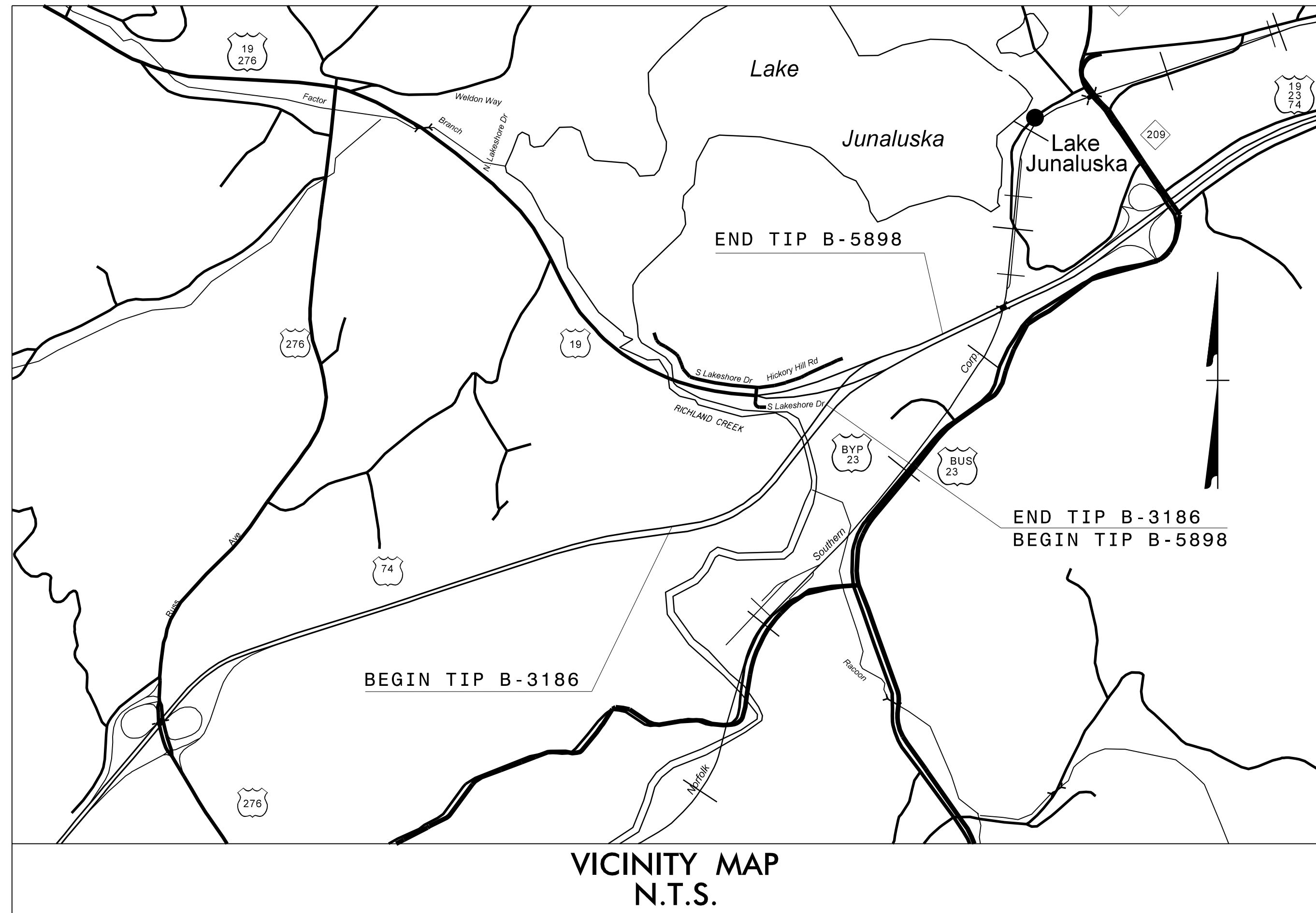
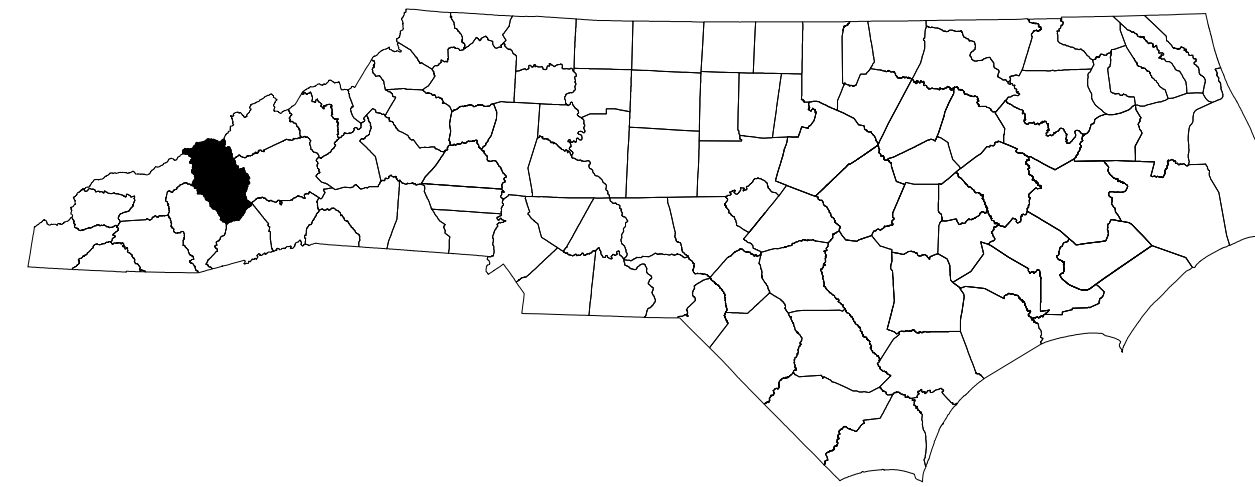


STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

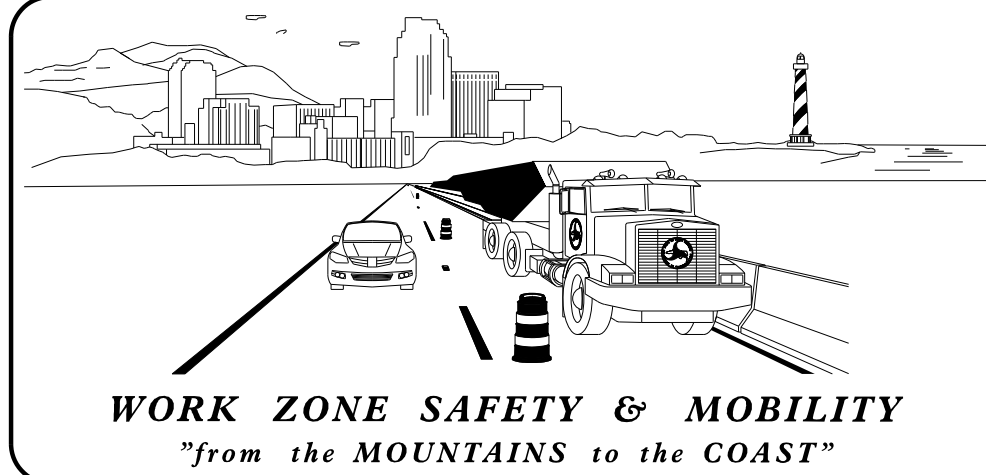
TRANSPORTATION MANAGEMENT PLAN
HAYWOOD COUNTY



VICINITY MAP
N.T.S.
LOCATION: US-74 BRIDGES OVER RICHLAND CREEK AND US-19

SHEET NO.	TITLE
TMP-01	TITLE SHEET, VICINITY MAP, AND INDEX OF SHEETS
TMP-01A	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, AND LEGEND
TMP-01B THRU 01C	TRANSPORTATION OPERATIONS PLAN: (MANAGEMENT STRATEGIES, GENERAL NOTES, AND LOCAL NOTES)
TMP-02	PORTABLE CONCRETE BARRIER AT TEMPORARY SHORING LOCATIONS
TMP-02A THRU 02D	TEMPORARY SHORING NOTES
TMP-02E	BEGIN WORK SIGN DESIGN
TMP-02F	TEMPORARY DETOUR - NB US19 (PHASE III)
TMP-02G	WZ VARIABLE SPEED LIMIT REDUCTION DETAILS
TMP-03 THRU 03A1	TEMPORARY TRAFFIC CONTROL PHASING
TMP-03B THRU 03C	TEMPORARY TYPICAL BRIDGE SECTIONS OVERVIEW
TMP-04 THRU 07	TEMPORARY TRAFFIC CONTROL PHASE I DETAILS
TMP-08 THRU 11	TEMPORARY TRAFFIC CONTROL PHASE II DETAILS
TMP-12 THRU 16	TEMPORARY TRAFFIC CONTROL PHASE III DETAILS
TMP-17	TEMPORARY TRAFFIC CONTROL PHASE IIA DETAILS
TMP-17A	TEMPORARY TRAFFIC CONTROL PHASE IIB DETAILS
TMP 18A	TEMPORARY TRAFFIC CONTROL PHASE IIIA DETAILS
TMP 18B	TEMPORARY TRAFFIC CONTROL PHASE IIIB DETAILS
TMP-19 THRU 22	TEMPORARY TRAFFIC CONTROL PHASE IV DETAILS
TMP-23 THRU 26	TEMPORARY TRAFFIC CONTROL PHASE V DETAILS

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jeffrey.thurston

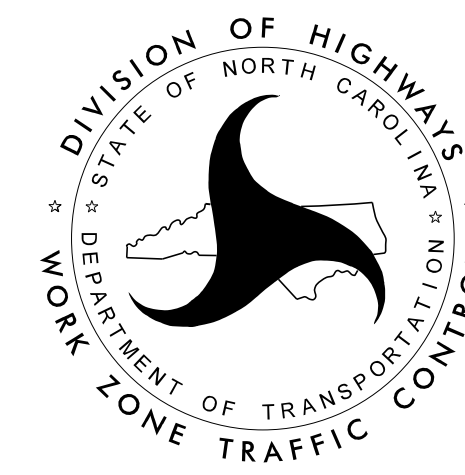


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DATE: 10/17/2023



ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS SHOWN IN "ROADWAY STANDARD DRAWINGS" - N.C. DEPARTMENT OF TRANSPORTATION - RALEIGH, N.C., DATED JANUARY 2018 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.01	WORK ZONE WARNING SIGNS
1101.02	TEMPORARY LANE CLOSURES
1101.03	TEMPORARY ROAD CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW BOARDS
1130.01	DRUMS
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGERS
1160.01	TEMPORARY CRASH CUSHION
1165.01	TRUCK MOUNTED ATTENUATOR
1170.01	PORTABLE CONCRETE BARRIER
1180.01	SKINNY DRUMS
1205.01	PAVEMENT MARKINGS - LINE TYPES AND OFFSETS
1205.02	PAVEMENT MARKINGS - TWO LANE AND MULTILANE ROADWAYS
1205.03	PAVEMENT MARKINGS - EXITS AND ENTRANCE RAMPS
1205.06	PAVEMENT MARKINGS - LANE DROPS
1205.08	PAVEMENT MARKINGS - SYMBOLS AND WORD MESSAGES
1205.12	PAVEMENT MARKINGS - BRIDGES
1205.13	PAVEMENT MARKINGS - LANE REDUCTIONS
1250.01	RAISED PAVEMENT MARKERS - INSTALLATION SPACING
1251.01	RAISED PAVEMENT MARKERS - (PERMANENT AND TEMPORARY)
1261.01	GUARDRAIL AND BARRIER DELINEATORS - INSTALLATION SPACING
1261.02	GUARDRAIL AND BARRIER DELINEATORS - TYPES AND MOUNTING
1262.01	GUARDRAIL END DELINEATION

LEGEND

GENERAL

- DIRECTION OF TRAFFIC FLOW
- EXIST. PVMT.
- NORTH ARROW
- PROPOSED PVMT.
- TEMP. SHORING (LOCATION PURPOSES ONLY)

- WORK AREA
- WEDGING & WIDENING
- TEMPORARY PAVEMENT
- PAVEMENT / BRIDGE REMOVAL

SIGNALS

- EXISTING
- PROPOSED
- TEMPORARY
- PORTABLE

PAVEMENT MARKINGS

- EXISTING LINES
- TEMPORARY LINES

TRAFFIC CONTROL DEVICES

- BARRICADE (TYPE III)
- CONE
- DRUM
- SKINNY DRUM
- TUBULAR MARKER
- TEMPORARY CRASH CUSHION
- FLASHING ARROW BOARD
- FLAGGER
- LAW ENFORCEMENT
- TRUCK MOUNTED ATTENUATOR (TMA)
- CHANGEABLE MESSAGE SIGN
- PORTABLE CONCRETE BARRIER (ANCHORED)

TEMPORARY SIGNING

- PORTABLE SIGN
- STATIONARY SIGN
- STATIONARY OR PORTABLE SIGN

PAVEMENT MARKERS

- CRYSTAL/CRYSTAL
- CRYSTAL/RED
- YELLOW/YELLOW

PAVEMENT MARKING SYMBOLS

- PAVEMENT MARKING SYMBOLS

TEMPORARY PAVEMENT MARKING

WORK ZONE PERFORMANCE *	
Z20	6" WHITE EDGELINE
Z21	6" WHITE SOLID LANE LINE
Z22	6" 10 FT. WHITE SKIP
Z23	6" 3 FT.-9 FT./SP WHITE MINISKIP
Z30	6" YELLOW EDGELINE
Z50	12" WHITE GORELINE
Z72	STRAIGHT ARROW
Z73	COMBO. LEFT/STRAIGHT ARROW

* INSTALL WORK ZONE PERFORMANCE COLD APPLIED PLASTIC (TYPE IV) ON CONCRETE BRIDGE DECK AREAS (SEE TMP-1B LOCAL NOTE 3)

NOTE: SEE WORK ZONE PERFORMANCE MARKING SPECIAL PROVISION

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ROADWAY STANDARD DRAWINGS & LEGEND

GENERAL NOTES

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS, AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS OR RESULT IN DUPLICATE OR UNDESIRE OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING, OR REMOVAL OF DEVICES AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT EXCEPT WHEN OTHERWISE NOTED IN THE PLAN OR DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS - REQUIRES INTERMEDIATE CONTRACT TIME PROJECT SPECIAL PROVISIONS

A) DO NOT CLOSE OR NARROW TRAVEL LANES FOR LONGER THAN TWO CONSECUTIVE DAYS AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
US 23/US 74 & US 19 RAMP	MONDAY-FRIDAY 6:00 AM - 9:00 AM 3:00 PM - 6:00 PM

B) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAY AND SPECIAL EVENTS AS FOLLOWS:

- FOR ANY UNEXPECTED OCCURRENCE THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
- FOR EASTER, ALL TIMES BETWEEN THE THURSDAY BEFORE AND THE MONDAY AFTER.
- FOR MEMORIAL DAY, ALL TIMES BETWEEN THE FRIDAY BEFORE AND THE TUESDAY AFTER.
- FOR INDEPENDENCE DAY, NOON THE DAY BEFORE INDEPENDENCE DAY THROUGH 8:00 A.M. THE DAY AFTER INDEPENDENCE DAY. IF INDEPENDENCE DAY IS ON A FRIDAY, SATURDAY, SUNDAY OR MONDAY, THEN FROM NOON THE THURSDAY BEFORE INDEPENDENCE DAY TO THE TUESDAY AFTER INDEPENDENCE DAY.
- FOR LABOR DAY, ALL TIMES BETWEEN THE FRIDAY BEFORE AND THE TUESDAY AFTER.
- FOR THANKSGIVING DAY, ALL TIMES BETWEEN THE TUESDAY BEFORE AND THE MONDAY AFTER.
- FOR CHRISTMAS AND NEW YEAR'S DAY, ALL TIMES BETWEEN THE FRIDAY BEFORE THE WEEK OF CHRISTMAS AND THE DAY AFTER NEW YEAR'S DAY. IF NEW YEAR'S DAY IS ON A FRIDAY, SATURDAY OR SUNDAY, OR MONDAY, THEN THE FOLLOWING TUESDAY AFTER NEW YEAR'S DAY.
- FOR LEAF SEASON, THE FIRST WEEKEND IN OCTOBER THROUGH THE FIRST WEEKEND IN NOVEMBER AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
US 23/US 74 & US 19	MONDAY-THURSDAY 6:00 AM - 7:00 PM
	FRIDAY 6:00 AM - 9:00 PM
	SATURDAY 9:00 AM - 9:00 PM
	SUNDAY NOON - 8:00 PM

- FOR THE NORTH CAROLINA MOUNTAIN STATE FAIR (TYPICALLY HELD FOR 10 DAYS STARTING THE FRIDAY AFTER LABOR DAY), BETWEEN THE HOURS OF 3:00 PM THE FRIDAY FOLLOWING LABOR DAY AND 7:00 AM THE FOLLOWING MONDAY AFTER THE FAIR CONCLUDES.

C) DO NOT STOP TRAFFIC AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS	OPERATION & DURATION
US 74/US 23	MONDAY - SUNDAY 6:00 AM - 6:00 PM	30 MINS OVERHEAD SIGN, TRAFFIC SHIFTS, PAVEMENT MARKER INSTALLATION
US 19 RAMP	MONDAY - SUNDAY 6:00 AM - 6:00 PM	30 MINS RESET PCB

D) DO NOT CONDUCT SINGLE VEHICLE HAULING AS FOLLOWS; INGRESS AND EGRESS FROM RAMPS WILL BE ALLOWED:

ROAD NAME	DAY AND TIME RESTRICTIONS
US 19 RAMP & US 74/US 23	MONDAY - FRIDAY 6:00 AM - 9:00 AM & 3:00 PM-6:00 PM

E) DO NOT CONDUCT MULTI-VEHICLE HAULING AS FOLLOWS; INGRESS AND EGRESS FROM RAMPS WILL BE ALLOWED:

ROAD NAME	DAY AND TIME RESTRICTIONS
US 19 RAMP & US 74/US 23	MONDAY - FRIDAY 6:00 AM - 9:00 AM & 3:00 PM-6:00 PM

F) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE HAULING OPERATION IS PROTECTED BY BARRIER OR GUARDRAIL OR AS DIRECTED BY THE ENGINEER.

LANE AND SHOULDER CLOSURE REQUIREMENTS

G) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED OR AS DIRECTED BY THE ENGINEER.

H) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 15 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR A LANE CLOSURE IS INSTALLED.

I) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

J) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS, OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.

K) DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY, RAMP, OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.

L) DO NOT INSTALL MORE THAN ONE LANE CLOSURE IN ANY ONE DIRECTION ON US 74/US 23.

PAVEMENT EDGE DROP OFF REQUIREMENTS

M) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS AN EDGE OF PAVEMENT DROP-OFF AS FOLLOWS:

BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.

BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

N) DO NOT EXCEED A DIFFERENCE OF 2 INCHES IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC FOR NOMINAL LIFTS OF 1.5 INCHES. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) 500 FT IN ADVANCE AND A MINIMUM OF EVERY HALF MILE THROUGHOUT THE UNEVEN AREA.

TRAFFIC PATTERN ALTERATIONS

O) NOTIFY THE ENGINEER THIRTY (30) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

P) FOR STATIONARY AND PORTABLE WORK ZONE SIGNS, SEE HIGH VISIBILITY DEVICES SPECIAL PROVISION.

Q) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 40 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.

R) PROVIDE SIGNING AND DEVICES REQUIRED TO CLOSE OR SHIFT LANES ACCORDING TO THE ROADWAY STANDARD DRAWINGS AND TRAFFIC CONTROL PLANS.

PROVIDE SIGNING AND DEVICES FOR THE OFF-SITE DETOUR ROUTE AS SHOWN IN THE TRAFFIC CONTROL PLAN.

S) COVER OR REMOVE ALL SIGNS AND DEVICES REQUIRED TO CLOSE OR SHIFT LANES WHEN ROAD CLOSURE OR LANE SHIFT IS NOT IN OPERATION.

T) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

U) INSTALL BLACK ON ORANGE "DIP" SIGNS (W8-2) AND/OR "BUMP" SIGNS (W8-1) 500 FT IN ADVANCE OF THE UNEVEN AREA, OR AS DIRECTED BY THE ENGINEER.

CONTINUED...

TRAFFIC MANAGEMENT STRATEGIES

THE FOLLOWING LISTED WORK ZONE STRATEGIES ARE RECOMMENDED FOR INCLUSION WITHIN THIS TRANSPORTATION MANAGEMENT PLAN (TMP).

TRAFFIC MANAGEMENT STRATEGIES

- LANE SHIFTS OR CLOSURES
- TEMPORARY RAMP CLOSURES WITH OFF-SITE DETOUR
- FULL ROADWAY CLOSURES FOR OVERHEAD WORK
- SHOULDER CLOSURES
- NIGHT WORK
- WORK HOUR RESTRICTIONS FOR PEAK HOUR TRAVEL AND SPECIAL EVENTS

WORK ZONE SAFETY & MOBILITY STRATEGIES

- SPEED LIMIT REDUCTION
- DIGITAL SPEED LIMIT SIGNS
- SEQUENTIAL FLASHING LIGHTS TEMPORARY LANE CLOSURES
- WORK ZONE PERFORMANCE PAVEMENT MARKINGS
- BLACK PAVEMENT MARKING TO COVER CONFLICTING MARKINGS BEYOND PAVING LIMITS

TRAFFIC / INCIDENT MANAGEMENT & SPEED ENFORCEMENT STRATEGIES:

- COORDINATION WITH LOCAL MEDIA
- INCREASED PENALTIES FOR WORK ZONE VIOLATIONS

CONTRACTING & INNOVATIVE CONSTRUCTION STRATEGIES:

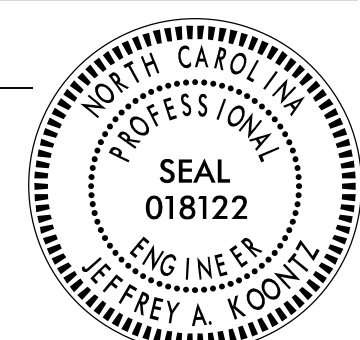
- INTERMEDIATE CONTRACT TIMES / LIQUIDATED DAMAGES

OVERVIEW

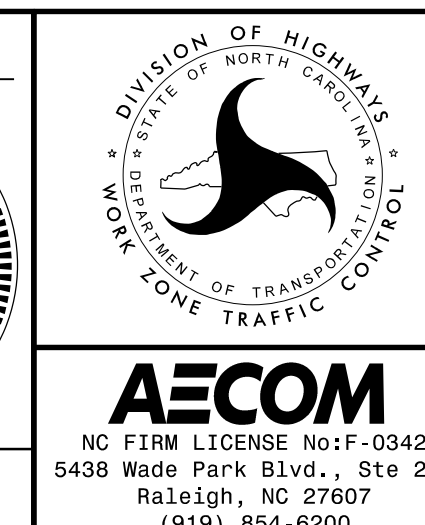
- US 74/US 23 CONSTRUCTED USING TEMPORARY ON-SITE DETOURS
- WESTBOUND US 74/US 23 FLYOVER BRIDGE AND BRIDGE OVER RICHLAND CREEK ARE CONSTRUCTED IN MULTIPLE STAGES.
- TEMPORARY SHORING REQUIRED
- SHORT TERM CLOSURES OF US 19 RAMP

DocuSigned by:
Jeffrey A. Koontz

APPROVED: *Jeffrey A. Koontz*
DATE: 11/9/2023



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PROJECT NOTES

**TRAFFIC
MANAGEMENT STRATEGIES**

GENERAL NOTES (CONTINUED)

LOCAL NOTES

TRAFFIC BARRIER

- V) INSTALL TEMPORARY BARRIER ACCORDING TO THE TRANSPORTATION MANAGEMENT PLANS A MAXIMUM OF TWO (2) WEEKS PRIOR TO BEGINNING WORK IN ANY LOCATION. TEMPORARY BARRIER OFFSET DIMENSIONS ARE PROVIDED TO THE TRAFFIC SIDE OF THE TEMPORARY BARRIER FACE. ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION PROCEED IN A CONTINUOUS MANNER TO COMPLETE THE PROPOSED WORK IN THAT LOCATION UNLESS OTHERWISE STATED IN THE TRANSPORTATION MANAGEMENT PLANS OR AS DIRECTED BY THE ENGINEER.

DO NOT PLACE BARRIER DIRECTLY ON ANY SURFACE OTHER THAN ASPHALT OR CONCRETE.

ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION AND NO WORK IS PERFORMED BEHIND THE TEMPORARY BARRIER FOR A PERIOD LONGER THAN TWO (2) MONTHS, REMOVE / RESET TEMPORARY BARRIER AT NO COST TO THE DEPARTMENT UNLESS OTHERWISE STATED IN THE TRANSPORTATION MANAGEMENT PLANS, TEMPORARY BARRIER IS PROTECTING A HAZARD, OR AS DIRECTED BY THE ENGINEER.

INSTALL TEMPORARY BARRIER WITH THE TRAFFIC FLOW BEGINNING WITH THE UPSTREAM SIDE OF TRAFFIC. REMOVE TEMPORARY BARRIER AGAINST THE TRAFFIC FLOW BEGINNING WITH THE DOWNSTREAM SIDE OF TRAFFIC.

INSTALL AND SPACE DRUMS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH) TO CLOSE OR KEEP THE SECTION OF THE ROADWAY CLOSED UNTIL THE TEMPORARY BARRIER CAN BE PLACED OR AFTER THE TEMPORARY BARRIER IS REMOVED.

- W) PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER AT ALL TIMES DURING THE INSTALLATION AND REMOVAL OF THE BARRIER BY EITHER A TRUCK MOUNTED ATTENUATOR (MAXIMUM 72 HOURS) OR A TEMPORARY CRASH CUSHION.

PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER FROM ONCOMING TRAFFIC AT ALL TIMES BY A TEMPORARY CRASH CUSHION UNLESS THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER IS OFFSET FROM ONCOMING TRAFFIC AS FOLLOWS OR AS SHOWN IN THE PLANS: (SEE ALSO 1101.05)

POSTED SPEED LIMIT	MINIMUM OFFSET
40 OR LESS	15 FT
45 - 50	20 FT
55	25 FT
60 MPH or HIGHER	30 FT

TRAFFIC CONTROL DEVICES

- X) WHEN LANE CLOSURES ARE NOT IN EFFECT SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER IN FEET THAN TWICE THE POSTED SPEED LIMIT (MPH) EXCEPT, 10 FT ON-CENTER IN RADII, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY. REFER TO STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES SECTIONS 1130 (DRUMS), 1135 (CONES) AND 1180 (SKINNY DRUMS) FOR ADDITIONAL REQUIREMENTS, AND HIGH VISIBILITY DEVICES SPECIAL PROVISION.
- Y) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY WIDTH REQUIRED.
- Z) PLACE ADDITIONAL SETS OF THREE CHANNELIZING DEVICES (DRUMS) PERPENDICULAR TO THE EDGE OF TRAVELWAY ON 500 FT CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC.

PAVEMENT MARKINGS AND MARKERS

- AA) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:

ROAD NAME	MARKING	MARKER
US 74 & US 19	WORK ZONE PERFORMANCE	TEMPORARY RAISED
US 74 & US 19	W.Z.PERF.COLD APPLIED PLASTIC(IV)-CONC BR. DECKS	TEMPORARY RAISED

- BB) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.

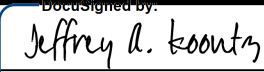

- CC) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS BY THE END OF EACH DAY'S OPERATION.



MISCELLANEOUS

- DD) LAW ENFORCEMENT MAY BE USED TO MAINTAIN TRAFFIC THROUGH THE WORK AREA AS DIRECTED BY THE ENGINEER.

- COORDINATE WITH THE ENGINEER AND EMERGENCY SERVICE PERSONNEL REGARDING ALL TRAFFIC ALTERATIONS.
- COORDINATE WITH THE ENGINEER TO ENSURE THAT THE OVERSIZE/OVERWEIGHT PERMIT UNIT IS AWARE OF ONGOING TRAFFIC OPERATIONS AND CLEAR ROADWAY WIDTHS THROUGHOUT CONSTRUCTION.
- IF USING REMOVABLE TAPE ON BRIDGE DECK, INSTALL TEMPORARY RAISED MARKERS HALF-ON/HALF-OFF AT 20' SPACING TO HOLD TAPE IN PLACE.
- REFER TO ROADWAY PLAN SHEETS FOR ALL PAVEMENT DETAILS.
- FOR DRUMS IN MERGE TAPERS, SEE SEQUENTIAL FLASHING WARNING LIGHTS SPECIAL PROVISION.
- WORK IN A CONTINUOUS MANNER WHEN TEMPORARY LANE CLOSURES ARE REQUIRED. DO NOT CLOSE ONE LANE OF US 74/US 23 FOR MORE THAN TWO CONSECUTIVE DAYS.
- COORDINATE AND PROVIDE 30 DAYS ADVANCE NOTICE TO THE TOWN OF WAYNESVILLE PRIOR TO CLOSURE OF THE GREENWAY. GREENWAY SHALL REMAIN CLOSED DURING CONSTRUCTION.
- DO NOT DISTURB EXISTING SANITARY SEWER LINE.
- COORDINATE WITH U-5839 (HAYWOOD) TO ENSURE THEIR OFF-SITE DETOUR SIGNS PLACED WITHIN B-3186/B-5898 PROJECT LIMITS ARE MAINTAINED.

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<h2>PROJECT NOTES</h2>

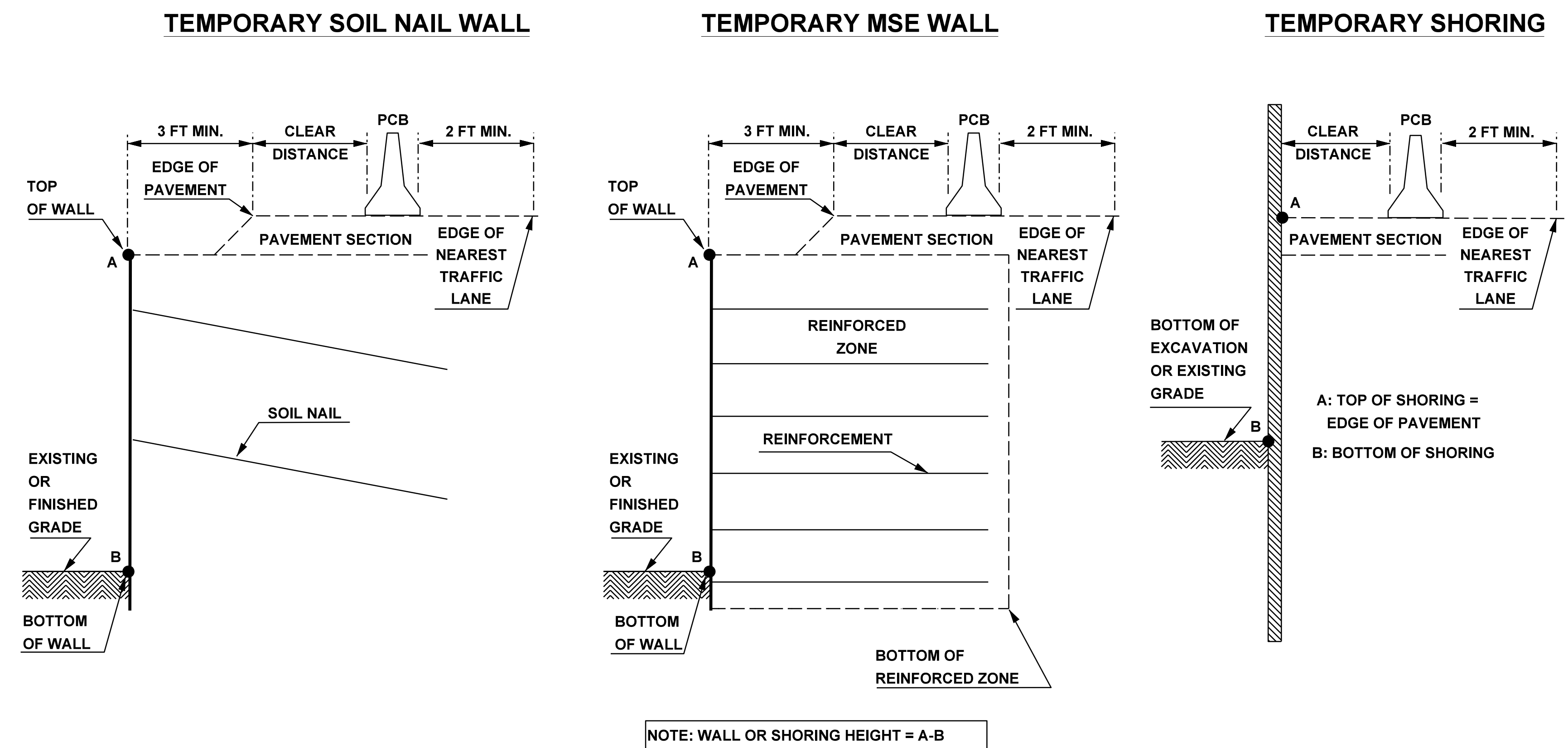


FIGURE A

NOTES

- 1- REFER TO THE TRAFFIC CONTROL PLANS FOR TEMPORARY SHORING LOCATIONS AND NOTES.
- 2- REFER TO THE "TEMPORARY SHORING" STANDARD PROVISION FOR INFORMATION ABOUT TEMPORARY SHORING AND PORTABLE CONCRETE BARRIER (PCB).
- 3- PCB IS REQUIRED IF TEMPORARY SHORING/WALL IS LOCATED WITHIN THE CLEAR ZONE IN ACCORDANCE WITH THE AASHTO ROADSIDE DESIGN GUIDE. DO NOT PLACE BARRIER DIRECTLY ON ANY SURFACE OTHER THAN ASPHALT OR CONCRETE. (CONTACT NCDOT PAVEMENT MANAGEMENT FOR APPLICABLE PAVEMENT DESIGN).
- 4- BASED ON THE CLEAR DISTANCE, OFFSET, DESIGN SPEED AND PAVEMENT TYPE, CHOOSE AN UNANCHORED OR ANCHORED PCB FROM THE TABLE SHOWN IN FIGURE B. CLEAR DISTANCE IS DEFINED AS SHOWN IN FIGURE A AND OFFSET IS DEFINED AS SHOWN IN FIGURE B.
- 5- AT THE CONTRACTOR'S OPTION OR IF THE MINIMUM REQUIRED CLEAR DISTANCE IS NOT AVAILABLE, SET PCB NEXT TO AND UP AGAINST THE TRAFFIC SIDE OF THE TEMPORARY SHORING/WALLS EXCEPT FOR BARRIER ABOVE TEMPORARY WALLS. PCB WITH THE MINIMUM REQUIRED CLEAR DISTANCE IS REQUIRED ABOVE TEMPORARY WALLS.
- 6- USE NCDOT PORTABLE CONCRETE BARRIER (PCB) IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1170.01 AND SECTION 1170 OF THE STANDARD SPECIFICATIONS.
- 7- SET PCB WITH A MINIMUM HORIZONTAL DISTANCE OF 2 FT BETWEEN THE FRONT FACE OF THE BARRIER AND THE EDGE OF THE NEAREST TRAFFIC LANE AS SHOWN IN FIGURE A UNLESS OTHERWISE SHOWN IN THE PLANS OR APPROVED BY THE ENGINEER.
- 8- FOR PCB ABOVE AND BEHIND TEMPORARY WALLS, PROVIDE A MINIMUM DISTANCE OF 3 FT BETWEEN THE EDGE OF PAVEMENT AND THE WALL FACE AS SHOWN IN FIGURE A. IF THIS MINIMUM REQUIRED DISTANCE IS NOT AVAILABLE, CONTACT THE ENGINEER.
- 9- TABLE SHOWN IN FIGURE B IS BASED ON NCDOT RESEARCH PROJECT NO. 2005-010 WITH VEHICLE TYPE USED FOR NCHRP 350 CRASH TESTS.

MINIMUM REQUIRED CLEAR DISTANCE, inches

	Pavement Type	Offset * ft	Design Speed, mph					
			<30	31-40	41-50	51-60	61-70	71-80
Unanchored PCB	Asphalt	<8	24	26	29	32	36	40
		8-14	26	28	31	35	38	42
		14-20	27	29	34	36	39	43
		20-26	28	31	35	38	40	44
		26-32	29	32	36	39	42	45
		32-38	30	34	38	41	43	46
		38-44	31	34	41	43	45	48
		44-50	31	35	41	43	46	49
	50-56	32	36	42	44	47	50	
	>56	32	36	42	45	47	51	
	Concrete	<8	17	18	21	22	25	26
		8-14	19	20	23	25	26	29
		14-20	22	22	24	26	28	31
		20-26	23	24	26	27	30	34
26-32		24	25	27	28	32	35	
32-38		24	26	27	30	33	36	
Anchored PCB	Asphalt	All Offsets	24 for All Design Speeds					
		Concrete (including bridge approach slabs)	All Offsets	12 for All Design Speeds				

* See Figure Below

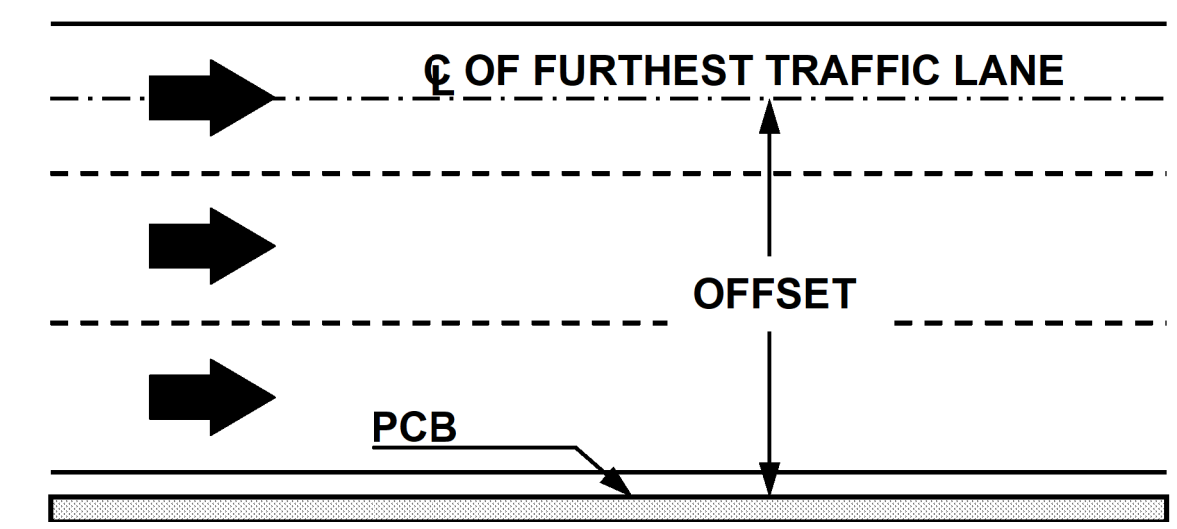


FIGURE B

9/6/2023 B3186-B5898.TC_TMP-02.dgn Jeffrey.Thurston

<p>APPROVED: <i>Jeffrey A. Kooty</i></p> <p>DATE: 10/17/2023</p>		<p>AECOM</p> <p>NC FIRM LICENSE No: F-0342 5438 Wade Park Blvd., Ste 200 Raleigh, NC 27607 (919) 854-6200</p>
<p>PORTABLE CONCRETE BARRIER AT TEMPORARY SHORING LOCATIONS</p>		
<p>DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED</p>		

TEMPORARY SHORING NOTES

TEMPORARY SHORING NO. ① (SEE SHEET TMP-05)

TEMPORARY SHORING NO. ② (SEE SHEET TMP-05)

TEMPORARY SHORING NO. ③ (SEE SHEET TMP-05)

FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.

FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.

FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.

DESIGN TEMPORARY SHORING FROM STATION 19+75 +/- -DET01EB-, 22.0' LT, TO STATION 22+20 +/- -DET01EB-, 22.0' LT, FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:

DESIGN TEMPORARY SHORING FROM STATION 21+10 +/- -DET01EB-, 21.5' RT, TO STATION 22+20 +/- -DET01EB-, 23.0' RT, FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:

DESIGN TEMPORARY SHORING FROM STATION 25+50 +/- -DET01EB-, 22.0' LT, TO STATION 26+50 +/- -DET01EB-, 22.0' LT, FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:

UNIT WEIGHT, $\gamma = 120$ PCF
 FRICTION ANGLE, $\phi = 30$
 COHESION, $c = 0$ PSF
 GROUNDWATER ELEVATION = 2573 FT.

UNIT WEIGHT, $\gamma = 120$ PCF
 FRICTION ANGLE, $\phi = 30$
 COHESION, $c = 0$ PSF
 GROUNDWATER ELEVATION = 2573 FT.

UNIT WEIGHT, $\gamma = 120$ PCF
 FRICTION ANGLE, $\phi = 30$
 COHESION, $c = 0$ PSF
 GROUNDWATER ELEVATION = 2573 FT.

BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.

BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.

BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.

LIMITED SUBSURFACE INFORMATION IS AVAILABLE IN THE VICINITY OF TEMPORARY SHORING FROM STATION 19+75 +/- -DET01EB-, 22.0' LT, TO STATION 22+20 +/- -DET01EB-, 22.0' LT. THE INFORMATION PROVIDED FOR TEMPORARY SHORING DESIGN WAS ASSUMED AND MAY NOT BE APPLICABLE TO THE ACTUAL SITE CONDITIONS ENCOUNTERED DURING CONSTRUCTION.

LIMITED SUBSURFACE INFORMATION IS AVAILABLE IN THE VICINITY OF TEMPORARY SHORING FROM STATION 21+10 +/- -DET01EB-, 21.5' RT, TO STATION 22+20 +/- -DET01EB-, 23.0' RT. THE INFORMATION PROVIDED FOR TEMPORARY SHORING DESIGN WAS ASSUMED AND MAY NOT BE APPLICABLE TO THE ACTUAL SITE CONDITIONS ENCOUNTERED DURING CONSTRUCTION.

LIMITED SUBSURFACE INFORMATION IS AVAILABLE IN THE VICINITY OF TEMPORARY SHORING FROM STATION 25+50 +/- -DET01EB-, 22.0' LT, TO STATION 26+50 +/- -DET01EB-, 22.0' LT. THE INFORMATION PROVIDED FOR TEMPORARY SHORING DESIGN WAS ASSUMED AND MAY NOT BE APPLICABLE TO THE ACTUAL SITE CONDITIONS ENCOUNTERED DURING CONSTRUCTION.

DRIVEN PILING FOR TEMPORARY SHORING FROM STATION 19+75 +/- -DET01EB-, 22.0' LT, TO STATION 22+20 +/- -DET01EB-, 22.0' LT, MAY NOT PENETRATE BELOW ELEVATION 2550 FT DUE TO OBSTRUCTIONS, VERY DENSE OR HARD SOIL, BOULDERS OR WEATHERED OR HARD ROCK.

DRIVEN PILING FOR TEMPORARY SHORING FROM STATION 21+10 +/- -DET01EB-, 21.5' RT, TO STATION 22+20 +/- -DET01EB-, 23.0' RT, MAY NOT PENETRATE BELOW ELEVATION 2550 FT DUE TO OBSTRUCTIONS, VERY DENSE OR HARD SOIL, BOULDERS OR WEATHERED OR HARD ROCK.

