

**NOTES**

PROVIDE 1 1/4" HIGH BEAM BOLSTERS UPPER AT 4'-0" CTS. ATOP THE METAL STAY IN PLACE FORMS TO SUPPORT THE BOTTOM MAT OF "A" BARS. WHEN USING REMOVABLE FORMS. PROVIDE CONTINUOUS HIGH CHAIRS FOR METAL DECK (CHCM) AT 4'-0" CTS. WITH A HEIGHT TO SUPPORT THE BOTTOM MAT OF "A" BARS A CLEAR DISTANCE OF 2 1/2" ABOVE THE TOP OF THE REMOVABLE FORM.

LONGITUDINAL STEEL MAY BE SHIFTED SLIGHTLY, AS NECESSARY, TO AVOID INTERFERENCE WITH STIRRUPS IN PRESTRESSED CONCRETE GIRDERS.

PREVIOUSLY CAST CONCRETE IN A CONTINUOUS UNIT SHALL HAVE ATTAINED A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI BEFORE ADDITIONAL CONCRETE IS CAST IN THE UNIT.

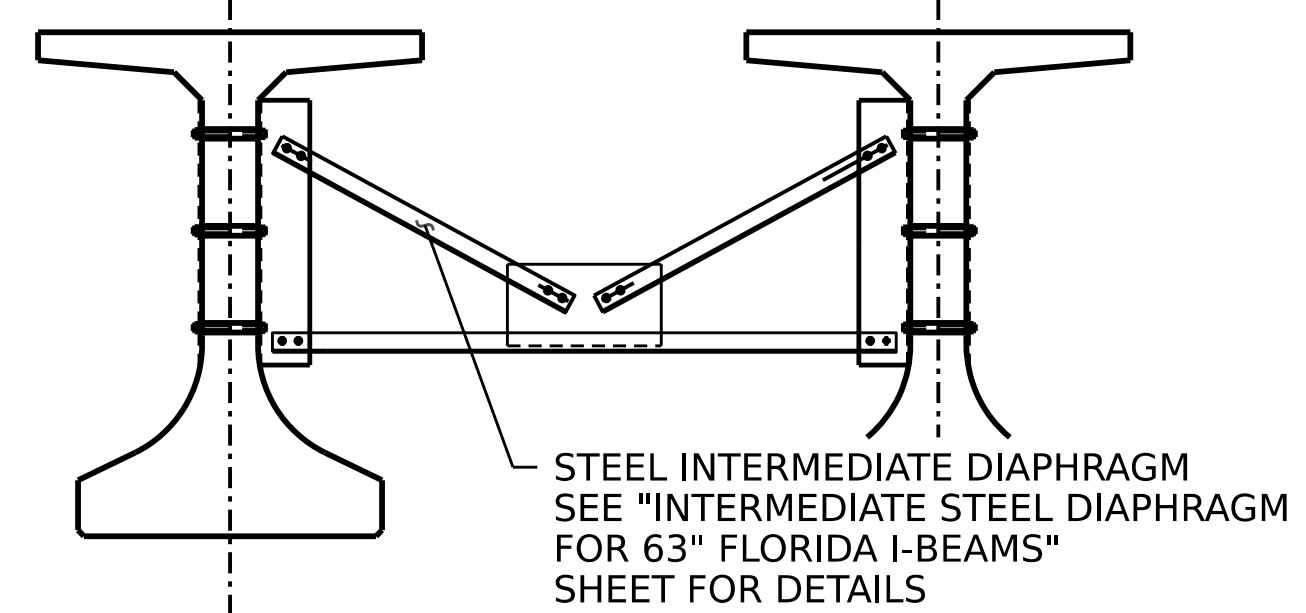
SEE CONCRETE BARRIER RAIL DETAILS SHEET FOR DETAILS OF STAGE III CONCRETE BARRIER REINFORCEMENT TO BE PLACED IN STAGE I. LONGITUDINAL "B" BARS MUST BE COORDINATED WITH BARRIER RAIL "D" BARS TO ENSURE NO CONFLICT DURING "D" BAR PLACEMENT.

BARRIER RAIL IN A CONTINUOUS UNIT SHALL NOT BE CAST UNTIL ALL SLAB CONCRETE IN THAT UNIT HAS BEEN CAST AND HAS REACHED A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI.

DOWELS SHALL BE PLACED IN THE SAME HORIZONTAL PLANE AS THE TOP AND BOTTOM SLAB REINFORCING STEEL.

