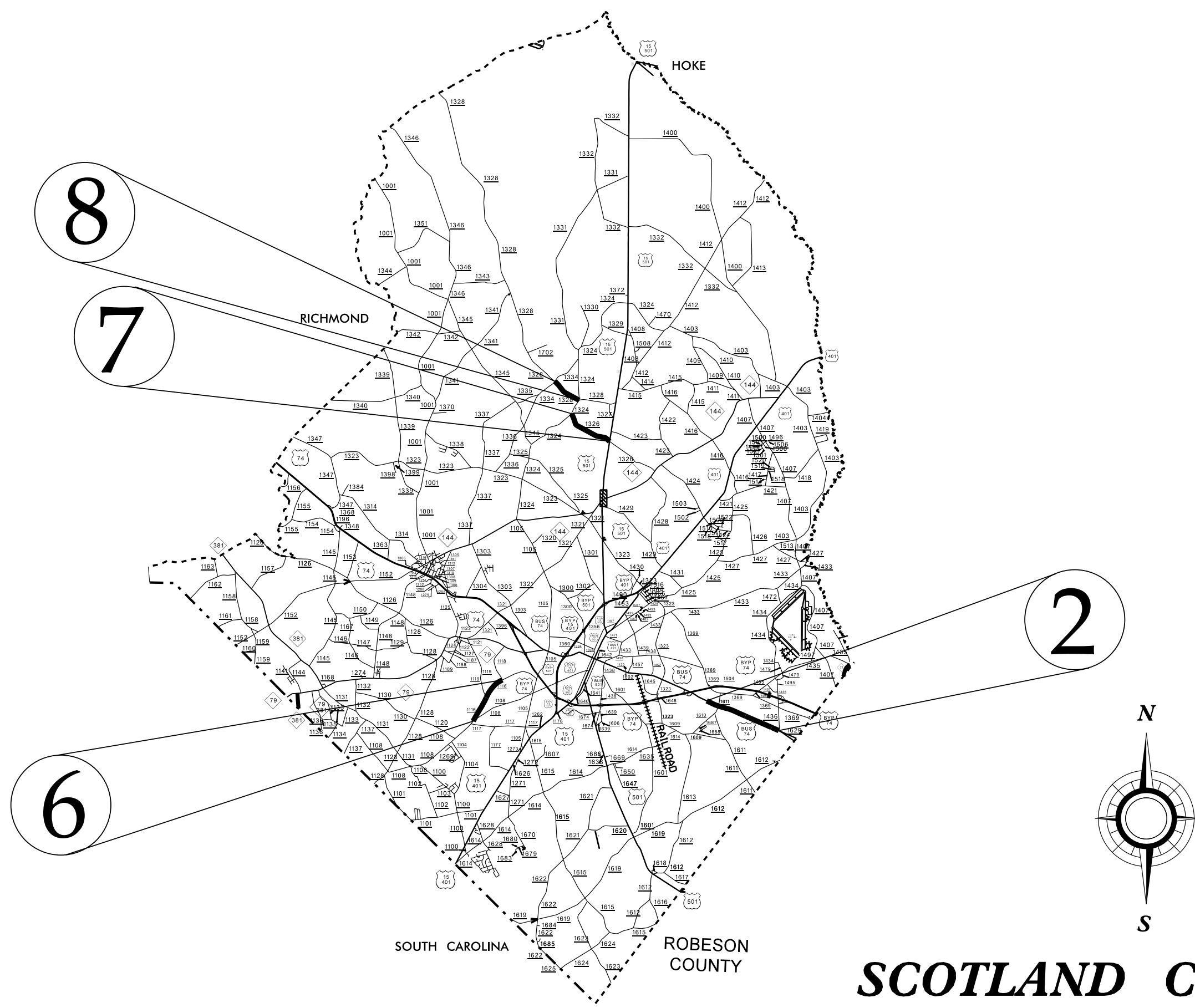
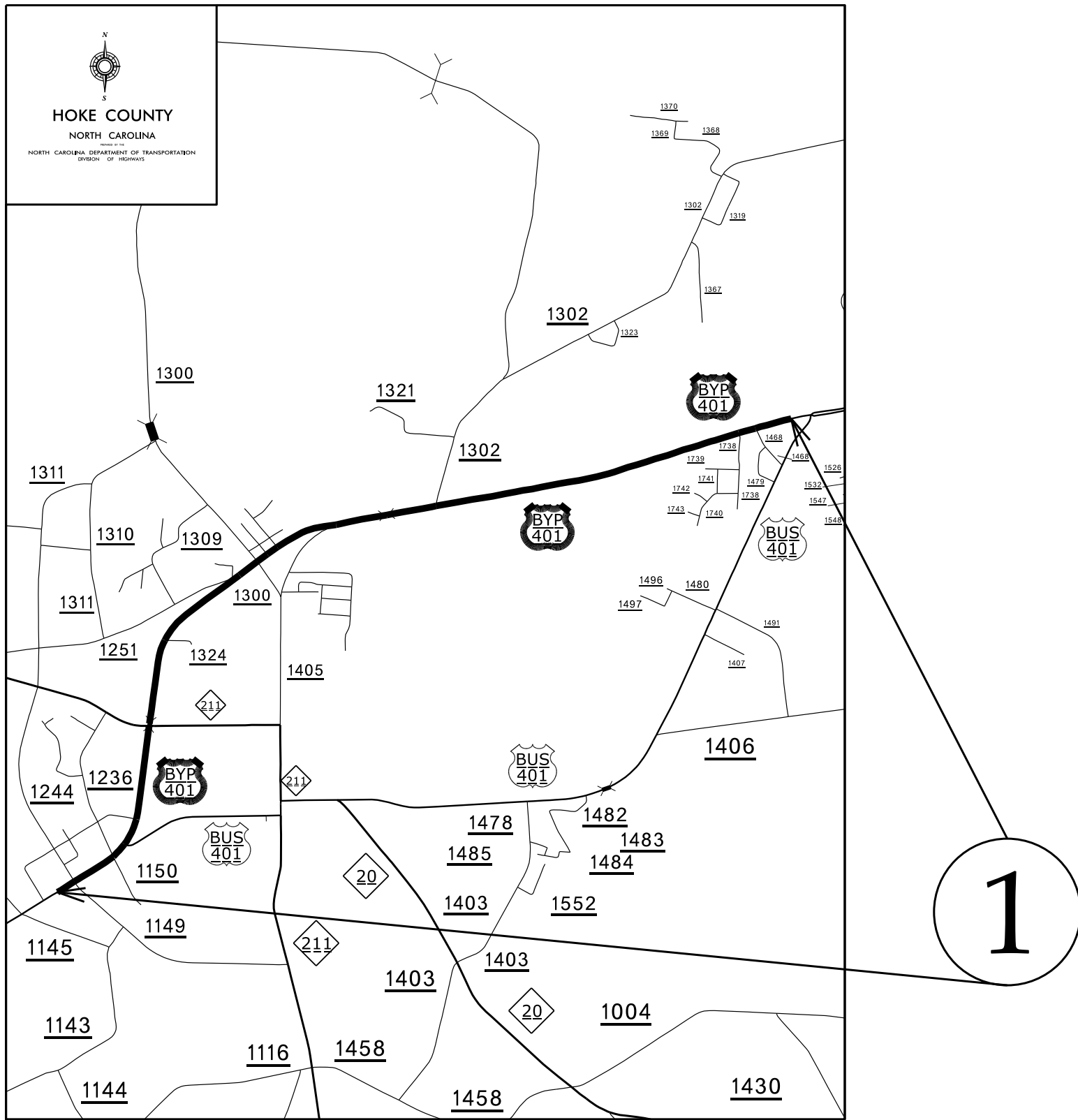


HOKE COUNTY

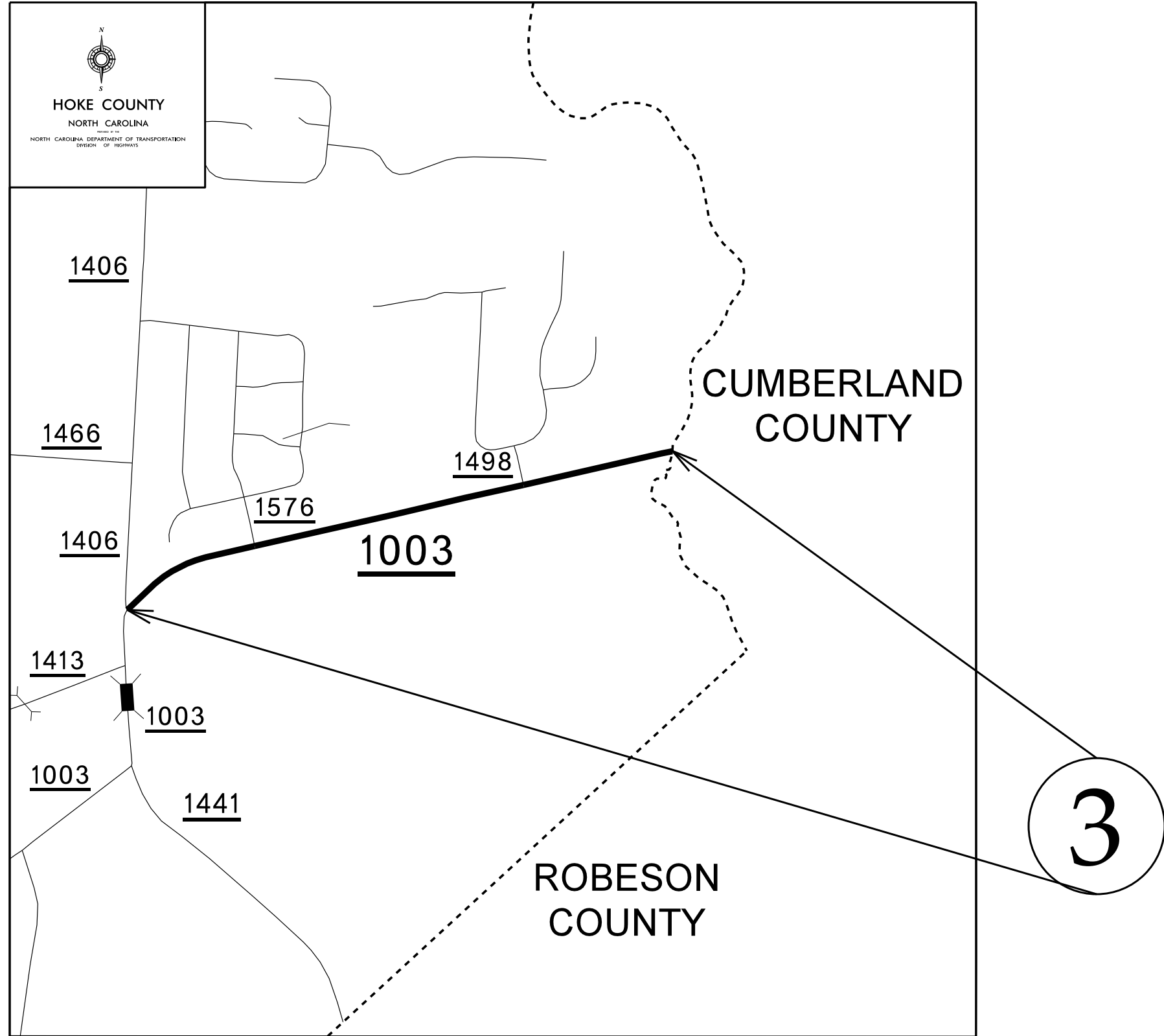


SCOTLAND COUNTY

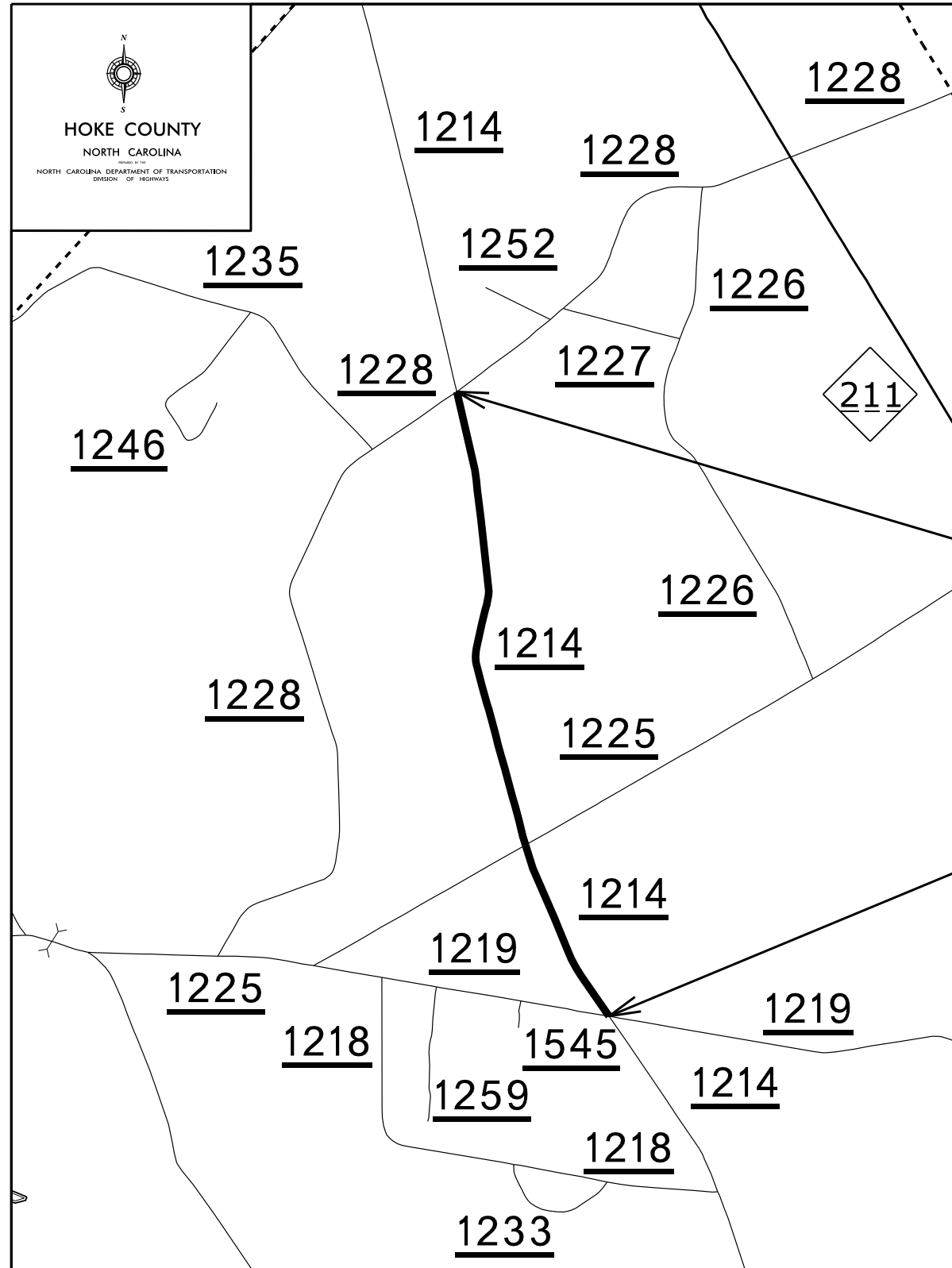
Map 1



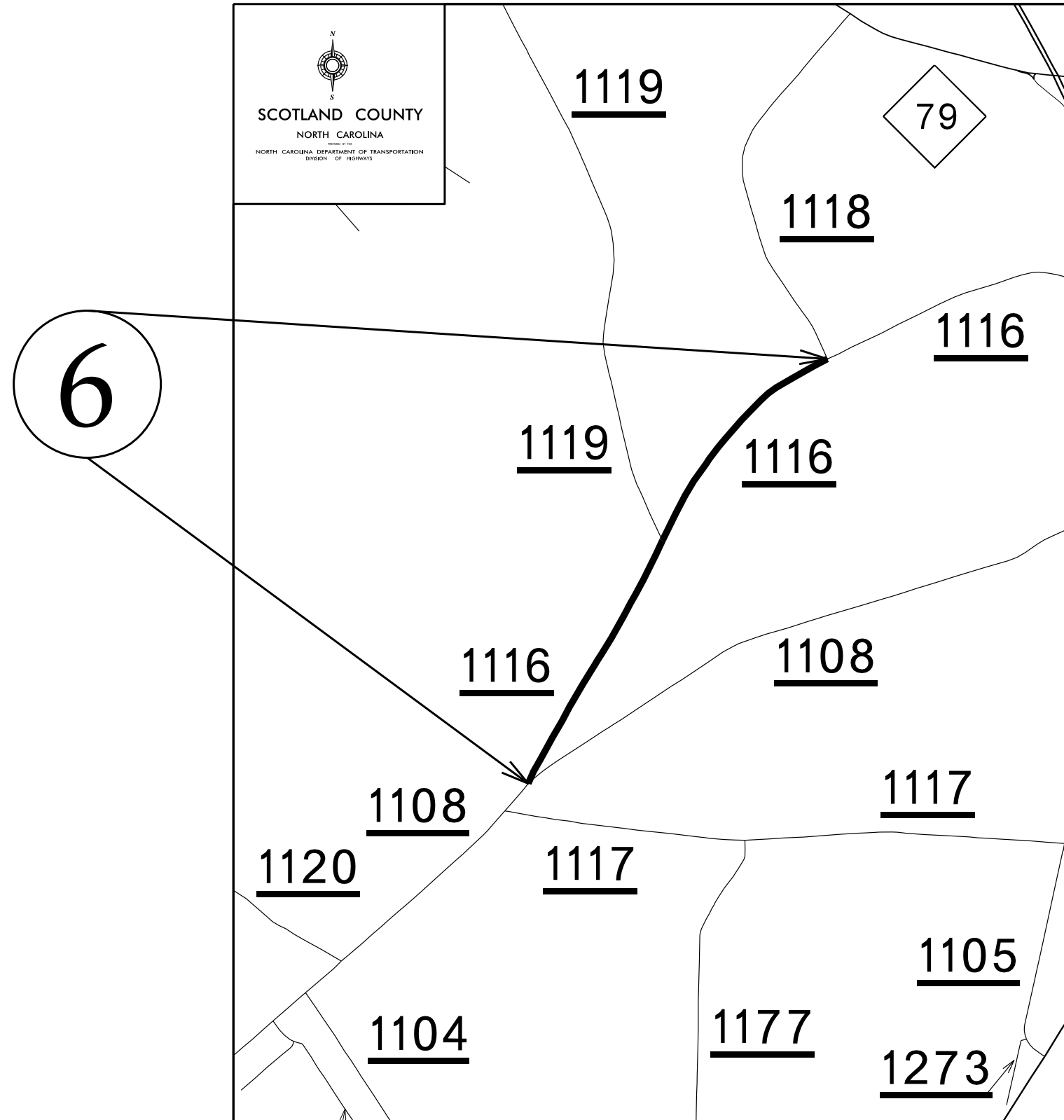
Map 3



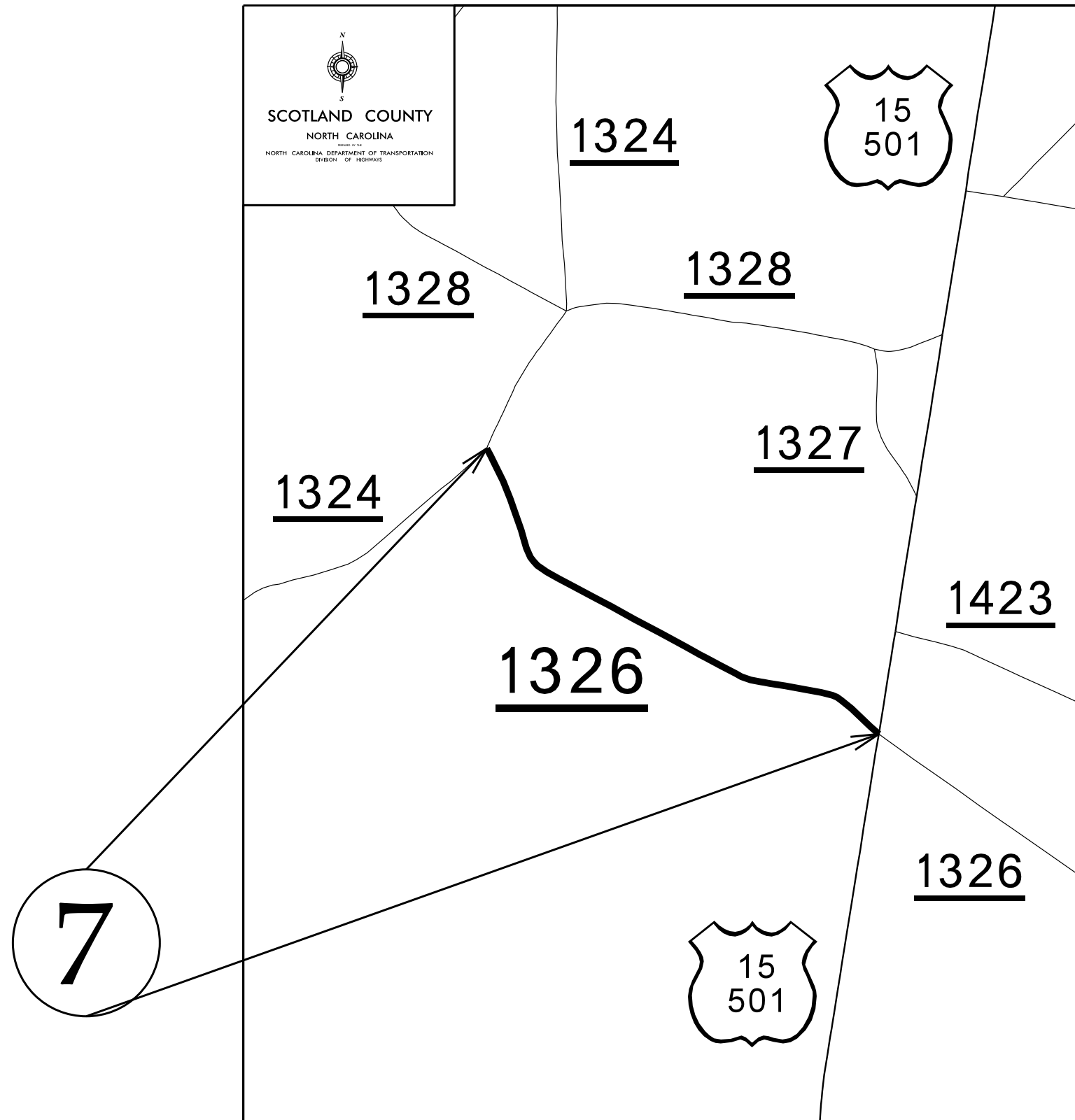
Map 4



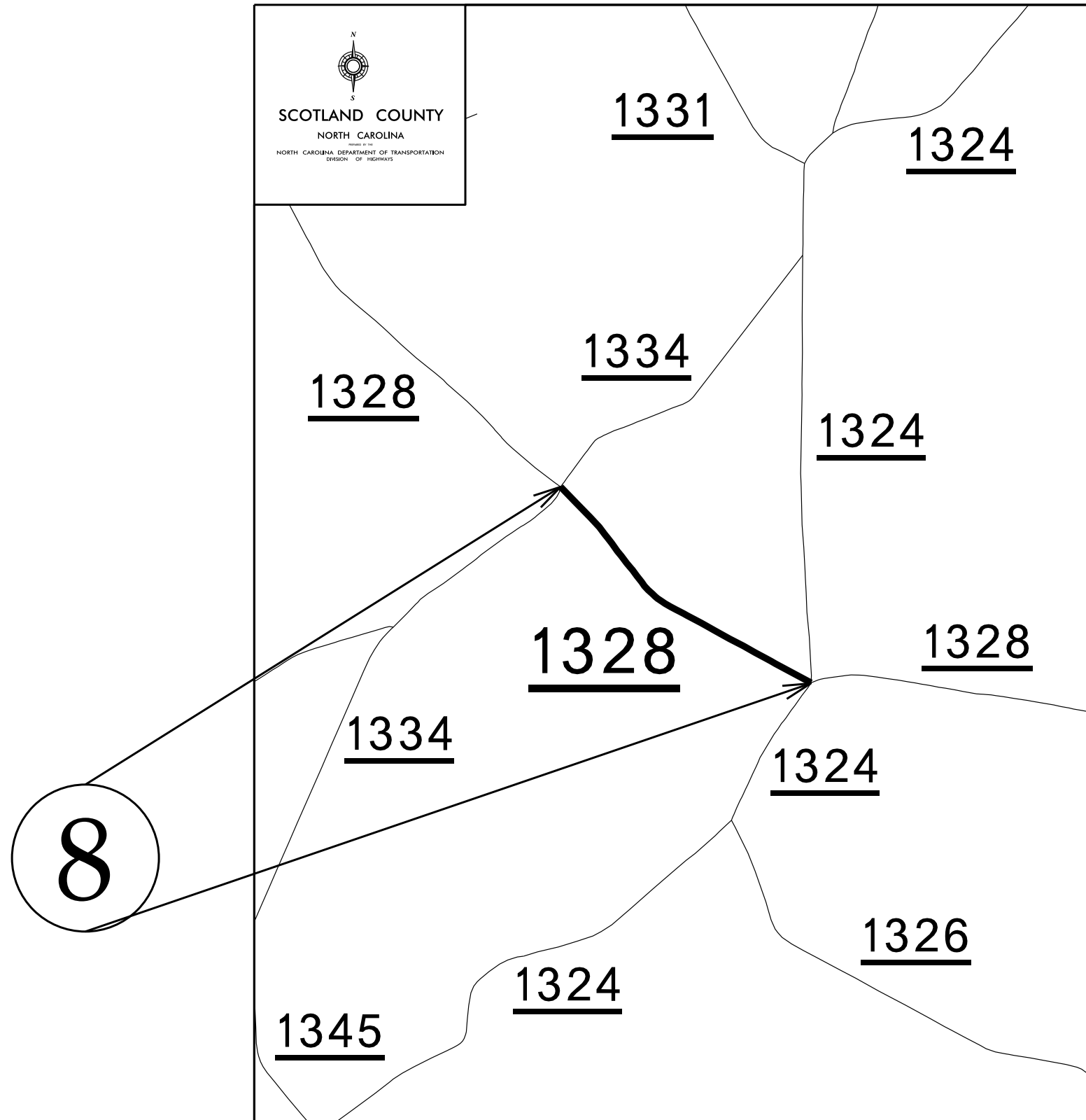
Map 6



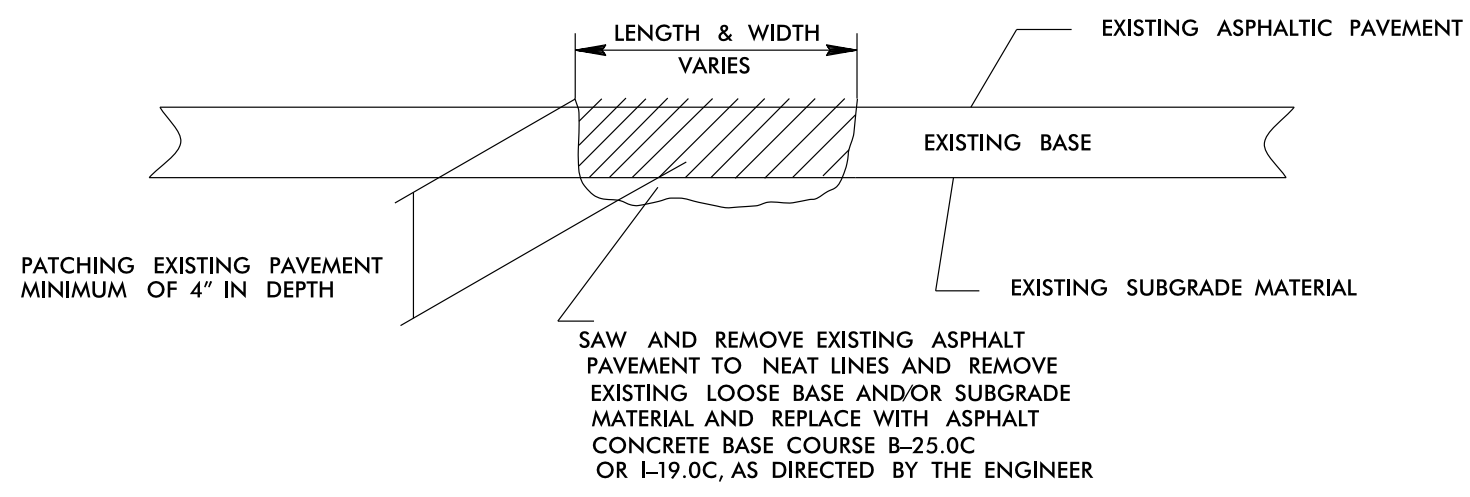