AT THE CONTRACTOR'S OPTION, USE STANDARD TEMPORARY SHORING FOR TEMPORARY SHORING FROM STATION 25+50 +/- -DET01EB-, 22.0' LT, TO STATION 26+50 +/- -DET01EB-, 22.0' LT. SEE GEOTECHNICAL STANDARD DETAIL NO. 1801.01 FOR STANDARD TEMPORARY SHORING AND DETAIL NO. 1801.02 FOR STANDARD WALLS.

AT THE CONTRACTOR'S OPTION, USE STANDARD TEMPORARY SHORING FOR TEMPORARY SHORING FROM STATION 19+75 +/- -DET01EB-, 22.0' LT, TO STATION 22+20 +/- -DET01EB-, 22.0' LT. SEE GEOTECHNICAL STANDARD DETAIL NO. 1801.01 FOR STANDARD TEMPORARY SHORING AND DETAIL NO. 1801.02 FOR STANDARD WALLS.

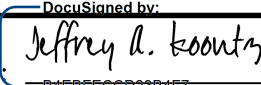
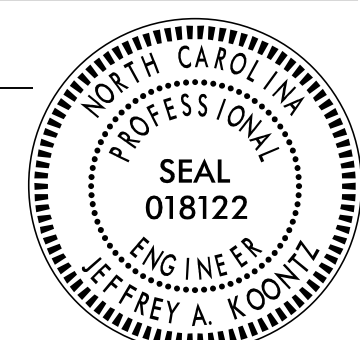

AT THE CONTRACTOR'S OPTION, USE STANDARD TEMPORARY SHORING FOR TEMPORARY SHORING FROM STATION 21+10 +/- -DET01EB-, 21.5' RT, TO STATION 22+20 +/- -DET01EB-, 23.0' RT. SEE GEOTECHNICAL STANDARD DETAIL NO. 1801.02 FOR STANDARD WALLS.

WHEN BACKFILL FOR RETAINING WALLS AND/OR BRIDGE APPROACH FILLS OVELAPS WITH THE REINFORCED ZONE OF TEMPORARY WALLS, USE SHORING BACKFILL OR BACKFILL MATERIAL REQUIRED FOR RETAINING WALLS AND/OR BRIDGE APPROACH FILLS, WHICHEVER IS BETTER, IN THE REINFORCED ZONE OF TEMPORARY WALLS.

WHEN BACKFILL FOR RETAINING WALLS AND/OR BRIDGE APPROACH FILLS OVELAPS WITH THE REINFORCED ZONE OF TEMPORARY WALLS, USE SHORING BACKFILL OR BACKFILL MATERIAL REQUIRED FOR RETAINING WALLS AND/OR BRIDGE APPROACH FILLS, WHICHEVER IS BETTER, IN THE REINFORCED ZONE OF TEMPORARY WALLS.

THE TEMPORARY SHORING NOTES SHOWN ON THIS SHEET WERE PROVIDED THROUGH A SEALED DOCUMENT PREPARED BY AECOM FOR THE GEOTECHNICAL ENGINEERING UNIT. THE DOCUMENT WAS SUBMITTED TO THE WZTC SECTION ON 8/31/2023 AND SEALED BY A PROFESSIONAL ENGINEER, RYAN PATRICK DOYLE, LICENSE #045161.

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APPROVED:  DATE: 10/17/2023		 AECOM <small>NC FIRM LICENSE No: F-0342 5438 Wade Park Blvd., Ste 200 Raleigh, NC 27607 (919) 854-6200</small>	<h2 style="margin: 0;">TEMPORARY SHORING NOTES</h2>
DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED			

TEMPORARY SHORING NOTES

TEMPORARY SHORING NO. 4 (SEE SHEET TMP-09)

TEMPORARY SHORING NO. 5 (SEE SHEET TMP-09)

TEMPORARY SHORING NO. 6 (SEE SHEET TMP-10)

FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.

FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.

FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.

DESIGN TEMPORARY SHORING FROM STATION 16+93 +/- -DET01WB-, 17.0' LT, TO STATION 23+75 +/- -DET01WB-, 17.0' LT, FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:

DESIGN TEMPORARY SHORING FROM STATION 25+75 +/- -DET01WB-, 17.0' LT, TO STATION 28+06 +/- -DET01WB-, 17.0' LT, FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:

DESIGN TEMPORARY SHORING FROM STATION 32+69 +/- -L_LT-, 8.0' RT, TO STATION 32+74 +/- -L_LT-, 7.0' LT, TO STATION 35+00 +/- -L_LT-, 7.0' LT, FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:

UNIT WEIGHT, $\gamma=120$ PCF
 FRICTION ANGLE, $\phi = 30$
 COHESION, $c = 0$ PSF
 GROUNDWATER ELEVATION = 2573 FT.

UNIT WEIGHT, $\gamma=120$ PCF
 FRICTION ANGLE, $\phi = 26$
 COHESION, $c = 0$ PSF
 GROUNDWATER ELEVATION = 2573 FT.

UNIT WEIGHT, $\gamma=120$ PCF
 FRICTION ANGLE, $\phi = 26$
 COHESION, $c = 0$ PSF
 GROUNDWATER ELEVATION = 2580 FT.

BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.

DESIGN TEMPORARY SHORING FROM STATION 28+06 +/- -DET01WB-, 17.0' LT, TO STATION 31+63 +/- -DET01WB-, 17.0' LT, FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:

BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.

LIMITED SUBSURFACE INFORMATION IS AVAILABLE IN THE VICINITY OF TEMPORARY SHORING FROM STATION 16+93 +/- -DET01WB-, 17.0' LT, TO STATION 23+75 +/- -DET01WB-, 17.0' LT. THE INFORMATION PROVIDED FOR TEMPORARY SHORING DESIGN WAS ASSUMED AND MAY NOT BE APPLICABLE TO THE ACTUAL SITE CONDITIONS ENCOUNTERED DURING CONSTRUCTION.

UNIT WEIGHT, $\gamma=120$ PCF
 FRICTION ANGLE, $\phi = 30$
 COHESION, $c = 0$ PSF
 GROUNDWATER ELEVATION = 2578 FT.

LIMITED SUBSURFACE INFORMATION IS AVAILABLE IN THE VICINITY OF TEMPORARY SHORING FROM STATION 32+69 +/- -L_LT-, 8.0' RT, TO STATION 32+74 +/- -L_LT-, 7.0' LT, TO STATION 35+00 +/- -L_LT-, 7.0' LT. THE INFORMATION PROVIDED FOR TEMPORARY SHORING DESIGN WAS ASSUMED AND MAY NOT BE APPLICABLE TO THE ACTUAL SITE CONDITIONS ENCOUNTERED DURING CONSTRUCTION.

DRIVEN PILING FOR TEMPORARY SHORING FROM STATION 16+93 +/- -DET01WB-, 17.0' LT, TO STATION 23+75 +/- -DET01WB-, 17.0' LT, MAY NOT PENETRATE BELOW ELEVATION 2578 FT DUE TO OBSTRUCTIONS, VERY DENSE OR HARD SOIL, BOULDERS OR WEATHERED OR HARD ROCK.

BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.

DRIVEN PILING FOR TEMPORARY SHORING FROM STATION 32+69 +/- -L_LT-, 8.0' RT, TO STATION 32+74 +/- -L_LT-, 7.0' LT, TO STATION 35+00 +/- -L_LT-, 7.0' LT, MAY NOT PENETRATE BELOW ELEVATION 2560 FT DUE TO OBSTRUCTIONS, VERY DENSE OR HARD SOIL, BOULDERS OR WEATHERED OR HARD ROCK.

AT THE CONTRACTOR'S OPTION, USE STANDARD TEMPORARY SHORING FOR TEMPORARY SHORING FROM STATION 16+93 +/- -DET01WB-, 17.0' LT, TO STATION 23+75 +/- -DET01WB-, 17.0' LT. SEE GEOTECHNICAL STANDARD DETAIL NO. 1801.02 FOR STANDARD TEMPORARY WALLS.

LIMITED SUBSURFACE INFORMATION IS AVAILABLE IN THE VICINITY OF TEMPORARY SHORING FROM STATION 25+75 +/- -DET01WB-, 17.0' LT, TO STATION 31+63 +/- -DET01WB-, 17.0' LT. THE INFORMATION PROVIDED FOR TEMPORARY SHORING DESIGN WAS ASSUMED AND MAY NOT BE APPLICABLE TO THE ACTUAL SITE CONDITIONS ENCOUNTERED DURING CONSTRUCTION.

AT THE CONTRACTOR'S OPTION, USE STANDARD TEMPORARY SHORING FOR TEMPORARY SHORING FROM STATION 32+69 +/- -L_LT-, 8.0' RT, TO STATION 32+74 +/- -L_LT-, 7.0' LT, TO STATION 35+00 +/- -L_LT-, 7.0' LT. SEE GEOTECHNICAL STANDARD DETAIL NO. 1801.02 FOR STANDARD TEMPORARY WALLS.

WHEN BACKFILL FOR RETAINING WALLS AND/OR BRIDGE APPROACH FILLS OVELAPS WITH THE REINFORCED ZONE OF TEMPORARY WALLS, USE SHORING BACKFILL OR BACKFILL MATERIAL REQUIRED FOR RETAINING WALLS AND/OR BRIDGE APPROACH FILLS, WHICHEVER IS BETTER, IN THE REINFORCED ZONE OF TEMPORARY WALLS.

DRIVEN PILING FOR TEMPORARY SHORING FROM STATION 25+75 +/- -DET01WB-, 17.0' LT, TO STATION 31+63 +/- -DET01WB-, 17.0' LT, MAY NOT PENETRATE BELOW ELEVATION 2560 FT DUE TO OBSTRUCTIONS, VERY DENSE OR HARD SOIL, BOULDERS OR WEATHERED OR HARD ROCK.

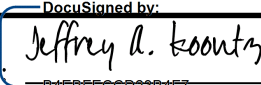



WHEN BACKFILL FOR RETAINING WALLS AND/OR BRIDGE APPROACH FILLS OVELAPS WITH THE REINFORCED ZONE OF TEMPORARY WALLS, USE SHORING BACKFILL OR BACKFILL MATERIAL REQUIRED FOR RETAINING WALLS AND/OR BRIDGE APPROACH FILLS, WHICHEVER IS BETTER, IN THE REINFORCED ZONE OF TEMPORARY WALLS.

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WHEN BACKFILL FOR RETAINING WALLS AND/OR BRIDGE APPROACH FILLS OVELAPS WITH THE REINFORCED ZONE OF TEMPORARY WALLS, USE SHORING BACKFILL OR BACKFILL MATERIAL REQUIRED FOR RETAINING WALLS AND/OR BRIDGE APPROACH FILLS, WHICHEVER IS BETTER, IN THE REINFORCED ZONE OF TEMPORARY WALLS.

THE TEMPORARY SHORING NOTES SHOWN ON THIS SHEET WERE PROVIDED THROUGH A SEALED DOCUMENT PREPARED BY AECOM FOR THE GEOTECHNICAL ENGINEERING UNIT. THE DOCUMENT WAS SUBMITTED TO THE WZTC SECTION ON 8/31/2023 AND SEALED BY A PROFESSIONAL ENGINEER, RYAN PATRICK DOYLE, LICENSE #045161.

9/6/2023 B3186-B5898.TC_TMP-02B.dgn jeffrey.thurston

APPROVED:  DATE: 10/17/2023		 	<h2 style="margin: 0;">TEMPORARY SHORING NOTES</h2>
DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED			

TEMPORARY SHORING NOTES

TEMPORARY SHORING NO. ⑦ (SEE SHEET TMP-09)

TEMPORARY SHORING NO. ⑧ (SEE SHEET TMP-09)

TEMPORARY SHORING NO. ⑨ (SEE SHEET TMP-20)

FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.

FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.

FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.

DESIGN TEMPORARY SHORING FROM STATION 23+35 +/- -L_LT-, 26' RT, TO STATION 23+19 +/- -L_LT-, 55' RT, FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:

DESIGN TEMPORARY SHORING FROM STATION 25+74 +/- -L_LT-, 26' RT, TO STATION 25+52 +/- -L_LT-, 64' RT, FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:

DESIGN TEMPORARY SHORING FROM STATION 22+20 +/- -DETO1EB-, 22.0' LT, TO STATION 23+10 +/- -DETO1EB-, 22.0' LT, FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:

UNIT WEIGHT, γ = 120 PCF
 FRICTION ANGLE, ϕ = 28
 COHESION, c = 0 PSF
 GROUNDWATER ELEVATION = 2573 FT.

UNIT WEIGHT, γ = 120 PCF
 FRICTION ANGLE, ϕ = 26
 COHESION, c = 0 PSF
 GROUNDWATER ELEVATION = 2573 FT.

UNIT WEIGHT, γ = 120 PCF
 FRICTION ANGLE, ϕ = 30
 COHESION, c = 0 PSF
 GROUNDWATER ELEVATION = 2573 FT.

BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.

BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.

BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.

LIMITED SUBSURFACE INFORMATION IS AVAILABLE IN THE VICINITY OF TEMPORARY SHORING FROM STATION 23+35 +/- -L_LT-, 26' RT, TO STATION 23+19 +/- -L_LT-, 55' RT. THE INFORMATION PROVIDED FOR TEMPORARY SHORING DESIGN WAS ASSUMED AND MAY NOT BE APPLICABLE TO THE ACTUAL SITE CONDITIONS ENCOUNTERED DURING CONSTRUCTION.

LIMITED SUBSURFACE INFORMATION IS AVAILABLE IN THE VICINITY OF TEMPORARY SHORING FROM STATION 25+74 +/- -L_LT-, 26' RT, TO STATION 25+52 +/- -L_LT-, 64' RT. THE INFORMATION PROVIDED FOR TEMPORARY SHORING DESIGN WAS ASSUMED AND MAY NOT BE APPLICABLE TO THE ACTUAL SITE CONDITIONS ENCOUNTERED DURING CONSTRUCTION.

LIMITED SUBSURFACE INFORMATION IS AVAILABLE IN THE VICINITY OF TEMPORARY SHORING FROM STATION 22+20 +/- -DETO1EB-, 22.0' LT, TO STATION 23+10 +/- -DETO1EB-, 22.0' LT. THE INFORMATION PROVIDED FOR TEMPORARY SHORING DESIGN WAS ASSUMED AND MAY NOT BE APPLICABLE TO THE ACTUAL SITE CONDITIONS ENCOUNTERED DURING CONSTRUCTION.

DRIVEN PILING FOR TEMPORARY SHORING FROM STATION 23+35 +/- -L_LT-, 26' RT, TO STATION 23+19 +/- -L_LT-, 55' RT, MAY NOT PENETRATE BELOW ELEVATION 2555 FT DUE TO OBSTRUCTIONS, VERY DENSE OR HARD SOIL, BOULDERS OR WEATHERED OR HARD ROCK.

DRIVEN PILING FOR TEMPORARY SHORING FROM STATION 25+74 +/- -L_LT-, 26' RT, TO STATION 25+52 +/- -L_LT-, 64' RT, MAY NOT PENETRATE BELOW ELEVATION 2553 FT DUE TO OBSTRUCTIONS, VERY DENSE OR HARD SOIL, BOULDERS OR WEATHERED OR HARD ROCK.

DRIVEN PILING FOR TEMPORARY SHORING FROM STATION 22+20 +/- -DETO1EB-, 22.0' LT, TO STATION 23+10 +/- -DETO1EB-, 22.0' LT, MAY NOT PENETRATE BELOW ELEVATION 2550 FT DUE TO OBSTRUCTIONS, VERY DENSE OR HARD SOIL, BOULDERS OR WEATHERED OR HARD ROCK.

AT THE CONTRACTOR'S OPTION, USE STANDARD TEMPORARY SHORING FOR TEMPORARY SHORING FROM STATION 23+35 +/- -L_LT-, 26' RT, TO STATION 23+19 +/- -L_LT-, 55' RT. SEE GEOTECHNICAL STANDARD DETAIL NO. 1801.02 FOR STANDARD TEMPORARY WALLS.

AT THE CONTRACTOR'S OPTION, USE STANDARD TEMPORARY SHORING FOR TEMPORARY SHORING FROM STATION 25+74 +/- -L_LT-, 26' RT, TO STATION 25+52 +/- -L_LT-, 64' RT. SEE GEOTECHNICAL STANDARD DETAIL NO. 1801.02 FOR STANDARD TEMPORARY WALLS.

AT THE CONTRACTOR'S OPTION, USE STANDARD TEMPORARY SHORING FOR TEMPORARY SHORING FROM STATION 22+20 +/- -DETO1EB-, 22.0' LT, TO STATION 23+10 +/- -DETO1EB-, 22.0' LT. SEE GEOTECHNICAL STANDARD DETAIL NO. 1801.01 FOR STANDARD TEMPORARY SHORING AND DETAIL NO. 1801.02 FOR STANDARD WALLS.

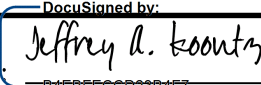


WHEN BACKFILL FOR RETAINING WALLS AND/OR BRIDGE APPROACH FILLS OVERLAPS WITH THE REINFORCED ZONE OF TEMPORARY WALLS, USE SHORING BACKFILL OR BACKFILL MATERIAL REQUIRED FOR RETAINING WALLS AND/OR BRIDGE APPROACH FILLS, WHICHEVER IS BETTER, IN THE REINFORCED ZONE OF TEMPORARY WALLS.

WHEN BACKFILL FOR RETAINING WALLS AND/OR BRIDGE APPROACH FILLS OVERLAPS WITH THE REINFORCED ZONE OF TEMPORARY WALLS, USE SHORING BACKFILL OR BACKFILL MATERIAL REQUIRED FOR RETAINING WALLS AND/OR BRIDGE APPROACH FILLS, WHICHEVER IS BETTER, IN THE REINFORCED ZONE OF TEMPORARY WALLS.

WHEN BACKFILL FOR RETAINING WALLS AND/OR BRIDGE APPROACH FILLS OVERLAPS WITH THE REINFORCED ZONE OF TEMPORARY WALLS, USE SHORING BACKFILL OR BACKFILL MATERIAL REQUIRED FOR RETAINING WALLS AND/OR BRIDGE APPROACH FILLS, WHICHEVER IS BETTER, IN THE REINFORCED ZONE OF TEMPORARY WALLS.

THE TEMPORARY SHORING NOTES SHOWN ON THIS SHEET WERE PROVIDED THROUGH A SEALED DOCUMENT PREPARED BY AECOM FOR THE GEOTECHNICAL ENGINEERING UNIT. THE DOCUMENT WAS SUBMITTED TO THE WZTC SECTION ON 8/31/2023 AND SEALED BY A PROFESSIONAL ENGINEER, RYAN PATRICK DOYLE, LICENSE #045161.

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APPROVED:  DATE: 10/17/2023		 AECOM <small>NC FIRM LICENSE No: F-0342 5438 Wade Park Blvd., Ste 200 Raleigh, NC 27607 (919) 854-6200</small>	<h2 style="margin: 0;">TEMPORARY SHORING NOTES</h2>
DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED			

TEMPORARY SHORING NOTES

TEMPORARY SHORING NO. 10 (SEE SHEET TMP-10)

TEMPORARY SHORING NO. 11 (SEE SHEET TMP-10)

FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.

FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.

DESIGN TEMPORARY SHORING FROM STATION 30+82 +/- -L_LT-, 13' +/- LT, TO STATION 31+45 +/- -L_LT-, 16' +/- LT, FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:

DESIGN TEMPORARY SHORING FROM STATION 32+24 +/- -L_LT-, 18' +/- LT, TO STATION 32+84 +/- -L_LT-, 18' +/- LT, FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:

UNIT WEIGHT, γ = 120 PCF
 FRICTION ANGLE, ϕ = 26
 COHESION, c = 0 PSF
 GROUNDWATER ELEVATION = 2578 FT.

UNIT WEIGHT, γ = 120 PCF
 FRICTION ANGLE, ϕ = 26
 COHESION, c = 0 PSF
 GROUNDWATER ELEVATION = 2580 FT.

BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.

BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.

LIMITED SUBSURFACE INFORMATION IS AVAILABLE IN THE VICINITY OF TEMPORARY SHORING FROM STATION 30+82 +/- -L_LT-, 13' +/- LT, TO STATION 31+45 +/- -L_LT-, 16' +/- LT. THE INFORMATION PROVIDED FOR TEMPORARY SHORING DESIGN WAS ASSUMED AND MAY NOT BE APPLICABLE TO THE ACTUAL SITE CONDITIONS ENCOUNTERED DURING CONSTRUCTION.

LIMITED SUBSURFACE INFORMATION IS AVAILABLE IN THE VICINITY OF TEMPORARY SHORING FROM STATION 32+24 +/- -L_LT-, 18' +/- LT, TO STATION 32+84 +/- -L_LT-, 18' +/- LT. THE INFORMATION PROVIDED FOR TEMPORARY SHORING DESIGN WAS ASSUMED AND MAY NOT BE APPLICABLE TO THE ACTUAL SITE CONDITIONS ENCOUNTERED DURING CONSTRUCTION.

DRIVEN PILING FOR TEMPORARY SHORING FROM STATION 30+82 +/- -L_LT-, 13' +/- LT, TO STATION 31+45 +/- -L_LT-, 16' +/- LT, MAY NOT PENETRATE BELOW ELEVATION 2560 FT DUE TO OBSTRUCTIONS, VERY DENSE OR HARD SOIL, BOULDERS OR WEATHERED OR HARD ROCK.

DRIVEN PILING FOR TEMPORARY SHORING FROM STATION 32+24 +/- -L_LT-, 18' +/- LT, TO STATION 32+84 +/- -L_LT-, 18' +/- LT, MAY NOT PENETRATE BELOW ELEVATION 2560 FT DUE TO OBSTRUCTIONS, VERY DENSE OR HARD SOIL, BOULDERS OR WEATHERED OR HARD ROCK.

AT THE CONTRACTOR'S OPTION, USE STANDARD TEMPORARY SHORING FOR TEMPORARY SHORING FROM STATION 30+82 +/- -L_LT-, 13' +/- LT, TO STATION 31+45 +/- -L_LT-, 16' +/- LT. SEE GEOTECHNICAL STANDARD DETAIL NO. 1801.02 FOR STANDARD TEMPORARY WALLS.

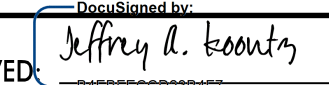
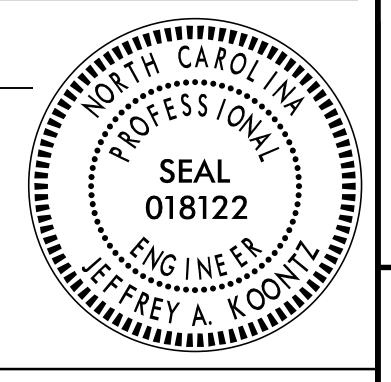


AT THE CONTRACTOR'S OPTION, USE STANDARD TEMPORARY SHORING FOR TEMPORARY SHORING FROM STATION 32+24 +/- -L_LT-, 18' +/- LT, TO STATION 32+84 +/- -L_LT-, 18' +/- LT. SEE GEOTECHNICAL STANDARD DETAIL NO. 1801.02 FOR STANDARD TEMPORARY WALLS.

WHEN BACKFILL FOR RETAINING WALLS AND/OR BRIDGE APPROACH FILLS OVELAPS WITH THE REINFORCED ZONE OF TEMPORARY WALLS, USE SHORING BACKFILL OR BACKFILL MATERIAL REQUIRED FOR RETAINING WALLS AND/OR BRIDGE APPROACH FILLS, WHICHEVER IS BETTER, IN THE REINFORCED ZONE OF TEMPORARY WALLS.

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THE TEMPORARY SHORING NOTES SHOWN ON THIS SHEET WERE PROVIDED THROUGH A SEALED DOCUMENT PREPARED BY AECOM FOR THE GEOTECHNICAL ENGINEERING UNIT. THE DOCUMENT WAS SUBMITTED TO THE WZTC SECTION ON 8/31/2023 AND SEALED BY A PROFESSIONAL ENGINEER, RYAN PATRICK DOYLE, LICENSE #045161.

9/6/2023 B3386-B5898.TC_TMP_02D.dgn jeffrey.thurston

APPROVED:  DATE: 10/17/2023			<h2 style="margin: 0;">TEMPORARY SHORING NOTES</h2>
DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED		 <p style="font-size: 8px;">NC FIRM LICENSE No: F-0342 5438 Wade Park Blvd., Ste 200 Raleigh, NC 27607 (919) 854-6200</p>	

SP 11299

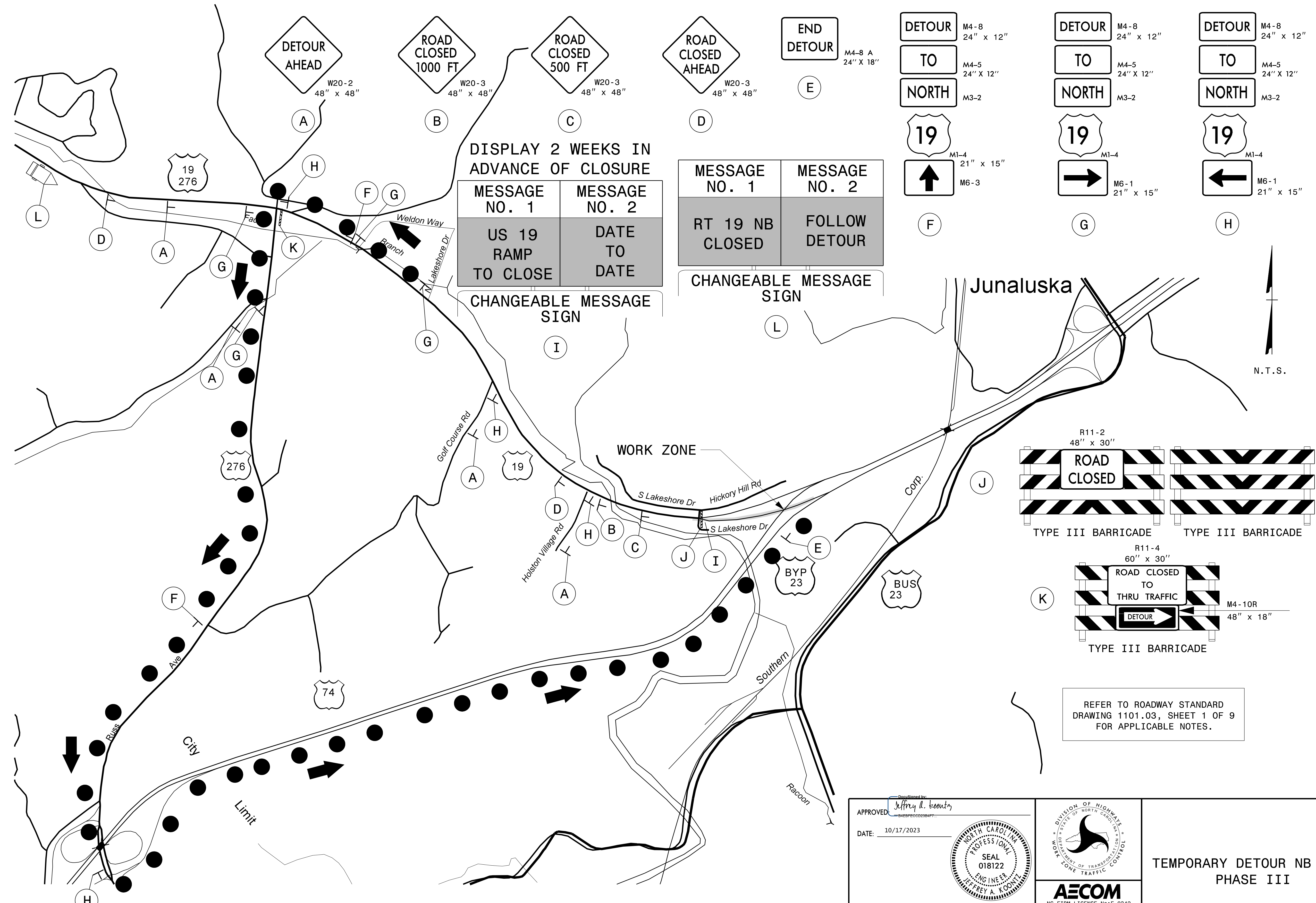
<p>SIGN NUMBER: 11299 BACKG COLOR: Fluorescent Orange</p> <p>TYPE: B COPY COLOR: Black</p> <p>QUANTITY: SEE PLANS</p> <p>SIGN WIDTH: 5'-6" HEIGHT: 5'-6" TOTAL AREA: 30.5 Sq.Ft.</p> <p>BORDER TYPE: INSET RECESS: 0.59" WIDTH: 0.75" RADII: 1.38"</p> <p>NO. Z BARS: N/A LENGTH: N/A</p>	<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>SYMBOL</th> <th>X</th> <th>Y</th> <th>WID</th> <th>HT</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> </tbody> </table>	SYMBOL	X	Y	WID	HT																																				<p>DESIGN BY: WJ CHECKED BY:</p> <p>PROJECT ID: ALL DIV: ALL DATE: Jun 22, 2011</p>	<p style="text-align: center;">BORDER R=1.38" TH=0.75" IN=0.59"</p> <p style="text-align: right; font-size: small;">Spacing Factor is 1 unless specified otherwise</p>																																																
SYMBOL	X	Y	WID	HT																																																																																							
<p>USE NOTES: 1,2</p> <p>1. Legend and border shall be direct applied black non-reflective sheeting.</p> <p>2. Background shall be Type VII, VIII, or IX (prismatic) fluorescent orange retroreflective sheeting.</p>																																																																																											
<p>LETTER POSITIONS</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th colspan="10">Letter spacings are to start of next letter</th> <th>Series/Size</th> </tr> <tr> <th colspan="10"></th> <th>Text Length</th> </tr> </thead> <tbody> <tr> <td></td><td>B</td><td>E</td><td>G</td><td>I</td><td>N</td><td></td><td></td><td></td><td></td><td>D 2000</td> </tr> <tr> <td>20.5</td><td>6</td><td>5.4</td><td>6.3</td><td>2.8</td><td>4.8</td><td>20.5</td><td></td><td></td><td></td><td>25.2</td> </tr> <tr> <td></td><td>R</td><td>O</td><td>A</td><td>D</td><td></td><td></td><td></td><td></td><td></td><td>D 2000</td> </tr> <tr> <td>21.4</td><td>5.8</td><td>5.9</td><td>7</td><td>4.8</td><td>21.4</td><td></td><td></td><td></td><td></td><td>23.5</td> </tr> <tr> <td></td><td>W</td><td>O</td><td>R</td><td>K</td><td></td><td></td><td></td><td></td><td></td><td>D 2000</td> </tr> <tr> <td>20.9</td><td>7.1</td><td>6.5</td><td>5.9</td><td>4.9</td><td>20.9</td><td></td><td></td><td></td><td></td><td>24.5</td> </tr> </tbody> </table>				Letter spacings are to start of next letter										Series/Size											Text Length		B	E	G	I	N					D 2000	20.5	6	5.4	6.3	2.8	4.8	20.5				25.2		R	O	A	D						D 2000	21.4	5.8	5.9	7	4.8	21.4					23.5		W	O	R	K						D 2000	20.9	7.1	6.5	5.9	4.9	20.9					24.5
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<p>FILENAME: BEGIN ROAD WORK2 NORTH CAROLINA D.O.T. SIGN DETAIL</p>																																																																																											

GENERAL NOTES FOR THE "BEGIN ROAD WORK" SIGN

- SIGN SP-11299 "BEGIN ROAD WORK" ONLY APPLIES TO FULL CONTROL AND PARTIAL CONTROL OF ACCESS ROADWAYS.
- WHEN USED, INSTALL SIGN SP-11299 "BEGIN ROAD WORK" ACCORDING TO DETAIL A ON ROADWAY STANDARD DRAWING 1101.01, SHEETS 1 & 2 OF 3.

9/6/2023
 B386-B5898.TC_TMP_02E.dgn
 Jeffrey Thurston

<p>APPROVED: <i>Jeffrey A. Kooty</i></p> <p>DATE: 10/17/2023</p>			<h2>BEGIN ROAD WORK SIGN DESIGN</h2>
<p>DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED</p>		<p>AECOM <small>NC FIRM LICENSE No: F-0342 5438 Wade Park Blvd., Ste 200 Raleigh, NC 27607 (919) 854-6200</small></p>	



DISPLAY 2 WEEKS IN ADVANCE OF CLOSURE

MESSAGE NO. 1	MESSAGE NO. 2
US 19 RAMP TO CLOSE	DATE TO DATE

CHANGEABLE MESSAGE SIGN

MESSAGE NO. 1	MESSAGE NO. 2
RT 19 NB CLOSED	FOLLOW DETOUR

CHANGEABLE MESSAGE SIGN

DETOUR M4-8 24" x 12"	DETOUR M4-8 24" x 12"	DETOUR M4-8 24" x 12"
TO M4-5 24" x 12"	TO M4-5 24" x 12"	TO M4-5 24" x 12"
NORTH M3-2	NORTH M3-2	NORTH M3-2
19 M1-4 21" x 15"	19 M1-4	19 M1-4
↑ M6-3	→ M6-1 21" x 15"	← M6-1 21" x 15"
(F)	(G)	(H)

R11-2
48" x 30"

ROAD CLOSED

TYPE III BARRICADE

R11-4
60" x 30"

ROAD CLOSED TO THRU TRAFFIC

DETOUR

M4-10R
48" x 18"

TYPE III BARRICADE

REFER TO ROADWAY STANDARD
DRAWING 1101.03, SHEET 1 OF 9
FOR APPLICABLE NOTES.

9/6/2023
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Jeffrey.thurston

APPROVED: *Jeffrey D. Kooty*

DATE: 10/17/2023

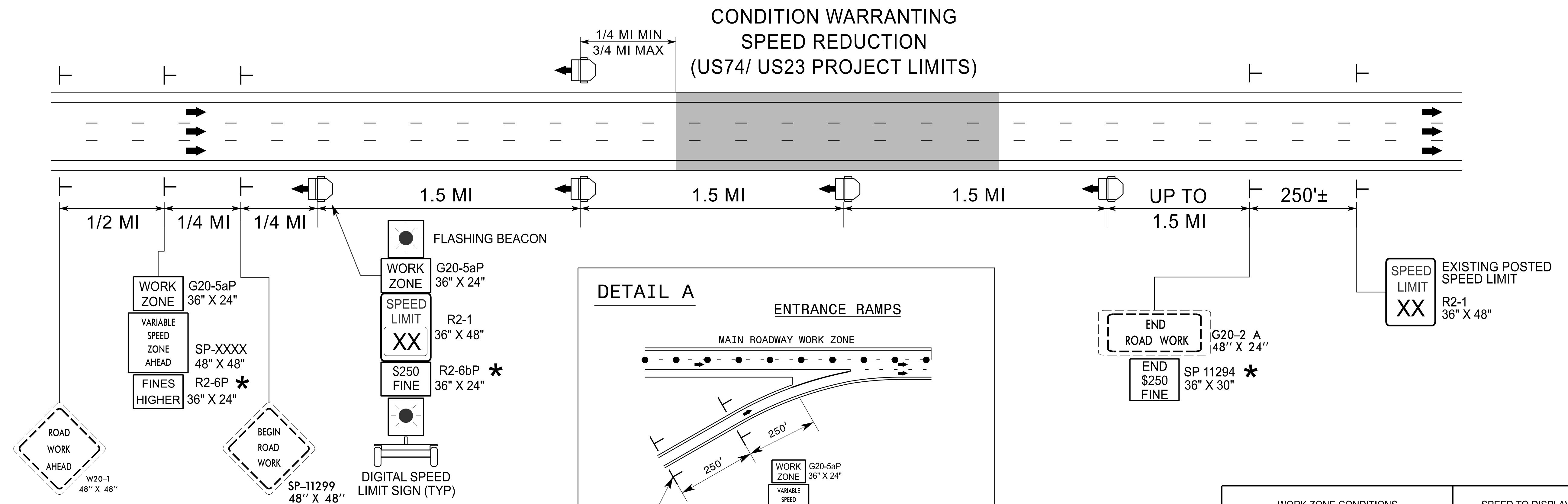
SEAL
018122
ENGINEER
JEFFREY A. KOOTY

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UNLESS ALL SIGNATURES COMPLETED

DIVISION OF HIGHWAYS
STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
WORK ZONE TRAFFIC CONTROL

AECOM
NC FIRM LICENSE No: F-0342
5438 Wade Park Blvd., Ste 200
Raleigh, NC 27607
(919) 854-6200

TEMPORARY DETOUR NB US19
PHASE III



* USE ONLY IF ORDINANCED. SEE NOTE 5 BELOW.

NOTES

1. THE DIGITAL SPEED LIMITS SIGNS WILL BE INSTALLED (TRAILER MOUNTED OR STATIONARY MOUNTED) IN ADVANCE OF AND SPACED APPROXIMATELY 1.5 MILES THROUGHOUT THE THE PROJECT LIMITS, UNLESS DIRECTED OTHERWISE.
2. WITHIN 1/4 TO 3/4 MILE UPSTREAM OF CONDITION WARRANTING A SPEED REDUCTION, PLACE A DIGITAL SPEED LIMIT SIGN ON BOTH THE INSIDE AND OUTSIDE SHOULDERS, UNLESS DIRECTED OTHERWISE BY THE ENGINEER. AT ALL OTHER LOCATIONS DOWNSTREAM, PLACE A SINGLE DIGITAL SPEED LIMIT SIGN ON THE OUTSIDE SHOULDER.

IF SIGNS ARE NOT HIGHLY VISIBLE TO ALL MOTORISTS, SUPPLEMENTAL DIGITAL SPEED LIMIT SIGNS ARE PERMITTED ON THE MEDIAN SHOULDER.
3. THE DIGITAL SPEED LIMIT SIGNS TAKE PRECEDENCE OVER EXISTING SPEED LIMIT SIGNS AND SHOULD REMAIN UPRIGHT AND VISIBLE AT ALL TIMES. ALL EXISTING SPEED LIMIT SIGNS SHALL BE COVERED OR REMOVED FOR DURATION OF THE PROJECT.
4. NCDOT HAS SOLE AUTHORITY OF THE SPEED LIMITS DISPLAYED ON THE DIGITAL SPEED LIMIT SIGNS.
5. THE WORK ZONE VARIABLE SPEED LIMIT AND THE \$250 SPEEDING PENALTY ARE SEPARATE ORDINANCES THAT MUST BE SIGNED BY THE STATE TRAFFIC ENGINEER TO BE VALID AND ENFORCEABLE. WITHOUT SIGNED ORDINANCES, THE SPEED LIMIT ON A FACILITY SHALL REMAIN UNCHANGED AND/OR HIGHER FINES SIGNS SHALL NOT BE USED.
6. THE REDUCED SPEED SHALL BE DISPLAYED A MINIMUM OF 1/4 MILE AND A MAXIMUM OF 3/4 MILE IN ADVANCE OF AND THROUGHOUT THE AREA MEETING CONDITIONS LISTED IN THE CHART. THE EXISTING SPEED LIMIT SHALL BE DISPLAYED ON ALL OTHER DIGITAL SPEED LIMIT SIGNS.
7. THE SPEED DISPLAYED SHALL BE THE LOWER OF THE EXISTING SPEED LIMIT OR THE SPEED IN THE WORK ZONE CONDITION CHART.
8. THE BEACONS ON THE DIGITAL SPEED LIMIT SIGNS SHALL ONLY FLASH DURING TIMES THE SPEED IS REDUCED, AND REMAIN OFF AT ALL OTHER TIMES.

	WORK ZONE CONDITIONS	SPEED TO DISPLAY (SEE NOTE 6 & 7)
LANE CLOSURES	2 LANES REDUCED TO 1 LANE	50
	3 LANES REDUCED TO 1 LANE	50
	3 LANES REDUCED TO 2 LANES	50
	4 LANES REDUCED TO 1 LANE	50
	4 LANES REDUCED TO 2 LANES	50
	4 LANES REDUCED TO 3 LANES	50
CONTINUOUS BARRIER (LENGTH OF BARRIER GREATER THAN 1 MILE)	1 OPEN LANE WITH CONTINUOUS BARRIER ON BOTH SHOULDERS	50
	1 OPEN LANE WITH CONTINUOUS BARRIER ON 1 SHOULDER	50
	3 OR 2 OPEN LANES WITH CONTINUOUS BARRIER ON BOTH SHOULDERS	50
	3 OR 2 OPEN LANES WITH CONTINUOUS BARRIER ON 1 SHOULDER	50
	4 OPEN LANES WITH BARRIER CONTINUOUS ON BOTH SHOULDERS	50
	4 OPEN LANES WITH BARRIER CONTINUOUS ON 1 SHOULDER	50
	UNEVEN LANES	50

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APPROVED: *Jeffrey A. Kooft*
 DATE: 10/17/2023

SEAL 018122
 ENGINEER
 JEFFREY A. KOOFT

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

DIVISION OF HIGHWAYS
 STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 WORK ZONE TRAFFIC CONTROL

AECOM
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 5438 Wade Park Blvd., Ste 200
 Raleigh, NC 27607
 (919) 854-6200

WORK ZONE VARIABLE SPEED LIMIT REDUCTION

PROJ. REFERENCE NO.	SHEET NO.
B-3186 / B-5898	TMP- 03

PHASE I

STEP 1) PRIOR TO ANY CONSTRUCTION ACTIVITY, INSTALL WORK ZONE ADVANCE WARNING SIGNS ON ALL ROADS INVOLVED (US 74/US 23 AND US 19) AND WORK ZONE SPEED REDUCTION SIGNS AND DEVICES ACCORDING TO THE ROADWAY STANDARD DRAWING (RSD) 1101.01 AND DETAIL ON TMP-02F.

STEP 2) BEGIN THE FOLLOWING WORK AT ANY TIME DURING PHASE I AND COMPLETE PRIOR TO PHASE II:

CONSTRUCT NEW SIGN SUPPORT AND REPLACEMENT GUARDRAIL AND REMOVE EXISTING SIGN SUPPORT ON WESTBOUND US 74/US 23 FOR EXIT 103 (SEE SIGNING PLANS AND TMP-06) AS FOLLOWS:

PLACE DRUMS AND CMS FOR TEMPORARY CLOSURE OF WESTBOUND AUXILIARY LANE FOR WORK ON THE OUTSIDE SHOULDER (SEE TMP-06 AND TMP-07) AND USE 1101.02 (SHEET 4 OF 14) FOR TEMPORARY CLOSURE OF THE INSIDE WESTBOUND LANE FOR WORK ON THE INSIDE SHOULDER.

USE RSD 1101.03 (SHEET 9 OF 9) FOR ROLLING ROADBLOCK TO TEMPORARILY STOP TRAFFIC TO REMOVE AND INSTALL OVERHEAD SIGN STRUCTURE AND SIGNS (SEE TMP-06 AND TMP-07 AND SIGNING PLANS).

STEP 3) COORDINATE WITH TOWN OF WAYNESVILLE PARKS & RECREATION AND USE PEDESTRIAN BARRICADES TO CLOSE WAYNESVILLE GREENWAY PARK AT S. LAKESHORE DRIVE AND AT INDUSTRIAL PARK DRIVE TO PEDESTRIAN AND BICYCLE TRAFFIC (SEE DETAIL AND INSET ON TMP-05).

STEP 4) USE RSD 1101.02 (SHEET 4 OF 14) FOR TEMPORARY LANE CLOSURE TO REMOVE EXISTING MARKINGS AND PLACE TEMPORARY PAVEMENT MARKINGS AND MARKERS ON EASTBOUND US 74/US 23 ACCORDING TO THE FOLLOWING STATIONS: (SEE TMP-04 THRU TMP-07)

-L_RT- STA. 6+50 +/- TO STA. 38+26 +/-

AND SHIFT EASTBOUND US 74/US 23 TRAFFIC INTO NEW TEMPORARY PATTERN.

STEP 5) USE RSD 1101.02 (SHEET 4 OF 14) FOR TEMPORARY OUTSIDE LANE CLOSURE ON EASTBOUND US 74/US 23 AND PLACE PCB ALONG OUTSIDE SHOULDER ACCORDING TO THE FOLLOWING STATIONS: (SEE TMP-04 THRU TMP-06)

-L_RT- STA. 8+54 +/- TO STA. 35+74 +/- RT

STEP 6) WORK BEHIND BARRIER TO CONSTRUCT EASTBOUND US 74/US 23 TEMPORARY DETOUR (-DET01EB-) UP TO EXISTING EDGE OF PAVEMENT, INCLUDING FINAL DRAINAGE, TEMPORARY DRAINAGE FOR RICHLAND CREEK TRIBUTARY, TEMPORARY BRIDGE, TEMPORARY SHORING 1, 2, AND 3, AND RETAINING WALL 3, AND RELOCATE GROUND MOUNTED SIGNS ACCORDING TO THE FOLLOWING STATIONS: (SEE OVERVIEW ON TMP-03B AND DETAILS ON TMP-04 THRU TMP-06)

-DET01EB- STA. 10+12 +/- TO STA. 35+70 +/-

WORK BEHIND BARRIER OR GUARDRAIL, USE RSD 1101.02 (SHEET 4 OF 14) FOR TEMPORARY LANE CLOSURE, OR RSD 1101.04 FOR TEMPORARY SHOULDER CLOSURE AS NEEDED FOR TRENCHLESS INSTALLATION OF PHASE I DRAINAGE CROSSINGS.

STEP 7) WORK BEHIND BARRIER TO PLACE PCB AND TEMPORARY PAVEMENT MARKING AND MARKERS ON EASTBOUND US 74/US 23 DETOUR (-DET01EB-) ACCORDING TO THE FOLLOWING STATIONS: (SEE TMP-08 THRU TMP-10)

-DET01EB- STA. 10+10 +/- TO STA. 22+20 +/- RT (PCB)
 -DET01EB- STA. 16+10 +/- TO STA. 22+20 +/- LT (PCB)
 -DET01EB- STA. 25+50 +/- TO STA. 35+70 +/- RT (PCB)
 -DET01EB- STA. 25+50 +/- TO STA. 30+80 +/- LT (PCB)
 -DET01EB- STA. 12+00 +/- TO STA. 31+00 +/- (PAVEMENT MARKING)

STEP 8) USE RSD 1101.02 (SHEET 4 OF 14) FOR TEMPORARY LANE CLOSURES ON EASTBOUND US 74/US 23 TO PERFORM THE FOLLOWING TO TIE PAVEMENT BETWEEN US 74/US 23 (-L_RT-) AND DETOUR (-DET01EB-):

REMOVE PCB AT BEGINNING AND END OF DETOUR AND REPLACE WITH DRUMS.

PLACE PAVEMENT WEDGING BETWEEN EXISTING TRAVEL LANES AND DETOUR LANES TO TIE PAVEMENT ACCORDING TO THE FOLLOWING STATIONS: (SEE TMP-04 AND TMP-06)

-L_RT- STA. 8+54 +/- TO STA. 13+01 +/-
 -L_RT- STA. 30+89 +/- TO STA. 35+69 +/-

MAINTAIN EASTBOUND TRAFFIC IN ONE LANE ON US 74/23.

COMPLETE TEMPORARY PAVEMENT MARKINGS AND MARKERS ACCORDING TO THE FOLLOWING STATIONS: (SEE TMP-08 THRU TMP-10)

-DET01EB- STA. 8+00 +/- TO STA. 12+00 +/-
 -DET01EB- STA. 31+00 +/- TO STA. 40+00 +/-

AND SHIFT EASTBOUND US 74/US 23 TRAFFIC, ONE LANE AT A TIME, ONTO TEMPORARY EASTBOUND DETOUR (-DET01EB-) AS MARKINGS ARE COMPLETED. RESET DRUMS FOR NEW PATTERN.

STEP 9) USE RSD 1101.02 (SHEET 4 OF 14) FOR TEMPORARY LANE CLOSURES ON EASTBOUND DETOUR AND RESET PCB ACCORDING TO THE FOLLOWING STATIONS: (SEE TMP-08 AND TMP-10)

-L_RT- STA. 8+50 +/- TO -DET01EB - STA. 10+10 +/- RT
 -L_RT- STA. 9+70 +/- TO -DET01EB - STA. 16+10 +/- LT
 -DET01EB- STA. 30+80 +/- TO -DET01EB - STA. 35+00 +/- LT
 -DET01EB- STA. 35+70 +/- TO -DET01EB - STA. 38+00 +/- RT

PHASE II

STEP 1) USE RSD 1101.02 (SHEET 4 OF 14) FOR TEMPORARY LANE CLOSURES ON WESTBOUND US 74/US 23 TO REMOVE CONFLICTING MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING AND MARKERS ON WESTBOUND US 74/US 23 (-L_LT-) ACCORDING TO THE FOLLOWING STATIONS: (SEE TMP-08 THRU TMP-11)

-L_LT- STA. 10+00 +/- TO STA. 44+28 +/-

AND SHIFT WESTBOUND US 74/US 23 TRAFFIC INTO TEMPORARY PATTERN.

STEP 2) USE RSD 1101.02 (SHEET 4 OF 14) FOR TEMPORARY LANE CLOSURE OF INSIDE LANE OF WESTBOUND US 74/US 23 AND PLACE PCB ACCORDING TO THE FOLLOWING STATIONS: (SEE TMP-08 THRU TMP-11)

-L_LT- STA. 11+90 +/- TO STA. 30+70 +/- RT
 -L_LT- STA. 33+08 +/- TO STA. 41+78 +/- RT

STEP 3) BEGIN WORK BEHIND BARRIER TO CONSTRUCT WESTBOUND US 74/US 23 DETOUR (-DET01WB-), INCLUDING RETAINING WALL 4, STAGE 1 OF WESTBOUND FLYOVER BRIDGE ABUTMENTS, RETAINING WALLS 5 AND 6, STAGE 1 OF THE WESTBOUND BRIDGE OVER RICHLAND CREEK, REMOVAL OF WESTBOUND US 74/US 23 BRIDGE RAIL, AND REMOVAL OF EXISTING EASTBOUND BRIDGE OVER RICHLAND CREEK, ACCORDING TO THE FOLLOWING STATIONS: (SEE OVERVIEWS ON TMP-03B AND TMP-03C AND DETAILS ON TMP-08 THRU TMP-11)

-DET01WB- STA. 13+25 +/- TO -L_LT- STA. 40+74 +/-

EXISTING US 74/ US 23 EASTBOUND BRIDGE OVER RICHLAND CREEK MAY BE USED AS NEEDED IN THIS PHASE BUT REMOVED PRIOR TO PHASE III.

WORK BEHIND BARRIER OR GUARDRAIL, USE RSD 1101.02 (SHEET 4 OF 14) FOR TEMPORARY LANE CLOSURE, OR RSD 1101.04 FOR TEMPORARY SHOULDER CLOSURE AS NEEDED FOR TRENCHLESS INSTALLATION OF PHASE II DRAINAGE CROSSINGS.

STEP 4) PRIOR TO STAGE 1 CONSTRUCTION OF WESTBOUND US 74/US 23 FLYOVER BRIDGE OVER US 19 AND ASSOCIATED WALLS 5 AND 6, INSTALL AND COVER EASTBOUND US 19 RAMP DETOUR SIGNS AND PLACE CMS WITH ADVANCE NOTIFICATION OF CLOSURE (SEE TMP-02E).

INTERMEDIATE CONTRACT TIME

COMPLETE THE FOLLOWING WORK OF PHASE II, STEPS 5 THROUGH 7 FOR RAMP -Y1_RT- IN ACCORDANCE WITH THE INTERMEDIATE CONTRACT TIME OF 30 DAYS (SEE SPECIAL PROVISIONS).

STEP 5) UNCOVER EASTBOUND US 19 RAMP DETOUR SIGNS, PLACE BARRICADES TO CLOSE RAMP TO ALL TRAFFIC AND PERFORM THE FOLLOWING ON THE EASTBOUND US 19 RAMP: (SEE TMP-10)

CONSTRUCT THE PROPOSED RAMP SHOULDERS (-Y1_RT) UP TO EXISTING EDGE AND ELEVATION ACCORDING TO THE FOLLOWING STATIONS:

-Y1-RT STA. 16+83 +/- TO 23+90 +/- RT
 -Y1-RT STA. 25+00 +/- TO 28+83 +/- LT

CONSTRUCT PROPOSED RAMP INLETS AND PIPES ASSOCIATED WITH STRUCTURES 532, 533, TO 629 AND 608 TO 609. COVER 608 AND 609 WITH STEEL PLATE (SEE TMP-17) AND MAKE ALL PAVEMENT REPAIRS.

STEP 6) RESET PCB ACCORDING TO THE FOLLOWING STATIONS: (SEE TMP-17)

-Y1_RT- STA. 16+50 +/- TO STA. 25+50 +/- LT
 -Y1_RT- STA. 17+60 +/- TO 23+80 +/- RT

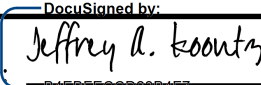
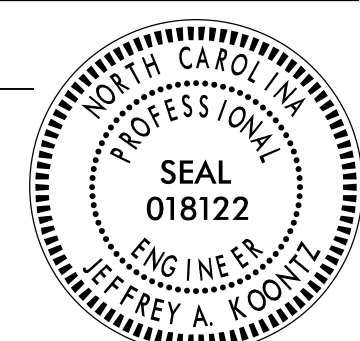

STEP 7) PLACE TEMPORARY PAVEMENT MARKING ACCORDING TO THE FOLLOWING STATIONS: (SEE TMP-17)

-Y1-RT STA. 15+00 +/- TO 28+83 +/-

REMOVE BARRICADES, COVER DETOUR SIGNS AND OPEN EASTBOUND RAMP TO TRAFFIC IN NEW TEMPORARY PATTERN.

STEP 8) WORK BEHIND BARRIER TO COMPLETE STAGE 1 OF WESTBOUND US 74/US 23 FLYOVER BRIDGE ABUTMENT #2, RETAINING WALL #7 AND CONSTRUCT THE LEFT SIDE OF US 19 EASTBOUND RAMP (-Y1_RT-) UP TO

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APPROVED:  DATE: 10/17/2023			<p>PROJECT PHASING</p>
<p style="text-align: center;">DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED</p>		<p style="text-align: center;">AECOM NC FIRM LICENSE No: F-0342 5438 Wade Park Blvd., Ste 200 Raleigh, NC 27607 (919) 854-6200</p>	

EXISTING EDGE AND ELEVATION, AND DRAINAGE ACCORDING TO THE FOLLOWING STATIONS: (SEE PHASE II-A DETAIL ON TMP-17)

-Y1-RT STA. 16+83 +/- TO 25+00 +/- LT

STEP 9) UNCOVER EASTBOUND US 19 RAMP DETOUR SIGNS, PLACE BARRICADES TO CLOSE RAMP TO ALL TRAFFIC AND PERFORM THE FOLLOWING ON THE EASTBOUND US 19 RAMP: (SEE TMP-17A)

RESET PCB AND PLACE TEMPORARY PAVEMENT MARKINGS ALONG RAMP ACCORDING TO THE FOLLOWING STATIONS: (SEE PHASE II-B DETAIL ON TMP-17A)

-Y1_RT- STA. 18+00 +/- TO STA. 25+15 +/- LT (PCB)
 -Y1_RT- STA. 18+00 +/- TO STA 23+80 +/- RT (PCB)
 -Y1_RT- STA. 16+83 +/- TO STA. 28+83 +/- LT (PVT MARKING)
 -Y1_RT- STA. 16+50 +/- TO STA. 25+20 +/- RT (PVT MARKING)

AND SHIFT RAMP TRAFFIC INTO NEW TEMPORARY PATTERN.

STEP 10) WORK BEHIND BARRIER TO COMPLETE STAGE 1 OF WESTBOUND US 74/US 23 FLYOVER BRIDGE ABUTMENT #1 AND RETAINING WALL 5 (SEE PHASE II-B DETAIL ON TMP-17A)

STEP 11) PLACE CMS FOR ADVANCE NOTICE OF TEMPORARY RAMP CLOSURE PRIOR TO HANGING GIRDERS.

INTERMEDIATE CONTRACT TIME
COMPLETE THE FOLLOWING WORK OF PHASE II, STEP 12 TO HANG STAGE 1 GIRDERS FOR THE WESTBOUND FLYOVER BRIDGE IN ACCORDANCE WITH THE INTERMEDIATE CONTRACT TIME OF 2 DAYS (SEE SPECIAL PROVISIONS).

STEP 12) UNCOVER EASTBOUND US 19 RAMP DETOUR SIGNS, PLACE BARRICADES TO CLOSE RAMP AND HANG STAGE 1 GIRDERS. (SEE TMP-10)

REMOVE BARRICADES AND OPEN RAMP TO TRAFFIC.

STEP 13) COMPLETE BRIDGE AND -DET01WB- CONSTRUCTION STARTED IN PREVIOUS STEPS OF PHASE II.

STEP 14) WORK BEHIND BARRIER AND PLACE TEMPORARY PAVEMENT MARKING AND MARKERS ON WESTBOUND DETOUR (-DET01WB-) ACCORDING TO THE FOLLOWING STATIONS (SEE TMP-12 THRU TMP-14)

-DET01WB- STA. 13+25 +/- TO STA. 35+00 +/-

AND PLACE PCB ACCORDING TO THE FOLLOWING STATIONS: (SEE TMP-08 THRU TMP-11)

-L_LT- STA. 15+90 +/- TO STA. 35+90 +/- LT
 -DET01WB- STA. 13+25 +/- TO -L_LT- STA. 23+35 +/- RT
 -DET01WB- STA. 26+75 +/- TO STA. 29+05 +/- RT

STEP 15) USE RSD 1101.02 (SHEET 4 OF 14) FOR TEMPORARY LANE CLOSURES OF WESTBOUND US 74/US 23 TO MAINTAIN ONE LANE AND PERFORM THE FOLLOWING:

REMOVE PCB AT BEGINNING AND END OF DETOUR AND REPLACE WITH DRUMS.

PLACE PAVEMENT WEDGING OVER EXISTING TRAVEL LANES AND DETOUR TO TIE PAVEMENT ACCORDING TO THE FOLLOWING STATIONS: (SEE TMP-08 THRU TMP-11)

-L_LT- STA. 12+81 +/- TO STA. 16+19 +/-
 -L_LT- STA. 35+52 +/- TO STA. 40+25 +/-

REMOVE CONFLICTING MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING AND MARKERS ON ACCORDING TO THE FOLLOWING STATIONS: (TMP-12 AND TMP-15)

-L_LT- STA. 3+89 +/- TO STA. 13+20 +/-
 -L_LT- STA. 35+00 +/- TO STA. 42+78 +/-

AND SHIFT WESTBOUND US 74/US 23 TRAFFIC, ONE LANE AT A TIME, ONTO TEMPORARY WESTBOUND US 74/US 23 DETOUR (-DET01WB-).

PHASE III

STEP 1) USE RSD 1101.02 (SHEET 4 OF 14) FOR TEMPORARY LANE CLOSURE ON WESTBOUND DETOUR (-DET01WB-) AND PLACE AND RESET PCB ACCORDING TO THE FOLLOWING STATIONS: (SEE TMP-12 THRU TMP-14)

-L_LT- STA. 5+89 +/- LT TO STA. 16+09 +/- RT
 -DET01WB- STA. 35+90 +/- TO STA. 37+70 +/- LT

STEP 2) WORK BEHIND BARRIER TO PERFORM THE FOLLOWING WORK: (SEE OVERVIEW ON TMP-03B AND DETAILS ON TMP-12 THRU TMP-14)

BEGIN CONSTRUCTION OF PROPOSED WESTBOUND US 74/US 23 (-L_LT-) UP TO EXISTING EDGE AND ELEVATION OF DET01WB PAVEMENT, INCLUDING STAGE 2 OF WESTBOUND BRIDGE OVER RICHLAND CREEK, RETAINING WALLS 1 AND 2, AND RELOCATE GROUND MOUNTED SIGNS, ACCORDING TO THE FOLLOWING STATIONS:

-L_LT- STA. 6+89 +/- TO STA. 31+00 +/- LT
 -L_LT- STA. 33+10 +/- TO STA. 35+63 +/- LT

REMOVE REMAINDER OF EXISTING WESTBOUND US 74/US 23 BRIDGE OVER RICHLAND CREEK.

WORK BEHIND BARRIER OR GUARDRAIL, USE RSD 1101.02 (SHEET 4 OF 14) FOR TEMPORARY LANE CLOSURE, OR RSD 1101.04 FOR TEMPORARY SHOULDER CLOSURE AS NEEDED FOR TRENCHLESS INSTALLATION OF DRAINAGE CROSSINGS.

PLACE CMS FOR ADVANCE NOTICE OF TEMPORARY RAMP CLOSURE PRIOR TO EXISTING FLYOVER BRIDGE REMOVAL AND STAGE 2 CONSTRUCTION OF WESTBOUND FLYOVER BRIDGE.

INTERMEDIATE CONTRACT TIME
COMPLETE THE FOLLOWING WORK OF PHASE III, STEP 3 FOR BRIDGE REMOVAL IN ACCORDANCE WITH THE INTERMEDIATE CONTRACT TIME OF 3 DAYS (SEE SPECIAL PROVISIONS).

STEP 3) UNCOVER EASTBOUND US 19 RAMP DETOUR SIGNS, PLACE BARRICADES TO CLOSE RAMP AND REMOVE OVERHEAD STRUCTURE OF THE EXISTING FLYOVER BRIDGE. (SEE DETAIL ON TMP-03C AND TMP-14)

REMOVE BARRICADES AND OPEN RAMP TO TRAFFIC.

STEP 4) WORK BEHIND BARRIER TO COMPLETE REMOVAL OF EXISTING FLYOVER BRIDGE AND CONSTRUCT STAGE 2 OF WESTBOUND FLYOVER BRIDGE (-L_RT-) ABUTMENT #1 AND WALL 5 (SEE PHASE III-A DETAIL ON TMP-18)

STEP 5) UNCOVER EASTBOUND US 19 RAMP DETOUR SIGNS, PLACE BARRICADES TO CLOSE RAMP TO ALL TRAFFIC AND PERFORM THE FOLLOWING ON THE EASTBOUND US 19 RAMP: (SEE TMP-18A)

RESET PCB AND PLACE TEMPORARY PAVEMENT MARKINGS ALONG RAMP ACCORDING TO THE FOLLOWING STATIONS: (SEE PHASE III-B DETAIL ON TMP-18A)

-Y1_RT- STA. 16+50 +/- STA. 25+15 +/- LT (PCB)
 -Y1_RT- STA. 18+00 +/- STA. 23+80 +/- RT (PCB)
 -Y1_RT- STA. 15+00 +/- TO 28+83 +/- LT (PVT MARKING)
 -Y1_RT- STA. 15+50 +/- TO 25+29 +/- RT (PVT MARKING)

AND SHIFT RAMP TRAFFIC INTO NEW TEMPORARY PATTERN.

STEP 6) WORK BEHIND BARRIER TO CONSTRUCT STAGE 2 OF WALL 7 AND FLYOVER BRIDGE ABUTMENT #2 (SEE PHASE III-B DETAIL ON TMP-18A).

PLACE CMS FOR ADVANCE NOTICE OF TEMPORARY RAMP CLOSURE PRIOR TO HANGING STAGE 2 GIRDERS ON WESTBOUND FLYOVER BRIDGE.

INTERMEDIATE CONTRACT TIME
COMPLETE THE FOLLOWING WORK OF PHASE III, STEP 7 TO HANG STAGE 2 GIRDERS FOR THE WESTBOUND FLYOVER BRIDGE IN ACCORDANCE WITH THE INTERMEDIATE CONTRACT TIME OF 2 DAYS (SEE SPECIAL PROVISIONS).

STEP 7) UNCOVER EASTBOUND US 19 RAMP DETOUR SIGNS, PLACE BARRICADES TO CLOSE RAMP AND HANG STAGE 2 GIRDERS ON WESTBOUND FLYOVER BRIDGE. (SEE TMP-10)

REMOVE BARRICADES AND OPEN RAMP TO TRAFFIC.

STEP 8) COMPLETE ALL ROADWAY CONSTRUCTION STARTED IN PREVIOUS STEPS OF PHASE III. WHEN FLYOVER BRIDGE IS COMPLETE, PERFORM STEP 8A.

STEP 8A) REMOVE PCB FROM RAMP (-Y1_RT). (SEE TMP-21)

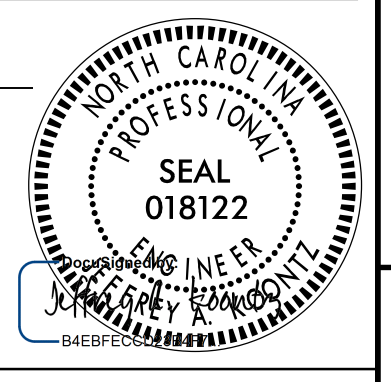


OPEN RAMP TO TRAFFIC IN EXISTING PATTERN UNTIL FINAL PAVEMENT IS PLACED.

STEP 9) WORK BEHIND BARRIER ON WESTBOUND US 74/US 23 (-L_LT-) AND PLACE TEMPORARY PAVEMENT MARKING AND MARKERS FOR ONE OUTSIDE LANE ACCORDING TO THE FOLLOWING STATIONS: (SEE TMP-19 THRU TMP-21)

-L_LT- STA. 15+00 +/- TO STA. 35+00 +/-

AND PLACE DRUMS ALONG THE NEW LANE.

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STEP 10) USE 1101.02 (SHEET 4 OF 14) TO CLOSE THE OUTSIDE LANE OF DET01WB AND MAINTAIN WESTBOUND TRAFFIC IN THE INSIDE LANE. REMOVE PCB AT THE BEGINNING AND END OF DET01WB AND PLACE TEMPORARY PAVEMENT MARKING AND MARKERS FOR ONE OUTSIDE WESTBOUND LANE ACCORDING TO THE FOLLOWING STATIONS: (SEE TMP-19 THRU TMP-22)

-L_LT- STA. 3+00 +/- TO STA. 15+00 +/-
 -L_LT- STA. 35+00 +/- TO STA. 47+47 +/-

AND SHIFT WESTBOUND US 74/ US 23 TRAFFIC INTO THE NEW TEMPORARY PATTERN IN THE OUTSIDE LANE OF -L_LT-.

STEP 11) RESET PCB ACCORDING TO THE FOLLOWING STATIONS: (SEE TMP-19 THRU TMP-22)

-L_LT- STA. 6+50 +/- TO STA. 36+90 +/- RT

STEP 12) PLACE TEMPORARY PAVEMENT MARKING TO COMPLETE THE INSIDE LANE OF WESTBOUND US 74/US 23 (-L_LT-) ACCORDING TO THE FOLLOWING STATIONS: (SEE TMP-19 THRU TMP-22)

-L_LT- STA. 3+00 +/- TO STA. 47+47 +/-

AND OPEN BOTH WESTBOUND LANES TO TRAFFIC IN A TEMPORARY PATTERN.

PHASE IV

STEP 1) WORK BEHIND BARRIER AND CONSTRUCT US 74/US 23 (-L_LT- AND -L_RT-) WITHIN THE TEMPORARY MEDIAN AREA, UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE, INCLUDING DRAINAGE, STAGE 3 CONSTRUCTION AND DEMOLITION OF WESTBOUND BRIDGE OVER RICHLAND CREEK, AND EASTBOUND BRIDGE OVER RICHLAND CREEK, ACCORDING TO THE FOLLOWING STATIONS: (SEE TMP-19 THRU TMP-21)

-L_RT- STA. 6+89 +/- TO -L_LT- STA. 34+00 +/-

WORK BEHIND BARRIER OR GUARDRAIL, USE RSD 1101.02 (SHEET 4 OF 14) FOR TEMPORARY LANE CLOSURE, OR RSD 1101.04 FOR TEMPORARY SHOULDER CLOSURE AS NEEDED FOR TRENCHLESS INSTALLATION OF DRAINAGE CROSSINGS.

STEP 2) WORK BEHIND BARRIER TO PLACE TEMPORARY PAVEMENT MARKING AND MARKERS ACCORING TO THE FOLLOWING STATIONS: (SEE TMP-23 THRU TMP-26)

-L_RT- STA. 13+00 +/- TO -L_RT- STA. 30+80 +/-

STEP 3) USE RSD 1101.04 (SHEET 4 OF 14) FOR TEMPORARY LANE CLOSURES TO PEFORM THE FOLLOWING:

REMOVE PCB AT BEGINNING AND END OF EASTBOUND DETOUR (-DET01EB-) AND REPLACE WITH DRUMS.

WEDGE OVER EXISTING DETOUR AND PROPOSED -L_RT- EASTBOUND TRAVEL LANES TO TIE PAVEMENT ACCORDING TO THE FOLLOWING STATION: (SEE TMP-19 AND TMP-21)

-L_RT- STA. 10+30 +/- TO STA. 13+00 +/-
 -L_RT- STA. 30+80 +/- TO STA. 35+55 +/-

REMOVE CONFLICTING PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKINGS AND MARKER ACCORDING TO THE FOLLOWING STATIONS: (SEE TMP-23 AND TMP-26)

-L_RT- STA. 6+50 +/- TO STA. 13+00 +/-
 -L_RT- STA. 30+80 +/- TO STA. 47+47 +/-
 -L_LT- STA. 6+89 +/- TO STA. 47+47 +/- (FINAL PATTERN)

AND SHIFT EASTBOUND US 74/US 23 TRAFFIC INTO TEMPORARY PATTERN ON -L_RT- AND WESTBOUND US 74/US 23 TRAFFIC INTO FINAL PATTERN.

STEP 4) USE 1101.02 (SHEET 4 OF 14) FOR TEMPORARY OUTSIDE LANE CLOSURE OF EASTBOUND US 74/ US 23 (-L_RT-) AND RESET PCB ACCORDING TO THE FOLLOWING STATIONS: (SEE TMP-23 THRU TMP-26)

-L_LT- STA. 6+10 +/- TO STA. 40+50 +/- RT

PHASE V

STEP 1) WORK BEHIND BARRIER AND REMOVE THE EASTBOUND DETOUR, COMPLETE CONSTRUCTION OF EASTBOUND US 74/US 23 (-L_RT-) SHOULDER, UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE, RESTORE THE JURISDICTIONAL STREAM, AND RELOCATE GROUND MOUNTED SIGN ACCORDING TO THE FOLLOWING STATIONS:

-L_RT- STA. 6+86 +/- TO STA. 40+04 +/- RT

STEP 2) USE 1101.02 (SHEET 4 OF 14) TO CLOSE THE OUTSIDE LANE OF EASTBOUND US 74/US 23 AND REMOVE PCB PLACED IN PHASE IV.

PHASE VI (NOT DEPICTED)

STEP 1) USE RSD 1101.02 (SHEET 4 OF 14) FOR TEMPORARY LANE CLOSURES AND PLACE FINAL LAYER OF SURFACE COURSE ON US 74/US 23 (-L_RT- AND -L_LT-) AND USE RSD 1205.01, 1205.02, 1205.03, 1205.12, 1250.01, 1253.01, AND PLACE FINAL PAVEMENT MARKING (THERMOPLASTIC) AND MARKERS (NON-CAST IRON SNOWPLOWABLE) ACCORDING TO THE FOLLOWING STATIONS: (SEE ROADWAY PLANS AND PAVEMENT MARKING PLANS)

-L_RT- STA. 6+86 +/- TO STA. 40+04 +/-
 -L_LT- STA. 6+89 +/- TO STA. 40+74 +/-

PLACE BLACK PAVEMENT MARKING (12") OVER ANY CONFLICTING TEMPORARY MARKINGS BEYOND THE FINAL PAVING LIMITS AND PLACE FINAL PAVEMENT MARKING (THERMOPLASTIC) IN ORIGINAL PATTERN.


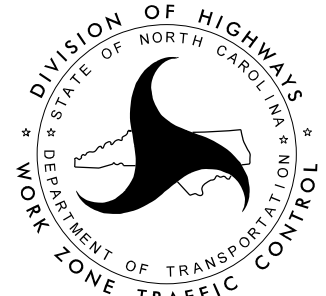
STEP 2) UNCOVER EASTBOUND US 19 RAMP DETOUR SIGNS, PLACE BARRICADES TO CLOSE RAMP TO ALL TRAFFIC AND PERFORM THE FOLLOWING ON THE EASTBOUND US 19 RAMP: (SEE ROADWAY PLANS AND PAVEMENT MARKING PLANS)

PLACE FINAL LAYER OF SURFACE COURSE ON EASTBOUND US 19 RAMP (-Y1-RT) AND USE RSD 1205.01, 1205.02, AND 1205.03 AND PLACE FINAL PAVEMENT MARKING (THERMOPLASTIC) ACCORDING TO THE FOLLOWING STATIONS: (SEE ROADWAY PLANS AND PAVEMENT MARKING PLANS)

-Y1-RT- STA. 16+83 +/- TO STA. 28+83 +/-

STEP 3) REMOVE ALL DEVICES AND SIGNS AND OPEN ALL LANES IN FINAL PATTERN.

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EXISTING CONDITION

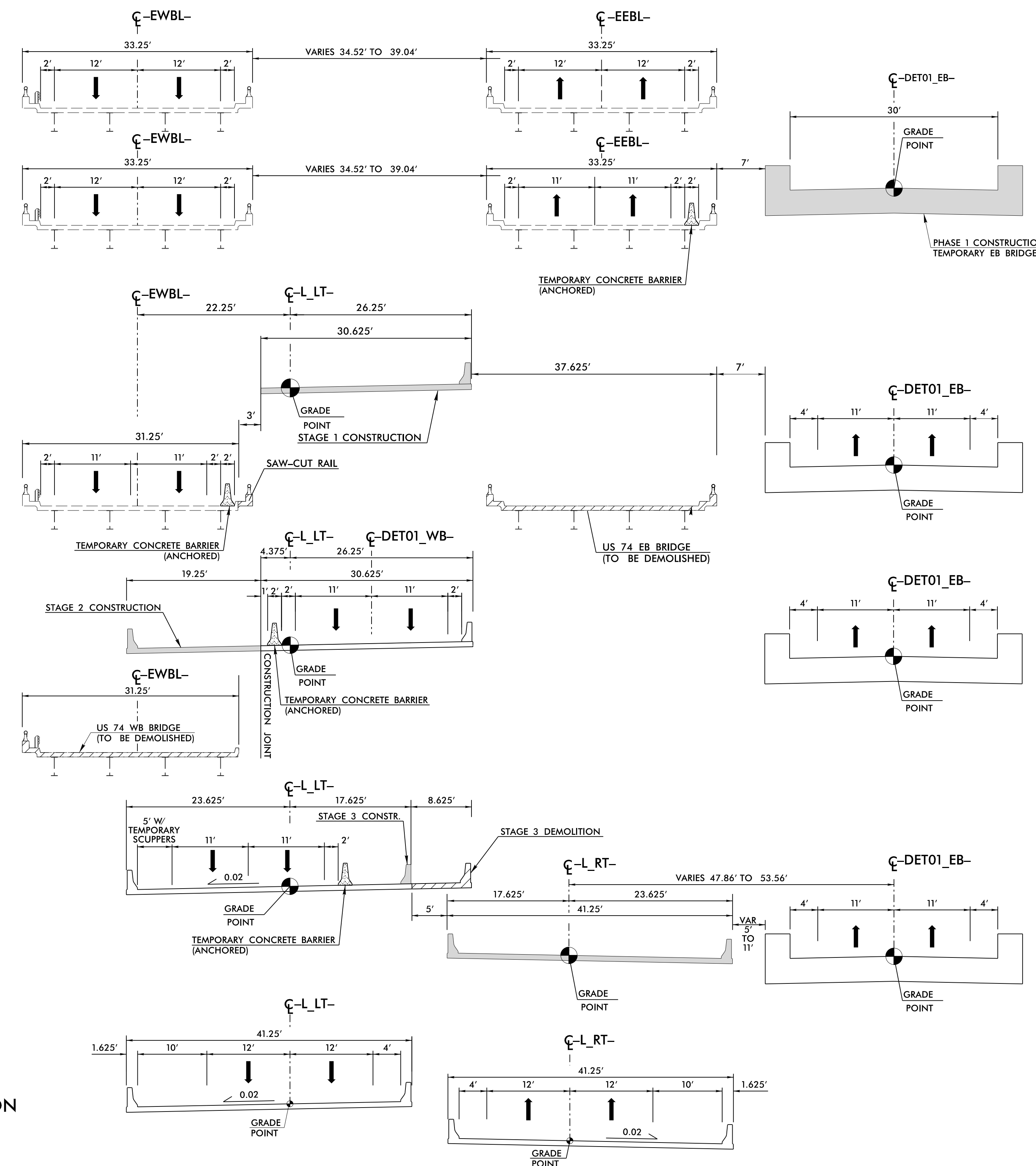
PHASE 1

PHASE 2

PHASE 3

PHASE 4

FINAL STRUCTURE CONFIGURATION



SECTION C-C
RICHLAND CREEK STRUCTURES
3' MIN. STRUCTURE SEPARATION
DURING CONSTRUCTION
BETWEEN EXIST AND PROPOSED
WB STRUCTURE

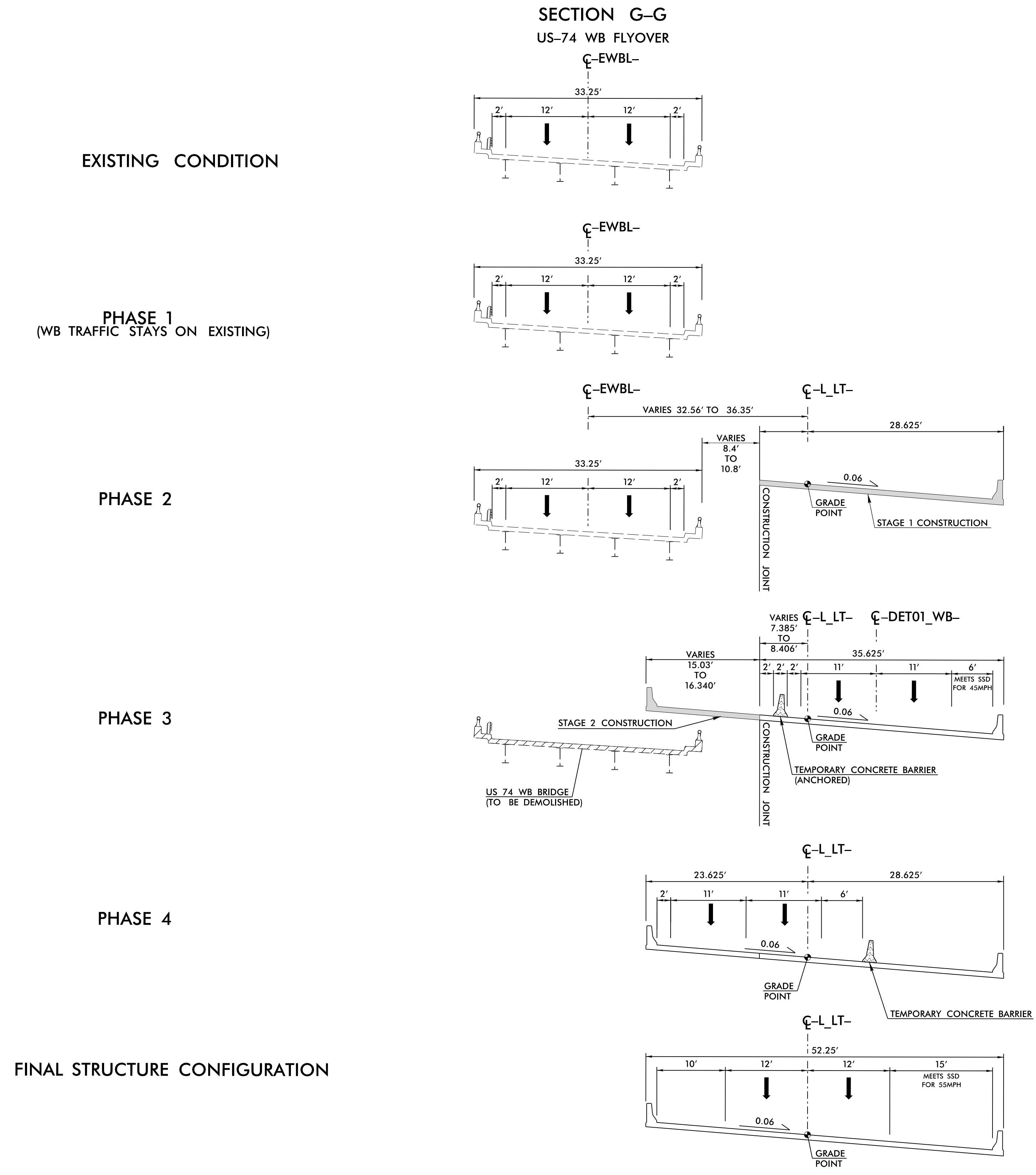
9/6/2023
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US 74/US 23
BRIDGES OVER RICHLAND CREEK
SECTION OVERVIEW



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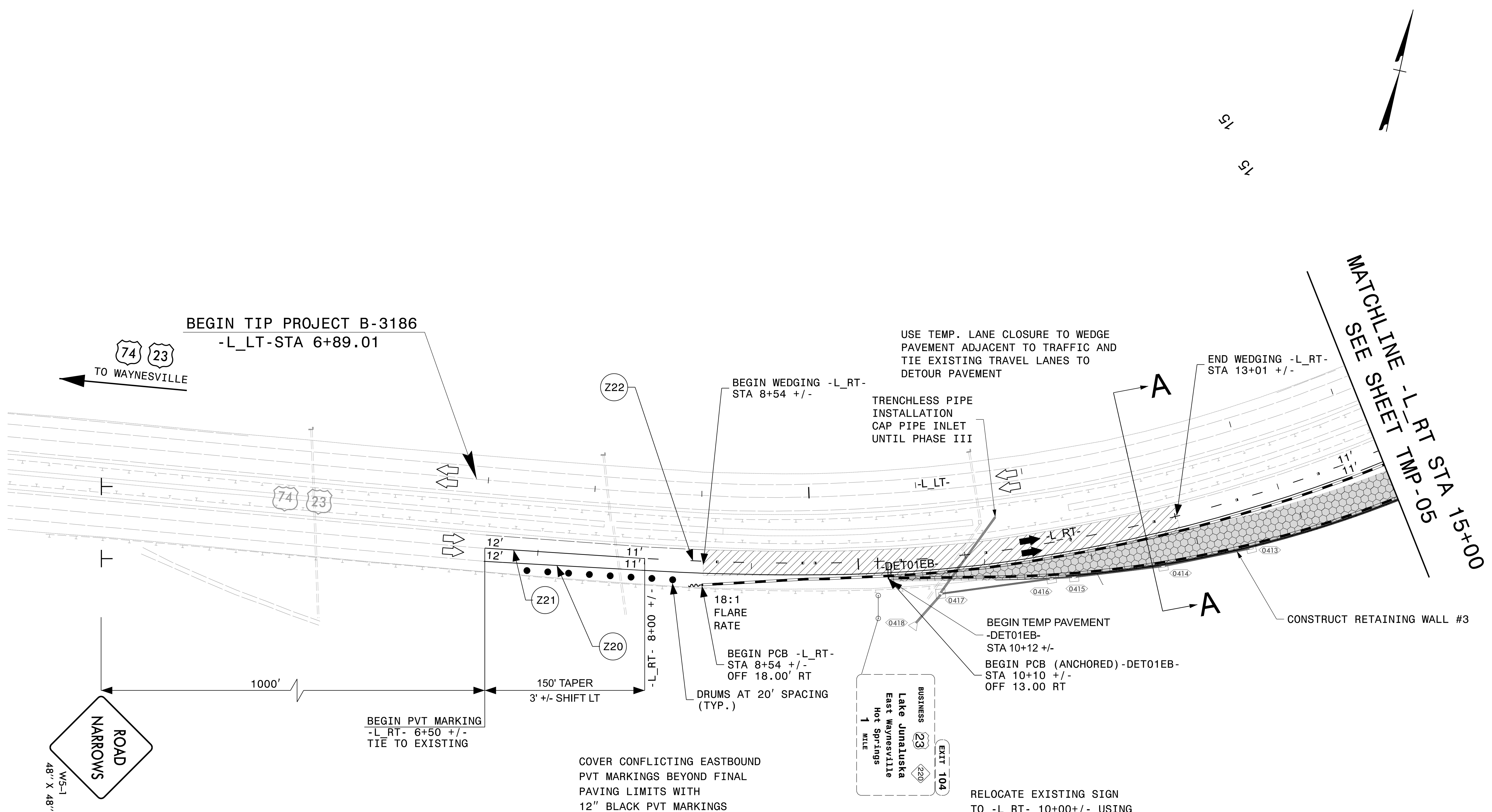
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**US 74/US 23
FLYOVER BRIDGE OVER US 19
SECTION OVERVIEW**

SEE TMP-04A FOR SECTION A-A DETAIL

PROJ. REFERENCE NO.	SHEET NO.
B-3186 / B-5898	TMP-04



	WORK AREA
	WEDGING & WIDENING
	TEMPORARY PAVEMENT
	PAVEMENT / BRIDGE REMOVAL

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10

BUSINESS 23
EXIT 104
Lake Junaluska
East Waynesville
Hot Springs
1 MILE

RELOCATE EXISTING SIGN TO -L_RT- 10+00 +/- USING TEMPORARY SUPPORTS

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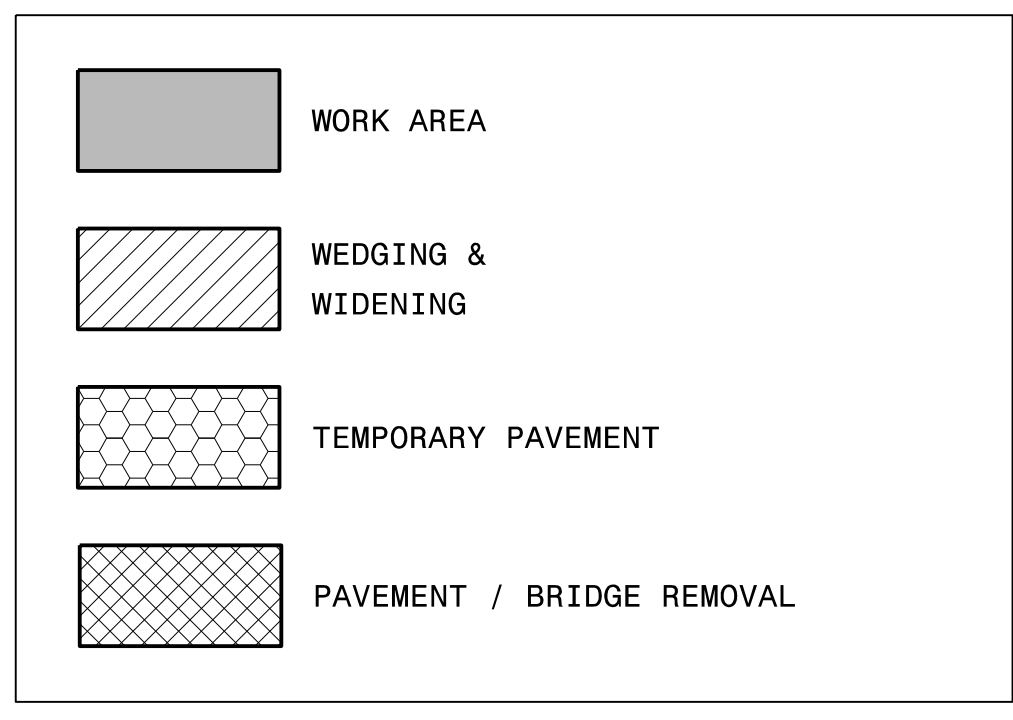
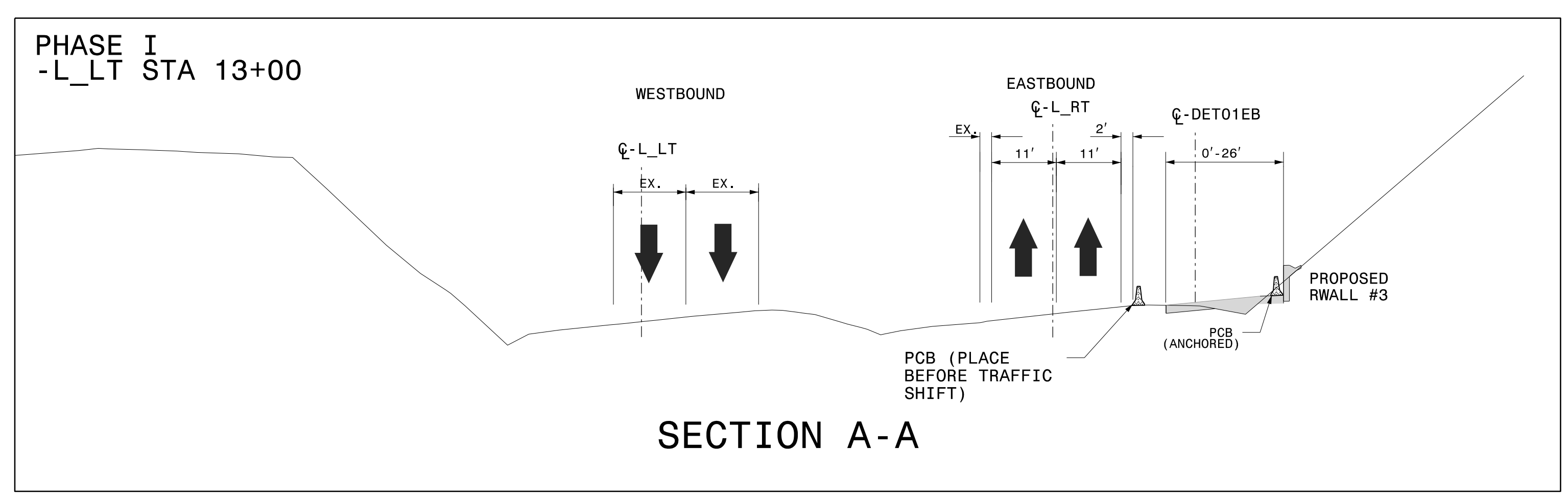
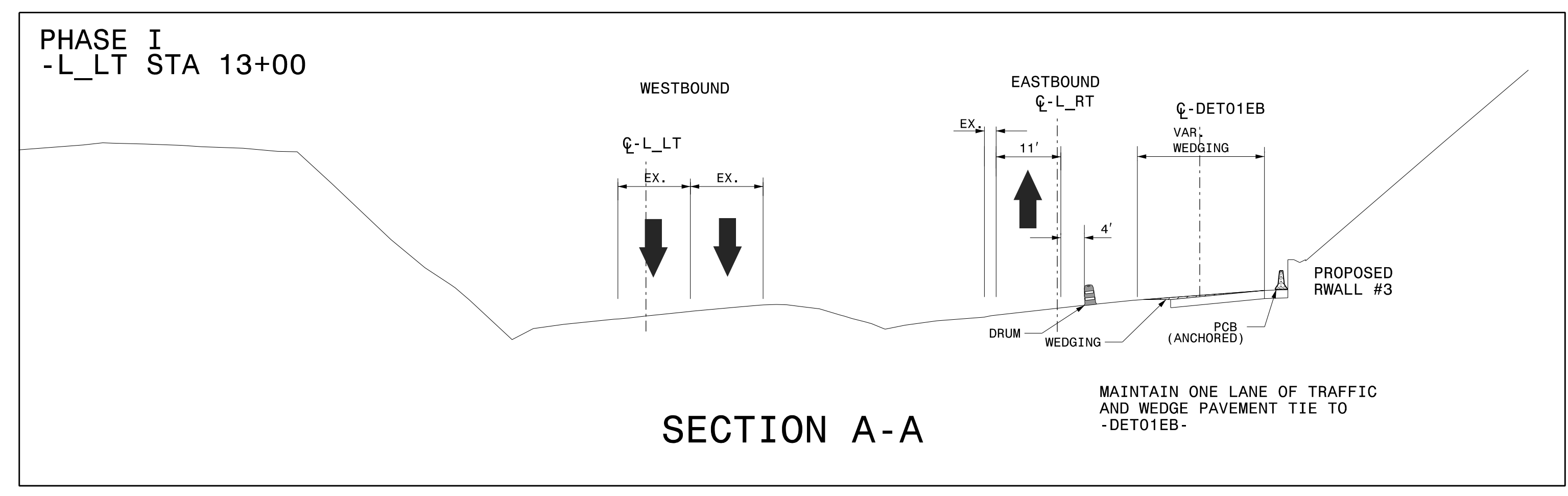
SEAL 018122
JEFFREY A. KOOFT
ENGINEER

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DIVISION OF HIGHWAYS
STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
WORK ZONE TRAFFIC CONTROL

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PHASE I DETAILS
US 74/US 23



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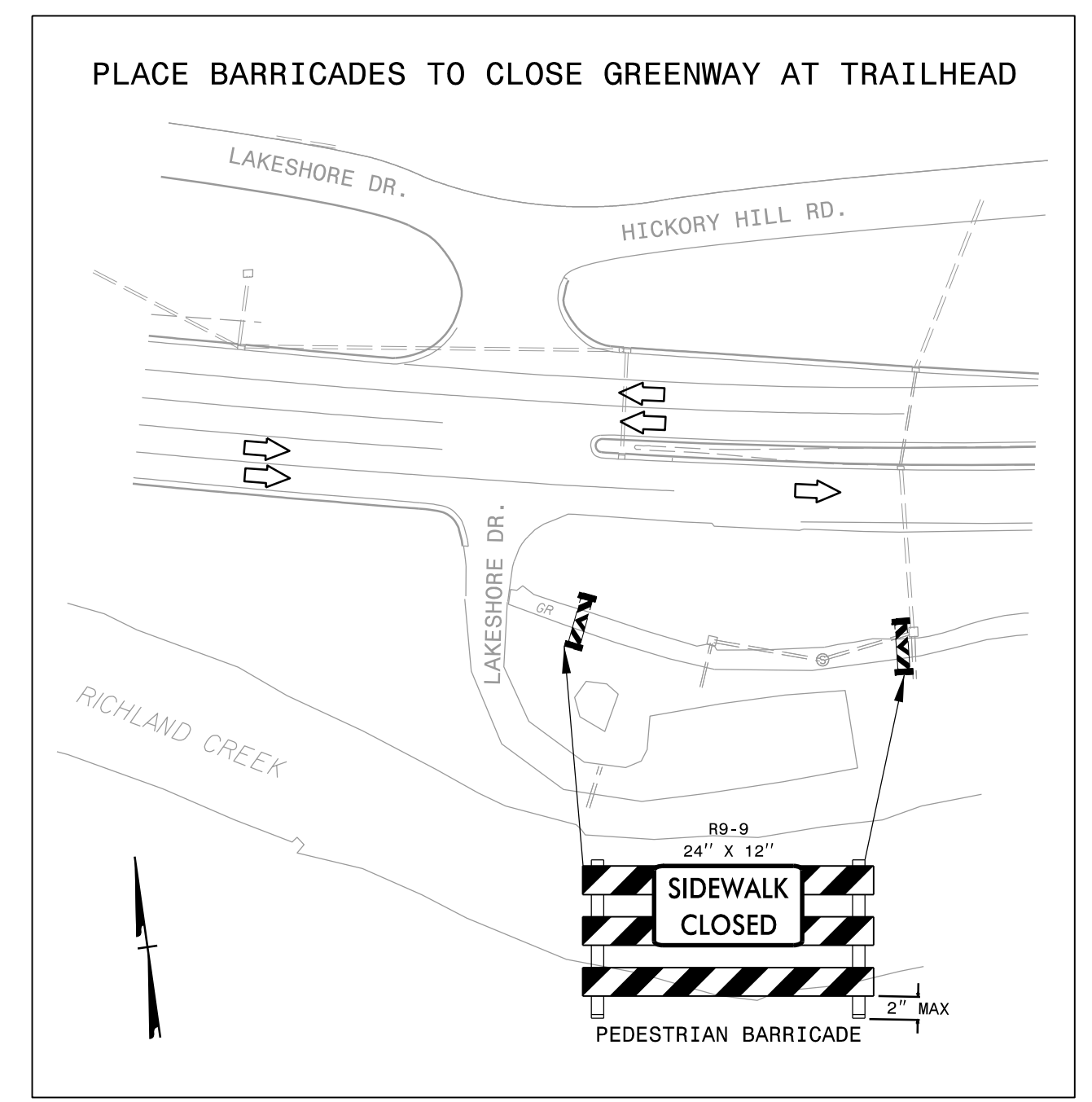
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PHASE I DETAILS
US 74/US 23

SEE TMP-05A FOR SECTION DETAILS

FOR NORTH SIDEWALK CLOSURE, SEE INSET DETAIL ON LEFT SIDE OF SHEET.



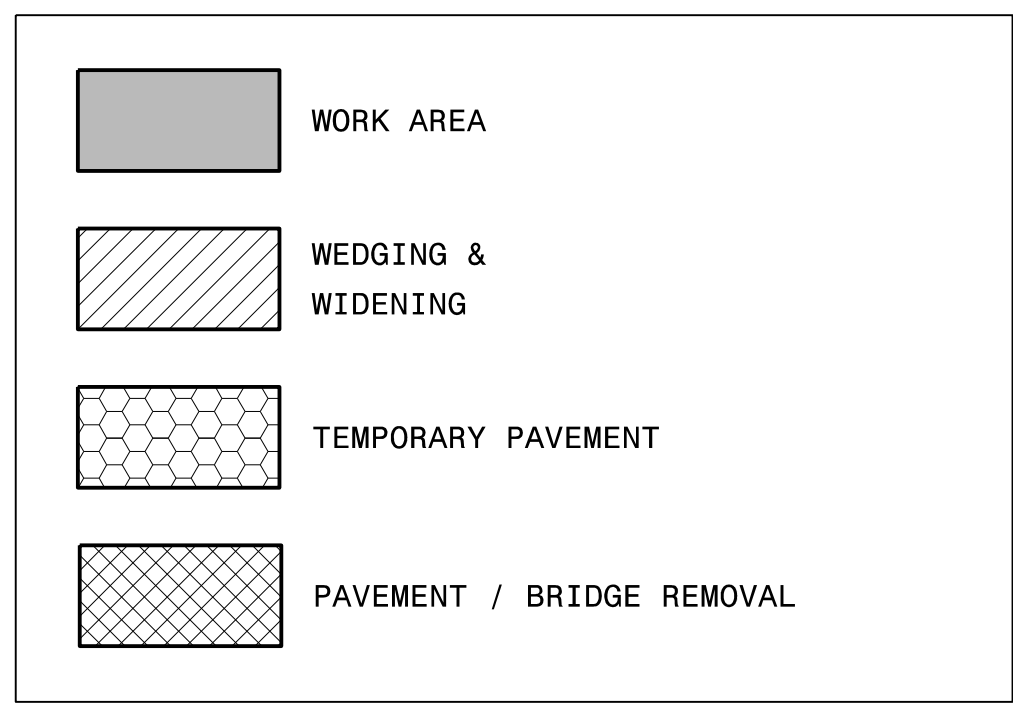
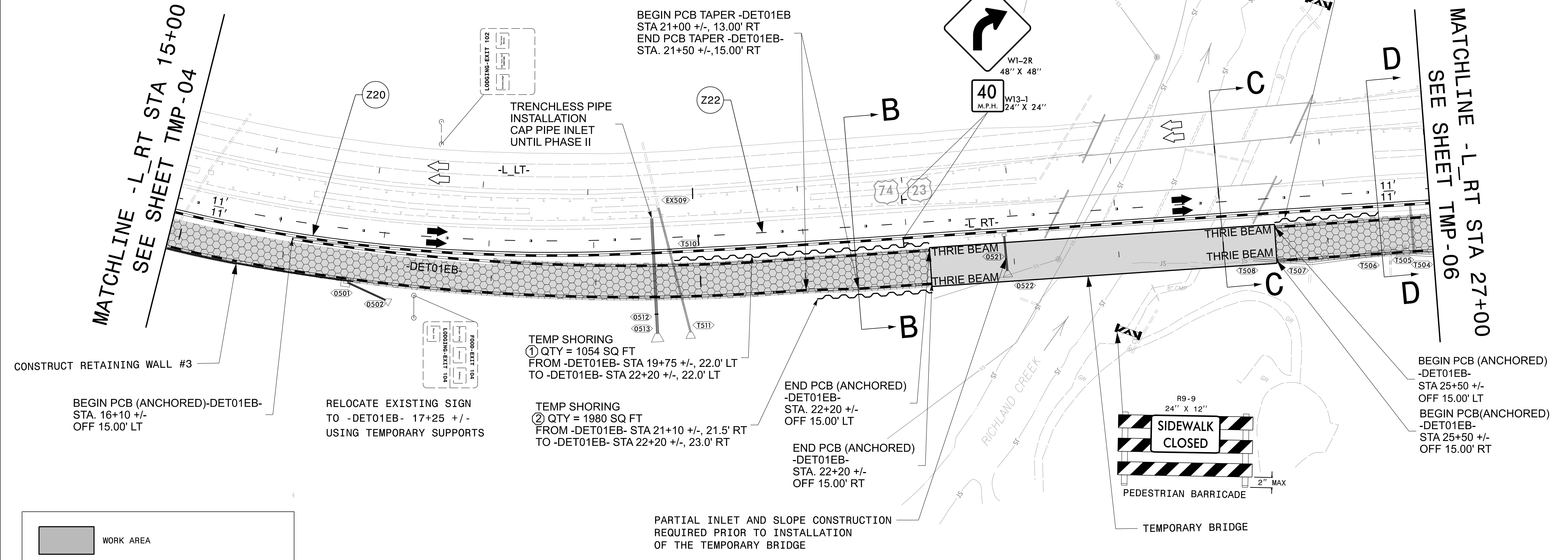
CUR DATA -DET01 EB-
 PIC 18+14.94
 $\Delta c = 14^{\circ}18'16.2''$ (LT)
 $D = 02^{\circ}47'41.7''$
 $Lc = 511.80$
 $Tc = 257.24$
 $R = 2,050$



TEMP SHORING
 ③ QTY = 200 SQ FT
 FROM -DET01EB- STA 25+50 +/-, 22.0' LT
 TO -DET01EB- STA 26+50 +/-, 22.0' LT

MATCHLINE -L_RT STA 15+00
 SEE SHEET TMP-04

MATCHLINE -L_RT STA 27+00
 SEE SHEET TMP-06



* TEMP GUARDRAIL ANCHOR UNIT, TYPE THRIE-BEAM

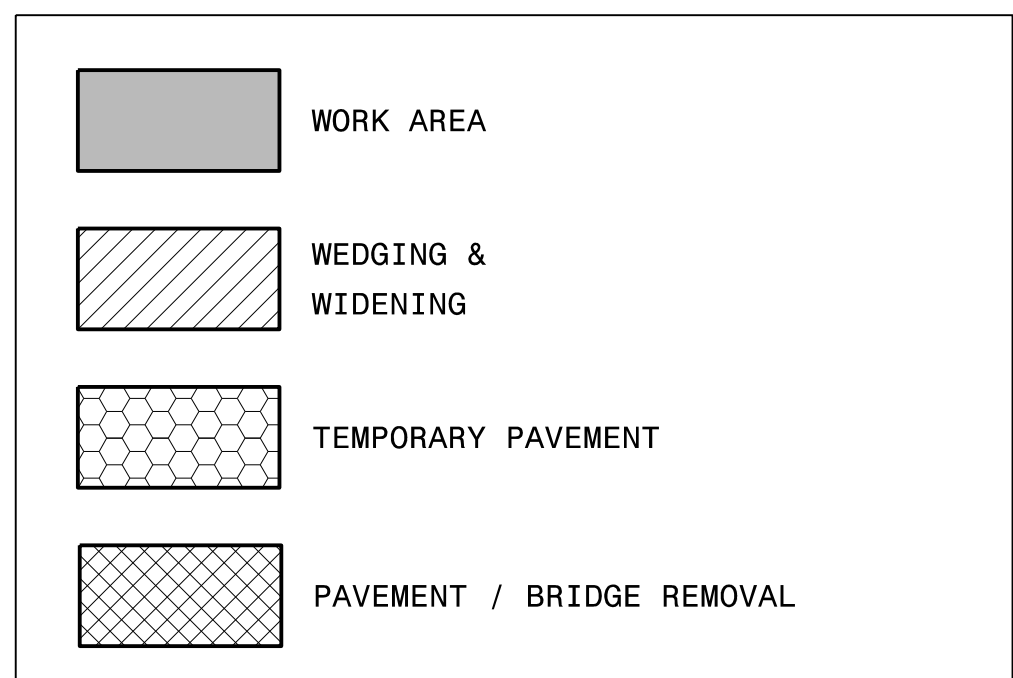
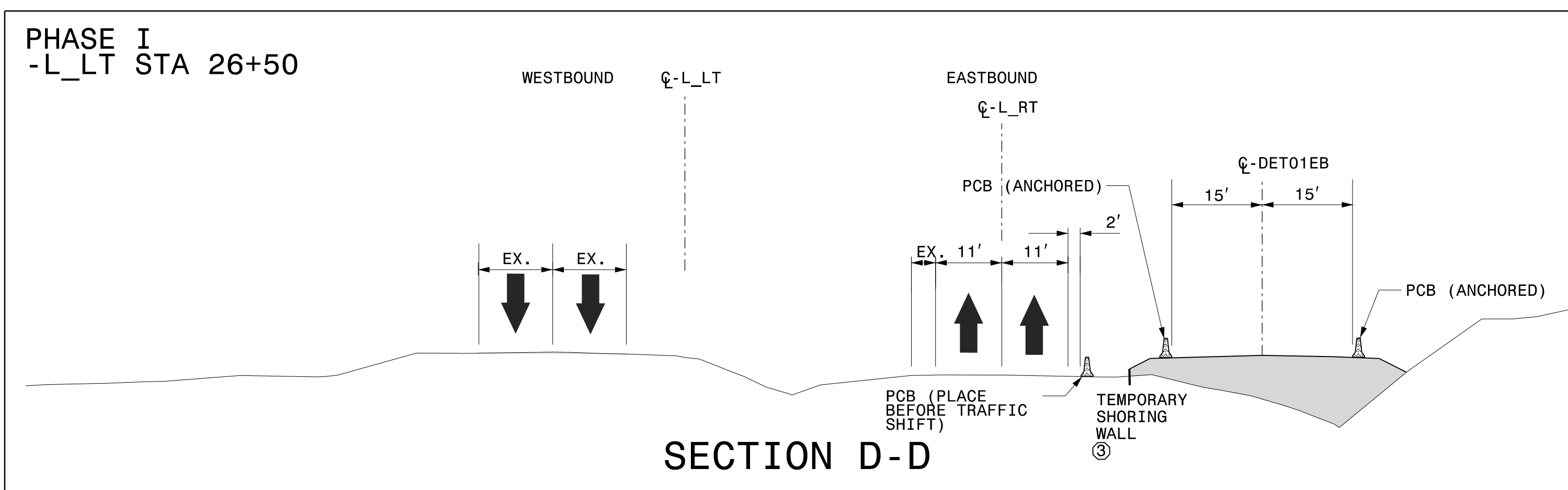
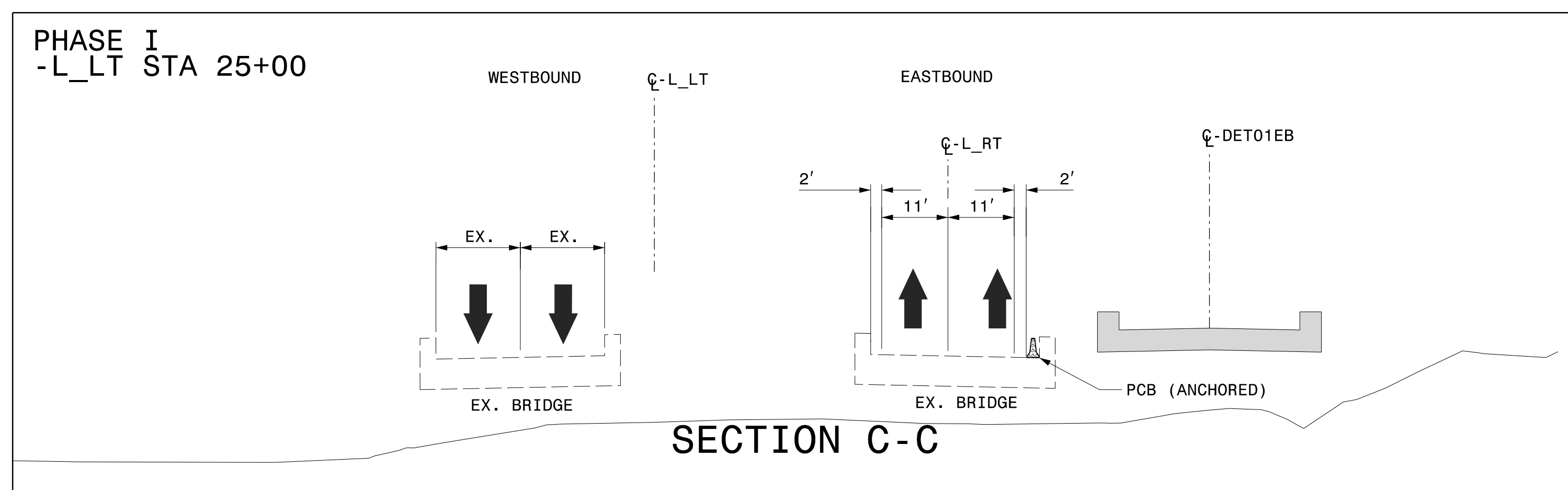
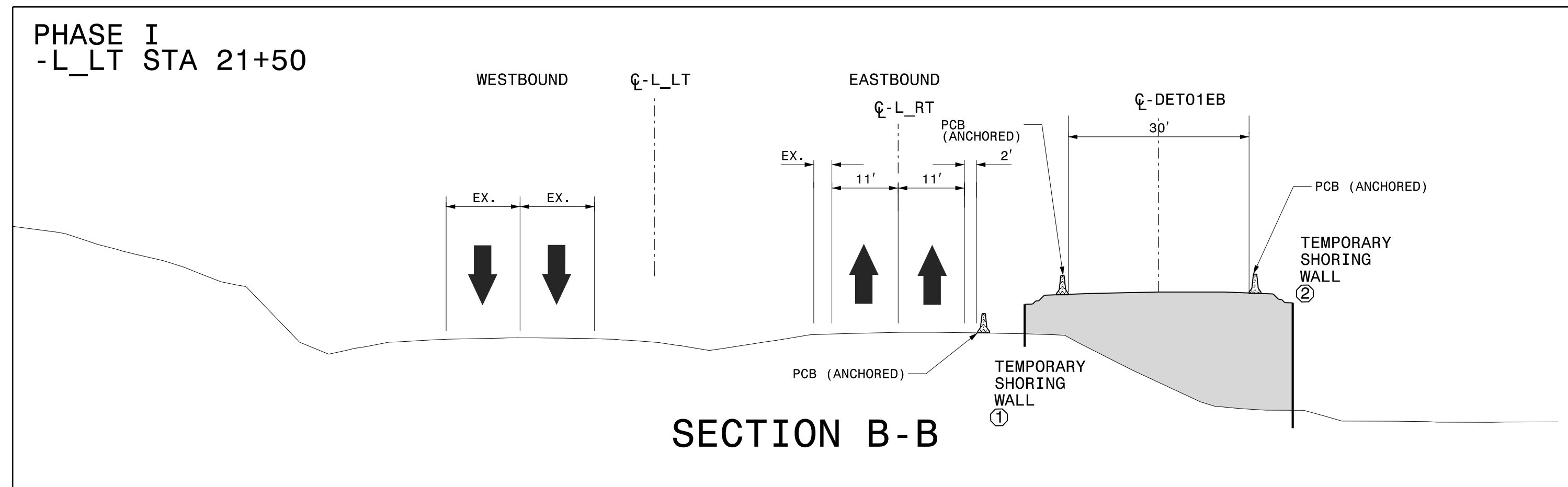
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PHASE I DETAILS
 US 74/US 23

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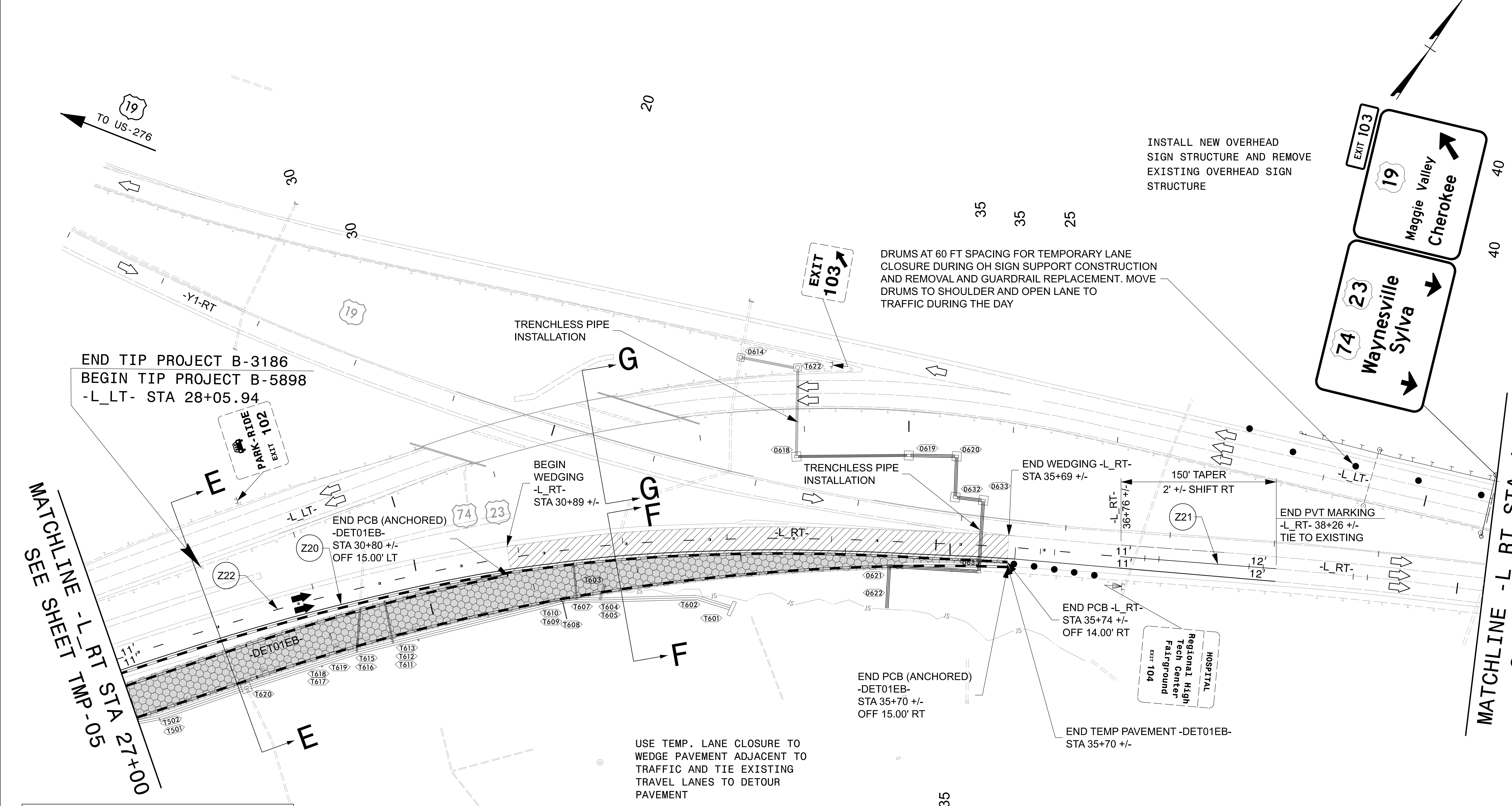
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PHASE I DETAILS
US 74/US 23

SEE TMP-06A FOR SECTION DETAILS

PROJ. REFERENCE NO.	SHEET NO.
B-3186 / B-5898	TMP-06



END TIP PROJECT B-3186
 BEGIN TIP PROJECT B-5898
 -L_LT- STA 28+05.94

MATCHLINE - L_RT STA 27+00
 SEE SHEET TMP-05

MATCHLINE - L_RT STA 40+25
 SEE SHEET TMP-07

	WORK AREA
	WEDGING & WIDENING
	TEMPORARY PAVEMENT
	PAVEMENT / BRIDGE REMOVAL

USE TEMP. LANE CLOSURE TO WEDGE PAVEMENT ADJACENT TO TRAFFIC AND TIE EXISTING TRAVEL LANES TO DETOUR PAVEMENT

DRUMS AT 60 FT SPACING FOR TEMPORARY LANE CLOSURE DURING OH SIGN SUPPORT CONSTRUCTION AND REMOVAL AND GUARDRAIL REPLACEMENT. MOVE DRUMS TO SHOULDER AND OPEN LANE TO TRAFFIC DURING THE DAY

INSTALL NEW OVERHEAD SIGN STRUCTURE AND REMOVE EXISTING OVERHEAD SIGN STRUCTURE

END WEDGING -L_RT- STA 35+69 +/-

END PCB -L_RT- STA 35+74 +/- OFF 14.00' RT

END PCB (ANCHORED) -DET01EB- STA 35+70 +/- OFF 15.00' RT

END TEMP PAVEMENT -DET01EB- STA 35+70 +/-

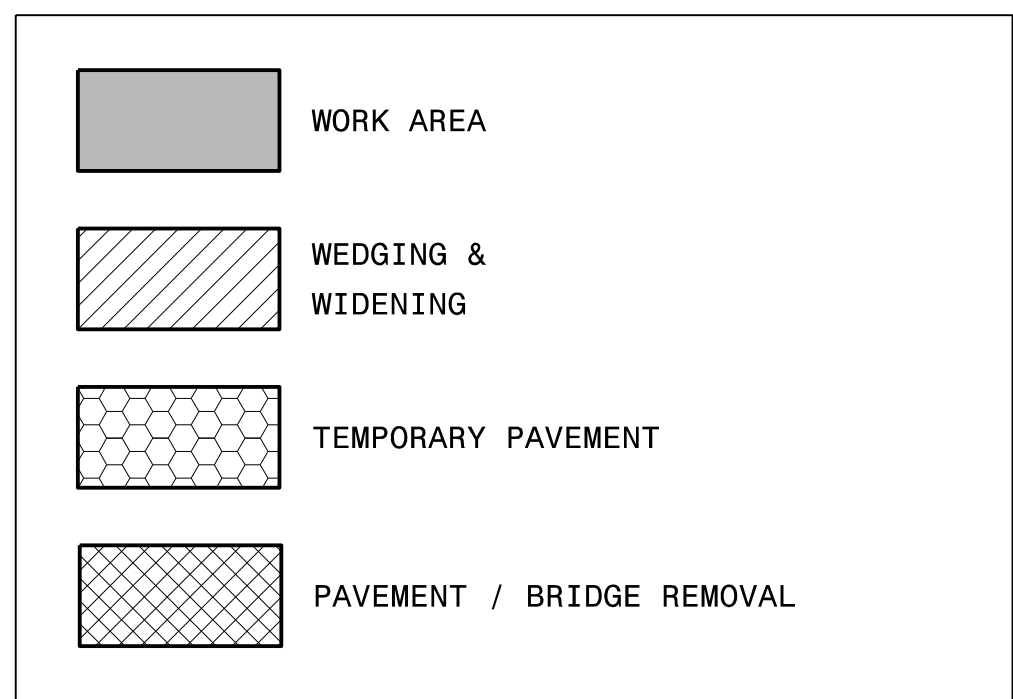
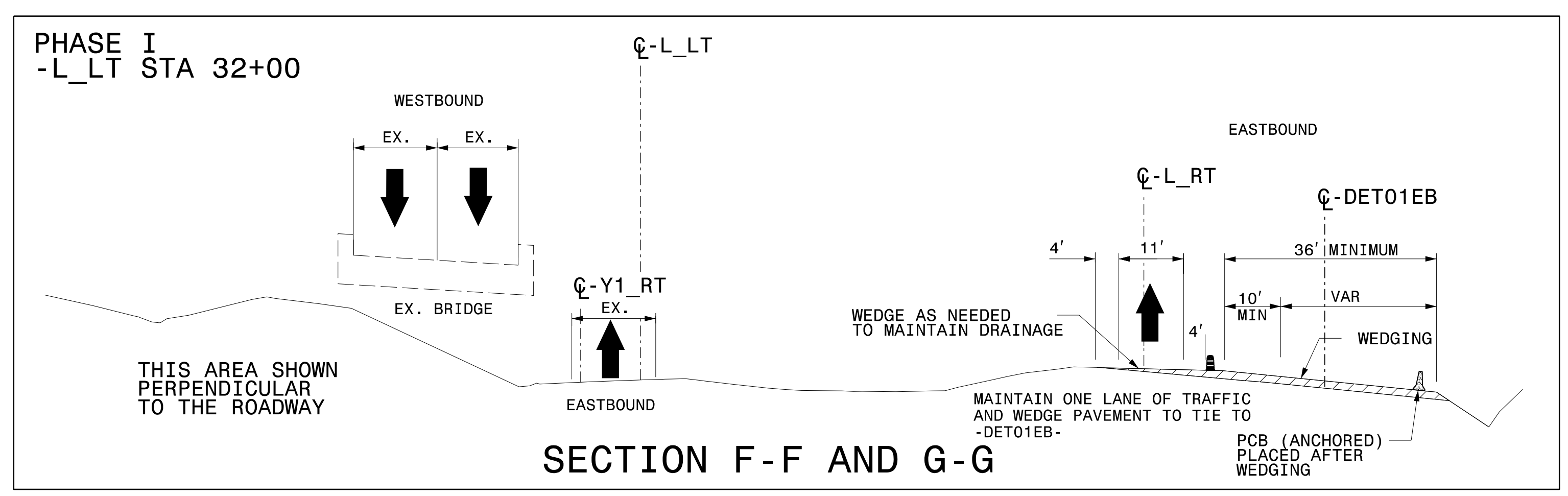
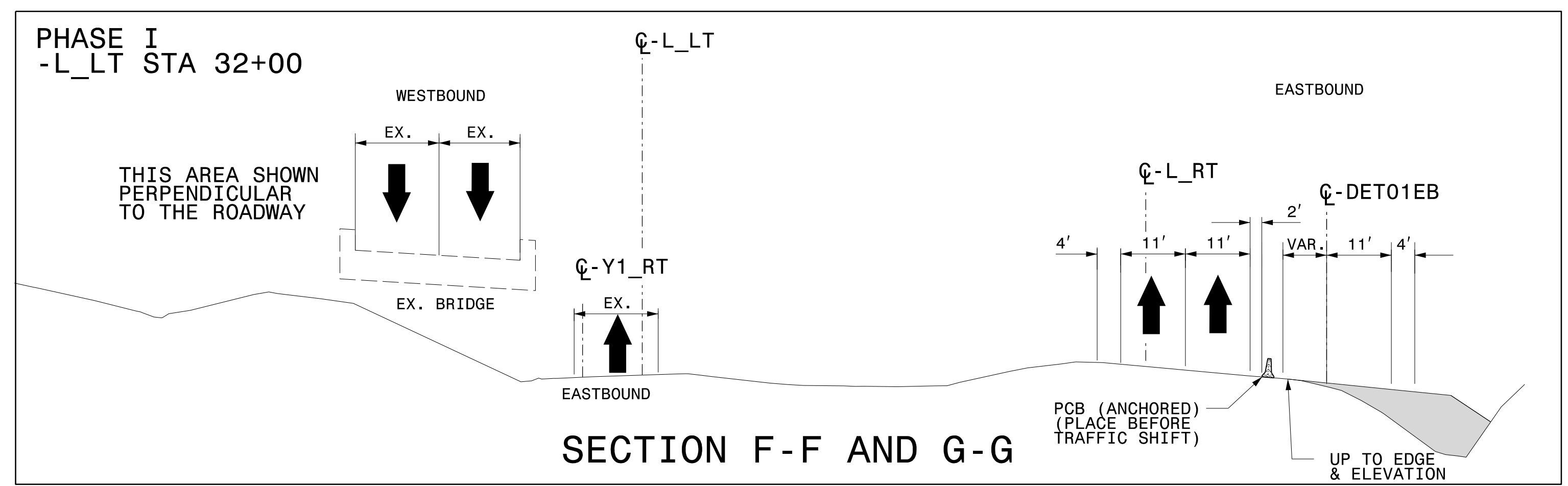
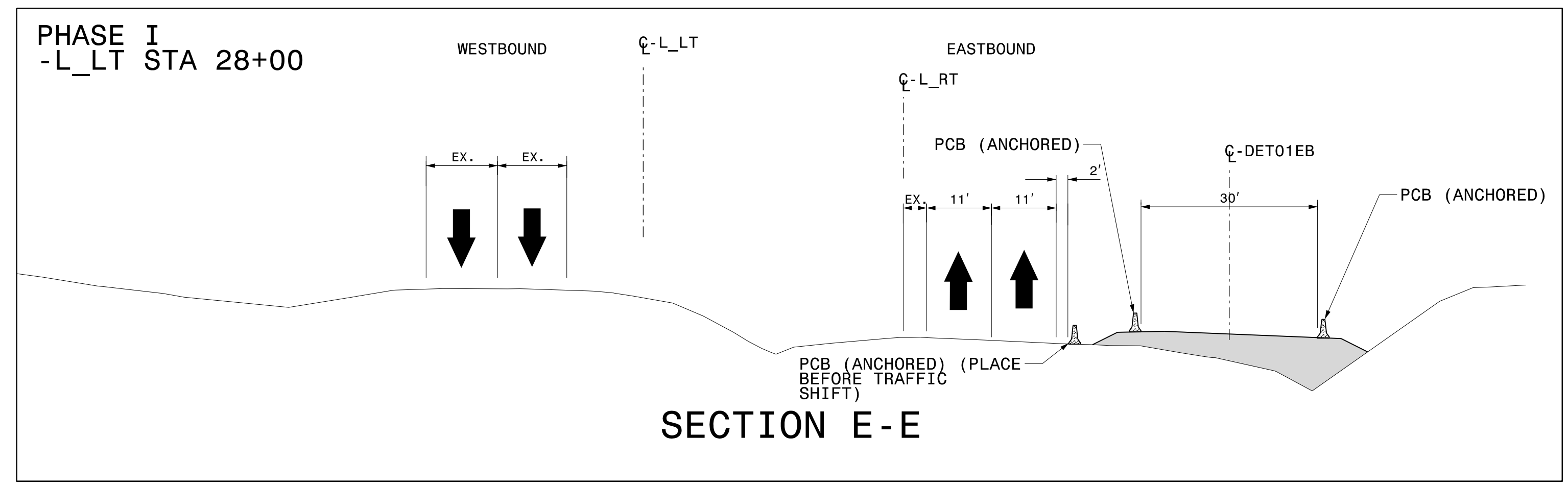
10/4/2023
 B3186-B5898_TC_TMP-06_Phase I.dgn
 jeff.koontz

APPROVED: *Jeffrey A. Koontz*
 DATE: 10/17/2023

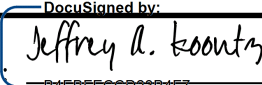
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PHASE I DETAILS
 US 74/US 23




9/6/2023
 B3386-B5898-TC-TMP-06A_Phase ISections.dgn
 jeffrey.thurston

APPROVED: 

DATE: 10/17/2023

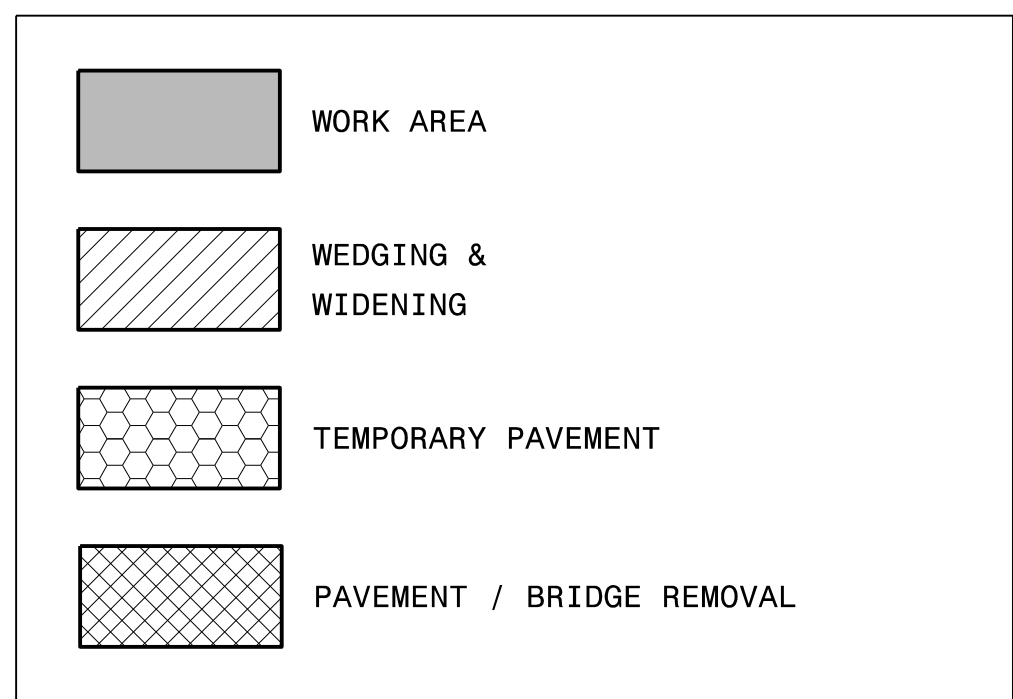


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PHASE I DETAILS
US 74/US 23



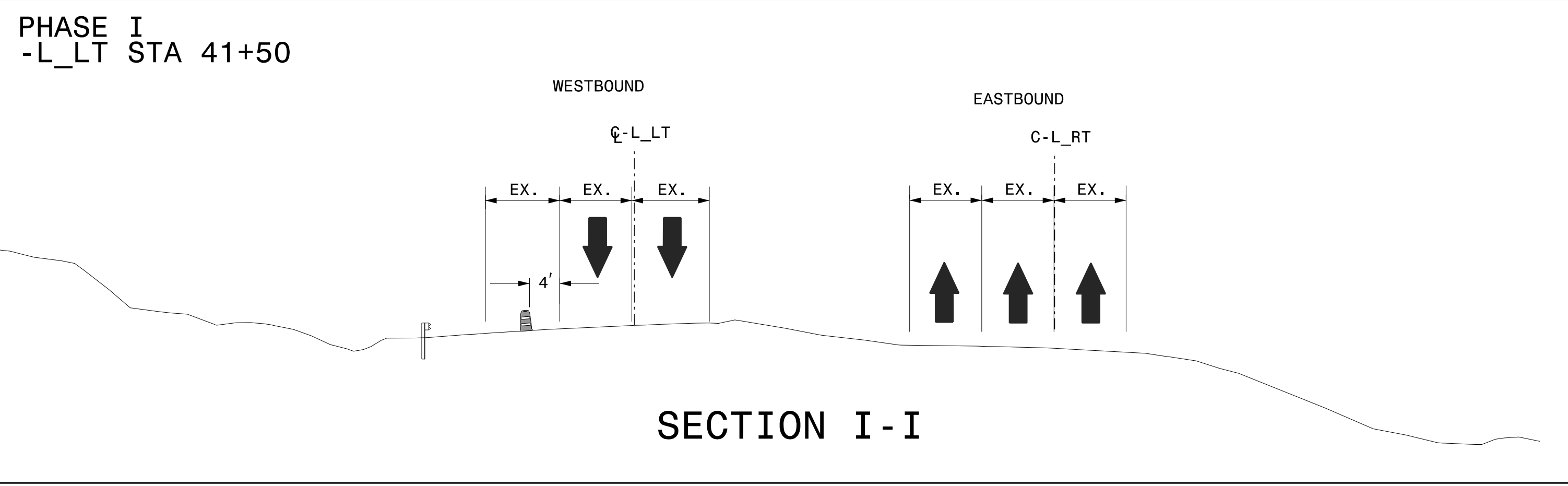
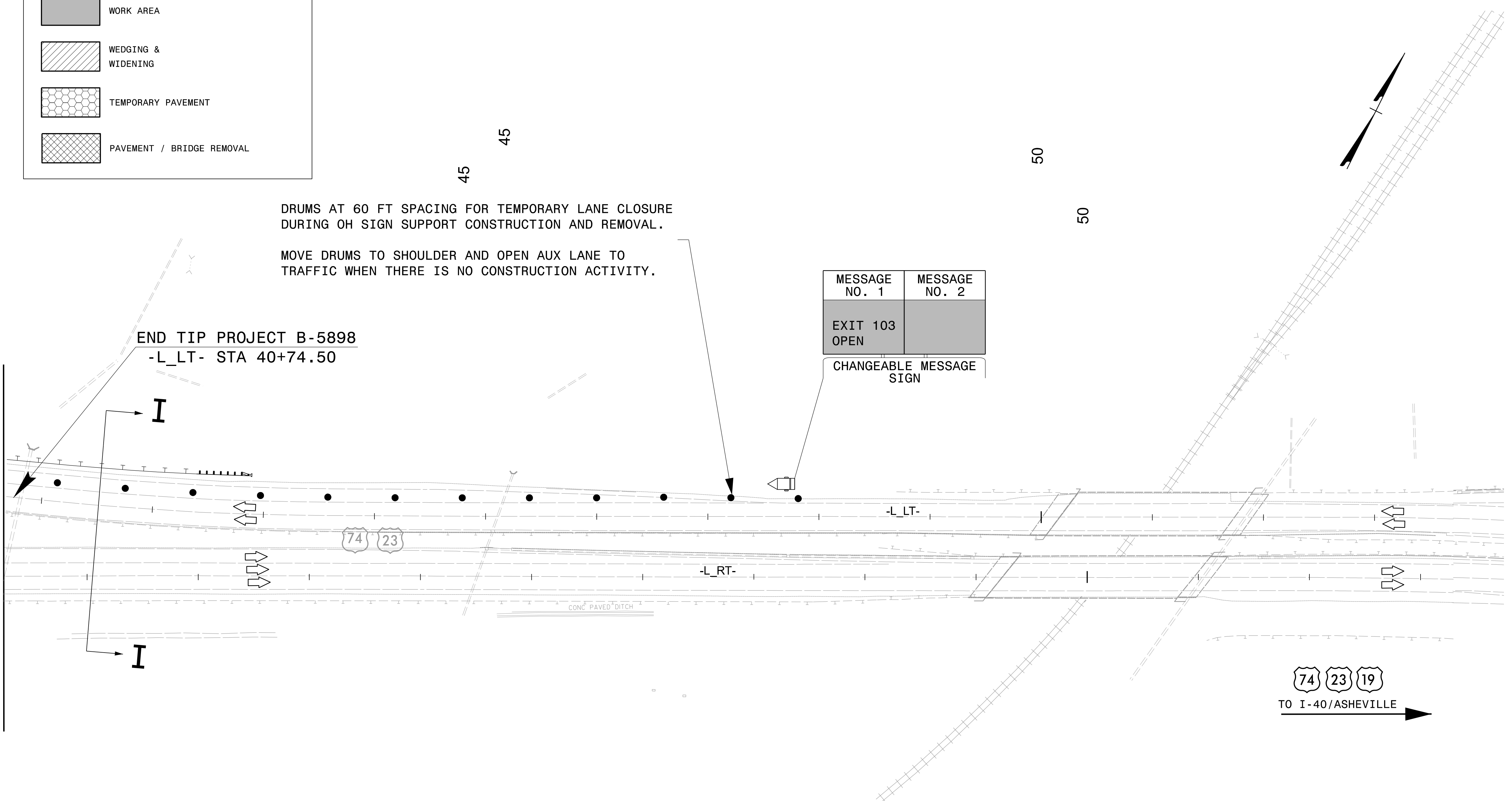
DRUMS AT 60 FT SPACING FOR TEMPORARY LANE CLOSURE DURING OH SIGN SUPPORT CONSTRUCTION AND REMOVAL.

MOVE DRUMS TO SHOULDER AND OPEN AUX LANE TO TRAFFIC WHEN THERE IS NO CONSTRUCTION ACTIVITY.

MESSAGE NO. 1	MESSAGE NO. 2
EXIT 103 OPEN	
CHANGEABLE MESSAGE SIGN	

MATCHLINE -L_RT STA 40+25
SEE SHEET TMP-06

END TIP PROJECT B-5898
-L_LT- STA 40+74.50



APPROVED: *Jeffrey A. Kooft*
DATE: 10/17/2023

DocuSigned by:
Jeffrey A. Kooft

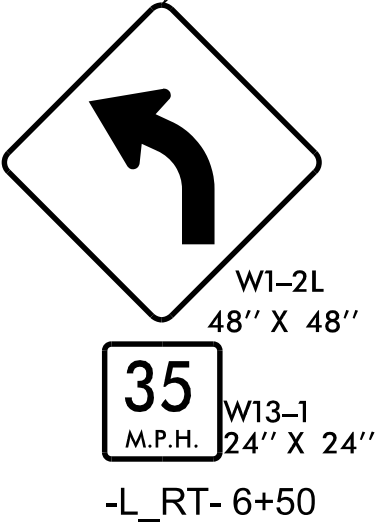
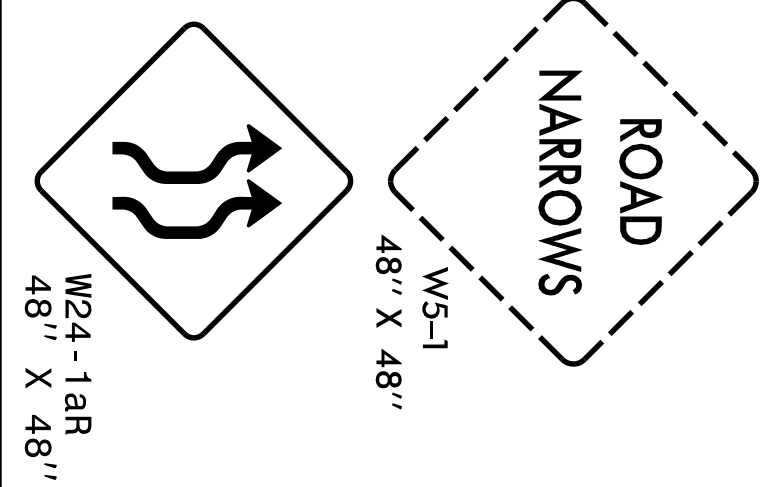
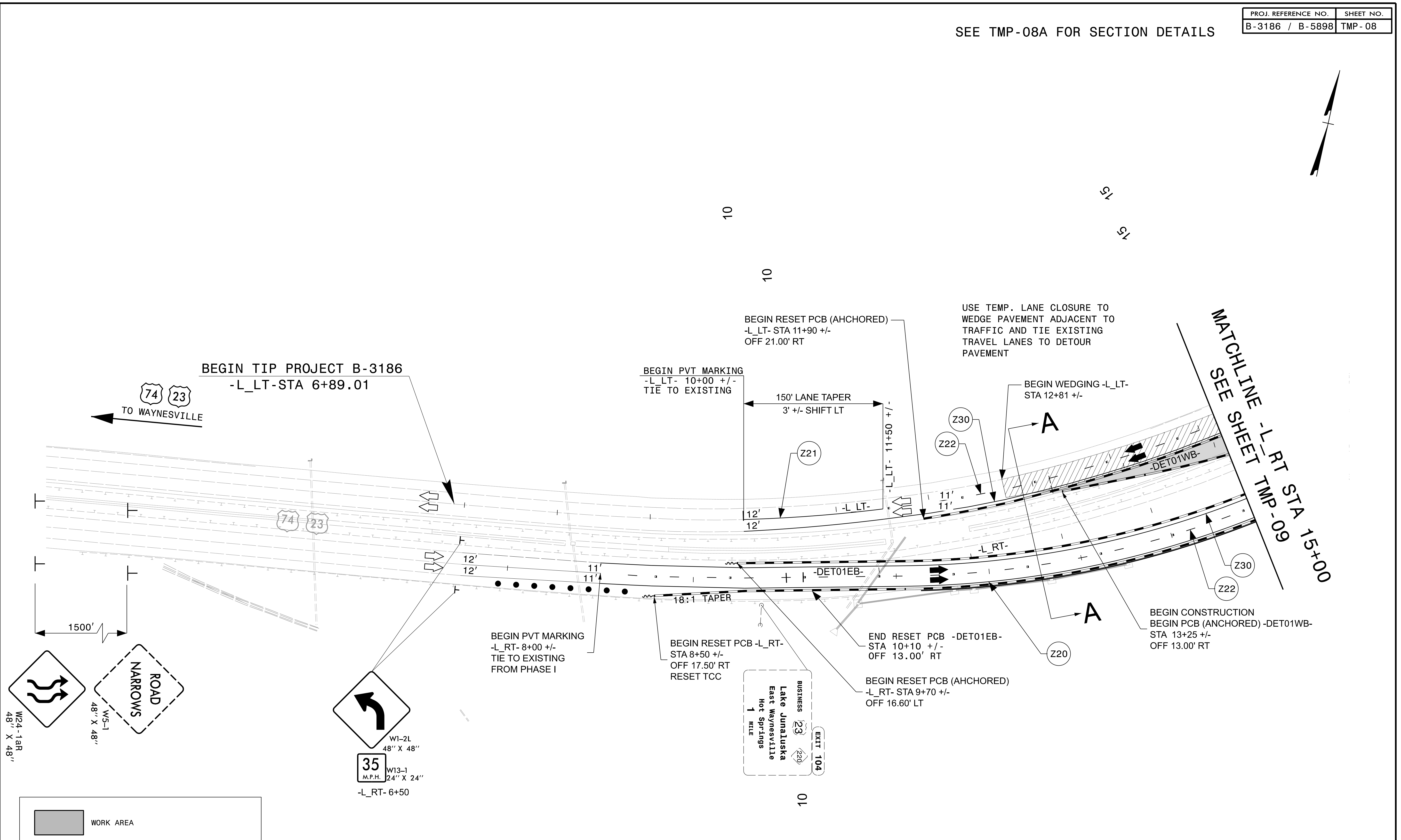
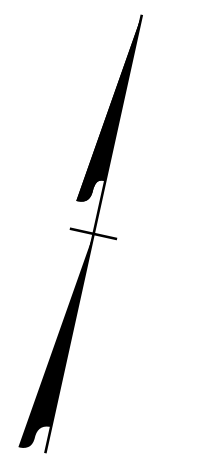
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PHASE I DETAILS
US 74/US 23

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B3186-B5898_TC_TMP_07_Phase I.dgn
Jeffrey.thurston

SEE TMP-08A FOR SECTION DETAILS

PROJ. REFERENCE NO.	SHEET NO.
B-3186 / B-5898	TMP-08



	WORK AREA
	WEDGING & WIDENING
	TEMPORARY PAVEMENT
	PAVEMENT / BRIDGE REMOVAL

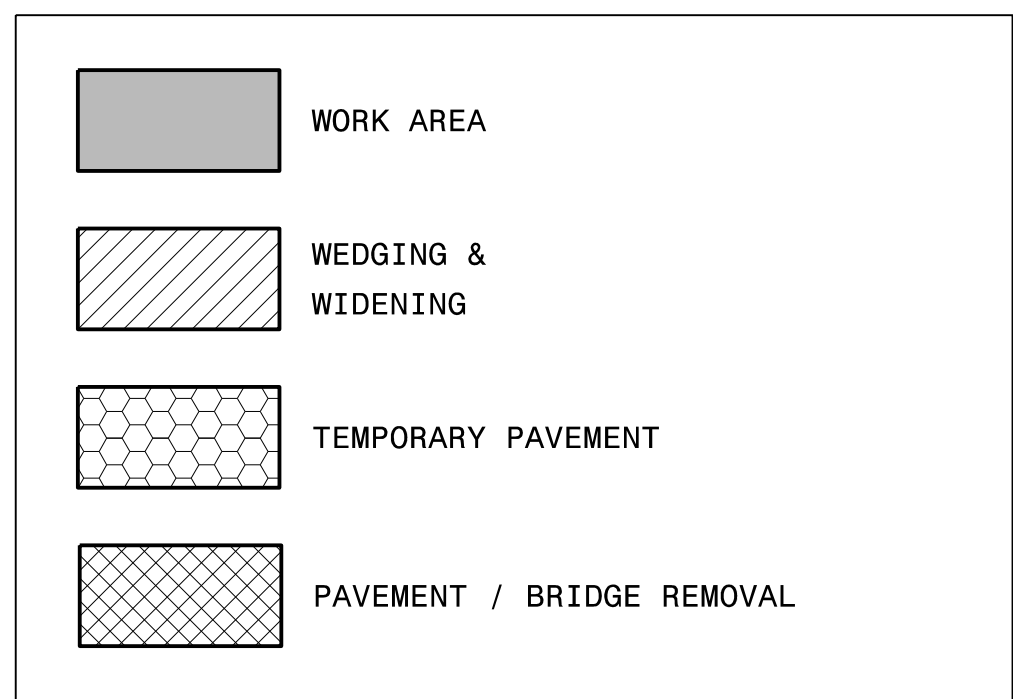
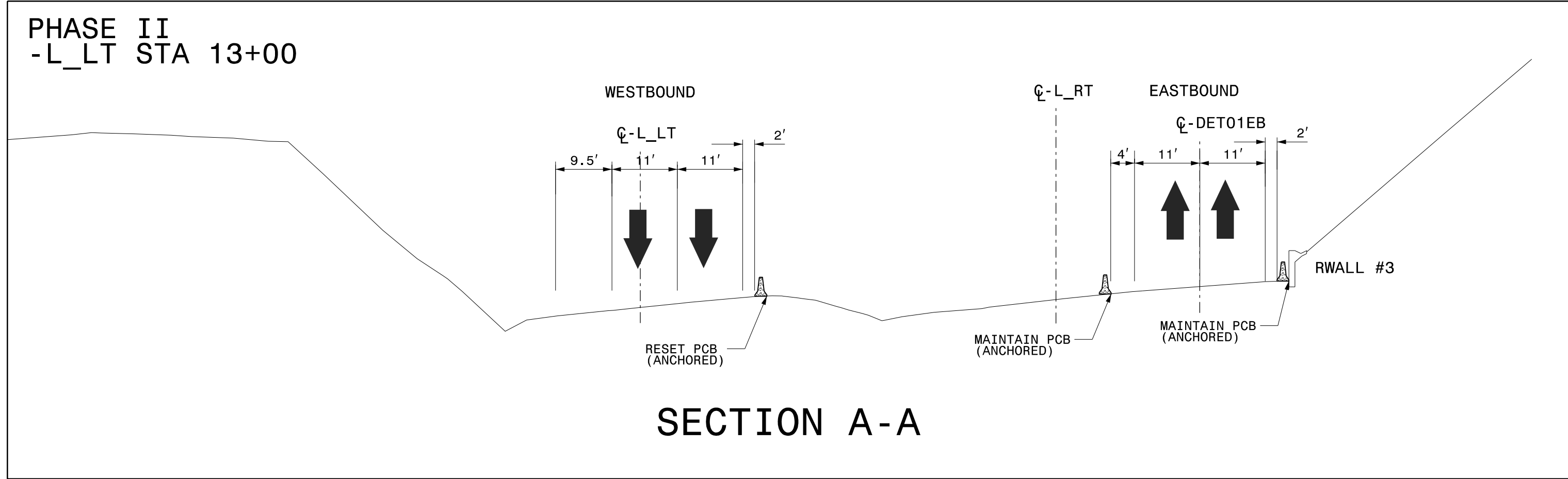
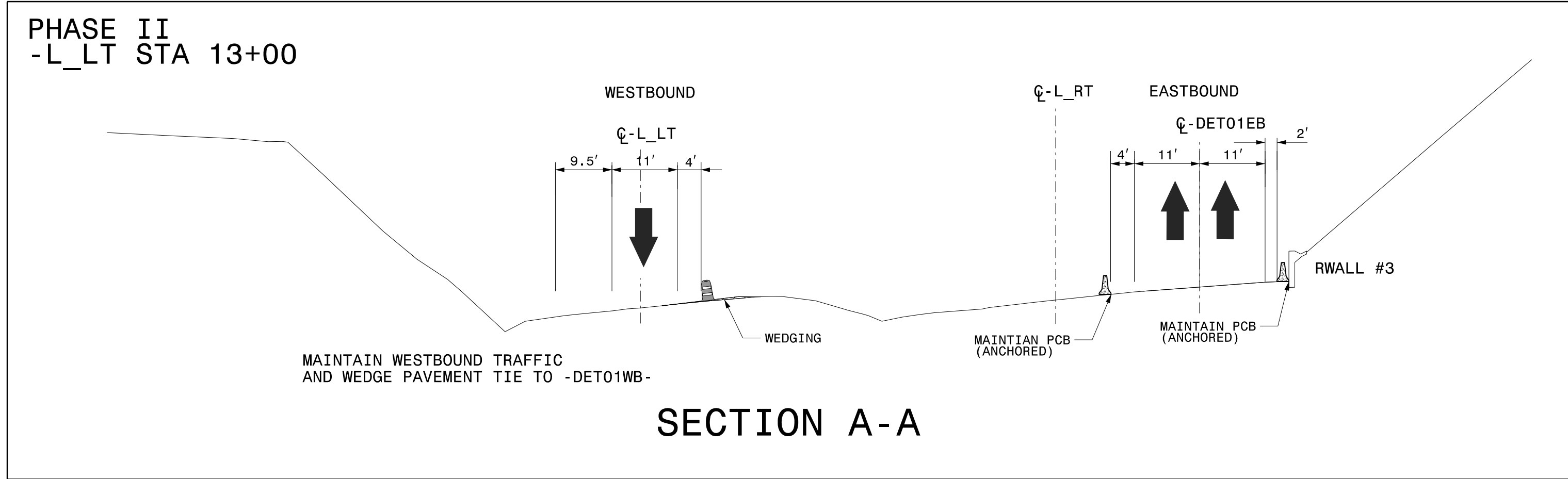
9/14/2023
 B3186-B5898-TC_TMP-08_Phase II.dgn
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**PHASE II DETAILS
 US 74/US 23**



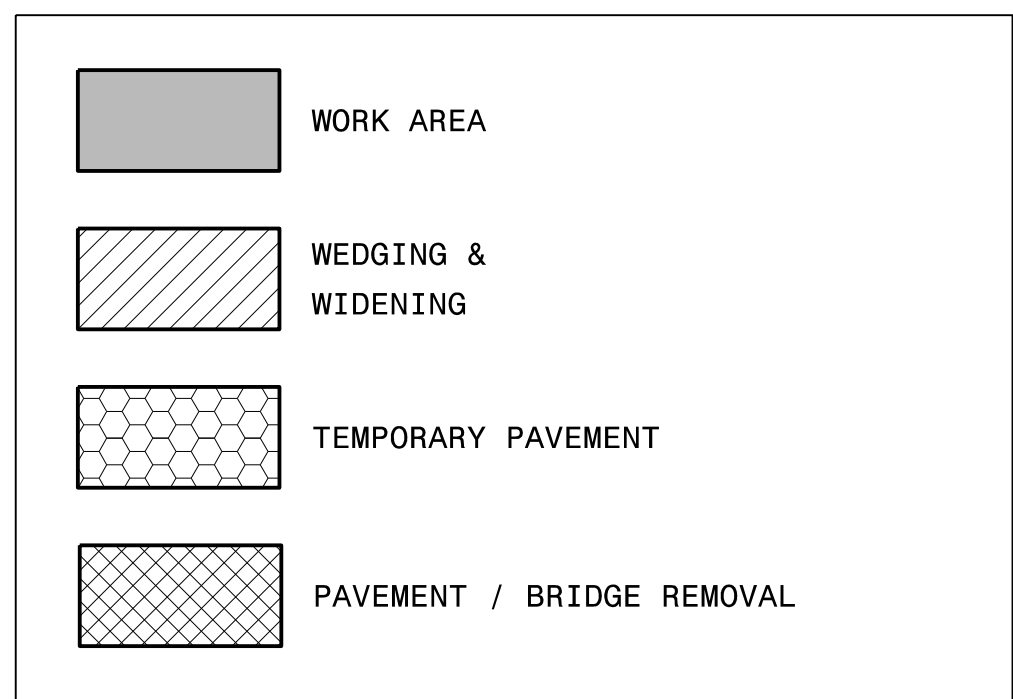
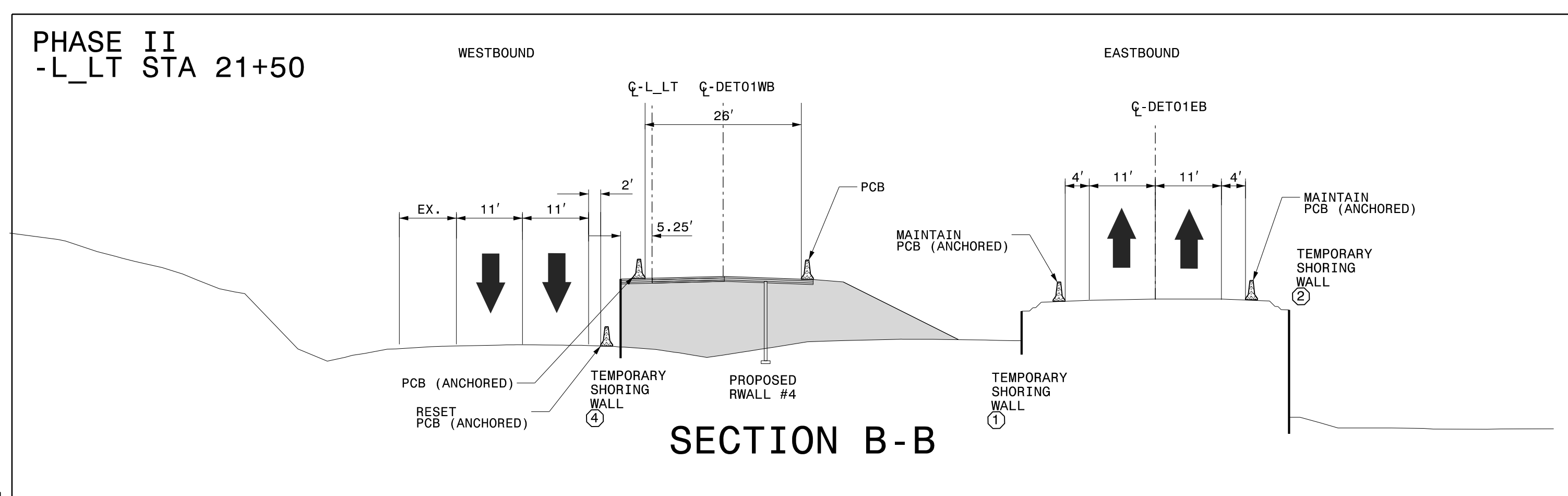
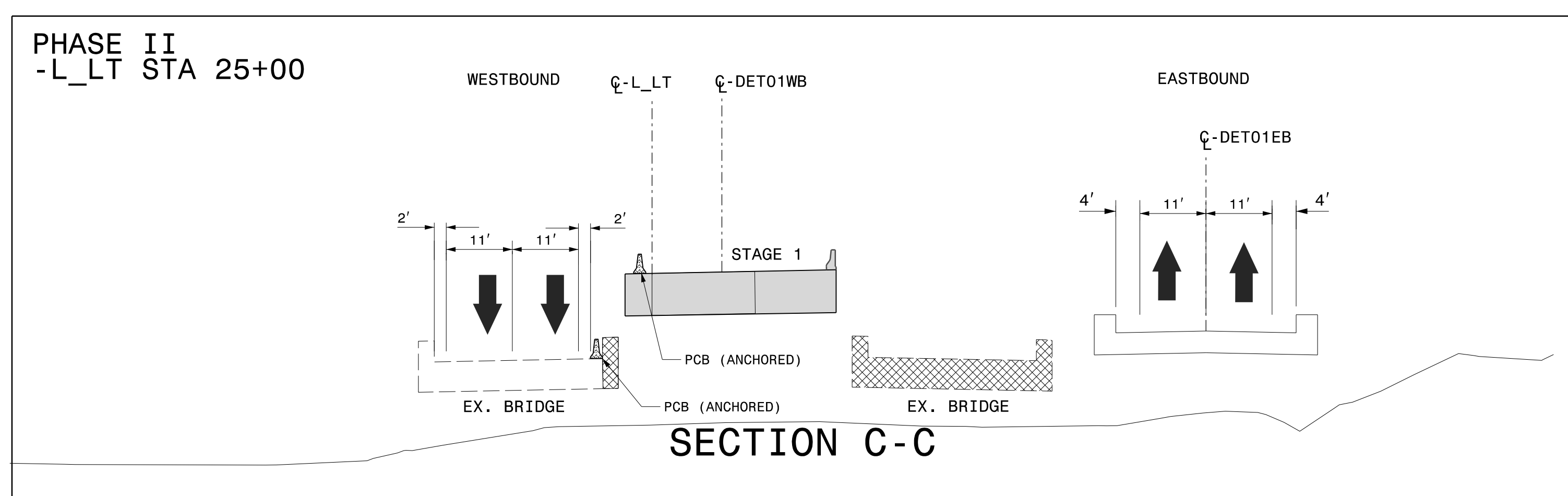
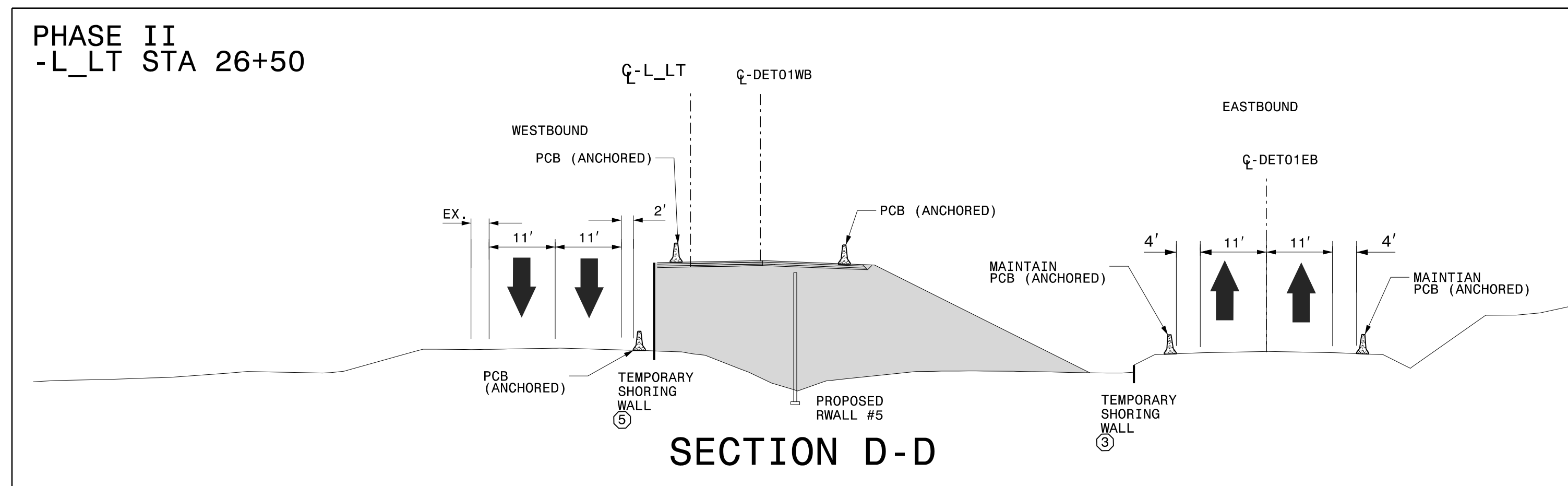
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B3186-B5898-TC_TMP-08A_Phase IISections.dgn
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PHASE II DETAILS
US 74/US 23



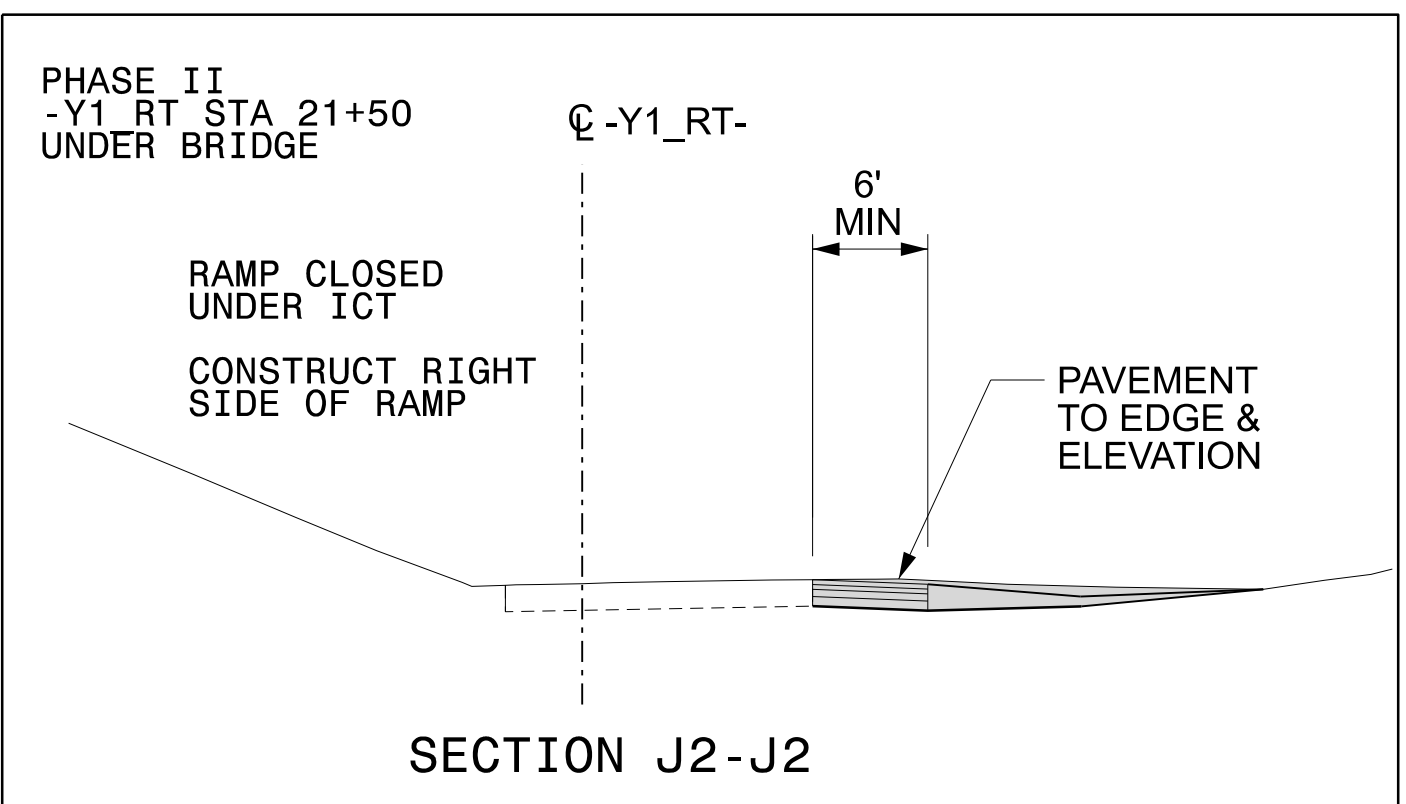
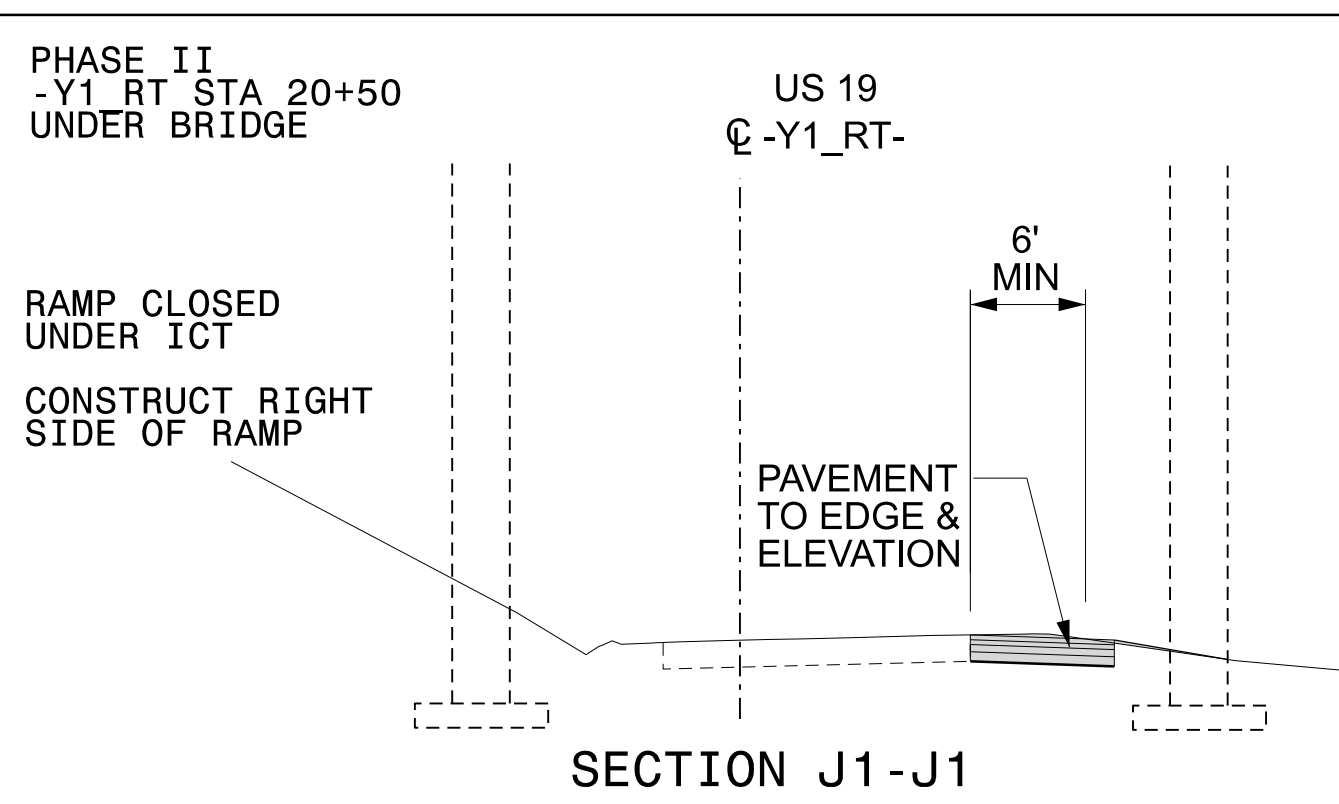
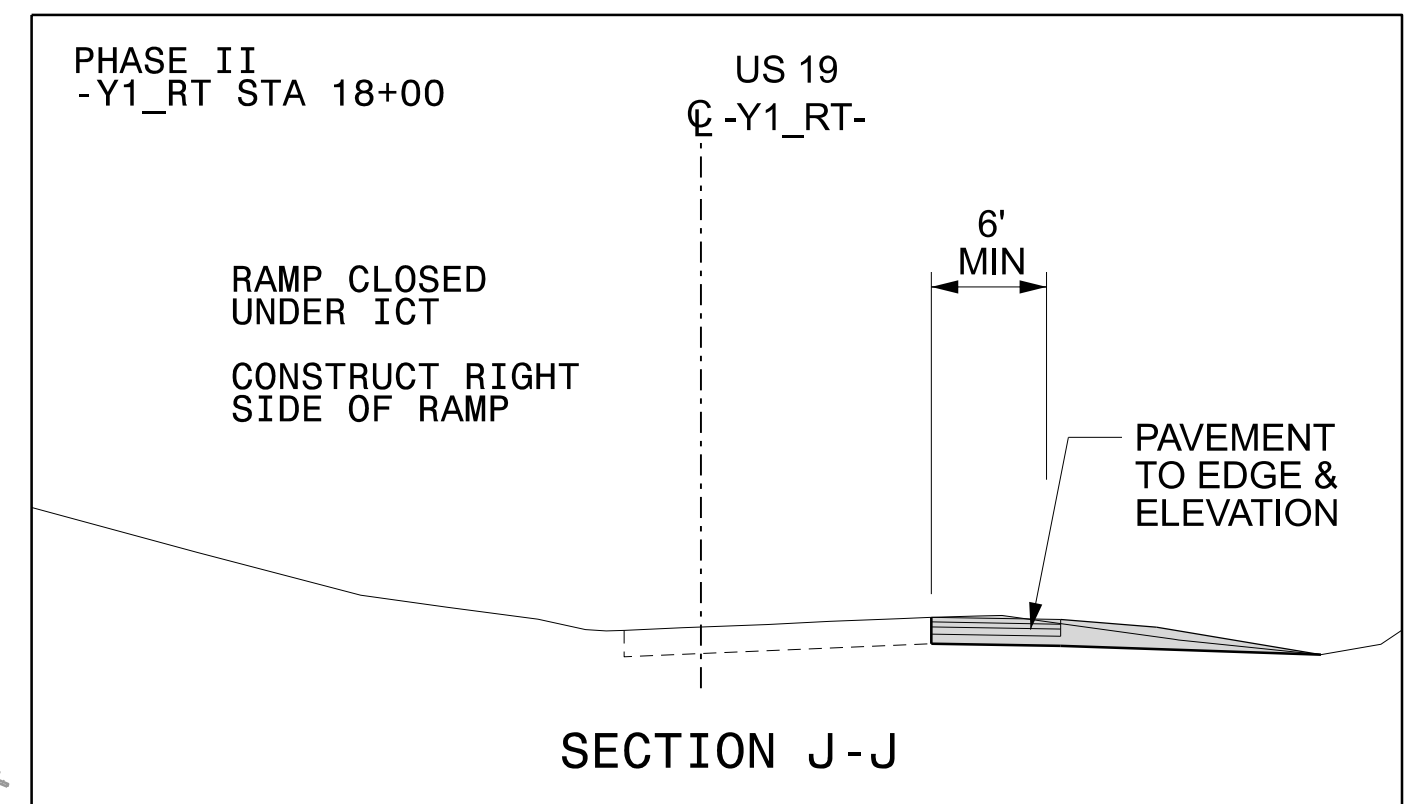
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 DATE: 10/17/2023

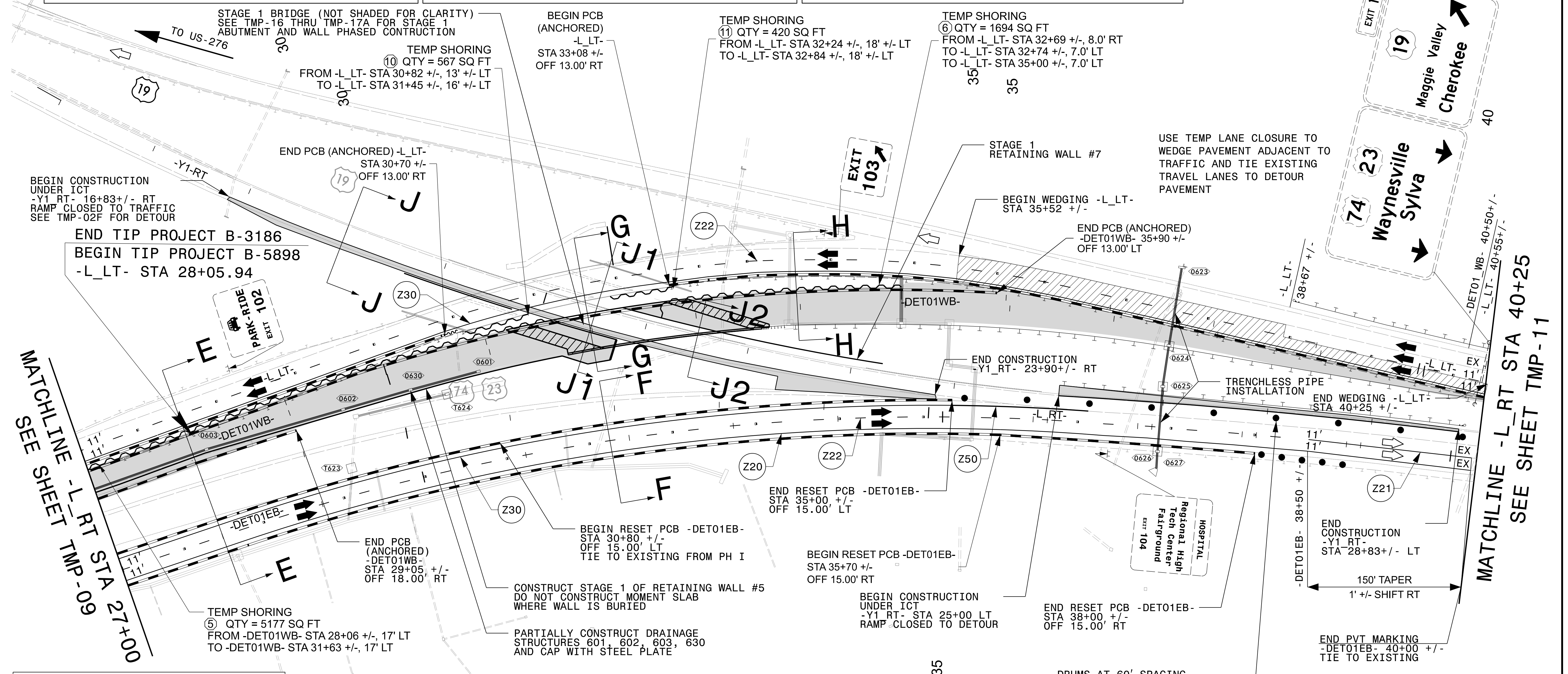
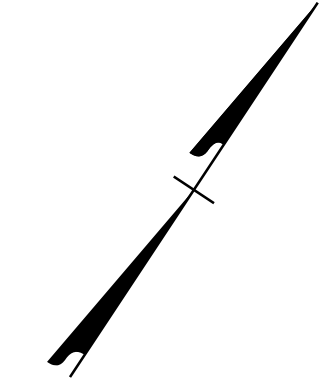
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PHASE II DETAILS
US 74/US 23



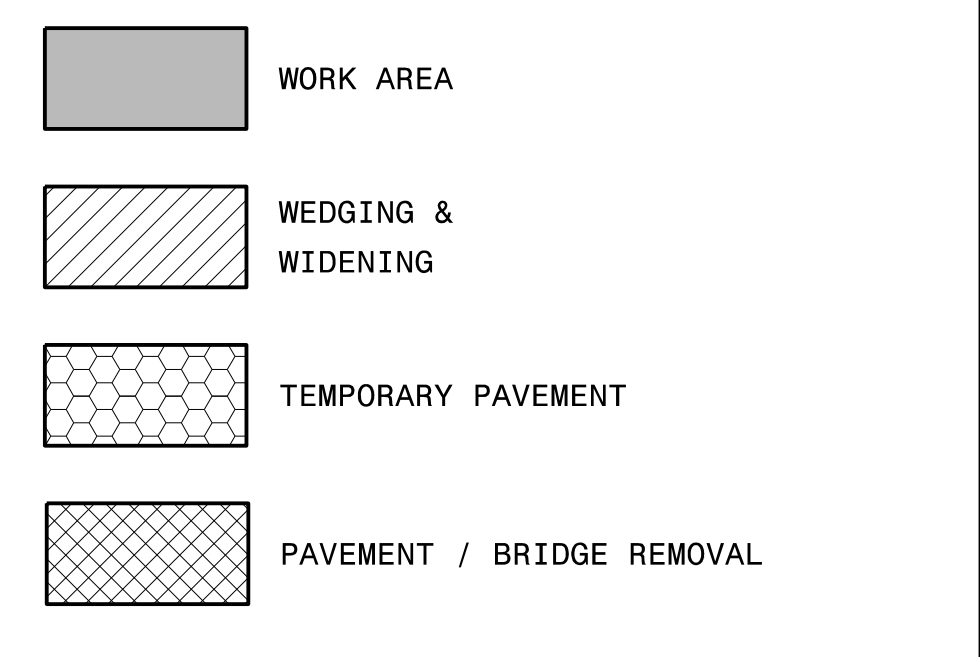
SEE TMP-10A FOR ADDITIONAL SECTION DETAILS



END TIP PROJECT B-3186
BEGIN TIP PROJECT B-5898
-L_LT- STA 28+05.94

MATCHLINE -L_RT STA 27+00
SEE SHEET TMP-06

MATCHLINE -L_RT STA 40+25
SEE SHEET TMP-11



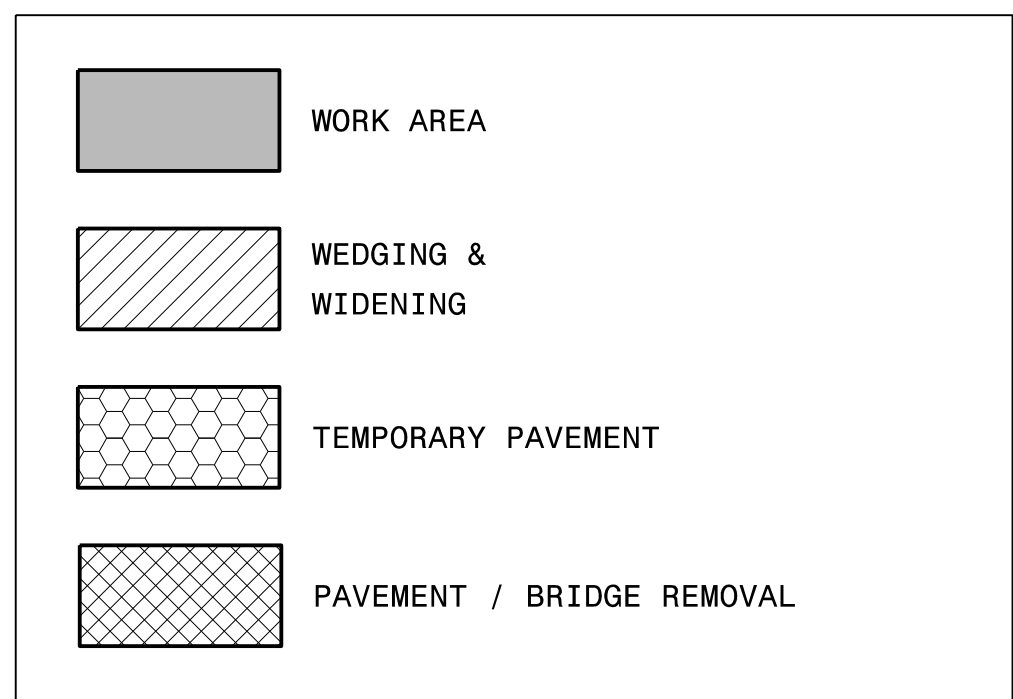
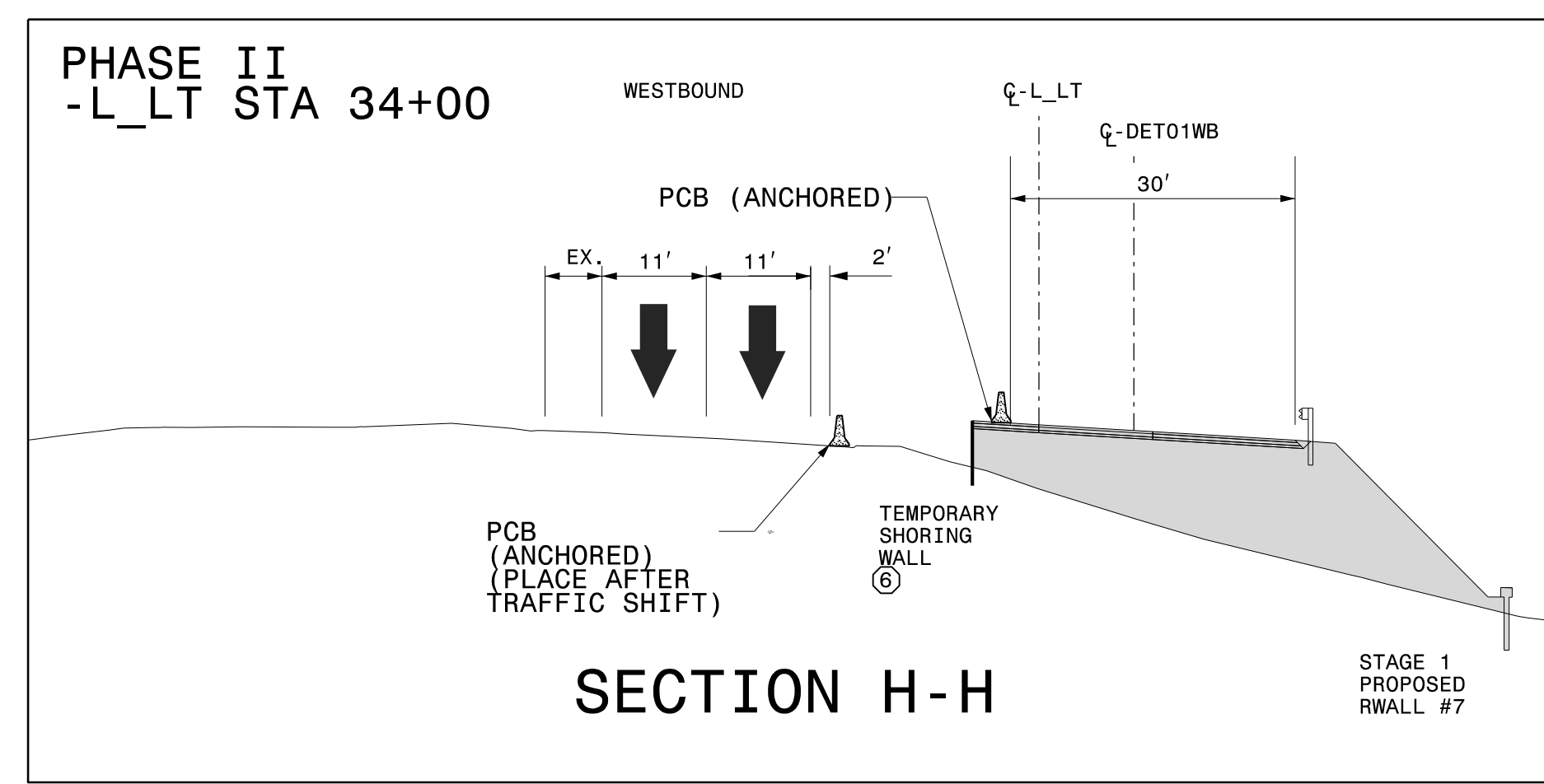
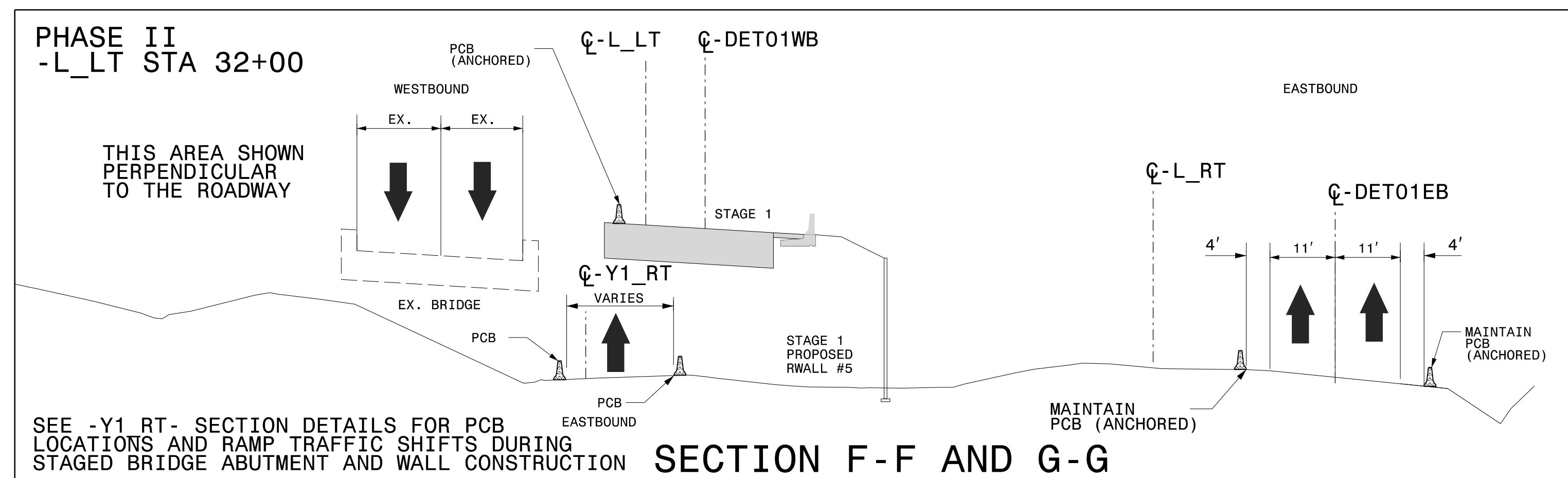
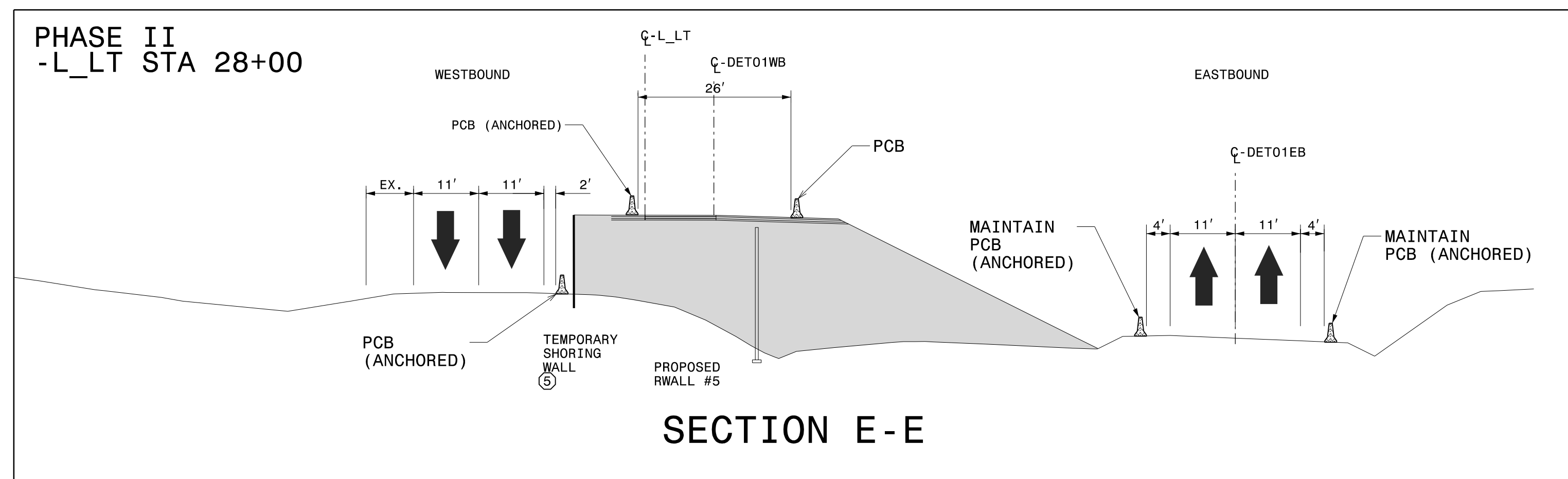
SEE TMP-16 THRU TMP-17A FOR PHASE II-A & B STAGE 1 ABUTMENT AND WALL PHASED DETAILS

APPROVED: *Jeffrey A. Koontz*
DATE: 10/17/2023
Professional Engineer Seal for Jeffrey A. Koontz, No. 018122, State of North Carolina.

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DEPARTMENT OF TRANSPORTATION & WORK
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PHASE II DETAILS
US 74/US 23

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B3186-B5898_TC_TMP-10_Phase II.dgn
Jeff.Koontz



APPROVED: *Jeffrey A. Kooft*

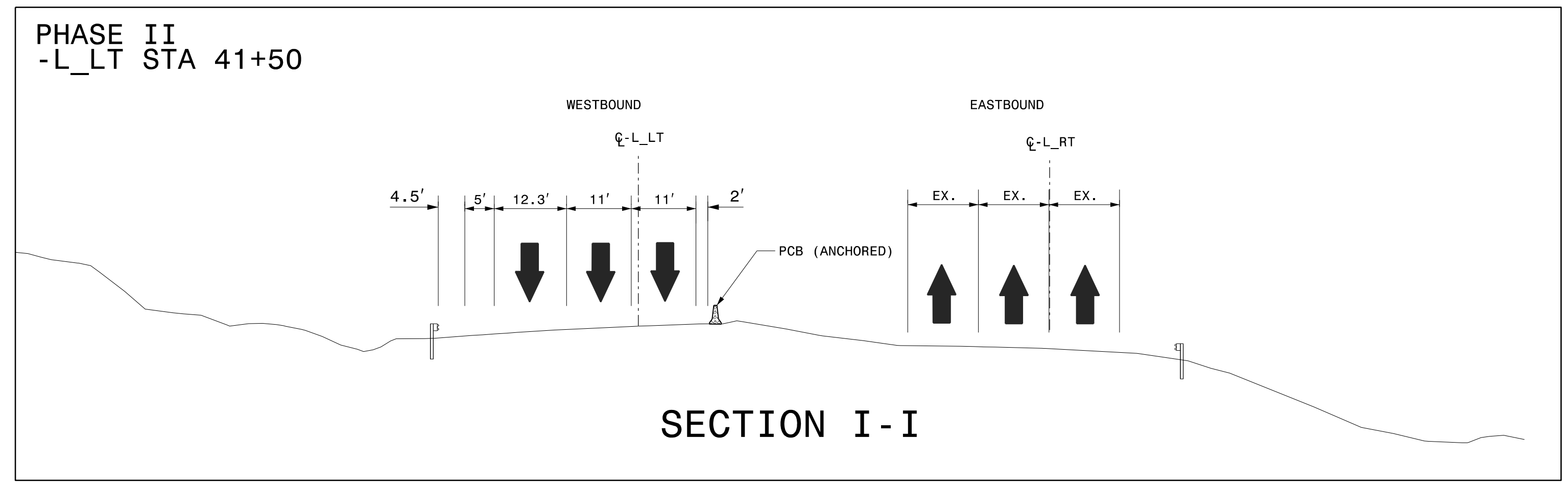
DATE: 10/17/2023

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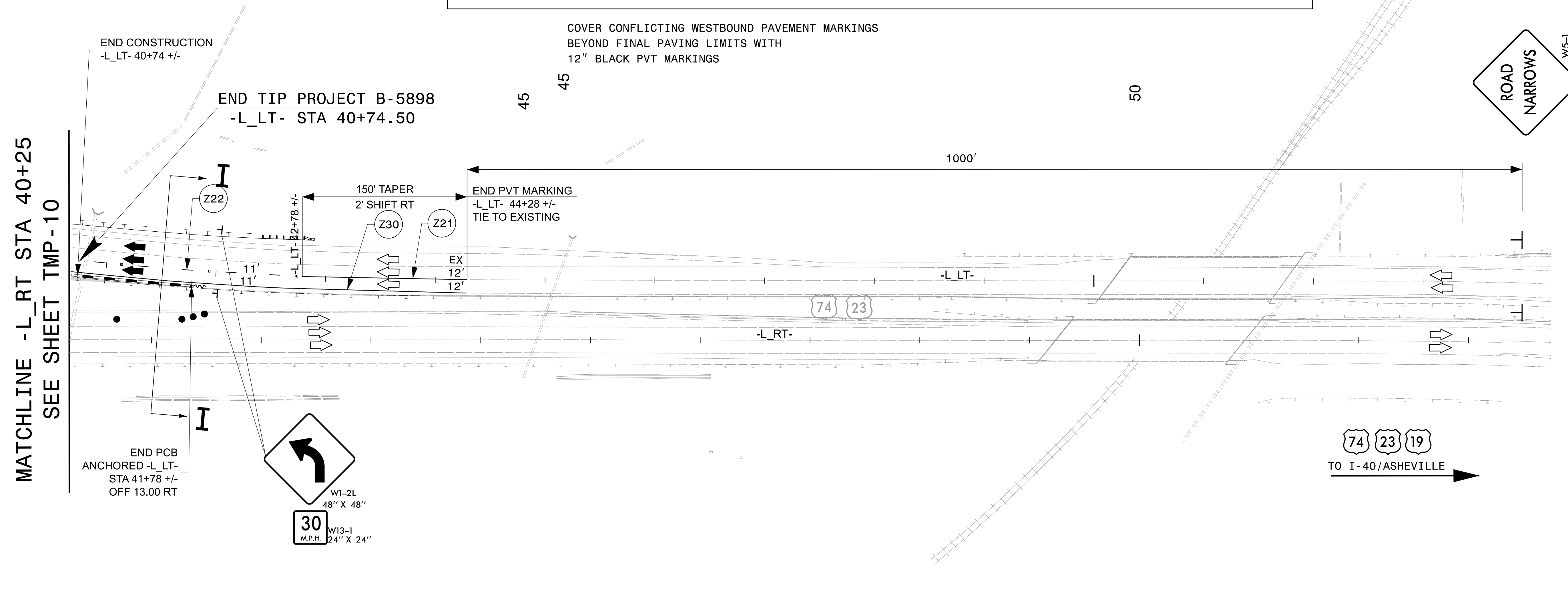
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**PHASE II DETAILS
US 74/US 23**

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COVER CONFLICTING WESTBOUND PAVEMENT MARKINGS
BEYOND FINAL PAVING LIMITS WITH
12" BLACK PVT MARKINGS



	WORK AREA
	WEDGING & WIDENING
	TEMPORARY PAVEMENT
	PAVEMENT / BRIDGE REMOVAL

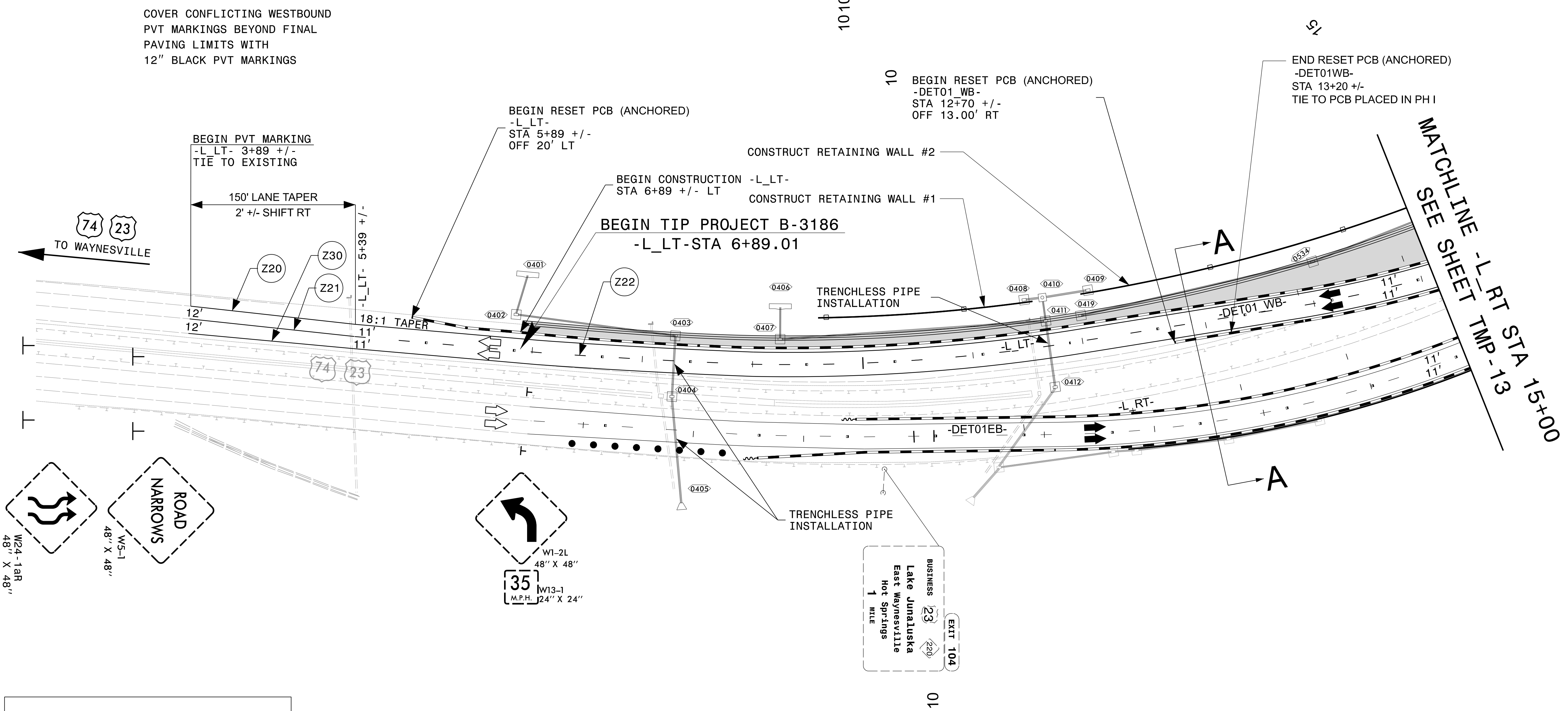
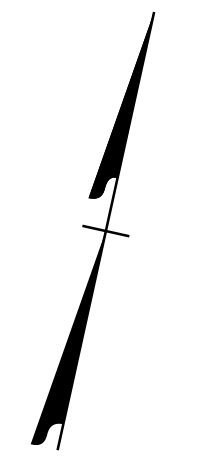
APPROVED: *Jeffrey A. Kooft*
DATE: 10/17/2023

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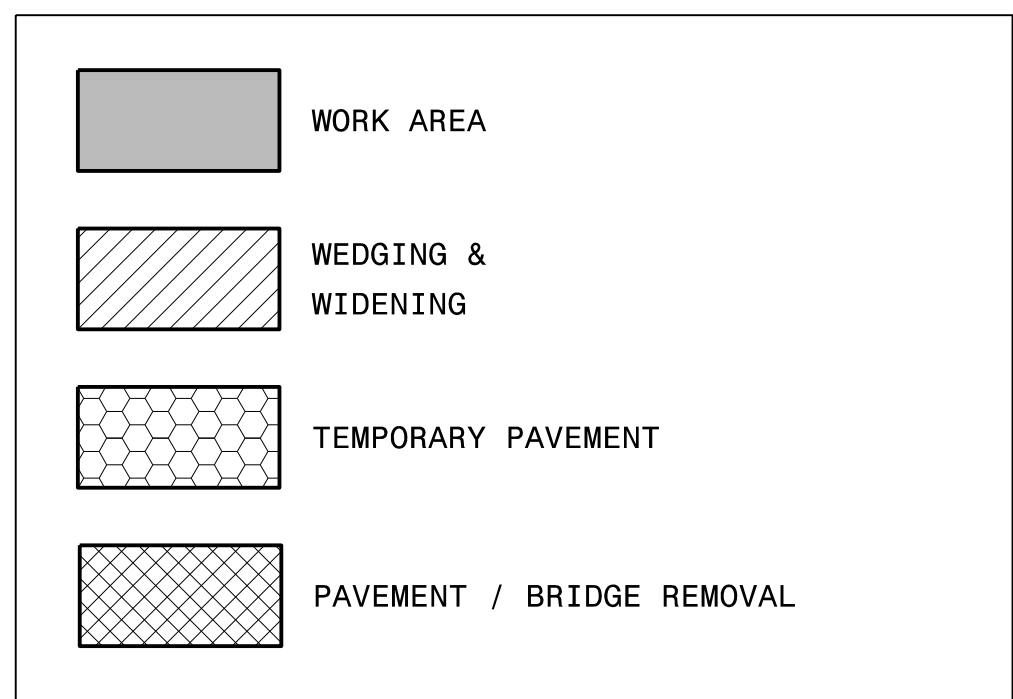
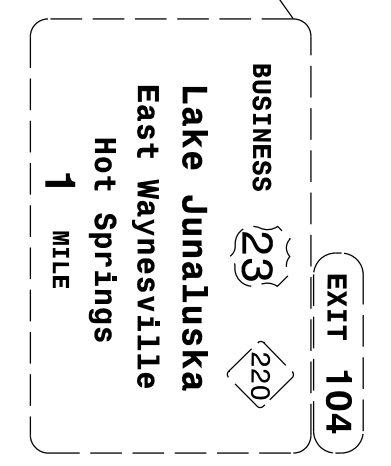
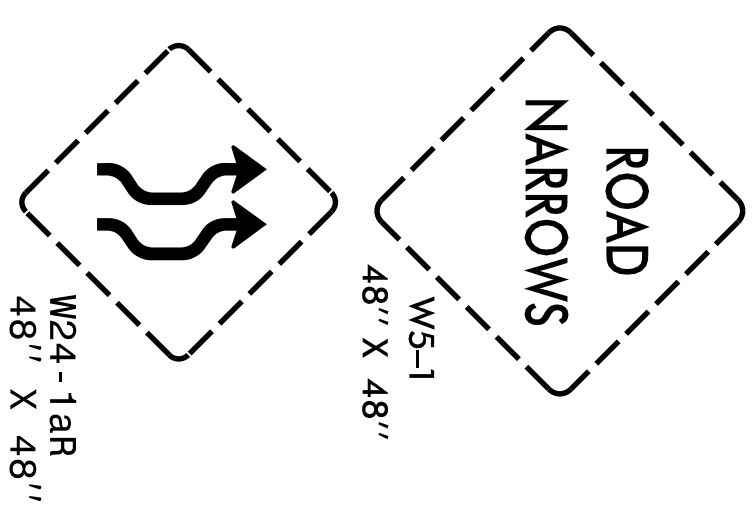
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PHASE II DETAILS
US 74/US 23

9/6/2023
 B3186-B5898_TC_TMP_II_Phase_II.dgn
 jeffrey.thurston



COVER CONFLICTING WESTBOUND
PVT MARKINGS BEYOND FINAL
PAVING LIMITS WITH
12" BLACK PVT MARKINGS



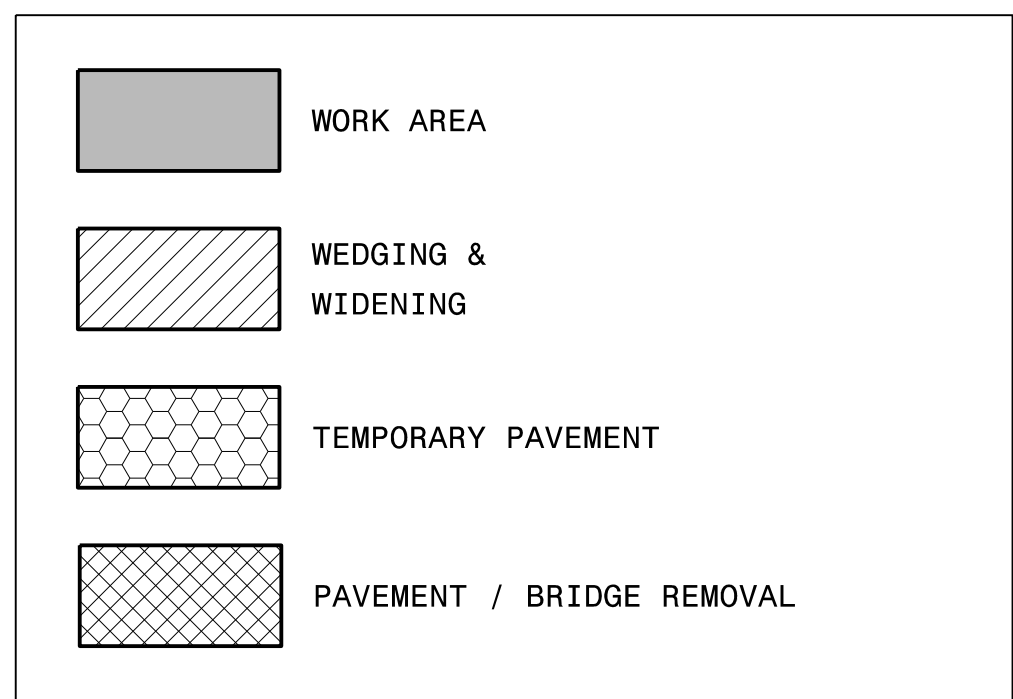
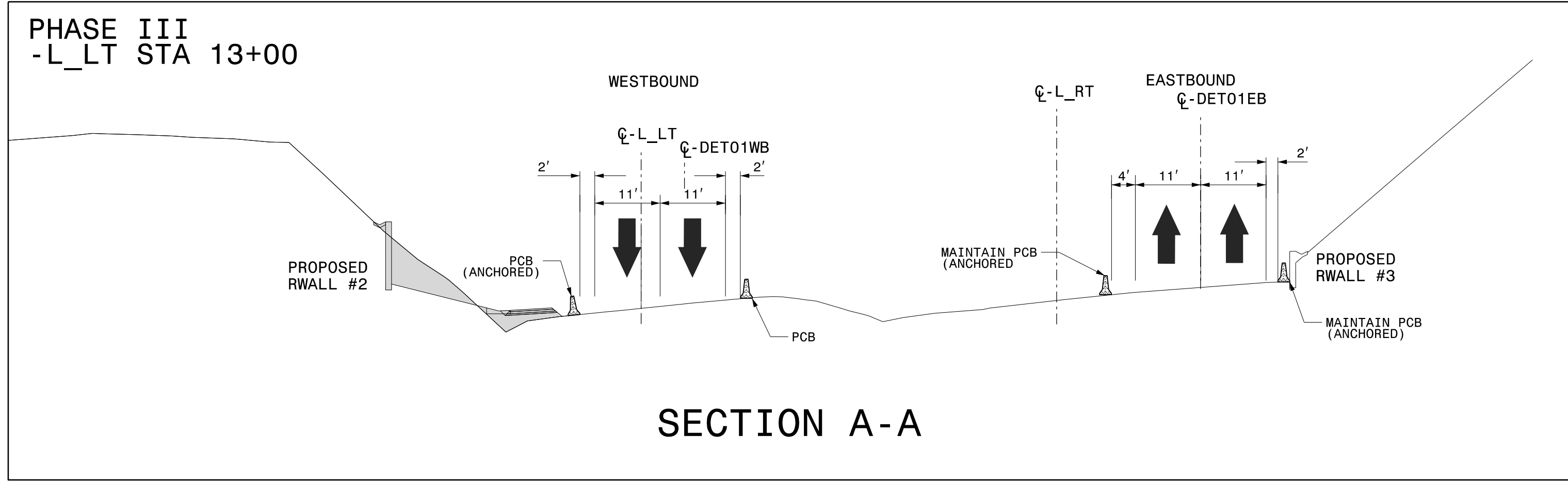
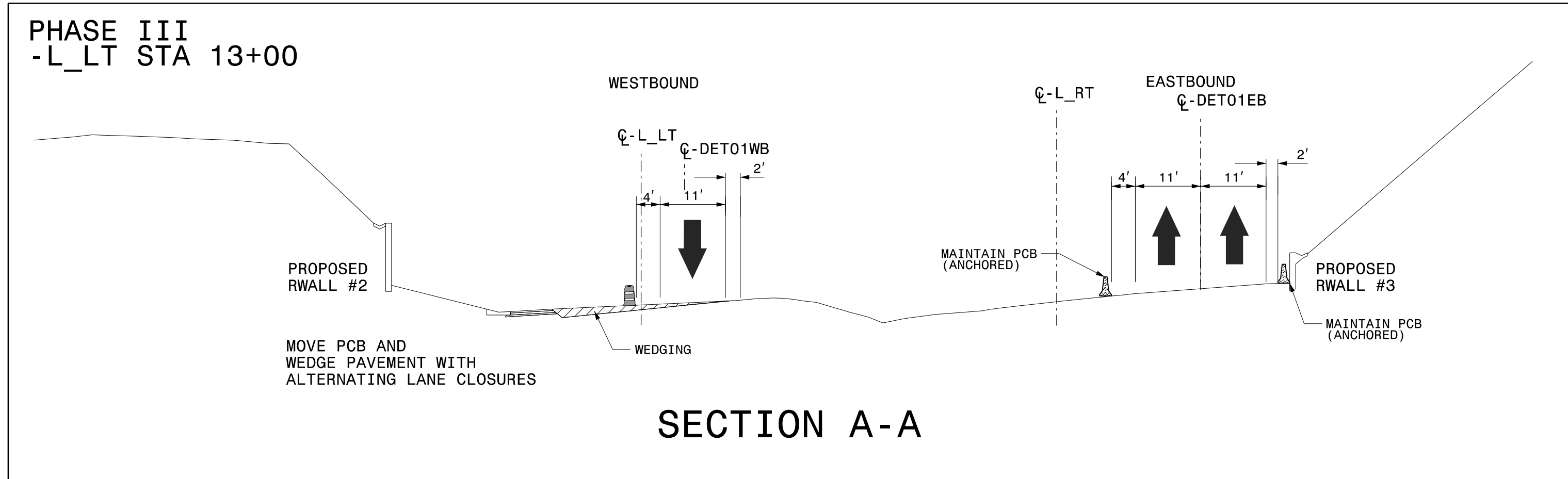
9/6/2023
 B3186-B5898.TC_TMP_12_Phase III.dgn
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PHASE III DETAILS
 US 74/US 23



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 B3186-B5898-TC-TMP-12A-Phase III Section.dgn
 jeffrey.thurston

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DocuSigned by: Jeffrey A. Kooty

DATE: 10/17/2023

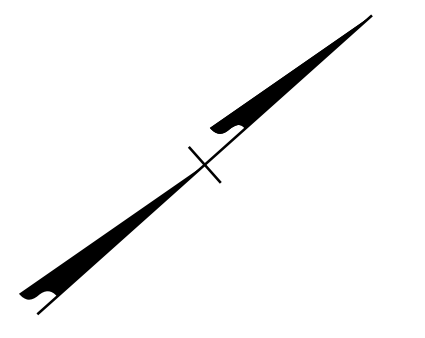
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PHASE III DETAILS
US 74/US 23

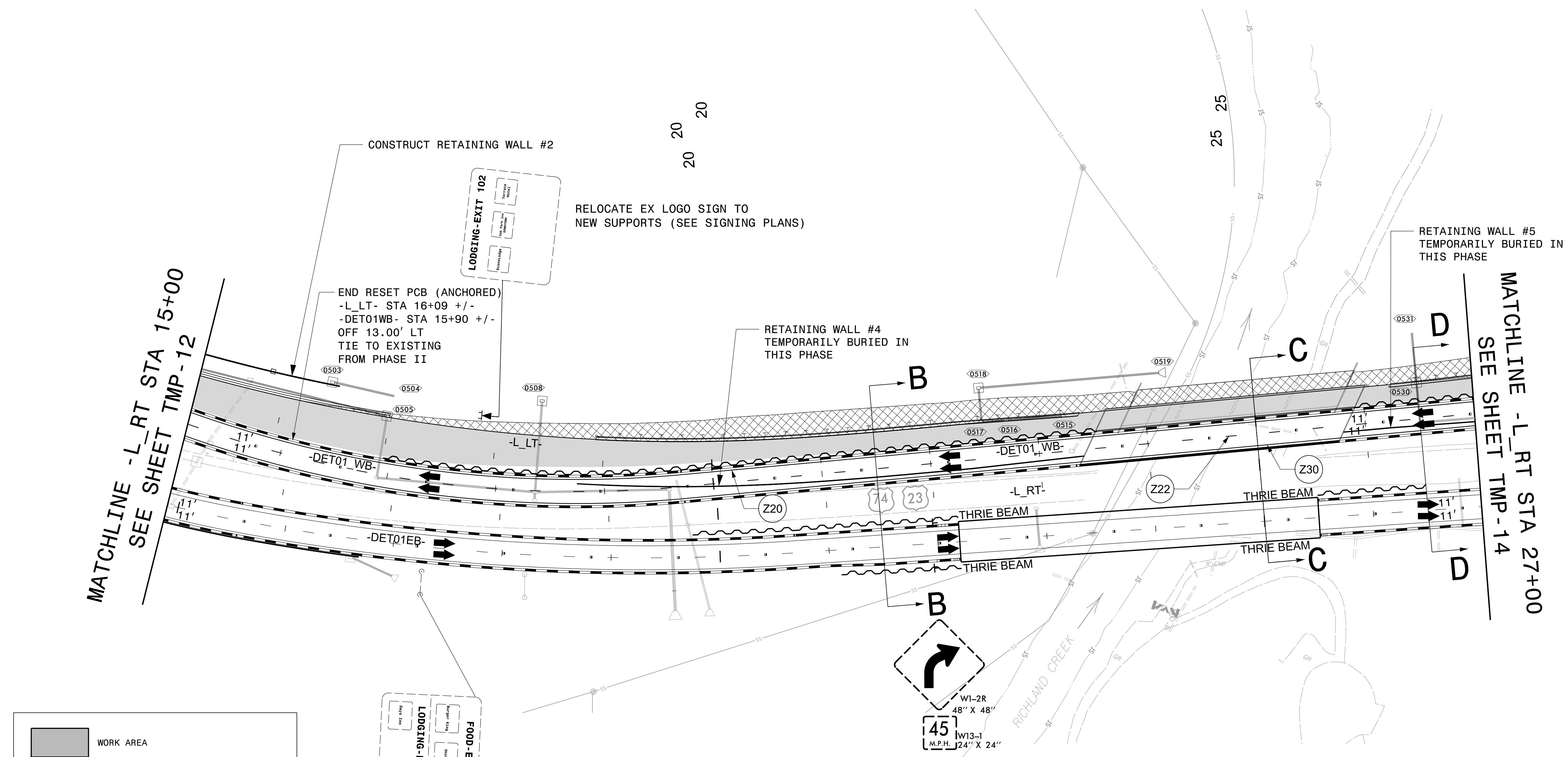
SEE TMP-13A FOR SECTION DETAILS

PROJ. REFERENCE NO.	SHEET NO.
B-3186 / B-5898	TMP-13

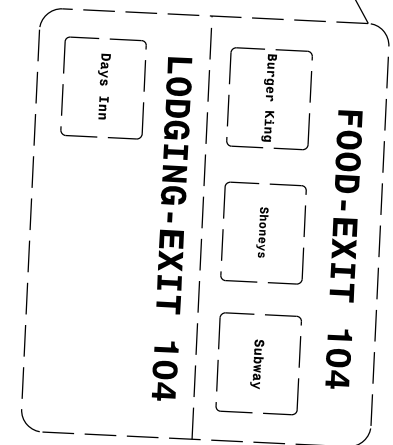


MATCHLINE -L_RT STA 15+00
SEE SHEET TMP-12

MATCHLINE -L_RT STA 27+00
SEE SHEET TMP-14



	WORK AREA
	WEDGING & WIDENING
	TEMPORARY PAVEMENT
	PAVEMENT / BRIDGE REMOVAL



APPROVED: *Jeffrey A. Koontz*

DATE: 10/17/2023

Seal: NORTH CAROLINA PROFESSIONAL SEAL 018122 ENGINEER JEFFREY A. KOONTZ

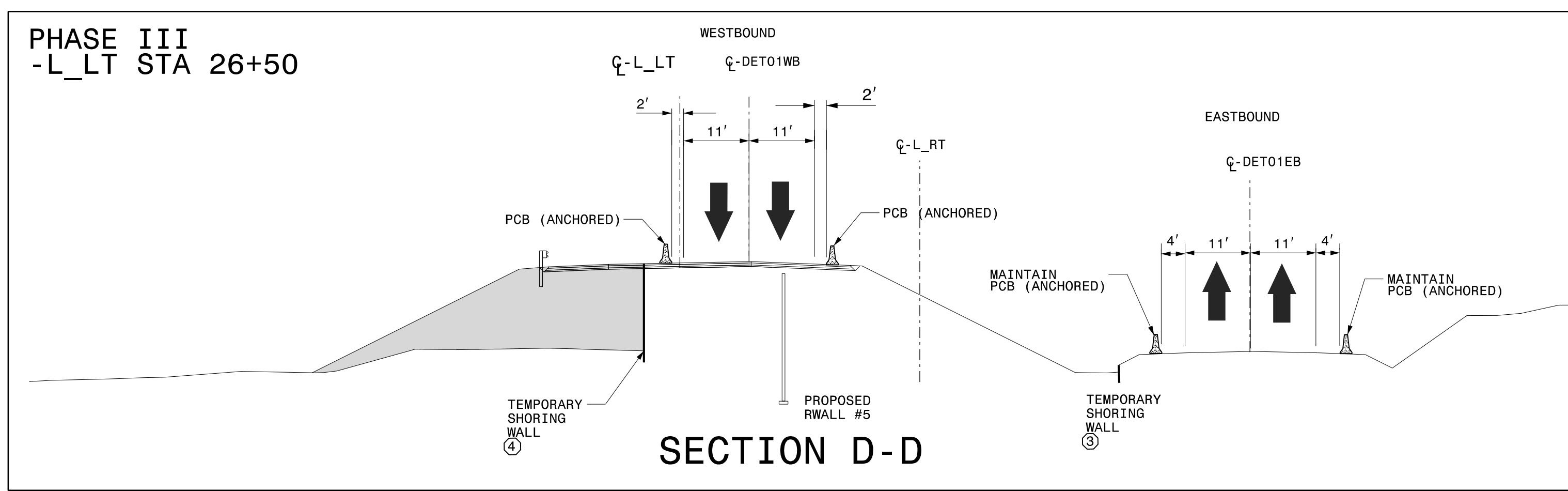
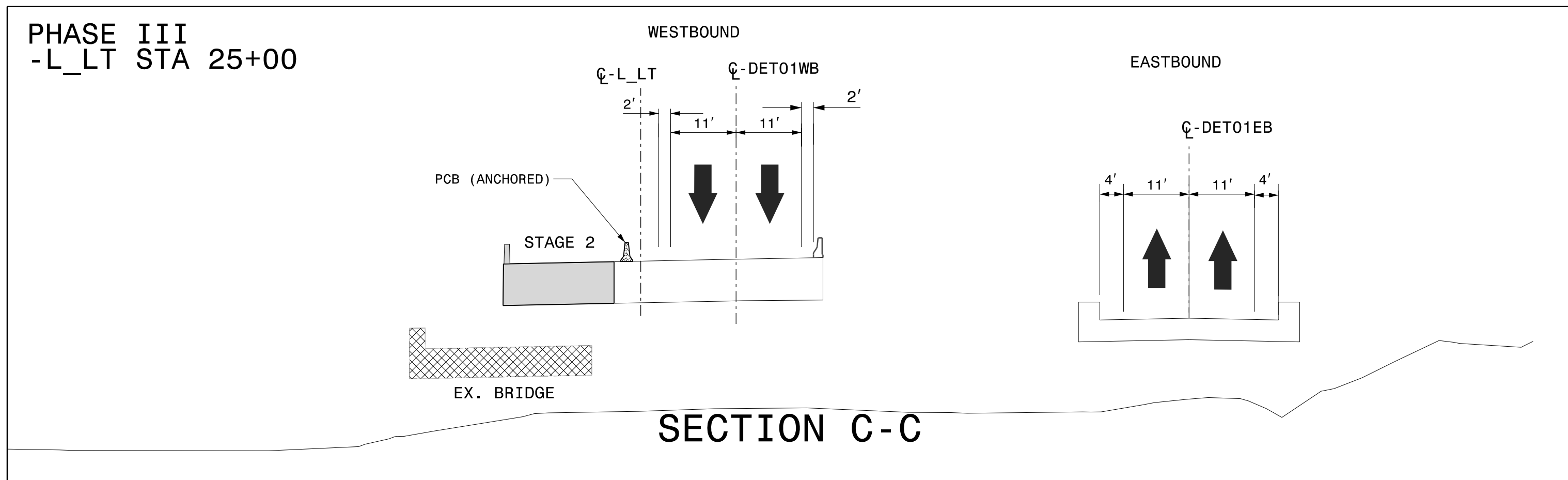
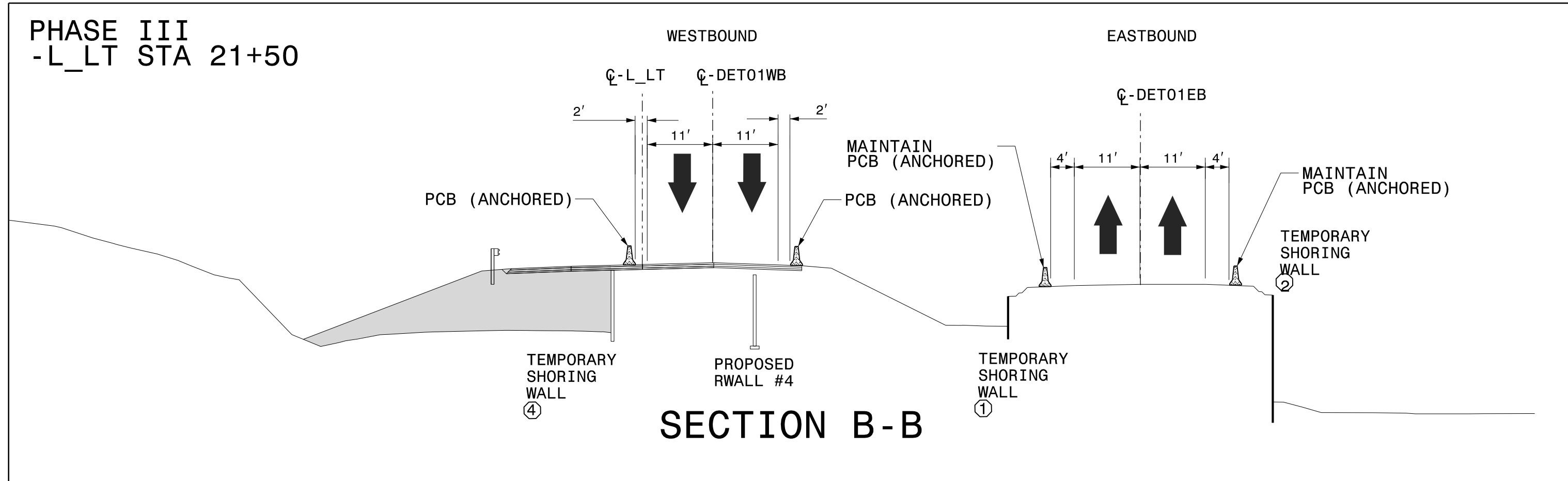
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PHASE III DETAILS
US 74/US 23

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B3186-B5898_TC_TMP_I3_Phase III.dgn
Jeff.Koontz



	WORK AREA
	WEDGING & WIDENING
	TEMPORARY PAVEMENT
	PAVEMENT / BRIDGE REMOVAL

9/14/2023
 B3186-B5898-TC-TMP-13A-Phase III Section.dgn
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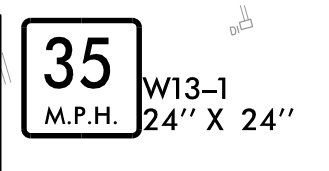
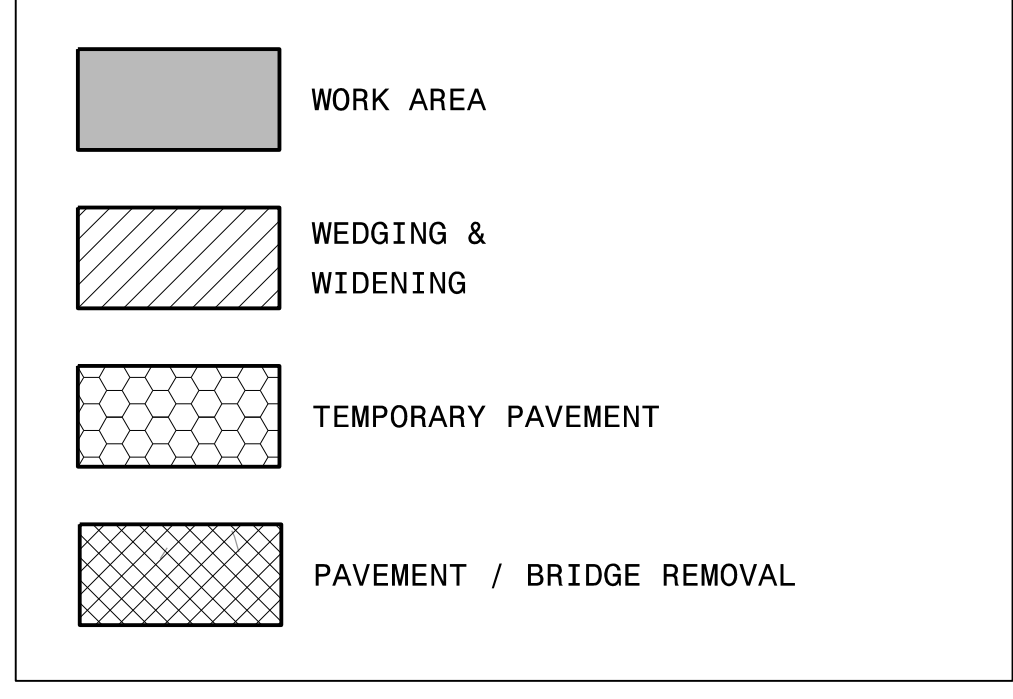
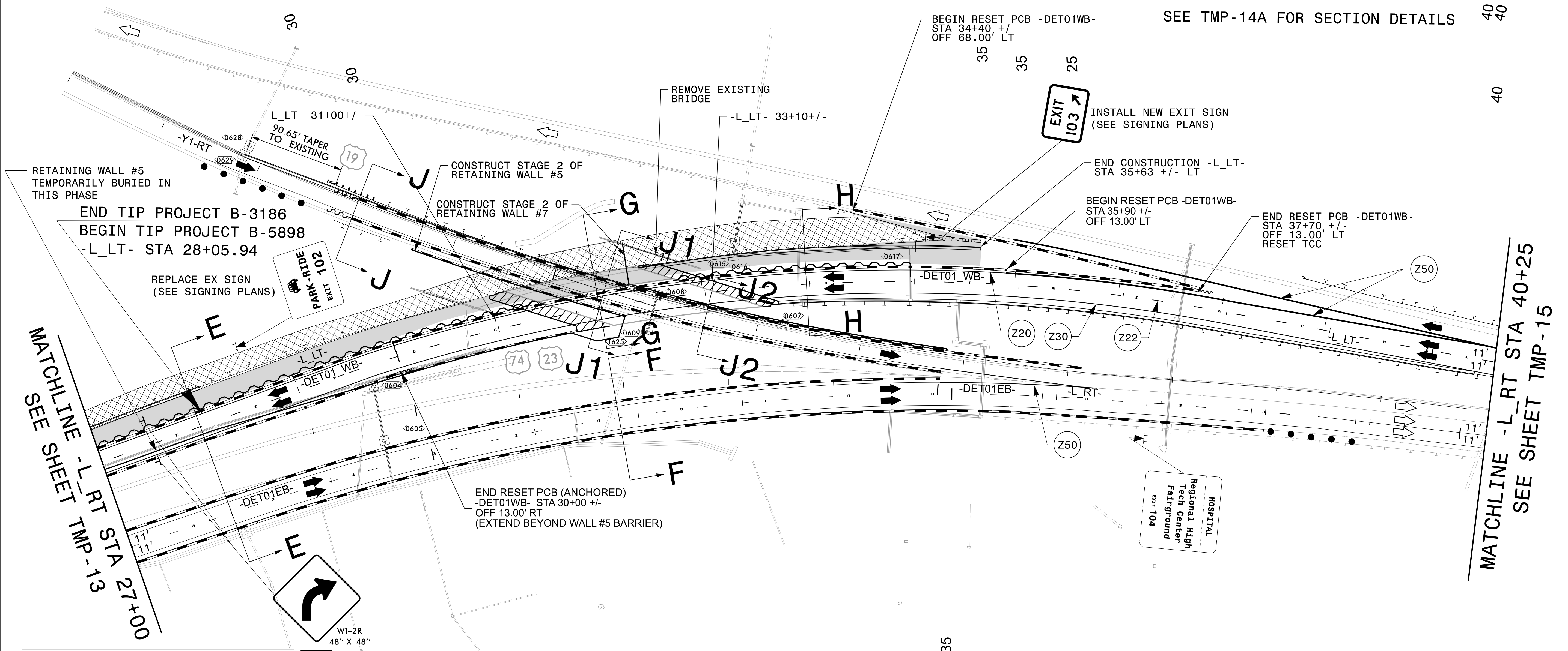
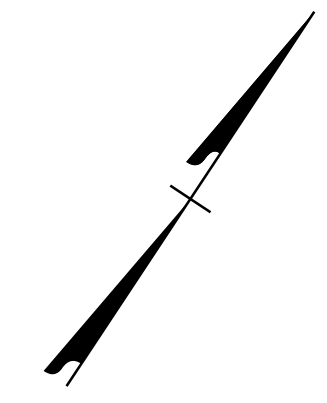
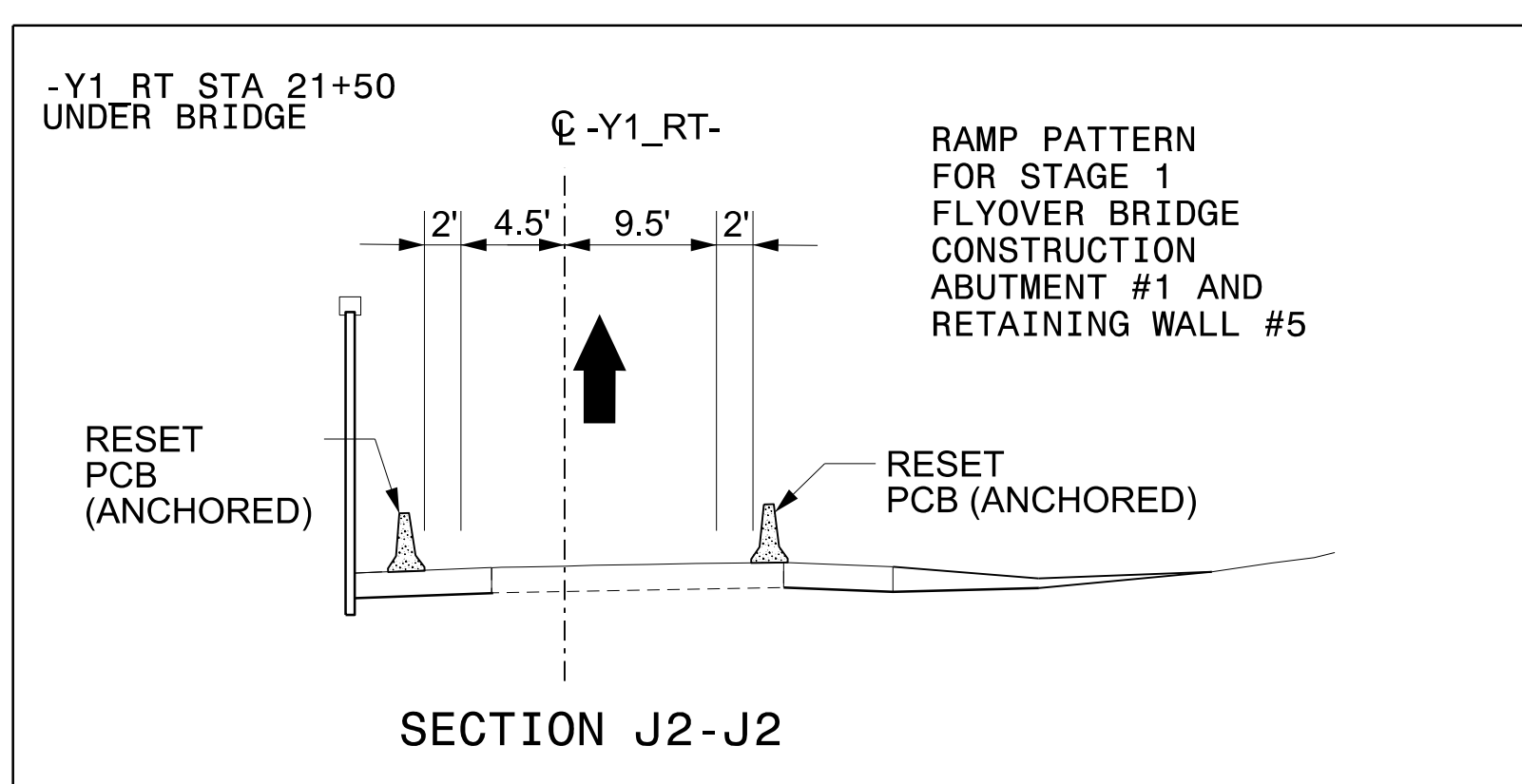
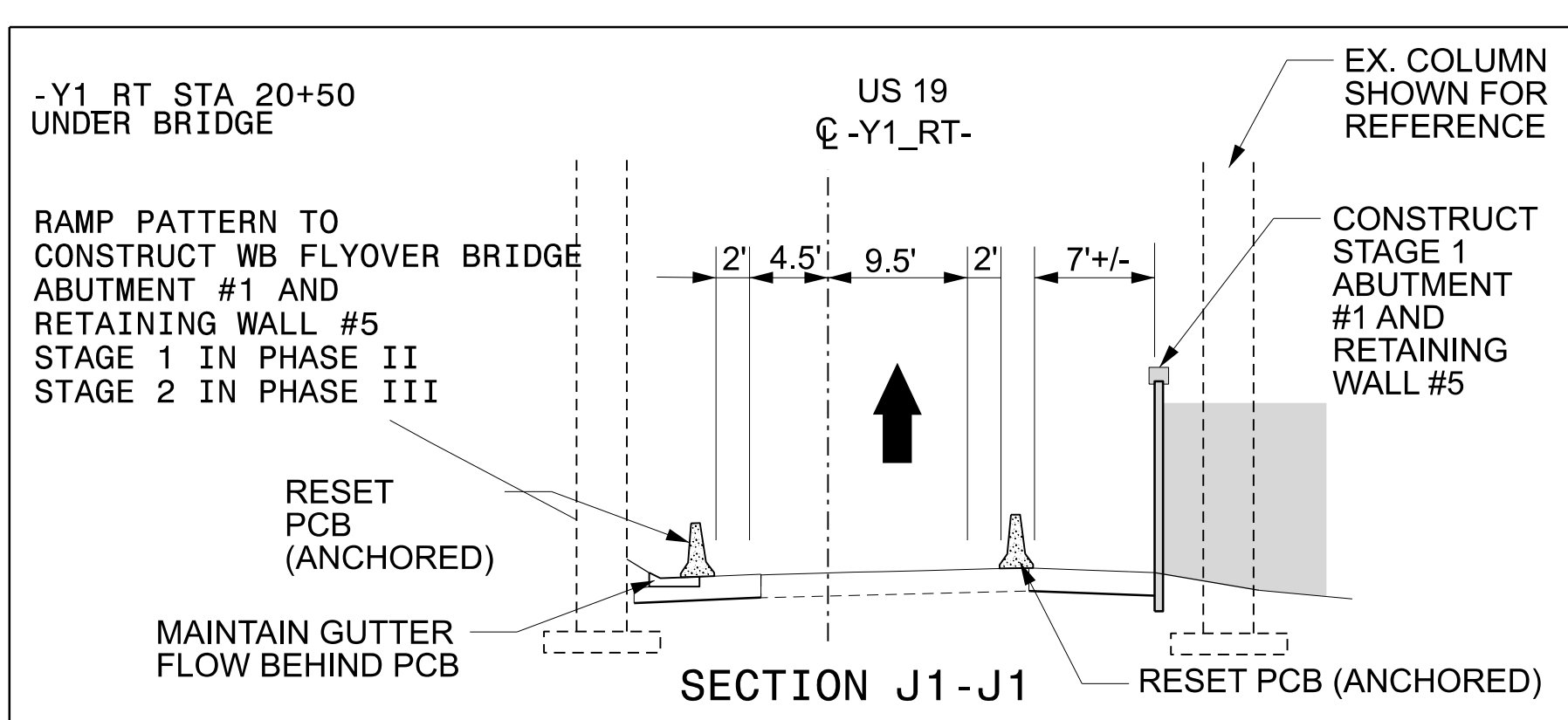
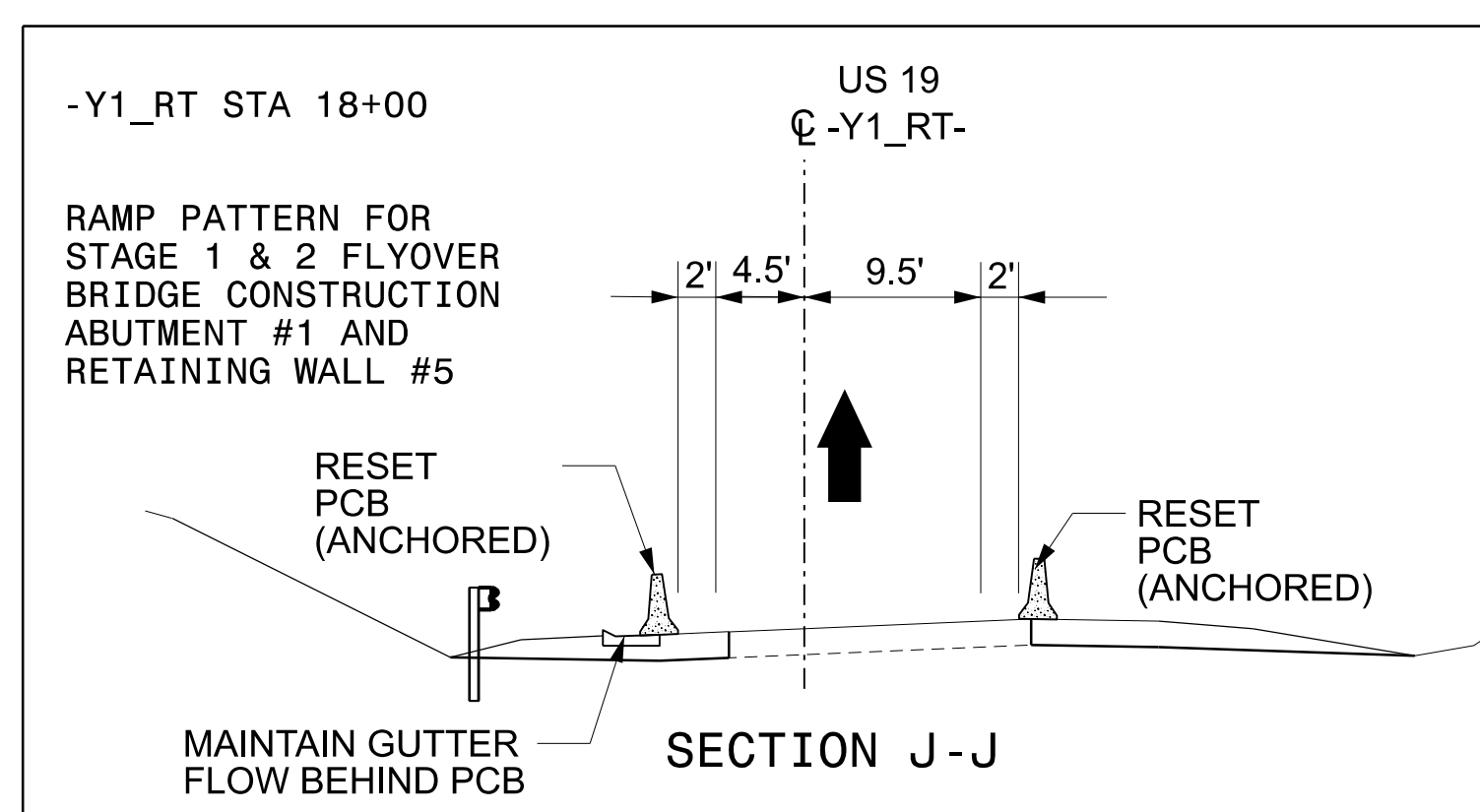
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PHASE III DETAILS
US 74/US 23



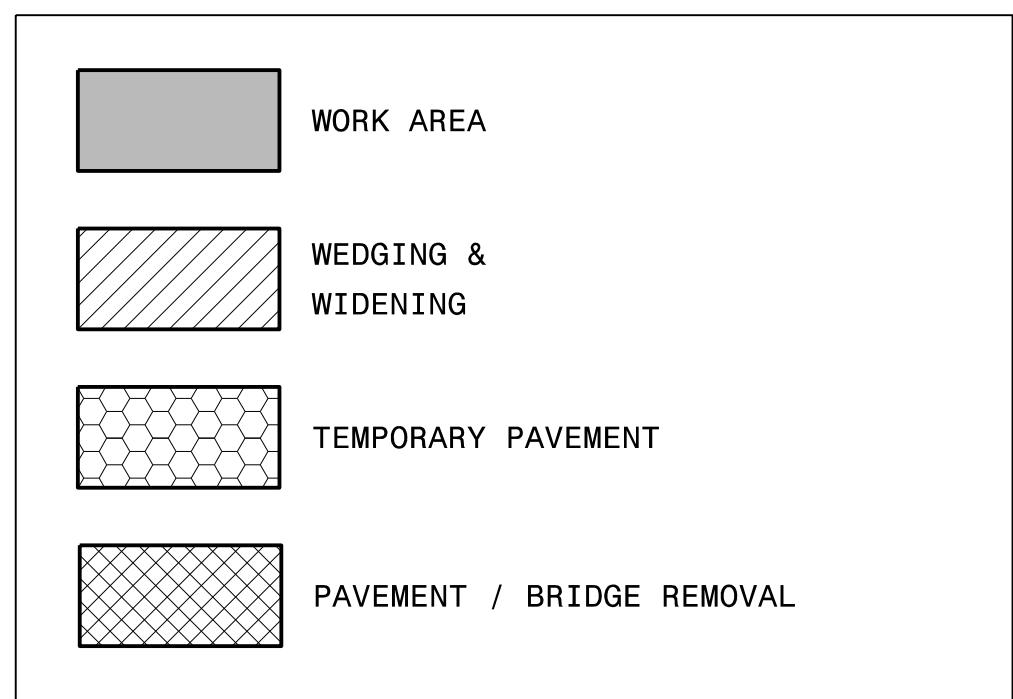
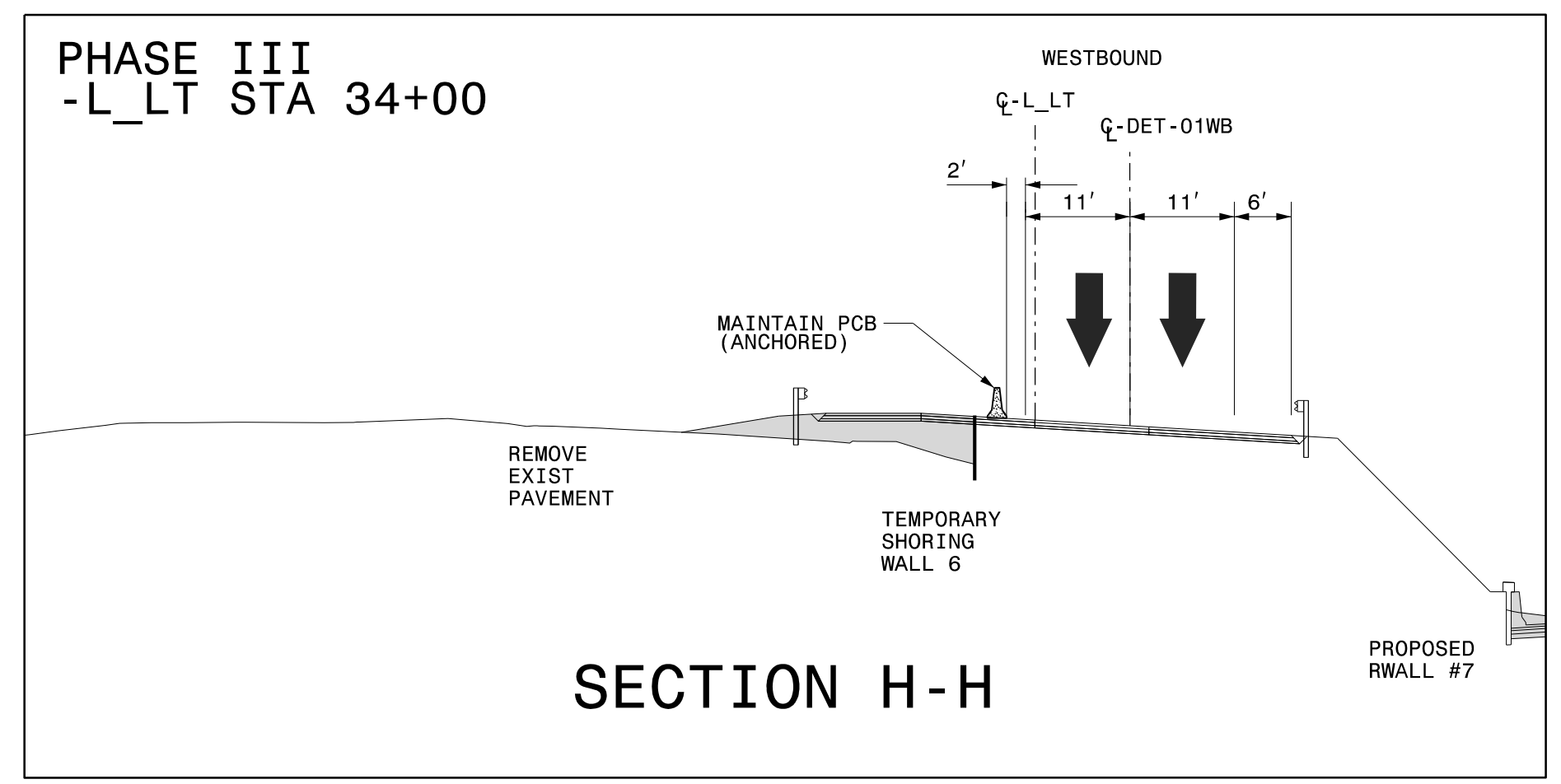
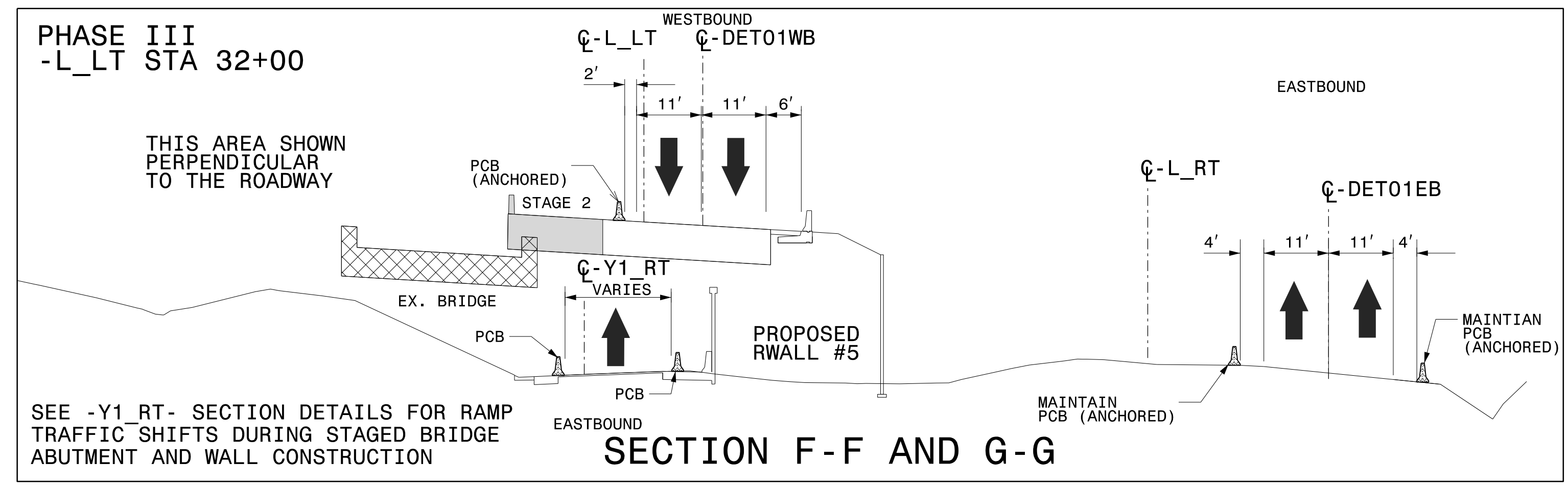
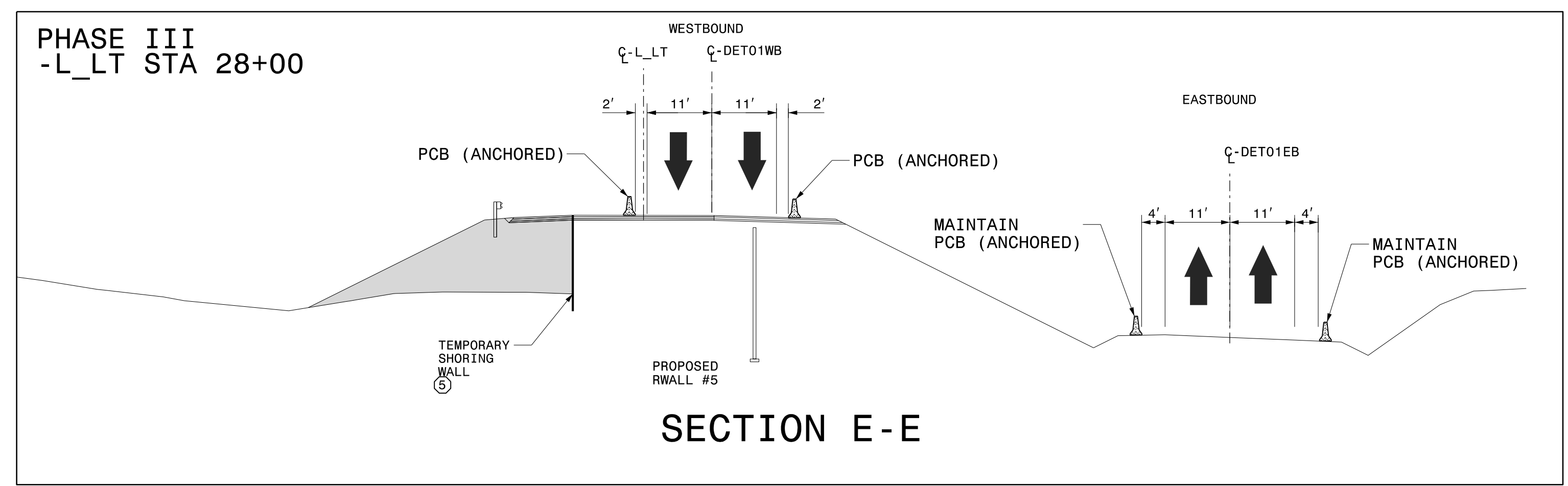
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US 74/US 23**

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 jeff.koontz



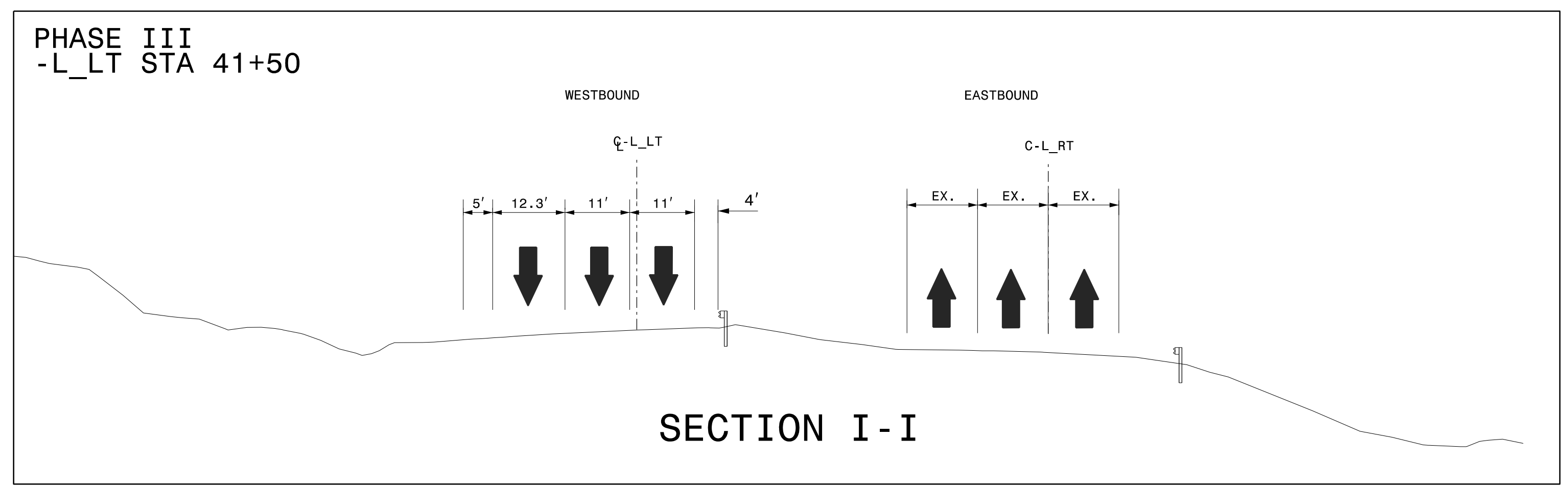
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 Jeffrey Thurston

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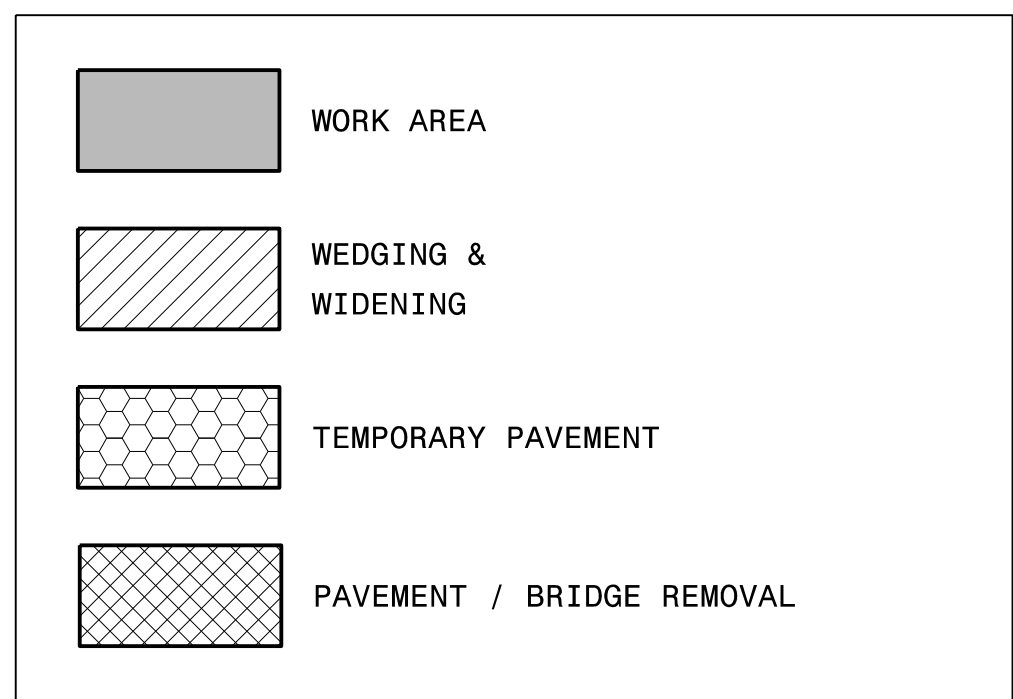
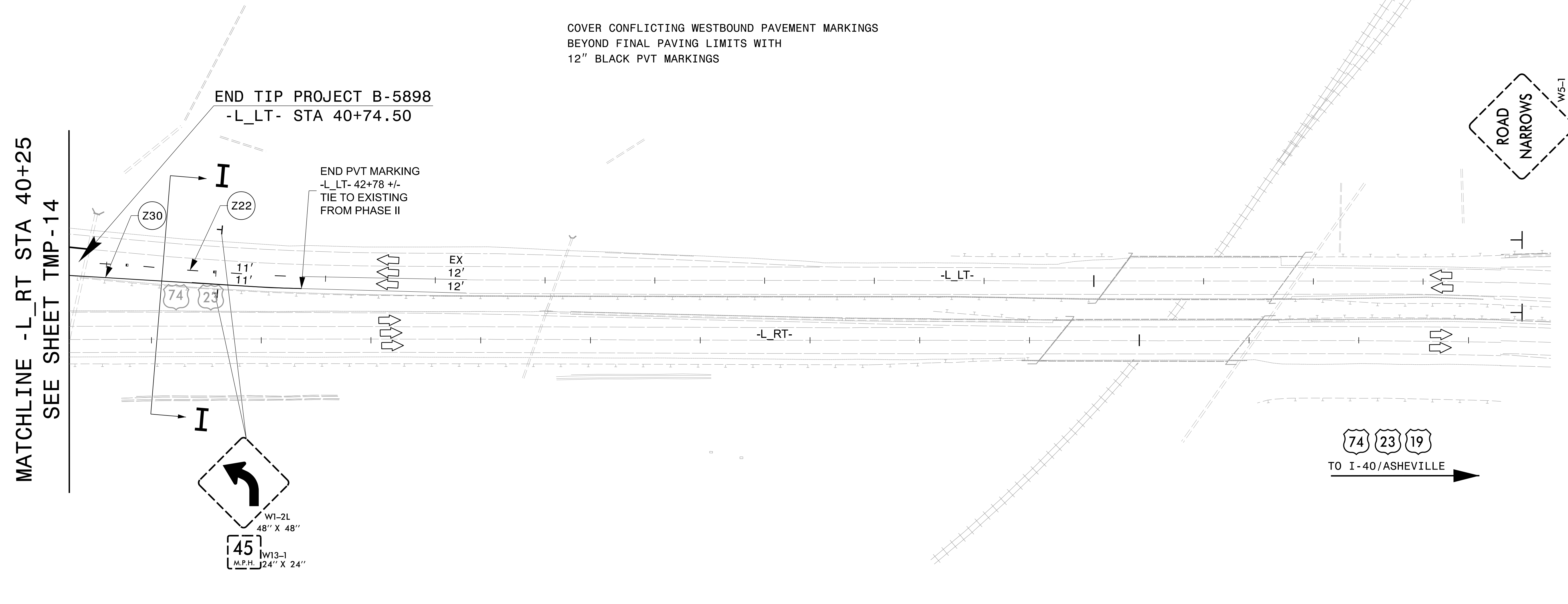
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**PHASE III DETAILS
US 74/US 23**



