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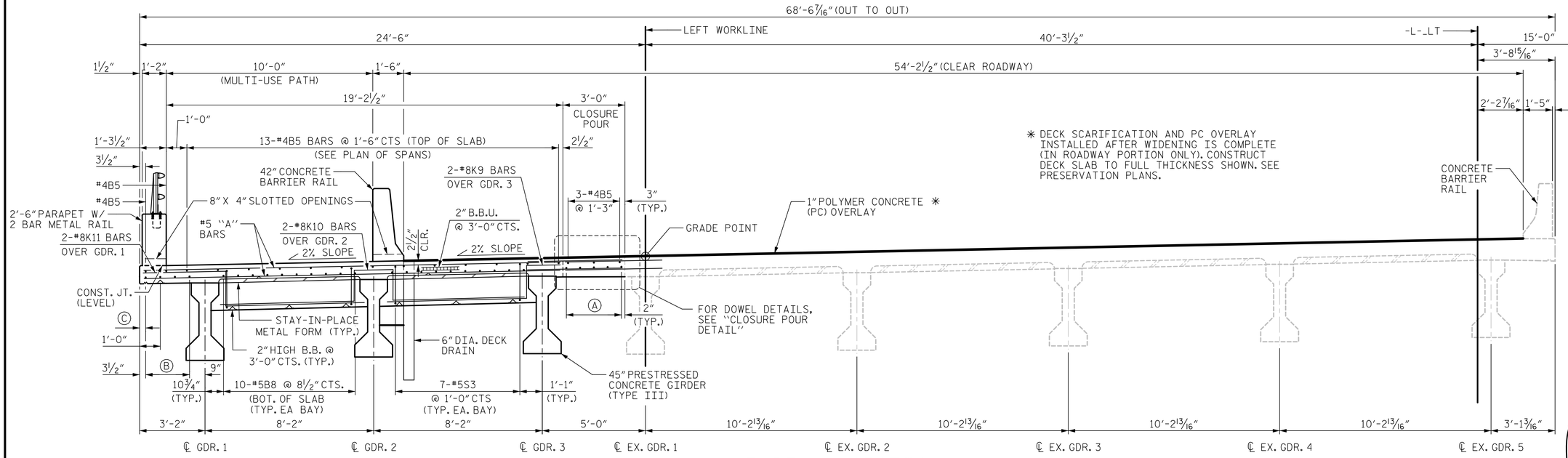
FOR NOTES, SEE "CONSTRUCTION SEQUENCE" SHEETS, SHEET 4 OF 4.

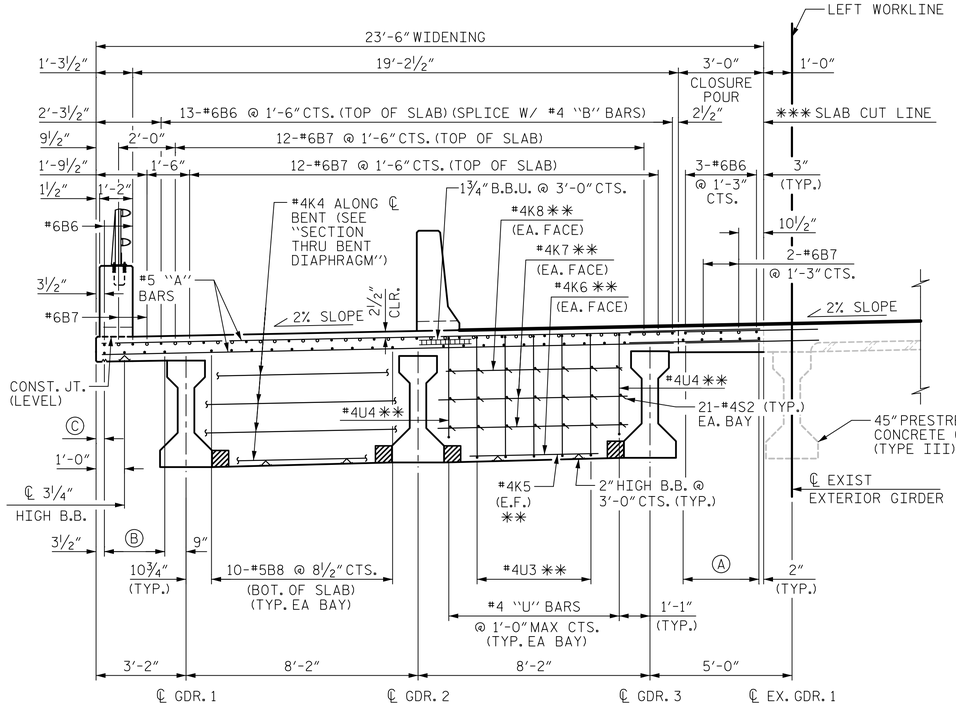


* DECK SCARIFICATION AND PC OVERLAY INSTALLED AFTER WIDENING IS COMPLETE (IN ROADWAY PORTION ONLY). CONSTRUCT DECK SLAB TO FULL THICKNESS SHOWN. SEE PRESERVATION PLANS.

*** PRIOR TO DECK REMOVAL, THE CONTRACTOR SHALL SUBMIT METHOD OF REMOVAL OF EXISTING DECK TO SLAB CUT LINE AS SHOWN IN PLANS. CONTRACTOR SHALL ENSURE MEASURES ARE TAKEN TO PROTECT DECK REINFORCING STEEL DURING REMOVAL OPERATIONS.

TYPICAL SECTION

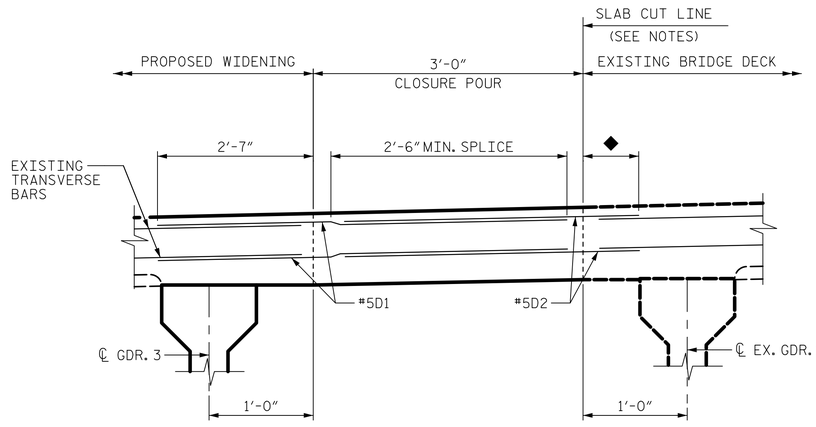
(END BENT 2 & BENT 2)
(SPAN C & SPAN D)



** (TYP. EA BAY)

PARTIAL TYPICAL SECTION

(BENT 3)



CLOSURE POUR DETAIL

(ALL SPANS)

◆ #6D2 DOWELS PLACED IN THE EXISTING DECK SHALL BE INSTALLED USING AN ADHESIVE ANCHORING SYSTEM. LEVEL ONE FIELD TESTING IS REQUIRED AND THE YIELD LOAD OF THE DOWEL IS 13.2 KIPS. OVERALL LENGTH SHALL PROVIDE A 2'-6" MIN. EXTENSION INTO CLOSURE POUR. EMBEDMENT LENGTH TO BE DETERMINED BY THE MANUFACTURER OF THE ADHESIVELY ANCHORED ANCHOR SYSTEM. #6D2 BAR LENGTH IS BASED ON A 10" EMBEDEMENT LENGTH. ADHESIVELY ANCHORED DOWEL TESTING SHALL BE IN ACCORDANCE WITH SECTION 420-13 OF THE STANDARD SPECIFICATIONS.

#6D1 AND #6D2 DOWELS SHALL BE PLACED IN THE SAME HORIZONTAL PLANE AS THE TOP AND BOTTOM SLAB REINFORCING STEEL.

Ⓐ 5-#5B8 @ 8" CTS. (BOT. OF SLAB) (2 BAR RUN)

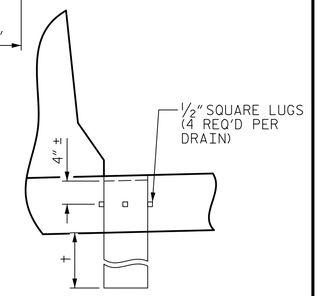
Ⓑ 4-#5B8 @ 8 1/2" CTS. (BOT. OF SLAB) (2 BAR RUN) (TYP. EA. OVERHANG)

Ⓒ 3 1/2" TO ℄ 2 - 1"Δ DRIP GROOVES

"B" BAR KEY

● = CONTINUOUS BAR RUN

○ = NON-CONTINUOUS BAR RUN FOR NEGATIVE MOMENT REGIONS, SEE "PLAN OF SPAN" SHEETS



DECK DRAIN DETAILS

† DRAIN TO EXTEND 2" BELOW BOTTOM OF GIRDER (SEE "PLAN OF SPANS" FOR LOCATION OF DECK DRAINS)

TOP OF FLOOR DRAIN TO BE SET 3/8" BELOW SURFACE OF SLAB.

4 - 1/2" SQUARE LUGS TO BE GLUED TO THE PVC PLASTIC PIPE AT EQUAL SPACES AROUND THE PIPE DRAIN APPROXIMATELY 4" FROM THE TOP OF THE PIPE.

THE 6" DIA. PVC PLASTIC PIPE AND FITTINGS SHALL BE SCHEDULE 40 AND CONFORM TO ASTM D1785.

PROJECT NO. U-5748

WAKE COUNTY

STATION: 24+88.00 -L-

SHEET 2 OF 3

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NORTH CAROLINA PROFESSIONAL ENGINEER SEAL 030474 JOHN C. MORRISON

DocuSigned by: John C. Morrison A2FDE142C82F4AB... 2/10/2023

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

SUPERSTRUCTURE

TYPICAL SECTIONS

(SOUTHBOUND LANES)

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S2-11
1			3			TOTAL SHEETS
2			4			119

DRAWN BY :	M. CATER	DATE : 10/2020
CHECKED BY :	J.C. MORRISON	DATE : 12/2022
DESIGNED BY :	D. RITACCO	DATE : 09/2020
DESIGN CHECKED BY :	J.C. MORRISON	DATE : 10/2020

USER: cafrm
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