Map 7



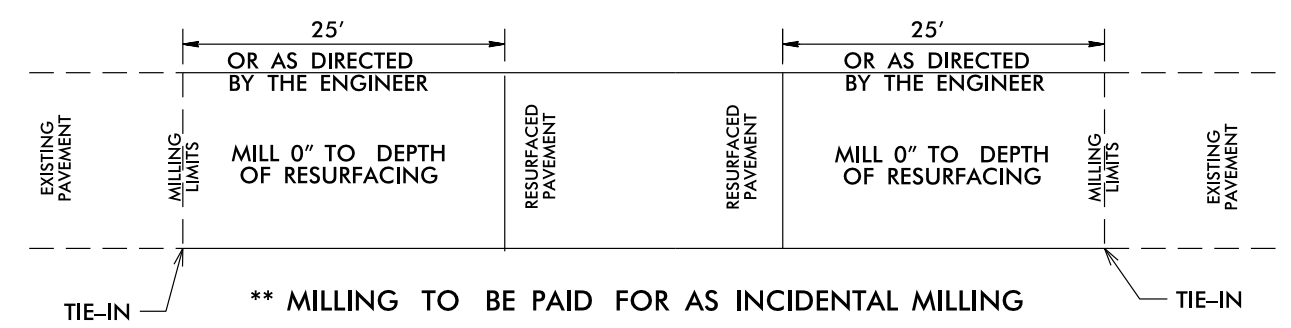
Map 8



DETAILS OF PATCHING EXISTING PAVEMENT PRIOR TO RESURFACING



PAVEMENT TIE-IN DETAIL

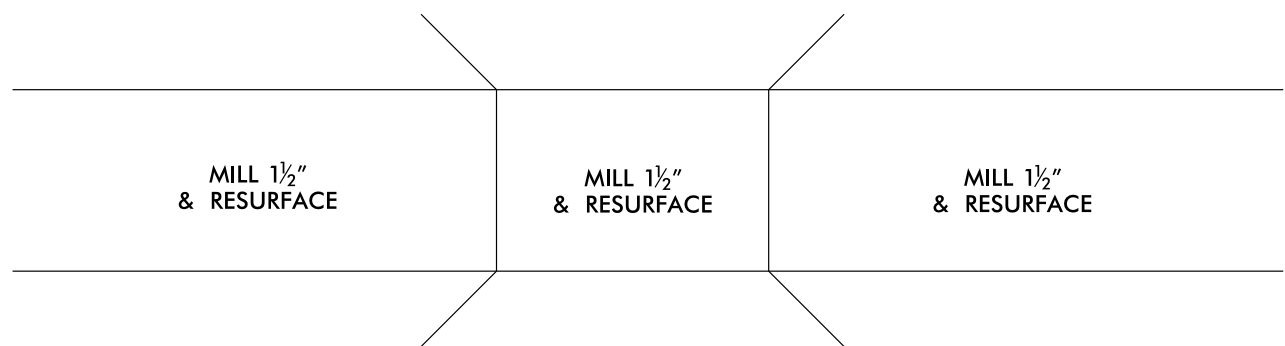


PROJECT NOTES

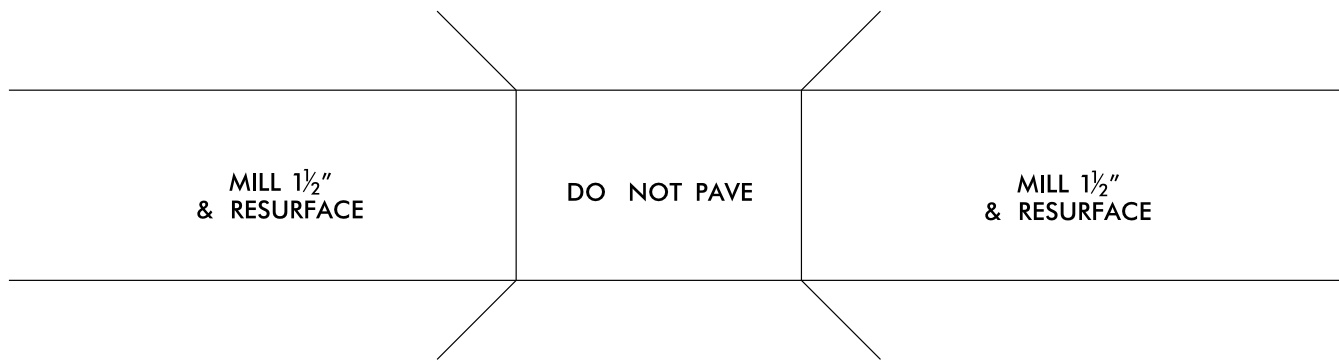
- ONE COAT OF PAINT MARKINGS TO BE APPLIED ON MAPS 4-8. PERMANENT MARKINGS TO BE HANDLED ON A LATER PROJECT.

WBS ELEMENT	SHEET NO.
2024CPT.08.08.10471	13
2024CPT.08.08.10831	
2024CPT.08.08.20471	
2024CPT.08.08.20831	

BRIDGE DETAILS



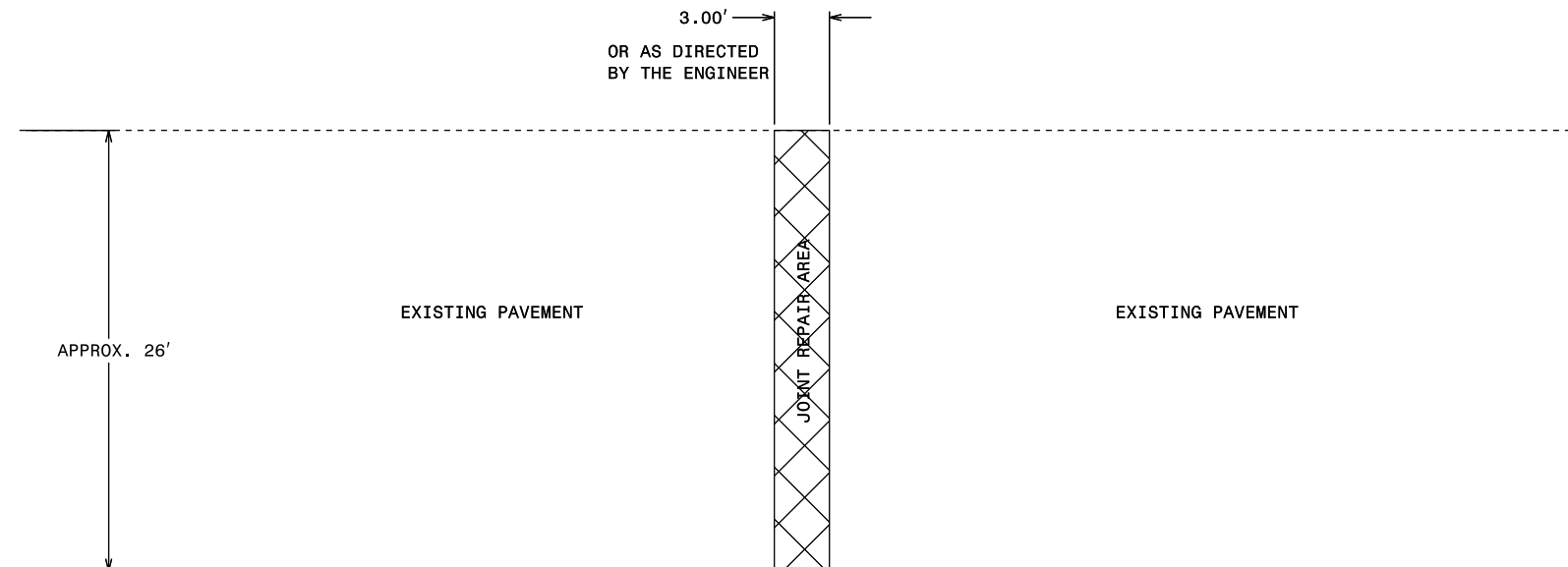
***BRIDGE DRAWING FOR
MAP #1 US 401
(BRIDGES #3 AND #5)***



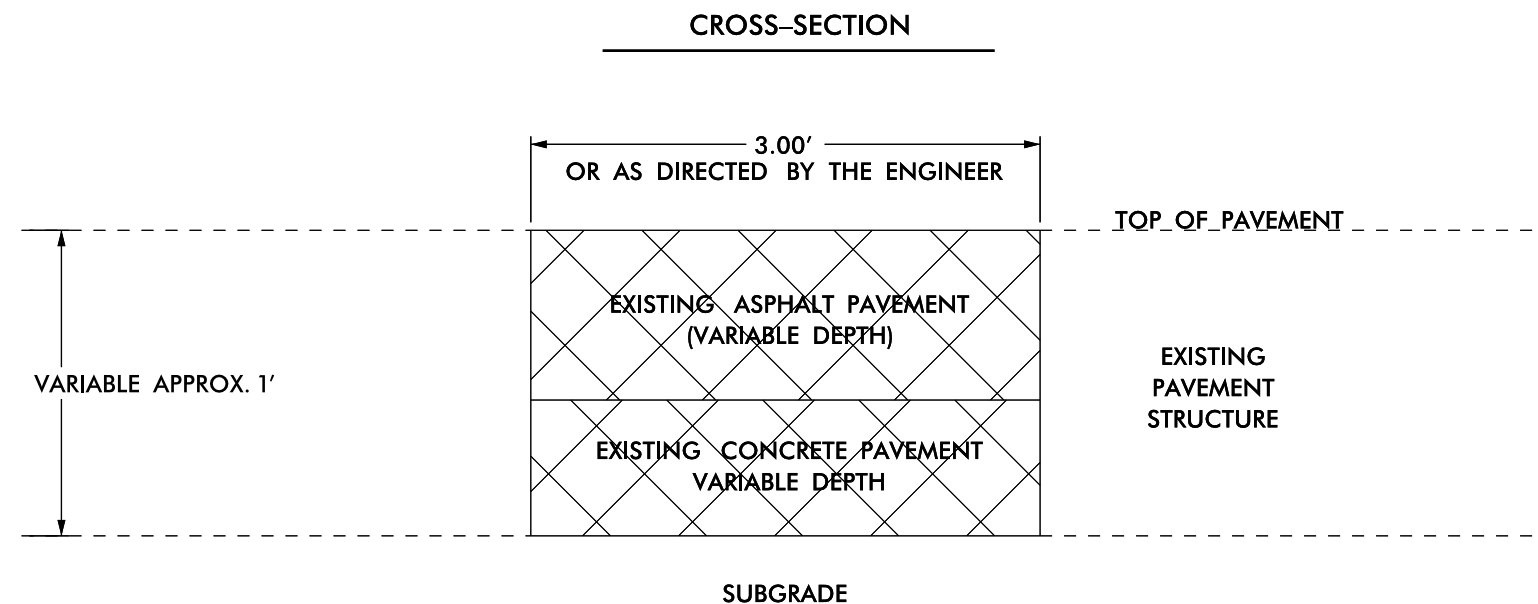
***BRIDGE DRAWING FOR
MAP #2 US 74 BUS
(BRIDGE #75)***

JOINT REPAIR DETAIL

JOINT SCHEDULE	
MAP	# JOINTS
#2 US 74 BUS.	21



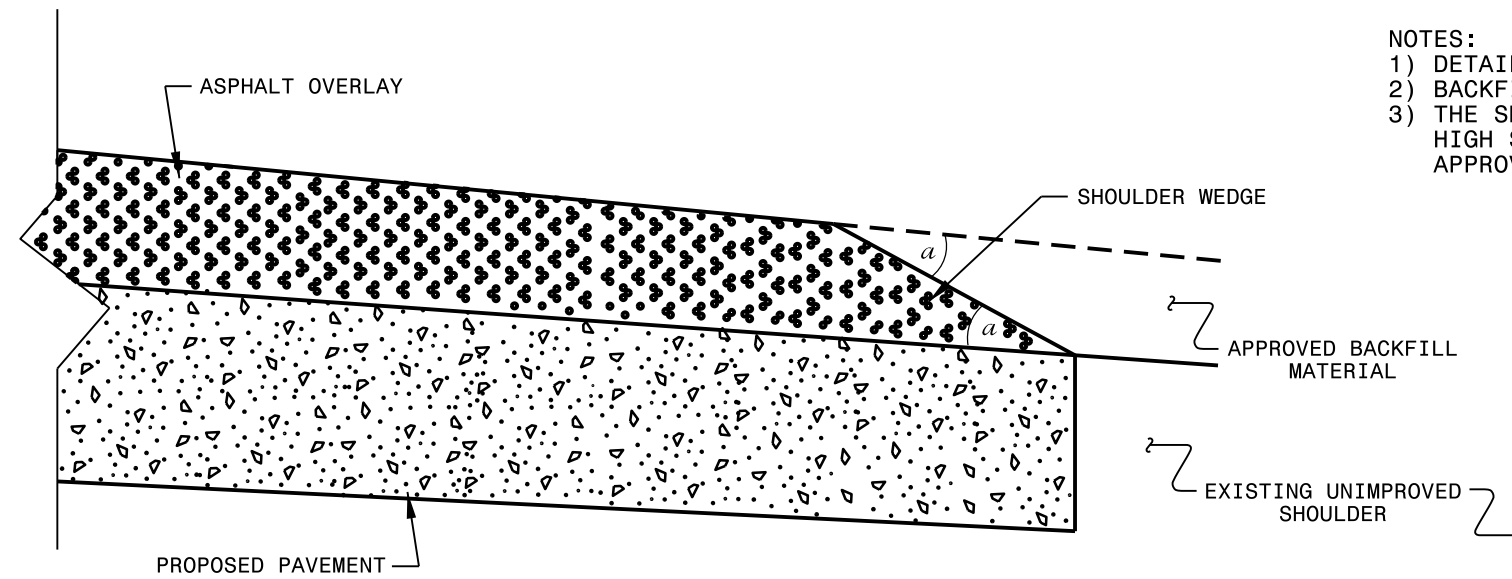
CONTRACTOR SHALL COORDINATE WITH RESIDENT ENGINEER'S OFFICE FOR LOCATION OF JOINTS TO BE REPAIRED.



NOTE:
 REMOVE ASPHALT AND CONCRETE AT JOINT LOCATIONS AS DIRECTED BY THE ENGINEER (BY SAWING CLEAN JOINTS).
 REMOVE A TOTAL WIDTH OF 3' (APPROX. 1.5' EACH SIDE OF JOINT).
 REMOVE AND REPLACE WITH ASPHALT CONCRETE BASE COURSE, TYPE B25.0C AND/OR ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0C.
 THERE WILL BE NO DIRECT PAY FOR THIS WORK AS IT WILL BE CONSIDERED INCIDENTAL TO TO THE LINE ITEM, JOINT REPAIR (TONNAGE)

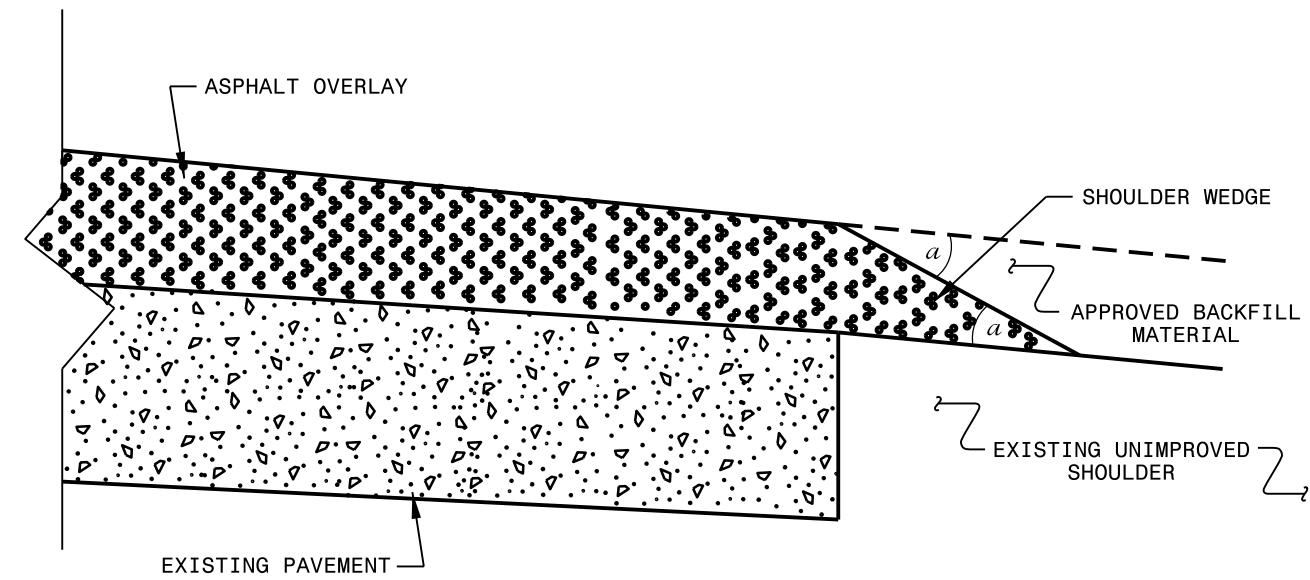
NOTES:

- 1) DETAIL DOES NOT APPLY TO OGAFB AND ULTRA-THIN BONDED WEARING COURSE.
- 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
- 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS, SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS APPROVED BY THE ENGINEER.



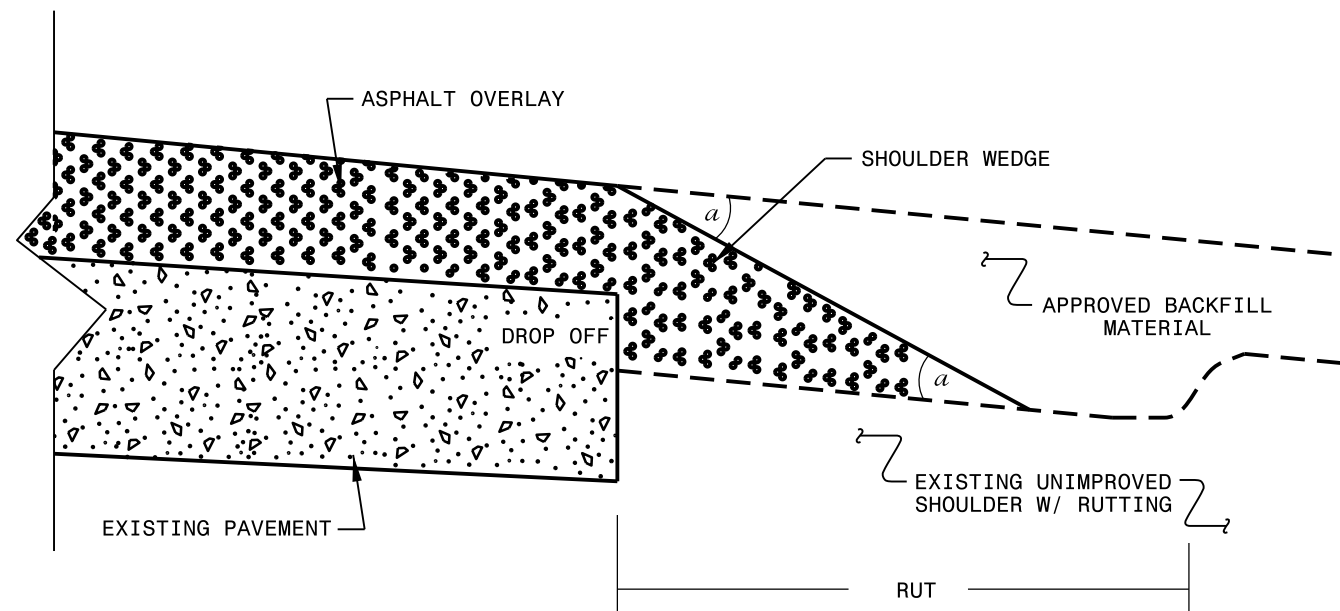
SHOULDER WEDGE DETAIL

(Resurfacing Projects w/ Widening or with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL

(Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL

(Resurfacing Adjacent to Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS AND DEVELOPMENT UNIT	
Office 919-707-6950 FAX 919-250-4119	
SHOULDER WEDGE DETAILS	
ORIGINAL BY: T.SPELL	DATE: 7-19-11
MODIFIED BY:	DATE: 2/2/16
CHECKED BY:	DATE:
FILE SPEC.: szusr/details/stand/shoulderwedgedetail.dgn	

PROJECT NO.	SHEET NO.
2024CPT.08.08.10471, ETC.	16

SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	BEGIN MP	END MP	1245000000-E	1260000000-E	1297000000-E	1330000000-E	1519000000-E	1523000000-E	1575000000-E	1704000000-E	1880000000-E	2830000000-N	2845000000-N	5255000000-N	7444000000-E	7456100000-E
														SHOULDER RECONSTRUCTION SMI	AGGREGATE SHOULDER BORROW TON	1.5" MILLING SY	INCIDENTAL MILLING SY	SURFACE COURSE, S9.5B TONS	SURFACE COURSE, S9.5C TONS	ASPHALT BINDER FOR PLANT MIX TONS	PATCHING EXISTING PAVEMENT TONS	GENERIC PAVING ITEM - JOINT REPAIR TON	ADJUST MANHOLES EA	ADJUST METER OR VALVE BOX EA	PORTABLE LIGHTING LS	INDUCTIVE LOOP SAWCUT LF	LEAD-IN CABLE (14-2) LF
2024CPT.08.08.10471	Hoke	1	US-401	FROM CJ 300' NORTH OF DUDLEY ST TO CJ 580' SOUTH OF US 401 BUS	1	2	2WU	NO	NO	4.87	42	7.86	12.73			86,118	22,370		10,024	591			1	1	1	3,204	100
TOTAL FOR MAP NO. 1										4.87						86,118	22,370		10,024	591		1	1	1	3,204	100	
TOTAL FOR PROJ NO. 2024CPT.08.08.10471										4.87						86,118	22,370		10,024	591		1	1	1	3,204	100	
2024CPT.08.08.10831	Scotland	2	US-74 BUS	FROM CJ 1100' WEST OF SR 1611 (N ROCKY FORD RD) TO CJ WEST OF SR 1436 (AIRPORT RD)	1	2	2WU	NO	NO	2.01	26	6.55	8.56			31,521	3,434		3,230	191							
TOTAL FOR MAP NO. 2										2.01						31,521	3,434		3,230	191		125					
TOTAL FOR PROJ NO. 2024CPT.08.08.10831										2.01						31,521	3,434		3,230	191		125					
2024CPT.08.08.20471	Hoke	3	SR-1003 / CAMDEN RD	FROM CUMBERLAND CO LINE TO SR 1406 (ROCKFISH RD)	2	2	2WU	NO	NO	0.98	22	0	0.98			1.96	275.00		895	1,239							
TOTAL FOR MAP NO. 3										0.98						1.96	275.00		895	1,239					1		
2024CPT.08.08.20471	Hoke	4	SR-1214 / CALLOWAY RD	FROM SR 1219 (ARMY RD) TO SR 1228 (RESERVATION RD)	2	2	2WU	NO	NO	2.38	22	5.39	7.77			4.76	667.00		488	2,839							
TOTAL FOR MAP NO. 4										2.38						4.76	667.00		488	2,839							
2024CPT.08.08.20471	Hoke	5	SR-1203 / TURNPIKE RD	FROM SR 1206 (L. MCLAUGHLIN RD) TO NC 211	2	2	2WU	NO	NO	1.35	22	6.31	7.66			2.70	378.00		672	1,750							
TOTAL FOR MAP NO. 5										1.35						2.70	378.00		672	1,750						1	
TOTAL FOR PROJ NO. 2024CPT.08.08.20471										4.71						9.42	1,320.00		2,055	5,828					2		
2024CPT.08.08.20831	Scotland	6	SR-1116 / BLUE WOODS RD	FROM SR 1118 (JIM CALHOUN RD) TO SR 1108 (XWAY RD)	2	2	2WU	NO	NO	1.47	20	1.1	2.57			2.94	412.00		749	1,655							
TOTAL FOR MAP NO. 6										1.47						2.94	412.00		749	1,655					1		
2024CPT.08.08.20831	Scotland	7	SR-1326 / MONROE RD	FROM US 15-501 (ABERDEEN RD) TO SR 1324 (SNEADTOWN RD)	2	2	2WU	NO	NO	1.42	19	1.6	3.02			2.84	398.00		422	1,437							
TOTAL FOR MAP NO. 7										1.42						2.84	398.00		422	1,437							
2024CPT.08.08.20831	Scotland	8	SR-1328 / HOFFMAN RD	FROM SR 1324 (SNEADTOWN RD) TO SR 1334 (CAMP MONROE RD)	2	2	2WU	NO	NO	0.9	21	1.07	1.97			1.80	252.00		624	1,096							
TOTAL FOR MAP NO. 8										0.9						1.80	252.00		624	1,096							
TOTAL FOR PROJ NO. 2024CPT.08.08.20831										3.79						7.58	1,062.00		1,795	4,188					1		
GRAND TOTAL										15.38						17.00	2,382.00		117,639	29,654					1	3,204	100

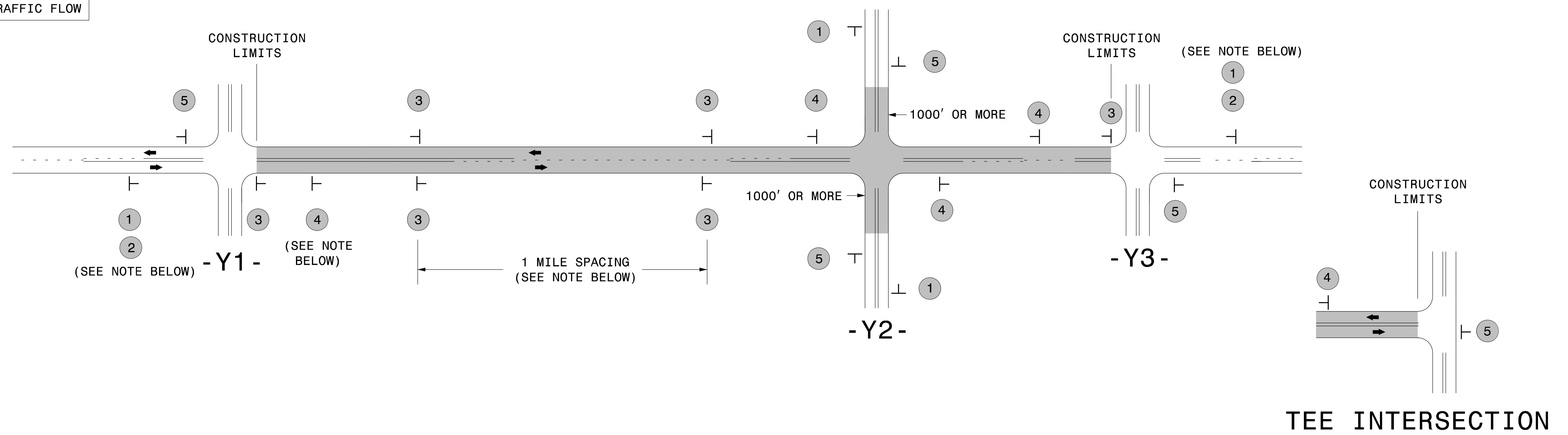
PROJECT NO.	SHEET NO.
2024CPT.08.08.10471, ETC.	17

THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	BEGIN MP	END MP	WORK ZONE ADVANCE/GENERAL WARNING SIGNING	TEMPORARY TRAFFIC CONTROL	LAW ENFORCEMENT	4685000000-E		4695000000-E		4700000000-E		4709000000-E		4720000000-E		4725000000-E					4810000000-E			4820000000-E			4825000000-E			4835000000-E			4840000000-N					4845000000-N					4850000000-E		4905100000-N	
															4" X 90 M WHITE THERMO	4" X 90 M YELLOW THERMO	8" X 90 M WHITE THERMO	12" X 90 M YELLOW THERMO	24" X 90 M WHITE THERMO	THERMO MSG SCHOOL 90 M	THERMO LT ARROW 90 M	THERMO RT ARROW 90 M	THERMO STR ARROW 90 M	THERMO STR & RT ARROW 90 M	THERMO STR & LT ARROW 90 M	4" WHITE PAINT	4" YELLOW PAINT	8" WHITE PAINT	12" YELLOW PAINT	24" WHITE PAINT	PAINT MSG SCHOOL	PAINT MSG STOP	PAINT MSG AHEAD	PAINT LT ARROW	PAINT RT ARROW	PAINT STR ARROW	PAINT STR & RT ARROW	PAINT STR & LT ARROW	4" LINE REMOVAL	NON-CAST IRON SNOWPLOWABLE PAVEMENT MARKER C&R	NON-CAST IRON SNOWPLOWABLE PAVEMENT MARKER Y&Y														
MI	FT	SF	LS	HR	LF	LF	LF	LF	LF	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA					
2024CPT.08.08.10471	Hoke	1	US-401	FROM CJ 300' NORTH OF DUDLEY ST TO CJ 580' SOUTH OF US 401 BUS	1	2	2WU	4.87	42	7.86	12.73	545	*	120	55,930	57,271	575	1,018	1,180	12	52	8	6	7	4	55,930	57,271	575	1,018	1,180	12	52	8	6	7	4	55,930	57,271	575	1,018	1,180	12	52	8	6	7	4	165	523						
TOTAL FOR MAP NO. 1							4.87					545		120	55,930	57,271	575	1,018	1,180	12	52	8	6	7	4	55,930	57,271	575	1,018	1,180	12	52	8	6	7	4	55,930	57,271	575	1,018	1,180	12	52	8	6	7	4	165	523						
TOTAL FOR PROJ NO. 2024CPT.08.08.10471							4.87					545	1.0000	120	55,930	57,271	575	1,018	1,180	12	52	8	6	7	4	55,930	57,271	575	1,018	1,180	12	52	8	6	7	4	55,930	57,271	575	1,018	1,180	12	52	8	6	7	4	165	523						
															113,201										77					113,201			12			77							688												
2024CPT.08.08.10831	Scotland	2	US-74 BUS	FROM CJ 1100' WEST OF SR 1611 (N ROCKY FORD RD) TO CJ WEST OF SR 1436 (AIRPORT RD)	1	2	2WU	2.01	26	6.55	8.56	230	*	21,786	12,155		113			4	1		4			22,918	12,295		113										4	1		4		636	24	171									
TOTAL FOR MAP NO. 2							2.01					230		21,786	12,155		113			4	1		4			22,918	12,295		113									4	1		4		636	24	171										
TOTAL FOR PROJ NO. 2024CPT.08.08.10831							2.01					230	1.0000	21,786	12,155		113			4	1		4			22,918	12,295		113								4	1		4		636	24	171											
															33,941										9					35,213			9			9							195												
2024CPT.08.08.20471	Hoke	3	SR-1003 / CAMDEN RD	FROM CUMBERLAND CO LINE TO SR 1406 (ROCKFISH RD)	2	2	2WU	0.98	22	0	0.98	104	*		9,692	7,551		93		2	2																							8	87										
TOTAL FOR MAP NO. 3							0.98					104			9,692	7,551		93		2	2																					8	87												
2024CPT.08.08.20471	Hoke	4	SR-1214 / CALLOWAY RD	FROM SR 1219 (ARMY RD) TO SR 1228 (RESERVATION RD)	2	2	2WU	2.38	22	5.39	7.77	266														24,906	21,606		41			4	5																						
TOTAL FOR MAP NO. 4							2.38					266														24,906	21,606		41			4	5																						
2024CPT.08.08.20471	Hoke	5	SR-1203 / TURNPIKE RD	FROM SR 1206 (L. MCLAUGHLIN RD) TO NC 211	2	2	2WU	1.35	22	6.31	7.66	146														14,010	10,800																												
TOTAL FOR MAP NO. 5							1.35					146														14,010	10,800																												
TOTAL FOR PROJ NO. 2024CPT.08.08.20471							4.71					516	1.0000		9,692	7,551		93			2	2				38,916	32,406		41			4	5											8	87										
															17,243										4					71,322			9			9							95												
2024CPT.08.08.20831	Scotland	6	SR-1116 / BLUE WOODS RD	FROM SR 1118 (JIM CALHOUN RD) TO SR 1108 (XWAY RD)	2	2	2WU	1.47	20	1.1	2.57	161	*													15,030	13,885																												
TOTAL FOR MAP NO. 6							1.47					161														15,030	13,885																												
2024CPT.08.08.20831	Scotland	7	SR-1326 / MONROE RD	FROM US 15-501 (ABERDEEN RD) TO SR 1324 (SNEADTOWN RD)	2	2	2WU	1.42	19	1.6	3.02	158														14,911	10,960																												
TOTAL FOR MAP NO. 7							1.42					158															14,911	10,960																											
2024CPT.08.08.20831	Scotland	8	SR-1328 / HOFFMAN RD	FROM SR 1324 (SNEADTOWN RD) TO SR 1334 (CAMP MONROE RD)	2	2	2WU	0.9	21	1.07	1.97	101													9,640	6,630																													
TOTAL FOR MAP NO. 8							0.9					101														9,640	6,630																												
TOTAL FOR PROJ NO. 2024CPT.08.08.20831							3.79					420	1.0000													39,581	31,475																												
																											90					290,792			21			86							978										
GRAND TOTAL								15.38					1,711	1.0000	120	87,408	76,977	575	1,224	1,180	12	58	11	6	11	4		157,345	133,447	575	1,131	1,221	12	4	5	56	9	6	11	4		636	197	781											

SIGNING FOR RESURFACING PROJECTS

LEGEND
 ┆ STATIONARY SIGN
 ← DIRECTION OF TRAFFIC FLOW



MAINLINE (-L-) SIGNING

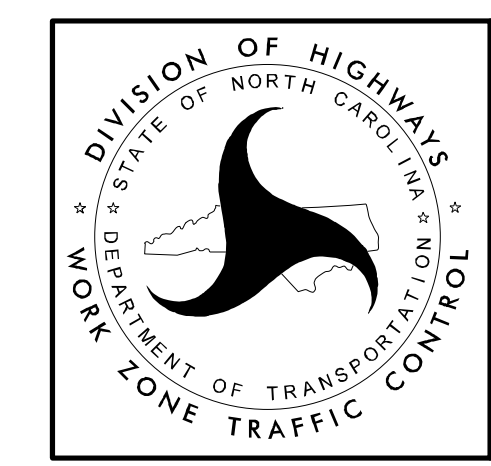
-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION		<p>PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</p> <p>#2 SIGN ONLY USED WHEN CONSTRUCTION LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)</p>	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div> <p>PLACED 500' IN ADVANCE OF FLAGGER.</p> </div> <div> <p>PLACED 250' IN ADVANCE OF FLAGGER.</p> </div> </div>
		<p>- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER.</p> <p>- AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.</p>	
		<p>- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS.</p> <p>- DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS.</p> <p>- INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE.</p> <p>- FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH.</p> <p>- A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</p> <p>- FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.</p>	
		<p>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.</p>	

THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.

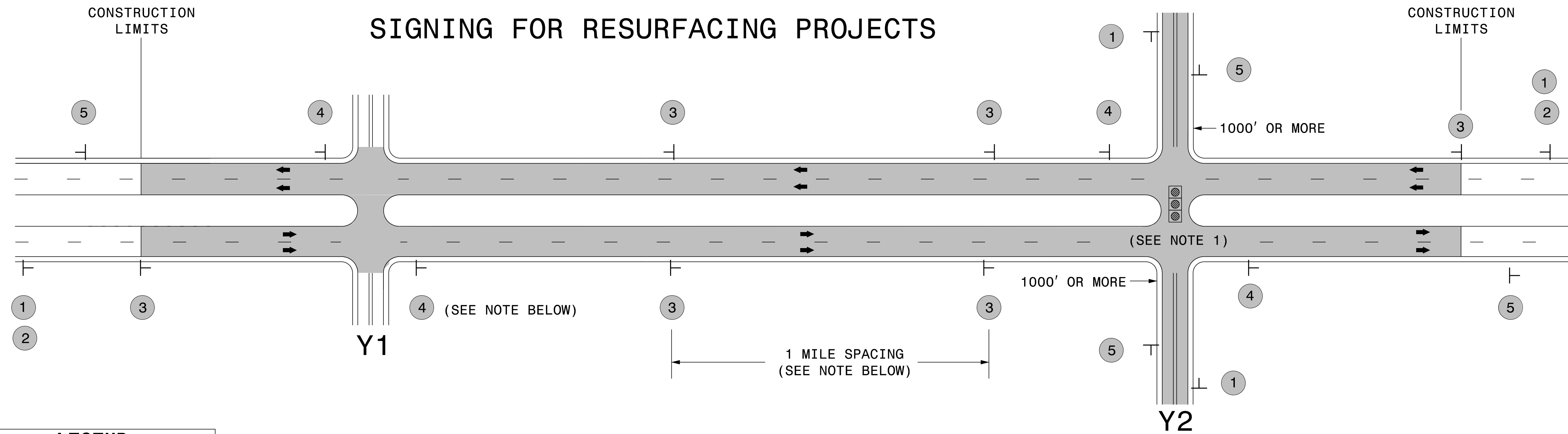
MAPS LESS THAN 2 MILES

FOR RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, NO STATIONARY SIGNS ARE REQUIRED. USE PORTABLE "ROAD UNDER CONSTRUCTION" OR "ROAD WORK AHEAD" SIGNS IN LIEU OF STATIONARY ADVANCE WARNINGS SIGNS.



ADVANCE WARNING SIGNS FOR RURAL AND SUBURBAN 2-LANE ROADWAY RESURFACING

5/15/2017 S:\TMU\WZTC\Resurfacing\2L2W & AST Resurfacing Details\Resurfacing_AdvWarn_2Ln.dgn User:kadai

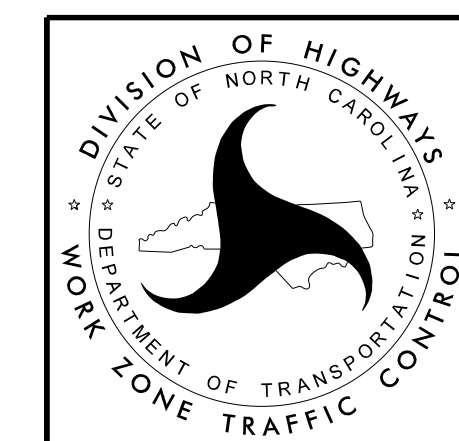


LEGEND	
┆	STATIONARY SIGN
←	DIRECTION OF TRAFFIC FLOW

MAINLINE (-L-) SIGNING

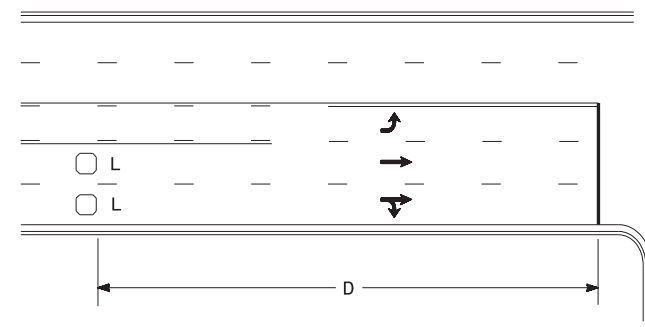
-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	MAINLINE (-L-) SIGNING		-Y- LINE SIGNING	
	1	 <small>W20-1 48" X 48"</small>	<p>PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</p>	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around; align-items: center;"> <div style="text-align: center;"> <small>W20-1 48" X 48"</small> </div> <div style="text-align: center;"> <small>W20-7 A 48" X 48"</small> </div> </div> <p>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p>
	2	 <small>W7-3aP 24" X 18"</small>	<p>#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)</p>	
	3	 <small>SP 13107 48" X 48"</small>	<p>PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. IF NO -Y- LINES EXIST, PLACE 2ND SET 1/2 MILE FROM THE CONSTRUCTION LIMITS AND THEN SPACE 1 MILE THEREAFTER.</p>	
	4	 <small>SP 13106 48" X 48"</small>	<p>THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</p>	
5	 <small>G20-2 A 48" X 24"</small>	<p>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.</p>		
			<p>NOTES:</p> <ol style="list-style-type: none"> 1) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION. 	



**RESURFACING
ADVANCE WARNING SIGNS
FOR RURAL AND SUBURBAN
MULTI-LANE ROADWAYS
W/ SHOULDER SECTIONS**

High Speed Detection (≥40 mph)

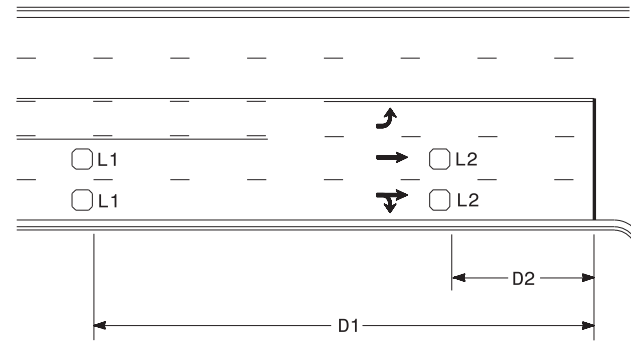


Speed Limit mph	D ft
40	250
45	300
50	355
55	420

L = 6ft X 6ft
Wired separately

Volume Density Operation

OR

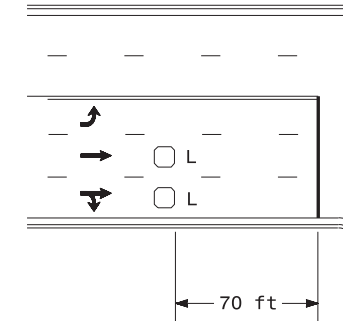


Speed Limit mph	D1 ft	D2 ft
40	250	80
45	300	90
50	355	100
55	420	110

L1 = 6ft X 6ft
Wired in series
L2 = 6ft X 6ft
Wired in series

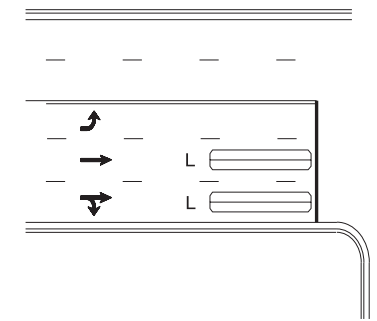
"Stretch" Operation

Low Speed Detection (≤35 mph)



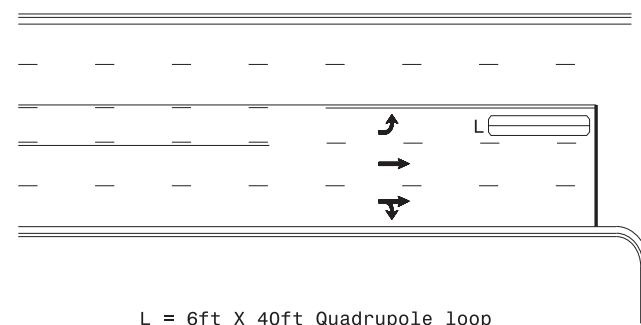
L = 6ft X 6ft
Wired in series

OR



L = 6ft X 40ft
Quadrupole loop, wired separately

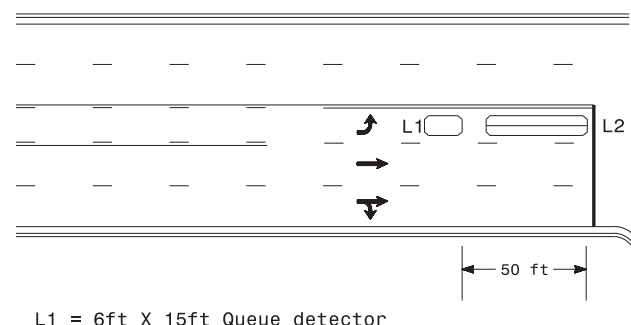
Left Turn Lane Detection



L = 6ft X 40ft Quadrupole loop

Presence Loop Detection

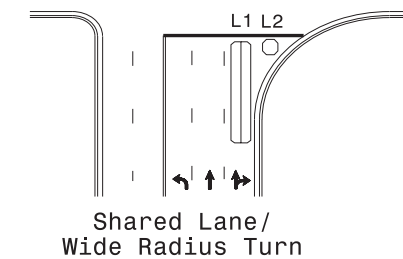
OR



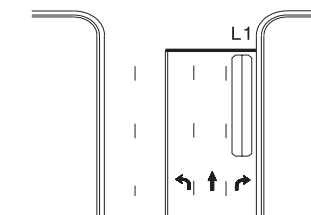
L1 = 6ft X 15ft Queue detector
L2 = 6ft X 40ft Quadrupole loop

Queue Loop Detection

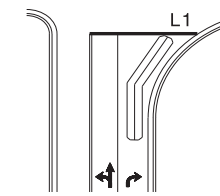
Right Turn Lane Detection



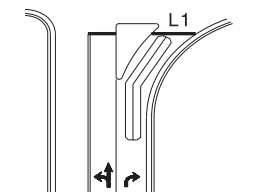
L1 = 6ft X 40ft Quadrupole loop
L2 = 6ft X 6ft [Minimum] Presence loop
Wired separately



Standard Turn

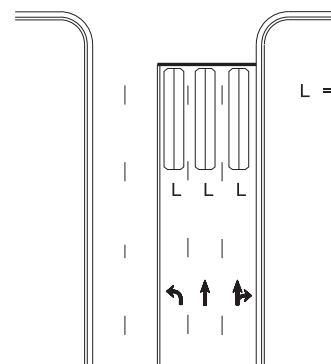


Wide Radius Turn



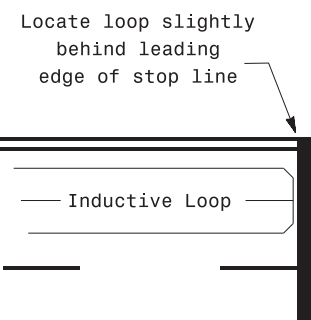
Channelized Turn

Side Street Detection



L = 6ft X 40ft
Quadrupole loop
Wired to separate
detectors/channels

Presence Loop Placement at Stop Lines



Note:
Loop may be located in advance of stop line under any of the following conditions:
1) stop line is greater than 15' from edge of intersecting roadway
2) loop detects a permissive or protected/permissive left turn
3) for an exclusive right turn lane

Recommended Number of Turns

Single 6' X 6' loop
(when wired separately):

Length of Lead-in ft	Number of Turns
< 250	3
250-375	4
375-525	5
> 525	6

Quadrupole loops: Use 2-4-2 turns

6' X 15' Loops:
Lead-in < 150', use 2 turns
Lead-in > 150', use 3 turns

750 N. Greenfield Pkwy, Garner, NC 27529

Prepared in the Offices of:

Typical Signal Loop Locations	
PLAN DATE: September 2020	REVIEWED BY: JPG
PREPARED BY: PLA	REVIEWED BY:
SCALE: N/A	REVISIONS: INIT. DATE
<p>9/8/2020</p> <p>SIG. INVENTORY NO.</p>	