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BEYOND FINAL PAVING LIMITS WITH
12" BLACK PVT MARKINGS



9/6/2023
 B3186-B5898-TC_TMP-15_Phase III.dgn
 jeffrey.thurston

APPROVED: *Jeffrey A. Kooft*
DATE: 10/17/2023

SEAL 018122
JEFFREY A. KOOFT
ENGINEER

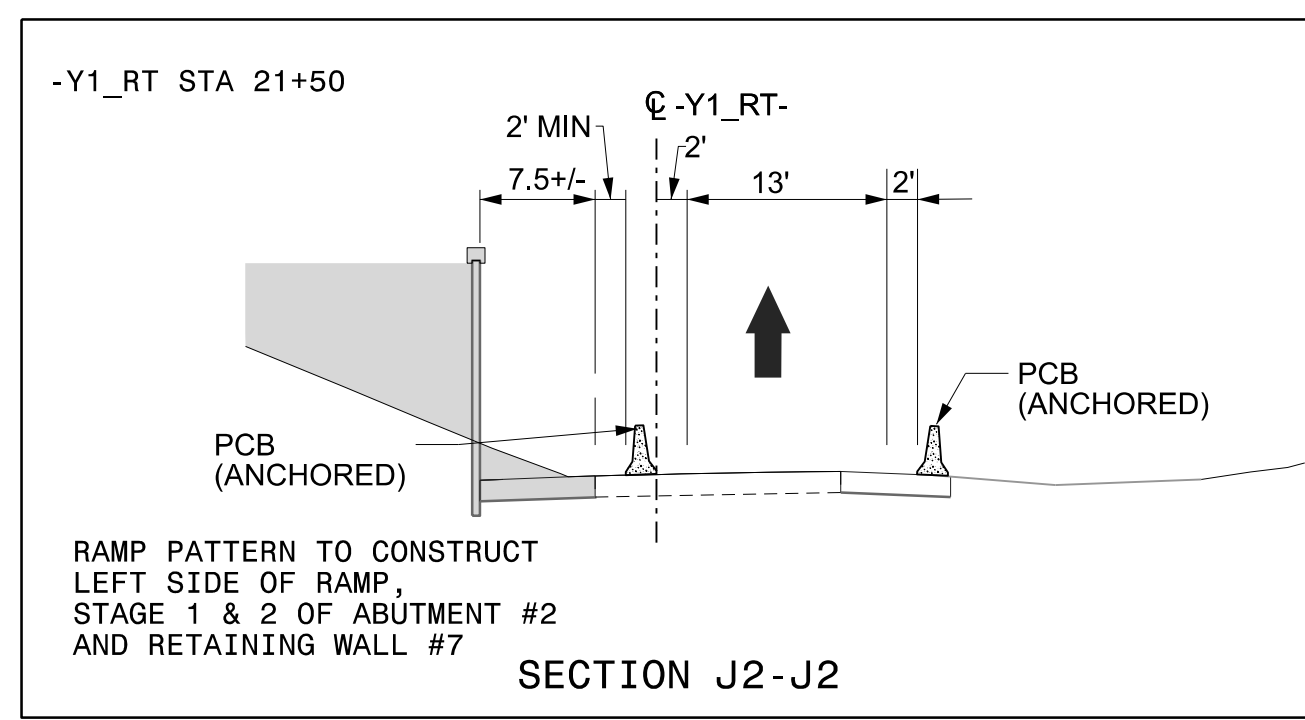
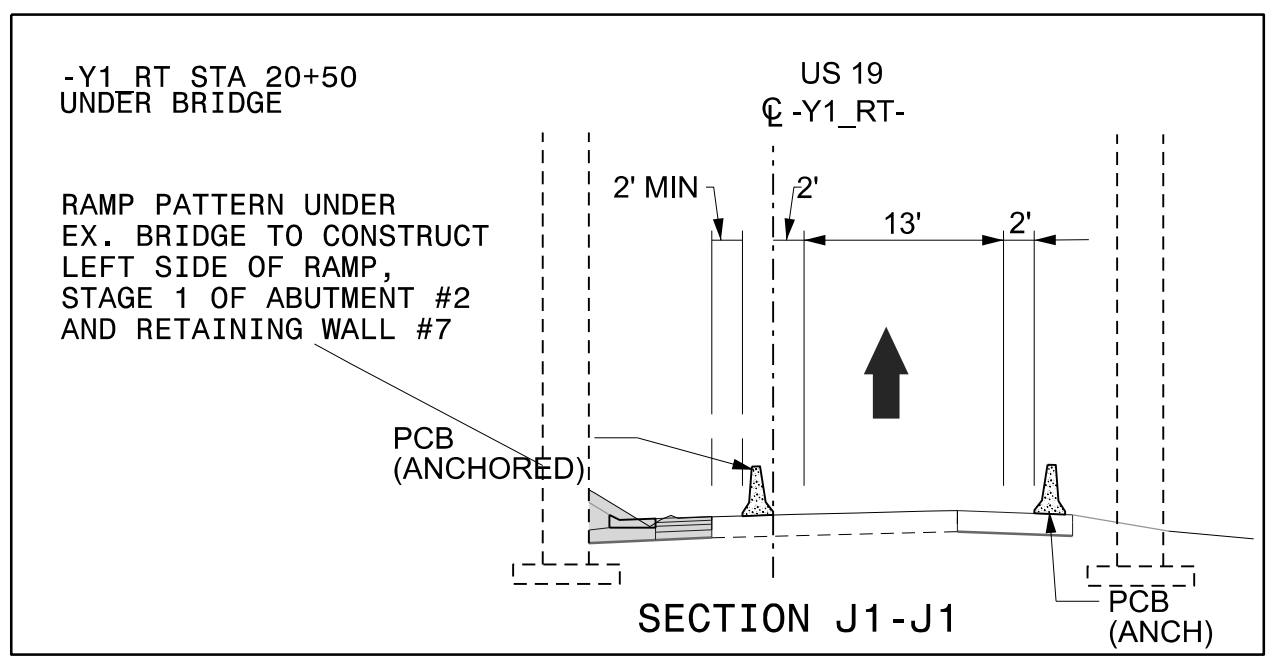
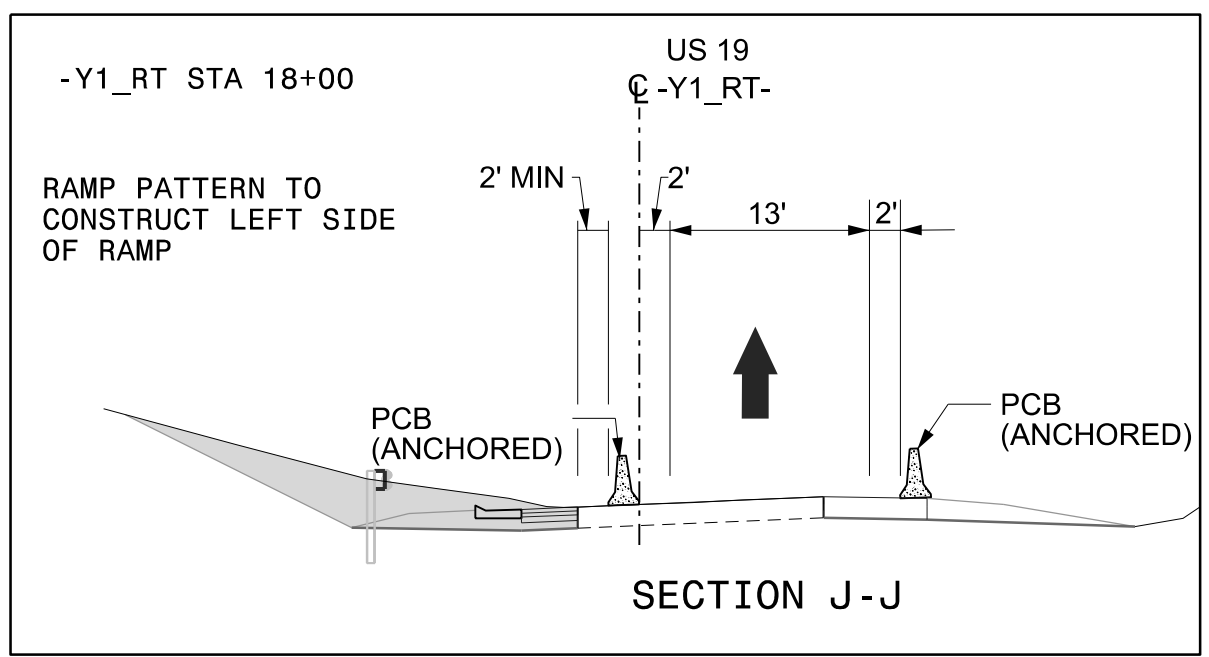
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DIVISION OF HIGHWAYS
STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION & WORK ZONE TRAFFIC CONTROL

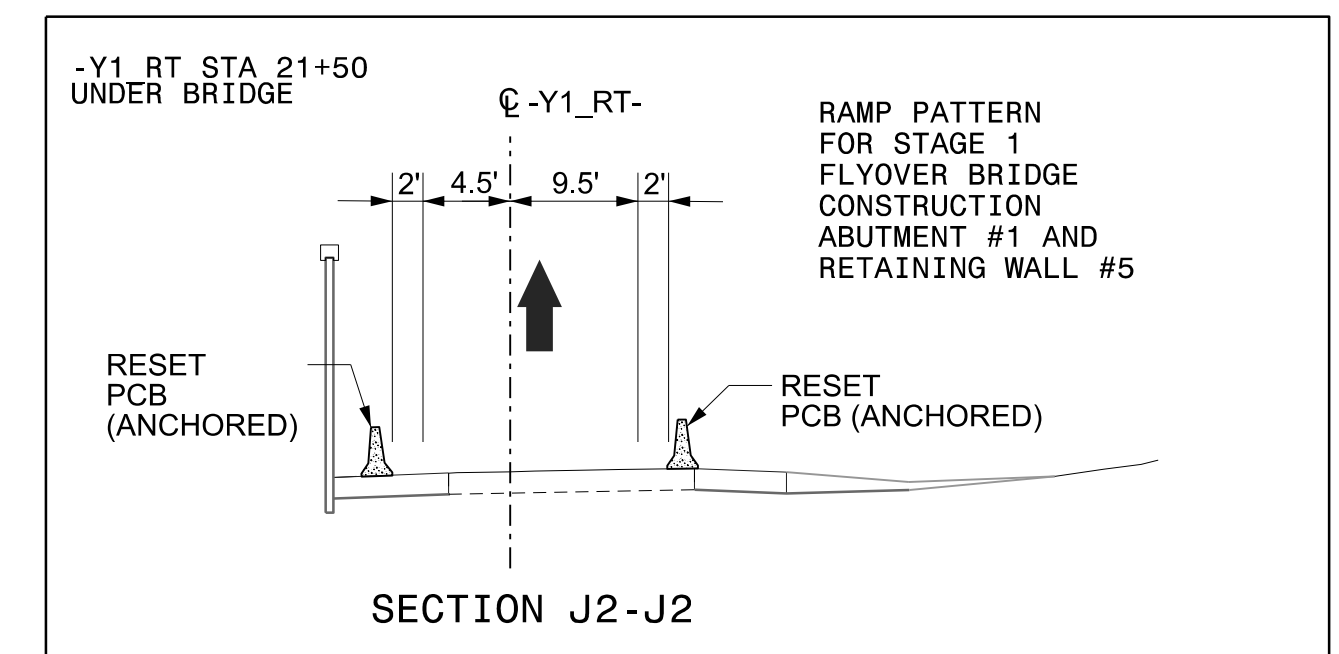
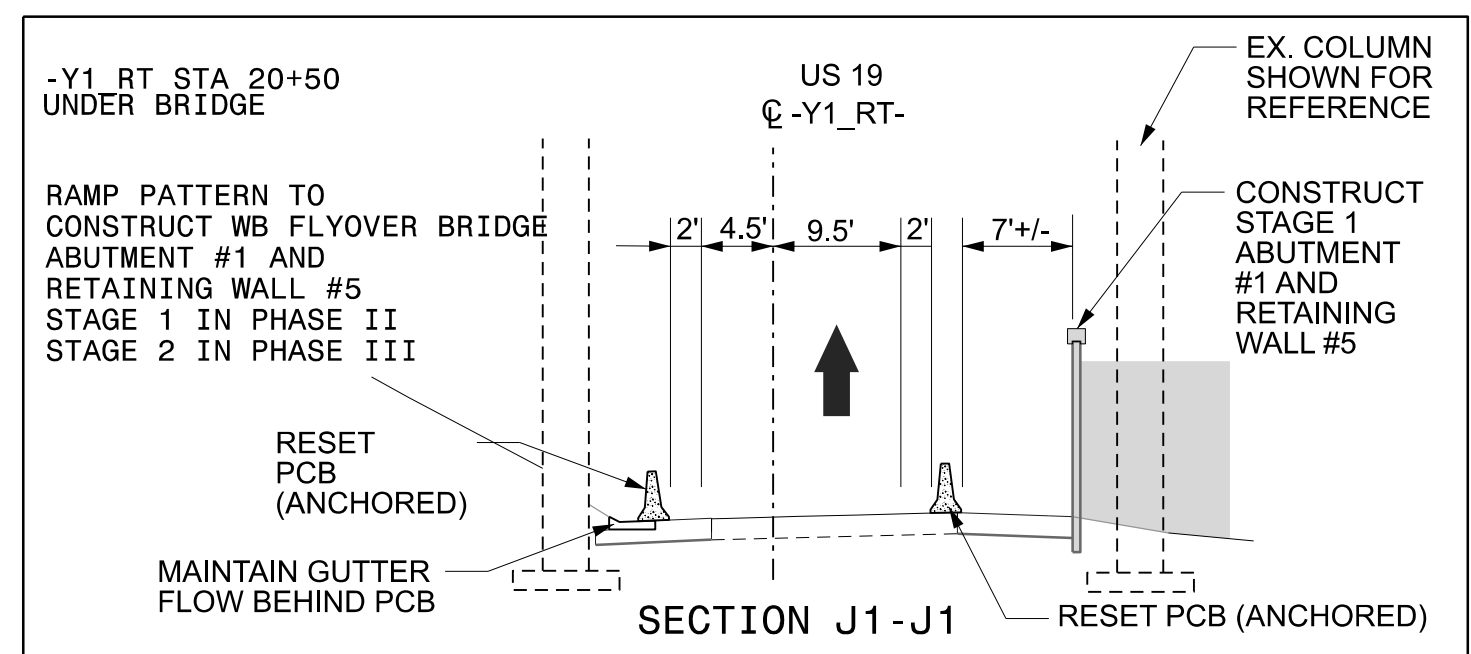
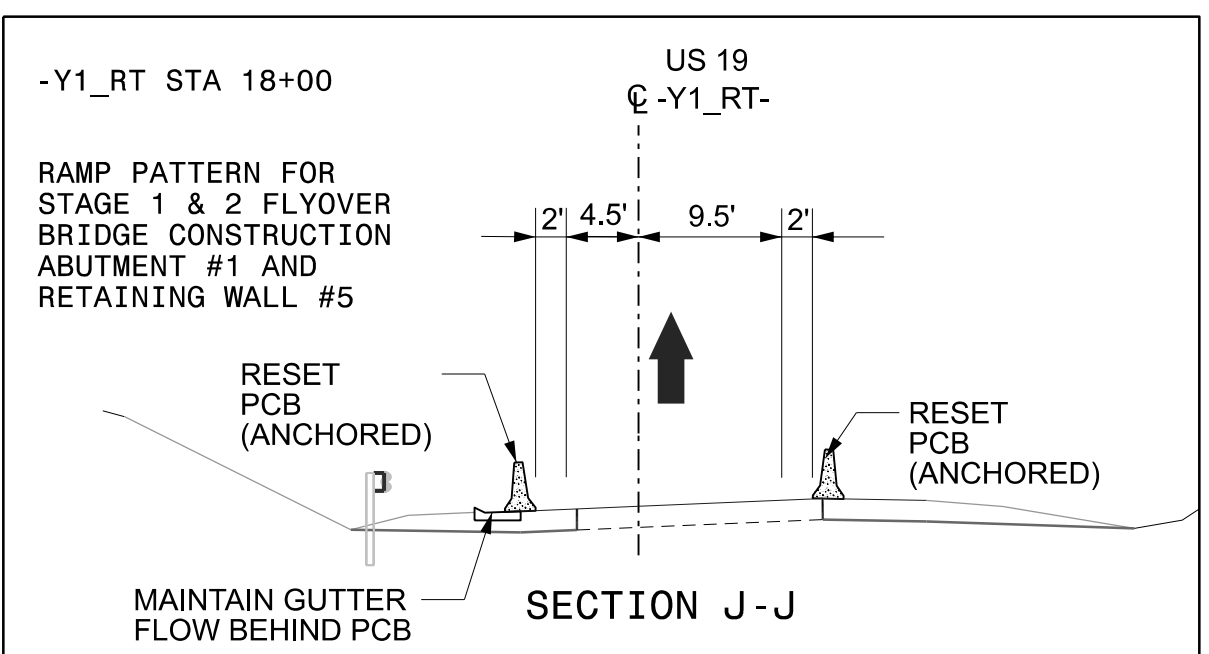
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PHASE III DETAILS
US 74/US 23

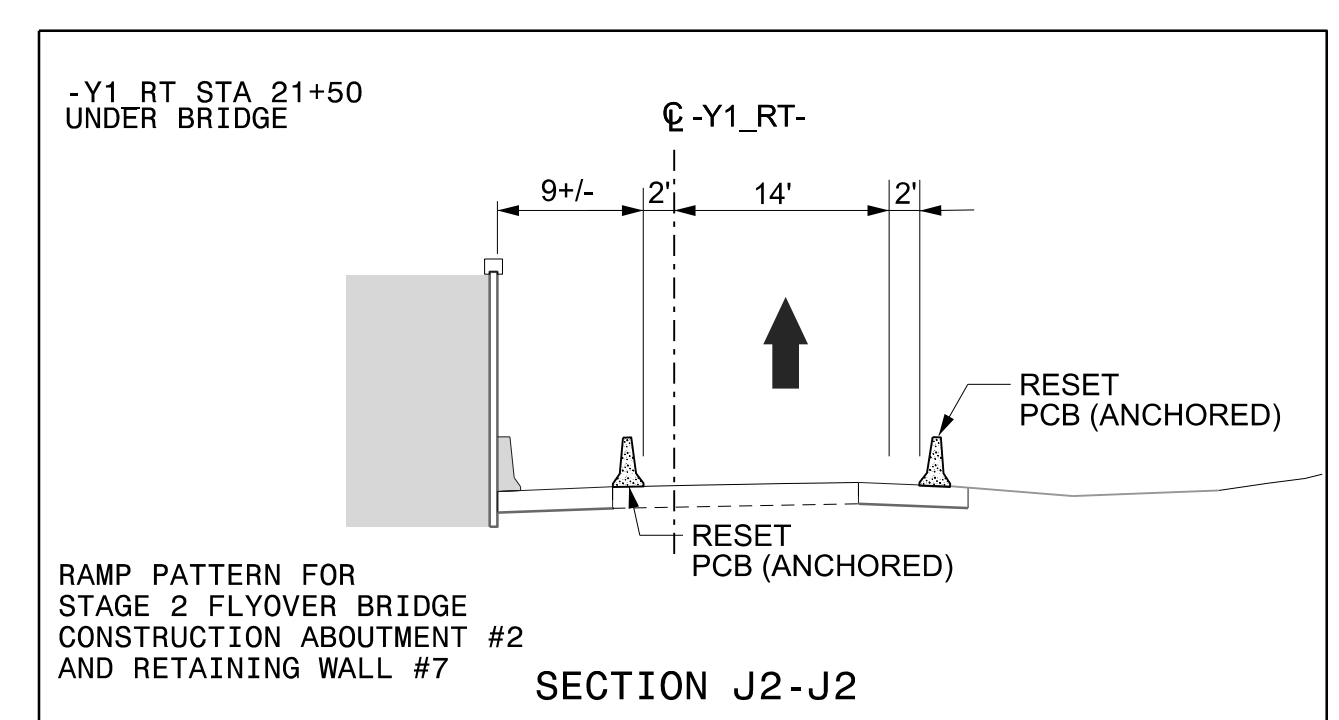
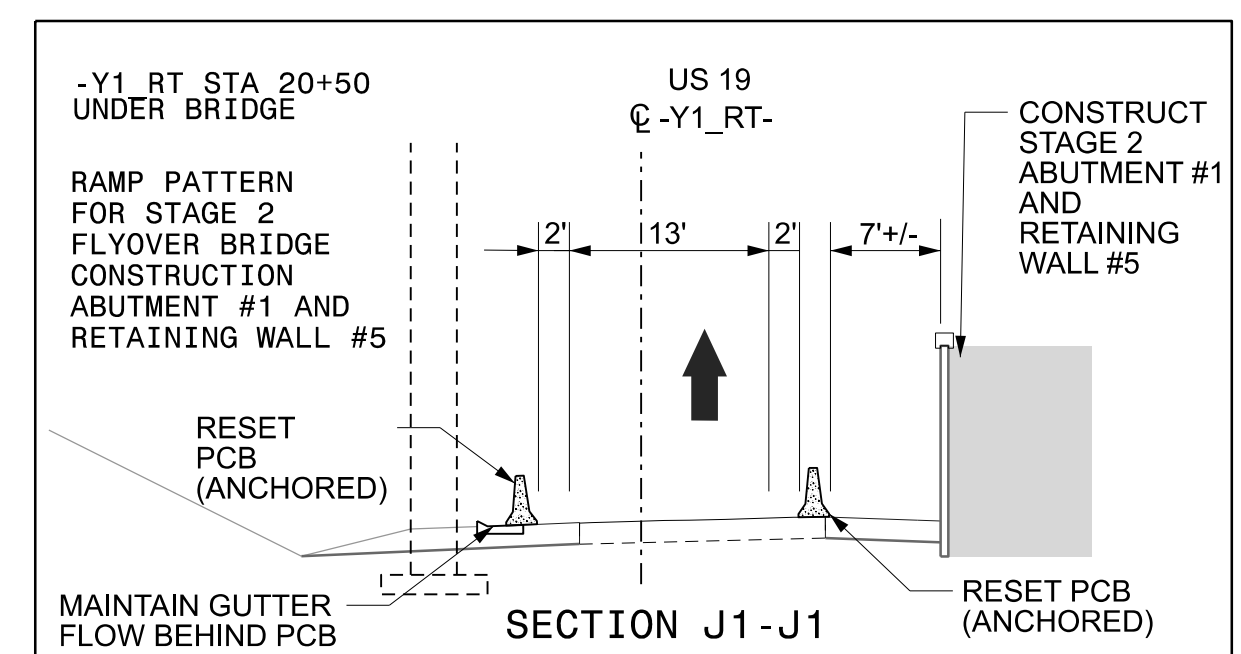
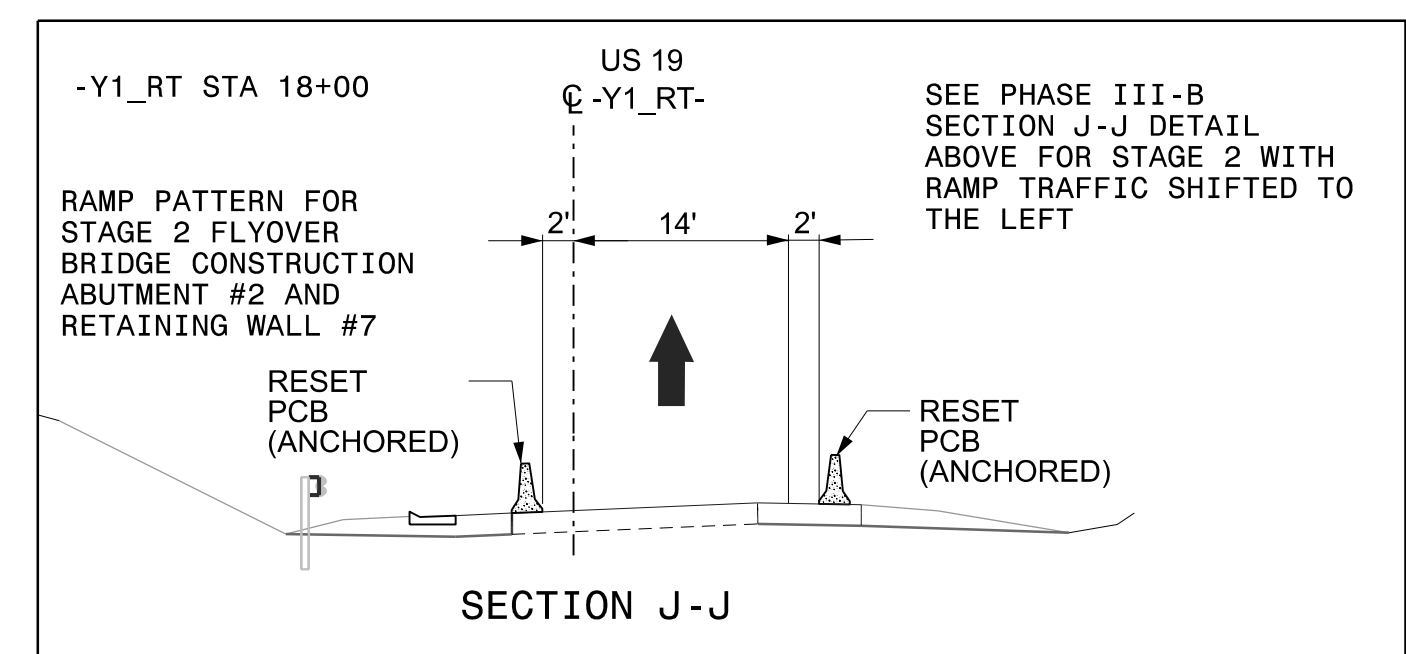
PHASE II-A (SEE TMP-17)
STAGE 1 & 2 US 74/US 23 FLYOVER BRIDGE
ABUTMENT #2 AND RETAINING WALL #7
RAMP TRAFFIC SHIFTED TO THE RIGHT



PHASE II-B (SEE TMP-17A)
STAGE 1 & 2 US 74/US 23 FLYOVER BRIDGE
ABUTMENT #1 AND RETAINING WALL #5
RAMP TRAFFIC SHIFTED TO THE LEFT



PHASE III-A & III-B (SEE TMP-18 & TMP 18A)
STAGE 2 US74/US23 FLYOVER BRIDGE
ABUTMENTS #1 & #2 AND RETAINING WALLS #5 & #7



9/6/2023
 B3186-B5898.TC_TMP_16_Phase III Section.dgn
 Jeffrey.Hurst

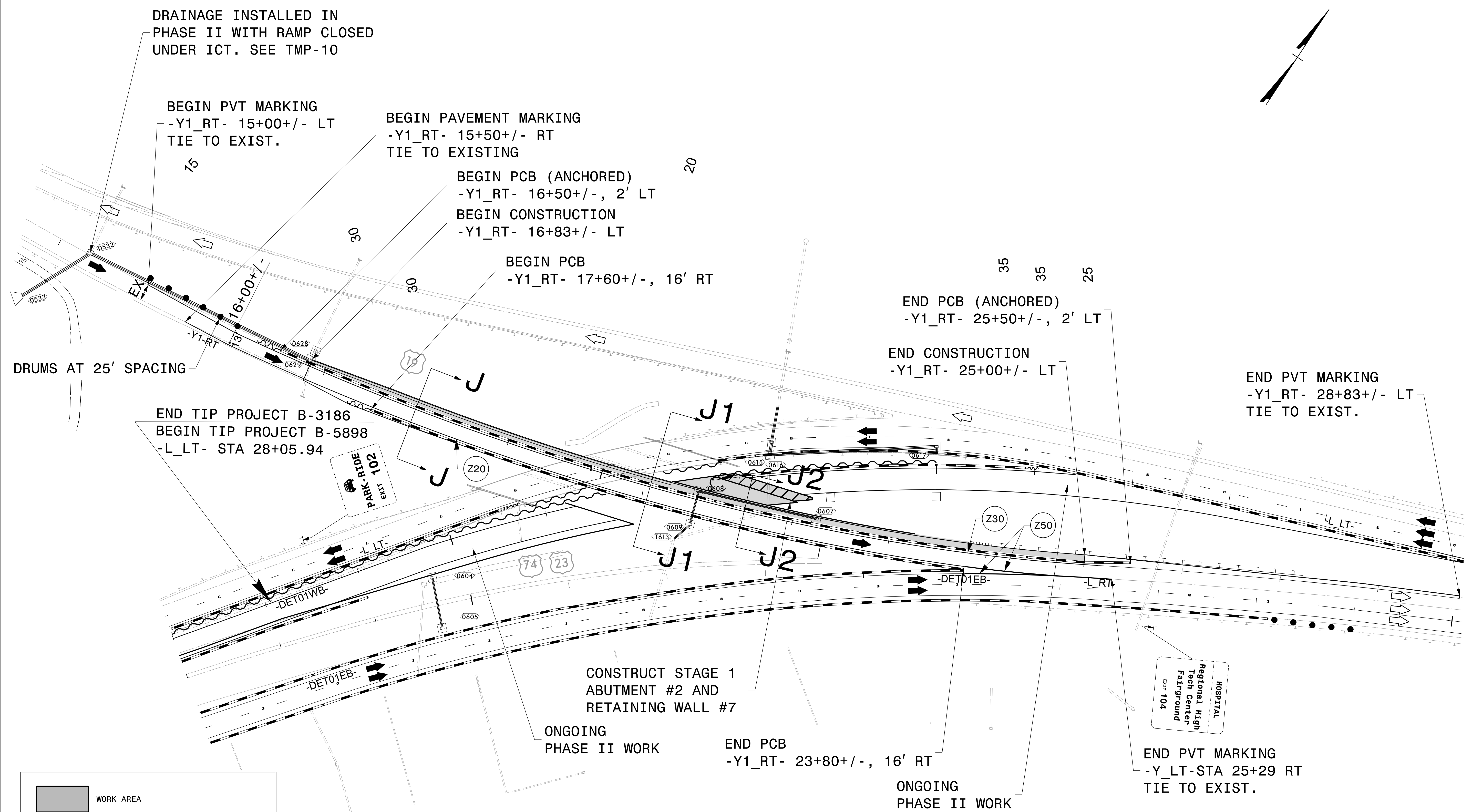
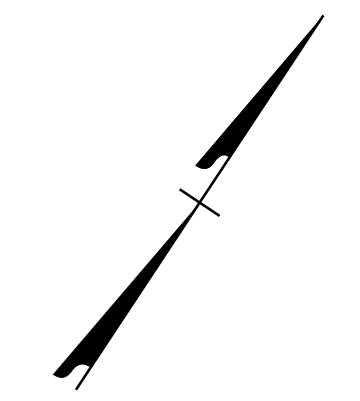
APPROVED: *Jeffrey H. Hurst*
 DATE: 10/17/2023

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PHASE II & III DETAILS
US 19 RAMP
WB FLYOVER BRIDGE &
RETAINING WALLS

SEE TMP-16 FOR SECTION DETAILS



	WORK AREA
	WEDGING & WIDENING
	TEMPORARY PAVEMENT
	PAVEMENT / BRIDGE REMOVAL

10/13/2023
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jon_ar_chambault

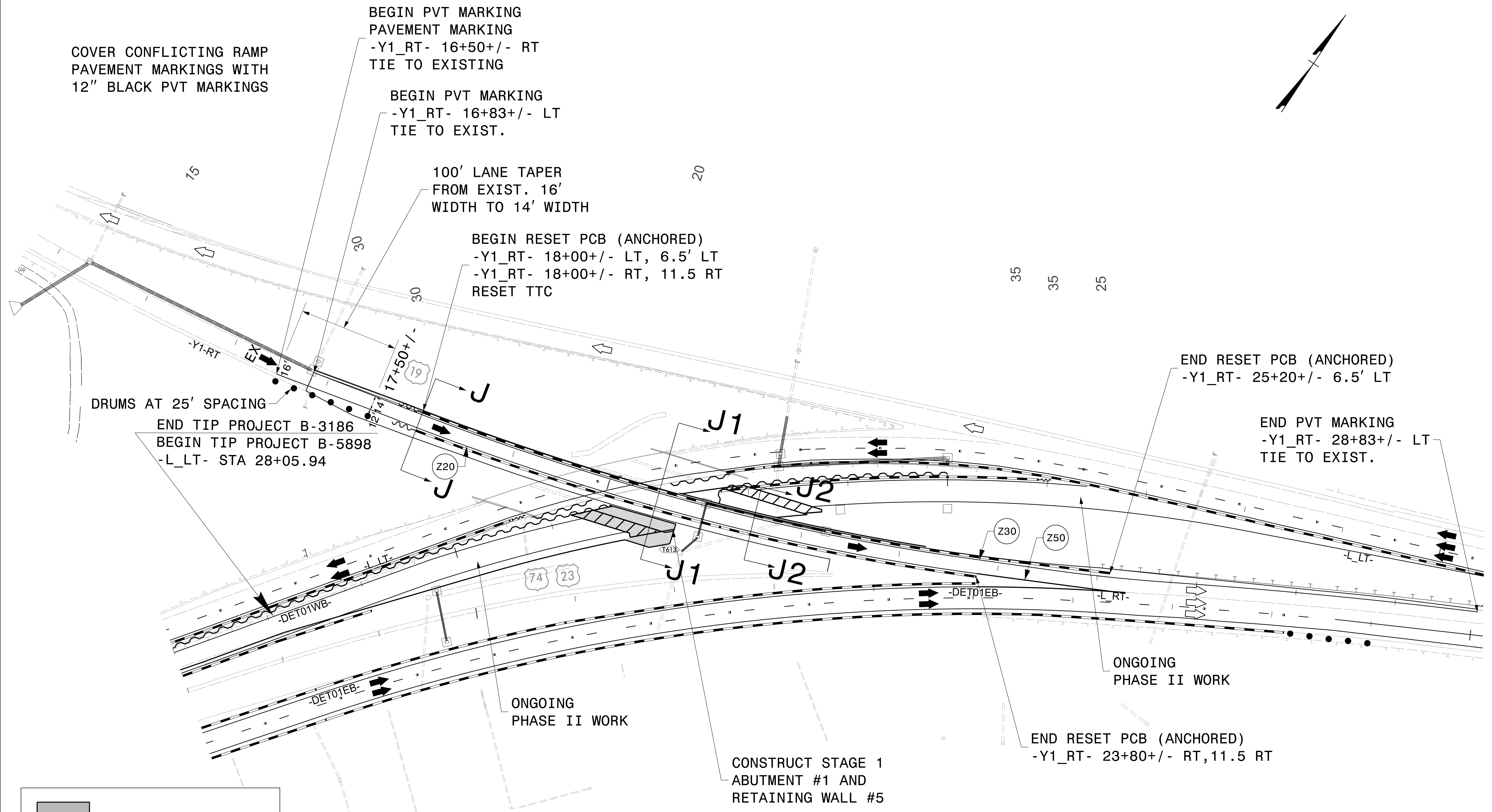
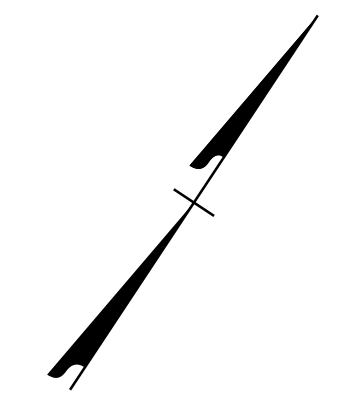
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PHASE II-A DETAILS
US 19 RAMP
US 74/US 23 STAGE 1
WB FLYOVER BRIDGE & WALL

SEE TMP-16 FOR SECTION DETAILS



	WORK AREA
	WEDGING & WIDENING
	TEMPORARY PAVEMENT
	PAVEMENT / BRIDGE REMOVAL

APPROVED: *Jeffrey A. Kooft*
 DATE: 10/17/2023

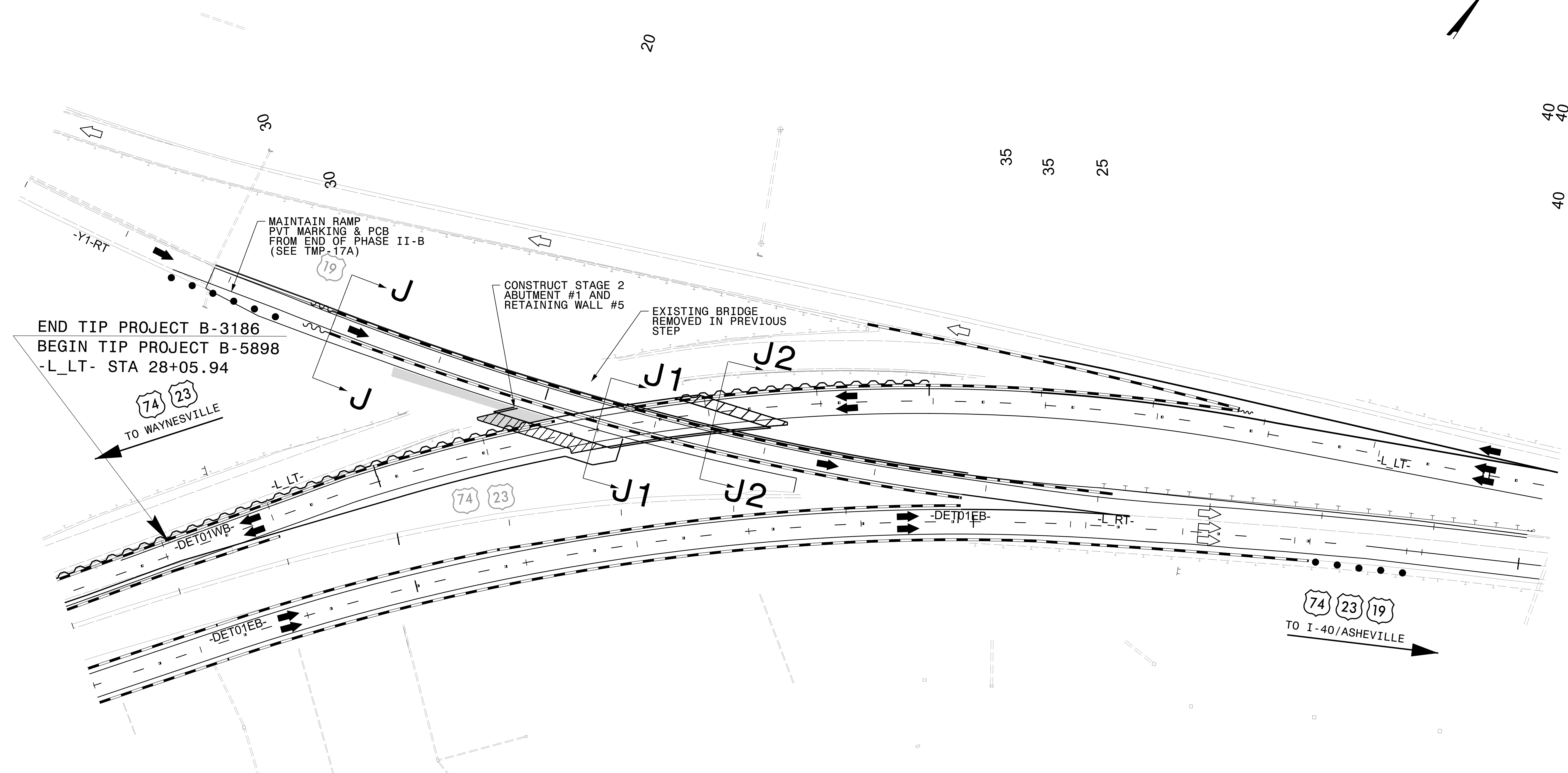
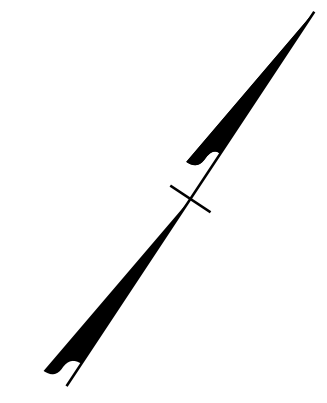
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PHASE II-B DETAILS
US 19 RAMP
US 74/US 23 STAGE 1
FLYOVER BRIDGE & WALL

9/6/2023
 B3186-B5898-TC_TMP_17A_Phase_1B.dgn
 jeffrey.thurston

SEE TMP-16 FOR SECTION DETAILS



	WORK AREA
	WEDGING & WIDENING
	TEMPORARY PAVEMENT
	PAVEMENT / BRIDGE REMOVAL

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 jeffrey.thurston

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JEFFREY A. KOOFT

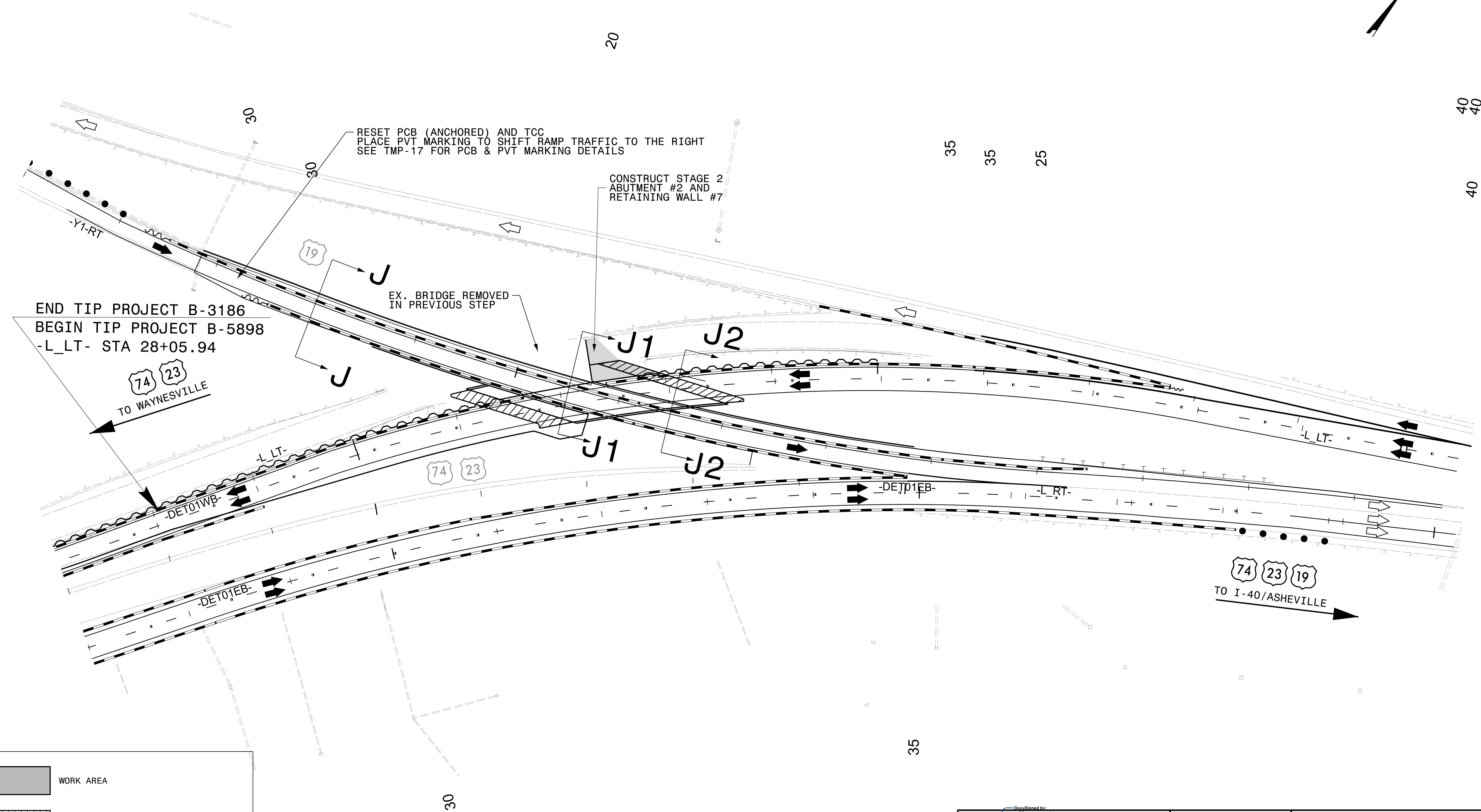
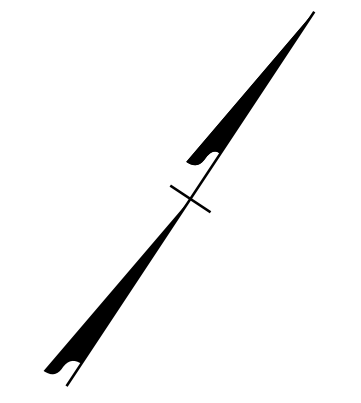
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**PHASE III-A DETAILS
US 74/US 23 STAGE 2
FLYOVER BRIDGE & WALL**

SEE TMP-16 FOR SECTION DETAILS



END TIP PROJECT B-3186
 BEGIN TIP PROJECT B-5898
 -L_LT- STA 28+05.94

	WORK AREA
	WEDGING & WIDENING
	TEMPORARY PAVEMENT
	PAVEMENT / BRIDGE REMOVAL

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 jon_ar_chambault

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JEFFREY A. KOOFTZ

DATE: 10/17/2023

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**PHASE III-B DETAILS
 US 74/US 23 STAGE 2
 FLYOVER BRIDGE & WALL**