SEE CONSTRUCTION SEQUENCE SHEETS FOR LOCATION OF TEMPORARY PORTABLE CONCRETE BARRIER (ANCHORED)

BUILD-UP BARS #5K26 AND #5K27 MAY BE PLACED DIAGONALLY IF NEEDED TO CLEAR THE REINFORCEMENT AT THE TOP OF BRIDGE DECK.



**PART TYPICAL SECTION**  
(SHOWING INTERMEDIATE STEEL DIAPHRAGMS)

**"B" BAR KEY**

- = NON-CONTINUOUS BAR RUN FOR NEGATIVE MOMENT REGIONS. SEE "PLAN OF SPANS" SHEET
- = CONTINUOUS BAR RUN SEE "PLAN OF SPANS" SHEET

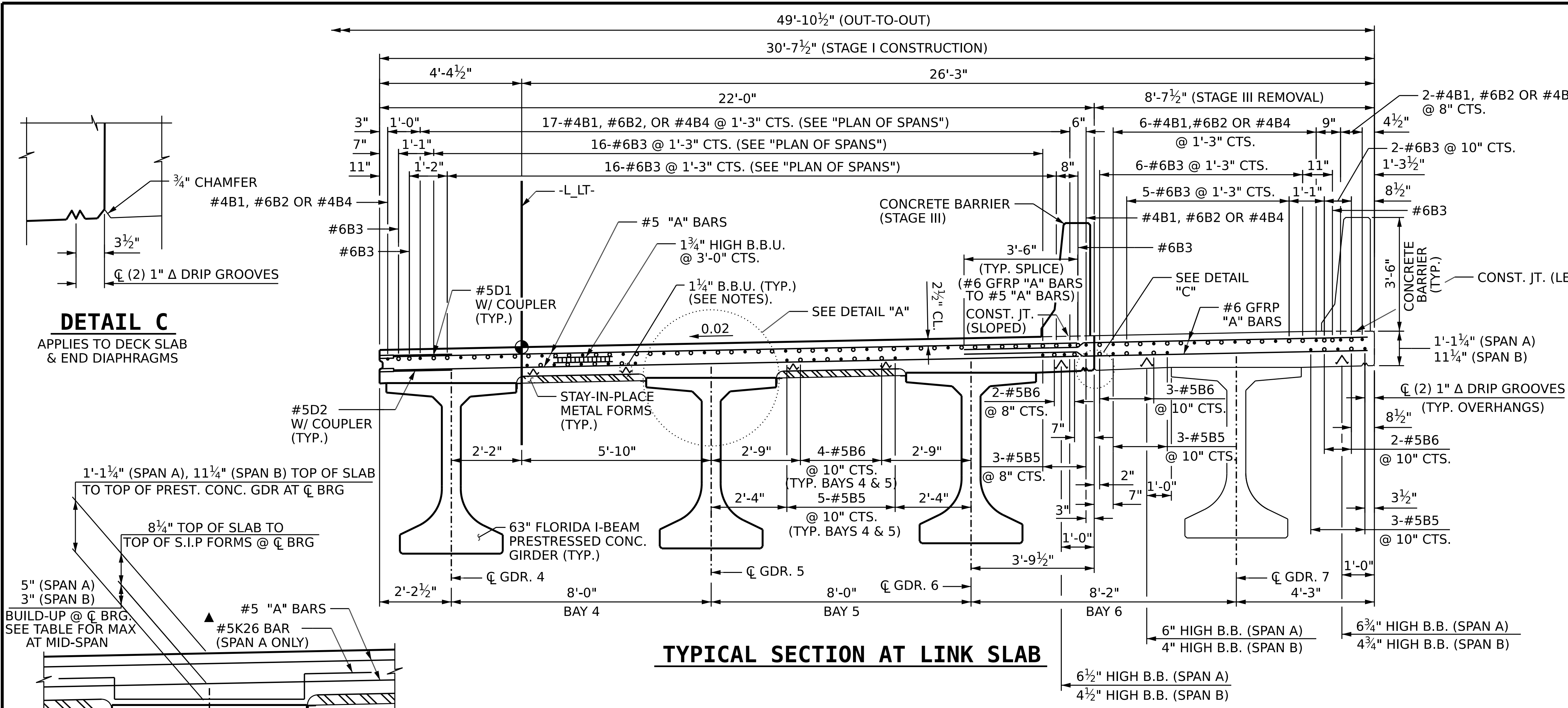
PROJECT NO. **B-3186 / B-5898**

**HAYWOOD** COUNTY

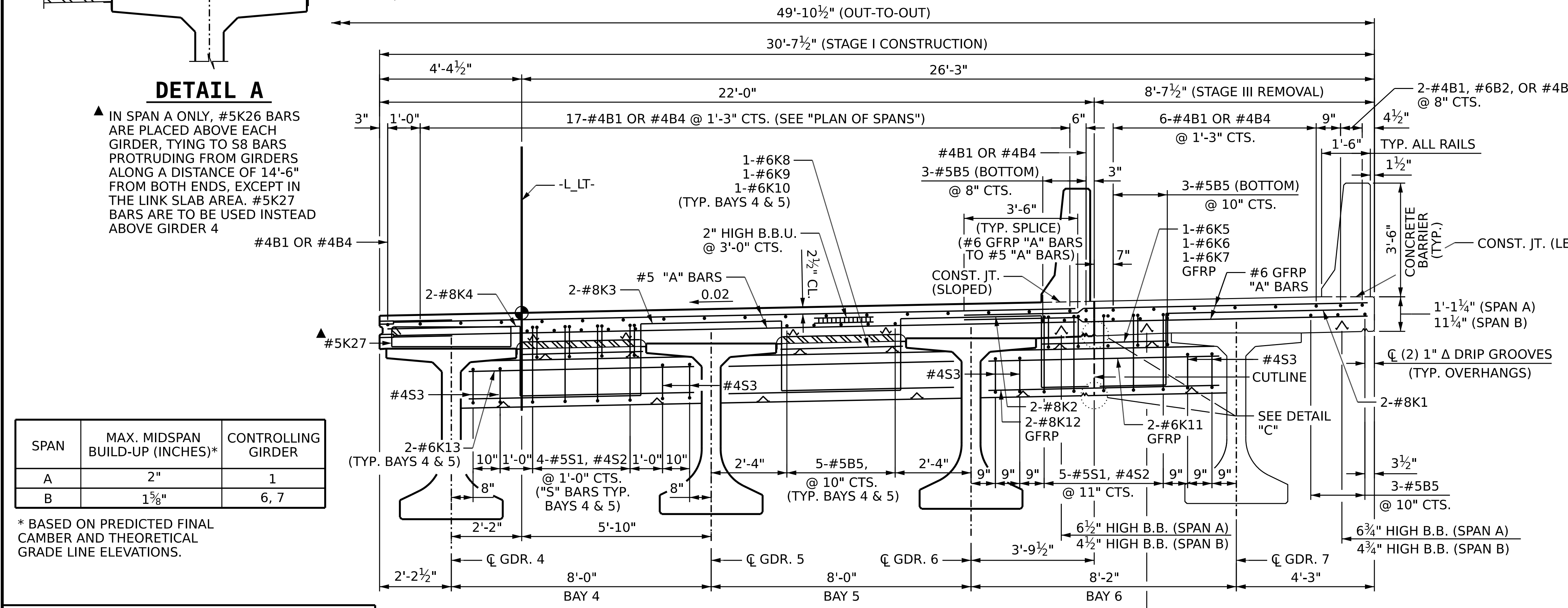
STATION: **24+70.00 -L\_LT-**

SHEET 1 OF 3

STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION RALEIGH					
SUPERSTRUCTURE					
TYPICAL SECTIONS STAGE I					
REVISIONS					
NO.	BY:	DATE:	NO.	BY:	DATE:
1			3		
2			4		
SHEET NO.					S3-08
TOTAL SHEETS					50



**TYPICAL SECTION AT LINK SLAB**



**TYPICAL SECTION AT END BENT**

**DETAIL A**  
▲ IN SPAN A ONLY, #5K26 BARS ARE PLACED ABOVE EACH GIRDER, TYING TO S8 BARS PROTRUDING FROM GIRDERS ALONG A DISTANCE OF 14'-6" FROM BOTH ENDS, EXCEPT IN THE LINK SLAB AREA. #5K27 BARS ARE TO BE USED INSTEAD ABOVE GIRDER 4

**DETAIL C**  
APPLIES TO DECK SLAB & END DIAPHRAGMS

SPAN	MAX. MIDSPAN BUILD-UP (INCHES)*	CONTROLLING GIRDER
A	2"	1
B	1 5/8"	6, 7

\* BASED ON PREDICTED FINAL CAMBER AND THEORETICAL GRADE LINE ELEVATIONS.

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 CHECKED BY: D. TUTTLE DATE: 06/2023  
 DESIGN ENGINEER OF RECORD: D. TUTTLE DATE: 06/2023

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED