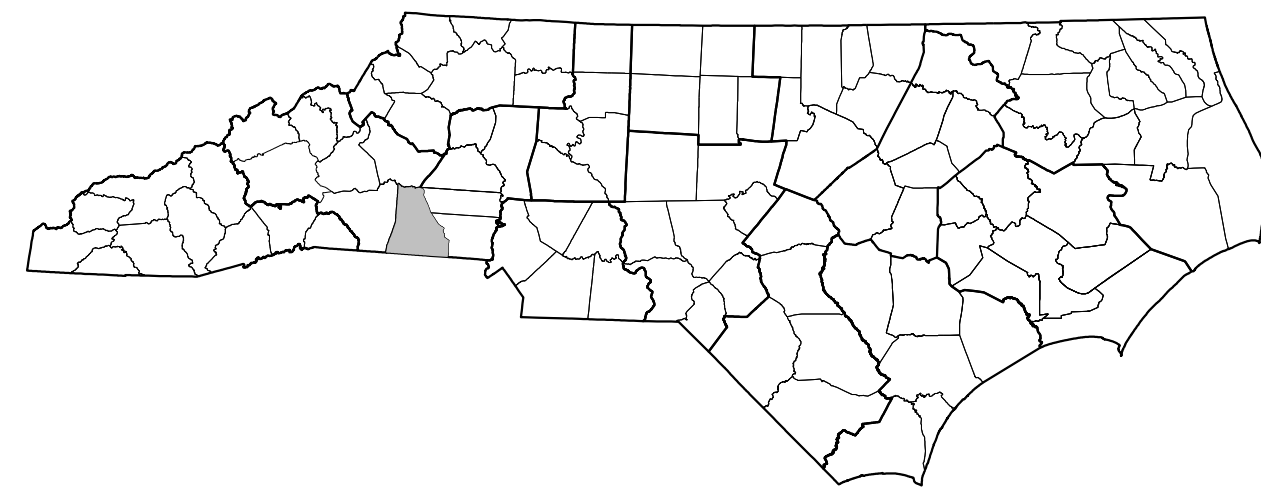


STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

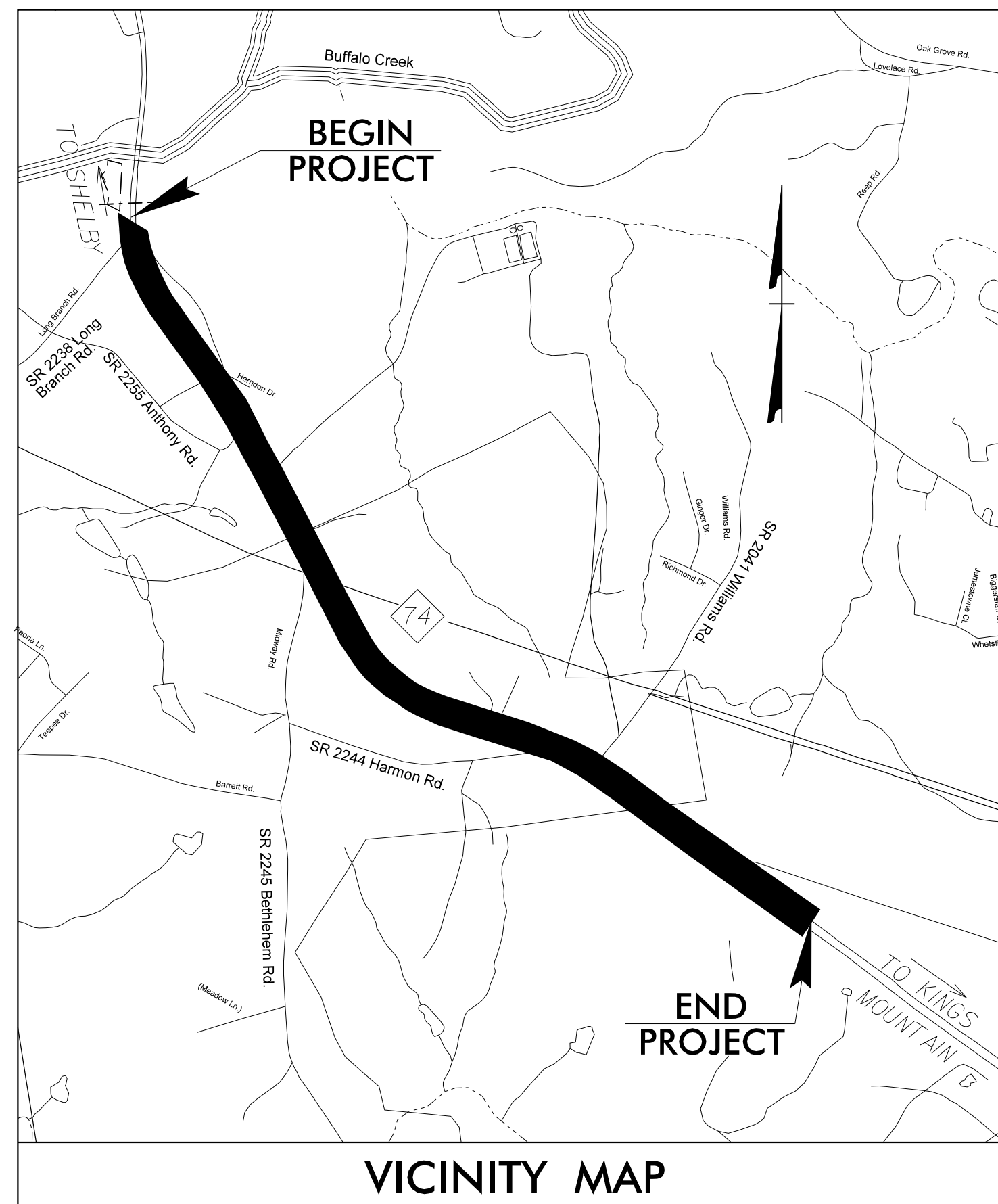
TRANSPORTATION MANAGEMENT PLAN

CLEVELAND COUNTY

DIVISION 12



**US 74 FROM EXISTING US 74 WEST OF SR 2238
(LONG BRANCH ROAD) TO WEST OF SR 1001**

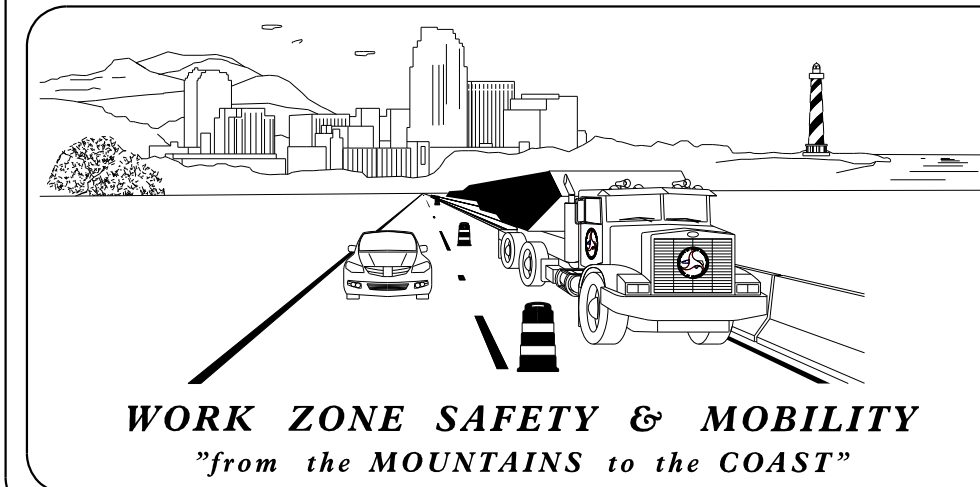


VICINITY MAP

SHEET NO.	TITLE
TMP-1	TITLE SHEET, VICINITY MAP, AND INDEX OF SHEETS
TMP-1A	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, AND LEGEND
TMP-1B, 1C	GENERAL NOTES
TMP-1D	SPECIAL SIGN DESIGNS
TMP-2	PORTABLE CONCRETE BARRIER AT TEMPORARY SHORING LOCATIONS
TMP-2A	TEMPORARY SHORING NOTES
TMP-3	TEMPORARY TRAFFIC CONTROL PHASING
TMP-4	-SR6- AND -SR7- PHASE 1 STEP 1A, 1B
TMP-5	-Y41- POTTS CREEK ROAD DETOUR ROUTE
TMP-6-15	-L- US 74 PHASE I STEP 1C
TMP-16	-SR9A- ANTHONY ROAD DETOUR ROUTE
TMP-17-19	-L- US 74 PHASE I STEP 2
TMP-20-22	-L- US 74 PHASE I STEP 3AB
TMP-23	-SR10- BETHLEHEM ROAD DETOUR ROUTE
TMP-24-24B	-L- US 74 PHASE I STEP 3C
TMP-24C-24E	-L- US 74 PHASE I STEP 3D
TMP-25-30	-L- US 74 PHASE II STEP 1
TMP-31-32	-L- US 74 PHASE II STEP 2
TMP-33	-L- US 74 PHASE III STEP 1
TMP-34-41	-L- US 74 PHASE III STEP 2

INDEX OF SHEETS

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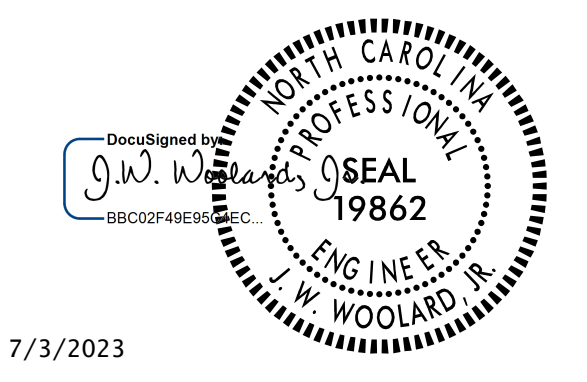
PLANS PREPARED BY:
 JAY W. WOOLARD, PE
 SENIOR TRANSPORTATION ENGINEER
 ANDREW N. GOOD
 TRANSPORTATION DESIGNER

NCDOT CONTACTS:
 THAD DUNCAN, PE
 NCDOT DIVISION 12
 PROJECT DESIGN ENGINEER



Stantec Consulting Services Inc.
 801 Jones Franklin Road, Suite 300
 Raleigh, NC 27606
 Tel. 919.851.6866
 Fax. 919.851.7024
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7/3/2023

TIP PROJECT: R-2707E

ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS" - PROJECT SERVICES UNIT - N.C. DEPARTMENT OF TRANSPORTATION - RALEIGH, N.C., DATED JANUARY 2018 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.01	WORK ZONE ADVANCE WARNING SIGNS
1101.02	TEMPORARY LANE CLOSURES
1101.03	TEMPORARY ROAD CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.06	WARNING SIGNS FOR BLASTING ZONES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW BOARDS
1130.01	DRUMS
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGING DEVICES
1160.01	TEMPORARY CRASH CUSHION
1165.01	WORK VEHICLE LIGHTING SYSTEMS AND TMA DELINEATION
1170.01	PORTABLE CONCRETE BARRIER
1180.01	SKINNY - DRUM
1205.01	PAVEMENT MARKINGS - LINE TYPES AND OFFSETS
1205.02	PAVEMENT MARKINGS - TWO LANE AND MULTILANE ROADWAYS
1205.12	PAVEMENT MARKINGS - BRIDGES
1261.01	GUARDRAIL AND BARRIER DELINEATORS - INSTALLATION SPACING
1261.02	GUARDRAIL AND BARRIER DELINEATORS - TYPES AND MOUNTING
1262.01	GUARDRAIL END DELINEATION
1264.01	OBJECT MARKERS - TYPES
1264.02	OBJECT MARKERS - INSTALLATION

TEMPORARY PAVEMENT MARKING SCHEDULE

SYMBOL	DESCRIPTION
PAINT PAVEMENT MARKING LINES (4")	
(P1)	- WHITE EDGELINE
(P3)	- 10 FT. WHITE SKIP
(P4)	- 3 FT.-9 FT./SP WHITE MINISKIP
(P10)	- YELLOW EDGELINE
(P13)	- YELLOW DOUBLE CENTER
PAINT PAVEMENT MARKING LINES (8")	
(P40)	- WHITE GORELINE
(P43)	- WHITE SOLID LANE LINE
PAINT PAVEMENT MARKING SYMBOLS	
(P71)	- RIGHT TURN ARROW
(P74)	- COMBO. RIGHT/STRAIGHT ARROW

LEGEND

GENERAL

- DIRECTION OF TRAFFIC FLOW
- EXIST. PVMT.
- PROPOSED PVMT.
- NORTH ARROW
- TEMP. SHORING (LOCATION PURPOSES ONLY)
- WORK AREA
- PREVIOUSLY STARTED / CONCURRENT CONSTRUCTION
- PAVEMENT REMOVAL
- TEMPORARY PAVEMENT

PAVEMENT MARKINGS

- EXISTING LINES
- EXISTING LINES
- EXISTING LINES
- EXISTING LINES
- EXISTING LINES
- EXISTING LINES
- EXISTING LINES
- EXISTING LINES
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PAVEMENT MARKING SYMBOLS

- EXISTING PAVEMENT MARKING SYMBOLS (HOLLOW)
- EXISTING PAVEMENT MARKING SYMBOLS (SOLID)
- EXISTING PAVEMENT MARKING ALPHANUMERIC CHARACTERS (HOLLOW)
- EXISTING PAVEMENT MARKING ALPHANUMERIC CHARACTERS (SOLID)

TRAFFIC CONTROL DEVICES

- | TEMPORARY DEVICES | PREVIOUSLY PLACED | |
|-------------------|-------------------|--------------------------------|
| | | BARRICADE (TYPE III) |
| | | CONE |
| | | DRUM |
| | | FLASHING ARROW BOARD |
| | | FLAGGER |
| | | LAW ENFORCEMENT |
| | | TRUCK MOUNTED ATTENUATOR (TMA) |
| | | CHANGEABLE MESSAGE SIGN |
| | | TEMPORARY CRASH CUSHION |
| | | PORTABLE CONCRETE BARRIER |

SIGNALS

- EXISTING
- PROPOSED
- TEMPORARY

TEMPORARY SIGNING

- | TEMPORARY SIGNS | PREVIOUSLY PLACED | |
|-----------------|-------------------|-----------------------------|
| | | PORTABLE SIGN |
| | | STATIONARY SIGN |
| | | STATIONARY OR PORTABLE SIGN |

PAVEMENT MARKERS

- CRYSTAL/CRYSTAL
- CRYSTAL/RED
- YELLOW/YELLOW

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GENERAL NOTES

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS, AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS OR RESULT IN DUPLICATE OR UNDESIRABLE OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING, OR REMOVAL OF DEVICES AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT EXCEPT WHEN OTHERWISE NOTED IN THE PLAN OR DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
-L- (US 74)	6:00 A.M.-8:00 P.M. MONDAY THRU SUNDAY

B) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS:

ROAD NAME
-L- (US 74)
ALL OTHER ROADS

HOLIDAY

- 1) FOR ANY UNEXPECTED OCCURRENCE THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
- 2) FOR NEW YEAR'S, BETWEEN THE HOURS OF 6:00 A.M. DECEMBER 31ST TO 8:00 P.M. JANUARY 2ND. IF NEW YEAR'S DAY IS ON A FRIDAY, SATURDAY, SUNDAY, OR MONDAY THEN UNTIL 8:00 P.M. THE FOLLOWING TUESDAY.
- 3) FOR EASTER, BETWEEN THE HOURS OF 6:00 A.M. THURSDAY AND 8:00 P.M. MONDAY.
- 4) FOR MEMORIAL DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY TO 8:00 P.M. TUESDAY.
- 5) FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 6:00 A.M. THE DAY BEFORE INDEPENDENCE DAY AND 8:00 P.M. THE DAY AFTER INDEPENDENCE DAY. IF INDEPENDENCE DAY IS ON A FRIDAY, SATURDAY, SUNDAY OR MONDAY; THEN BETWEEN THE HOURS OF 6:00 A.M. THE THURSDAY BEFORE INDEPENDENCE DAY AND 8:00 P.M. THE TUESDAY AFTER INDEPENDENCE DAY.
- 6) FOR LABOR DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY AND 8:00 P.M. TUESDAY.
- 7) FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 6:00 A.M. TUESDAY TO 8:00 P.M. MONDAY.
- 8) FOR CHRISTMAS, BETWEEN THE HOURS OF 6:00 A.M. THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 8:00 P.M. THE FOLLOWING TUESDAY AFTER THE WEEK OF CHRISTMAS.
- 9) FOR THE AMERICAN LEGION BASEBALL WORLD SERIES, BETWEEN THE HOURS 6:00 AM THE WEDNESDAY IN AUGUST BEFORE THE START OF THE AMERICAN LEGION BASEBALL WORLD SERIES AND 8:00 PM THE WEDNESDAY AFTER THE AMERICAN LEGION BASEBALL WORLD SERIES.
- 10) FOR THE CLEVELAND COUNTY FAIR, BETWEEN THE HOURS OF 6:00 AM THE DAY BEFORE THE BEGINNING OF THE CLEVELAND COUNTY FAIR AND 8:00 PM THE DAY AFTER THE END OF THE CLEVELAND COUNTY FAIR.

C) DO NOT STOP TRAFFIC OR USE ON SITE RAMP DETOURS AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS	DURATION AND OPERATION
-L- US 74	MONDAY THRU SUNDAY 5:00 A.M. TO 9:00 P.M	30 MINUTES FOR OVERHEAD GIRDER INSTALLATION
-L- US 74	MONDAY THRU SUNDAY 6:00 AM TO 7:00 PM	15 MINUTES FOR TRAFFIC SHIFTS

LANE AND SHOULDER CLOSURE REQUIREMENTS

- D) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED OR AS DIRECTED BY THE ENGINEER.
- E) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 15 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR A LANE CLOSURE IS INSTALLED.
- F) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- G) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRANSPORTATION MANAGEMENT PLANS, ROADWAY STANDARD DRAWINGS, OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.
- H) DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY, RAMP, OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.
- I) DO NOT INSTALL MORE THAN (2 MILES) OF LANE CLOSURE ON -L- (US 74) MEASURED FROM THE BEGINNING OF THE MERGE TAPER TO THE END OF THE LANE CLOSURE.
- J) DO NOT INSTALL MORE THAN ONE LANE CLOSURE IN ANY ONE DIRECTION ON -L- (US 74).

PAVEMENT EDGE DROP OFF REQUIREMENTS

- K) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPEN TRAVEL LANE THAT HAS A DROP-OFF AS FOLLOWS:
 - BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.
 - BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.
 - BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.
- L) DO NOT EXCEED A DIFFERENCE OF 2 INCHES IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC FOR NOMINAL LIFTS OF 1.5 INCHES. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) 500 FT IN ADVANCE AND A MINIMUM OF ONCE EVERY HALF MILE THROUGHOUT THE UNEVEN AREA.


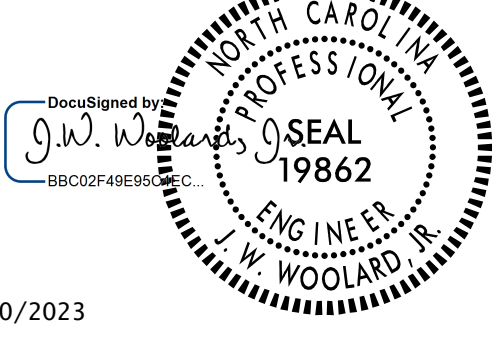
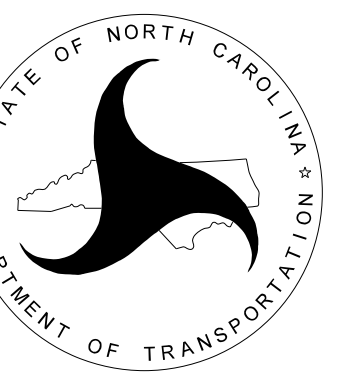
TRAFFIC PATTERN ALTERATIONS

- M) NOTIFY THE ENGINEER THIRTY (30) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

- N) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 40 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.
- O) PROVIDE SIGNING AND DEVICES REQUIRED TO CLOSE THE ROAD ACCORDING TO THE ROADWAY STANDARD DRAWINGS AND TRANSPORTATION MANAGEMENT PLANS.
 - PROVIDE SIGNING REQUIRED FOR THE OFF-SITE DETOUR ROUTE AS SHOWN IN THE TRANSPORTATION MANAGEMENT PLANS.
- P) COVER OR REMOVE ALL SIGNS AND DEVICES REQUIRED TO CLOSE THE ROAD WHEN ROAD CLOSURE IS NOT IN OPERATION.
 - COVER OR REMOVE ALL SIGNS REQUIRED FOR THE OFF-SITE DETOUR WHEN THE DETOUR IS NOT IN OPERATION.
- Q) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.
- R) INSTALL BLACK ON ORANGE "DIP" SIGNS (W8-2) AND/OR "BUMP" SIGNS (W8-1) 500 FT IN ADVANCE OF THE UNEVEN AREA, OR AS DIRECTED BY THE ENGINEER.

4/20/2023
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GENERAL NOTES

TRAFFIC BARRIER

S) INSTALL TEMPORARY BARRIER ACCORDING TO THE TRANSPORTATION MANAGEMENT PLANS A MAXIMUM OF TWO (2) WEEKS PRIOR TO BEGINNING WORK IN ANY LOCATION. ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION PROCEED IN A CONTINUOUS MANNER TO COMPLETE THE PROPOSED WORK IN THAT LOCATION UNLESS OTHERWISE STATED IN THE TRANSPORTATION MANAGEMENT PLANS OR AS DIRECTED BY THE ENGINEER.

DO NOT PLACE BARRIER DIRECTLY ON ANY SURFACE OTHER THAN ASPHALT OR CONCRETE.

ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION AND NO WORK IS PERFORMED BEHIND THE TEMPORARY BARRIER FOR A PERIOD LONGER THAN TWO (2) MONTHS, REMOVE/RESET TEMPORARY BARRIER AT NO COST TO THE DEPARTMENT UNLESS OTHERWISE STATED IN THE TRANSPORTATION MANAGEMENT PLANS, TEMPORARY BARRIER IS PROTECTING A HAZARD, OR AS DIRECTED BY THE ENGINEER.

INSTALL TEMPORARY BARRIER WITH THE TRAFFIC FLOW BEGINNING WITH THE UPSTREAM SIDE OF TRAFFIC. REMOVE TEMPORARY BARRIER AGAINST THE TRAFFIC FLOW BEGINNING WITH THE DOWNSTREAM SIDE OF TRAFFIC.

INSTALL AND SPACE DRUMS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH) TO CLOSE OR KEEP THE SECTION OF THE ROADWAY CLOSED UNTIL THE TEMPORARY BARRIER CAN BE PLACED OR AFTER THE TEMPORARY BARRIER IS REMOVED.

T) PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER AT ALL TIMES DURING THE INSTALLATION AND REMOVAL OF THE BARRIER BY EITHER A TRUCK MOUNTED ATTENUATOR (MAXIMUM 72 HOURS) OR A TEMPORARY CRASH CUSHION.

PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER FROM ONCOMING TRAFFIC AT ALL TIMES BY A TEMPORARY CRASH CUSHION UNLESS THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER IS OFFSET FROM ONCOMING TRAFFIC AS FOLLOWS OR AS SHOWN IN THE PLANS:

POSTED SPEED LIMIT	MINIMUM OFFSET
40 OR LESS	15 FT
45-50	20 FT
55	25 FT
60 MPH or HIGHER	30 FT

TRAFFIC CONTROL DEVICES

U) WHEN LANE CLOSURES ARE NOT IN EFFECT SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER IN FEET THAN TWICE THE POSTED SPEED LIMIT (MPH), EXCEPT 10 FT ON-CENTER IN RADII, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY. REFER TO STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES SECTIONS 1130 (DRUMS) FOR ADDITIONAL REQUIREMENTS.

V) PLACE TYPE III BARRICADES WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY.

W) PLACE ADDITIONAL SETS OF THREE CHANNELIZING DEVICES DRUMS PERPENDICULAR TO THE EDGE OF TRAVELWAY ON (500 FT) CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC.

PAVEMENT MARKINGS AND MARKERS

X) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:

ROAD NAME	MARKING	MARKER
ALL ROADS	PAINT	NONE
BRIDGES	COLD APPLIED PLASTIC TYPE IV	TEMPORARY RAISED

Y) PLACE ONE APPLICATION OF PAINT FOR TEMPORARY TRAFFIC PATTERNS. PLACE A SECOND APPLICATION OF PAINT SIX (6) MONTHS AFTER THE INITIAL APPLICATION AND EVERY SIX MONTHS AS DIRECTED BY THE ENGINEER.

Z) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.

AA) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS BY THE END OF EACH DAY'S OPERATION.

BB) TRACE THE (EXISTING AND/OR PROPOSED) MONOLITHIC ISLAND LOCATIONS WITH THE PROPER COLOR PAVEMENT MARKING PRIOR TO (REMOVAL AND/OR INSTALLATION). PLACE DRUMS TO DELINEATE ANY (EXISTING AND/OR PROPOSED) MONOLITHIC ISLANDS (AFTER REMOVAL AND/OR BEFORE INSTALLATION).

MISCELLANEOUS

CC) IN THE EVENT A TIE-IN CANNOT BE MADE IN ONE DAY'S TIME, BRING THE TIE-IN AREA TO AN APPROPRIATE ROADWAY ELEVATION AS DETERMINED BY THE ENGINEER. PLACE BLACK ON ORANGE "LOOSE GRAVEL" SIGNS (W8-7) AND BLACK ON ORANGE "PAVEMENT ENDS" SIGNS (W8-3) (500 FT) AND (1000 FT) RESPECTIVELY IN ADVANCE OF THE UNEVEN AREAS. USE DRUMS TO DELINEATE THE EDGE OF ROADWAY ALONG UNPAVED AREAS.

DD) ALL CURB RAMP LOCATIONS SHALL BE DERIVED FROM STATIONING SHOWN ON PAVEMENT MARKING PLANS OR AS DIRECTED BY THE ENGINEER IN COORDINATION WITH THE SIGNING AND DELINEATION UNIT.

EE) CONTRACTOR SHALL MAINTAIN SIDEWALK ACCESS AT ALL TIMES AS STATED IN THE PHASING. CONTRACTOR SHALL BE RESPONSIBLE TO PROVIDE TEMPORARY SIDEWALKS (CONCRETE, ASPHALT, OR OTHER SUITABLE MATERIAL AS APPROVED BY THE ENGINEER) AT ALL LOCATIONS WHERE THE OPEN PEDESTRIAN TRAVELWAY HAS BEEN REMOVED FOR CONSTRUCTION OPERATIONS (UTILITIES, DRAINAGE, ETC.).

FF) MAINTAIN VEHICULAR ACCESS TO ALL DRIVEWAYS DURING THE LIFE OF THE CONTRACT, UNLESS OTHERWISE NOTED IN THE PHASING OR DIRECTED BY THE ENGINEER. USE INCIDENTAL STONE WHEN NECESSARY.

GG) ALL DIMENSIONS AND STATIONS IN THE TRANSPORTATION MANAGEMENT PLAN AND PHASING ARE APPROXIMATE (+/-); FIELD ADJUST AS NECESSARY OR AS DIRECTED BY THE ENGINEER.

HH) COMPLETE ANY PROPOSED OR TEMPORARY WIDENING IN SUCH A MANNER THAT PONDING OF WATER WILL NOT OCCUR IN THE TRAVEL LANE.

II) ENSURE THE OVERSIZE/OVERWEIGHT PERMIT UNIT (919) 814-3700 HAS BEEN ADVISED OF THE ONGOING TRAFFIC OPERATIONS THROUGH THE DIVISION OFFICE.


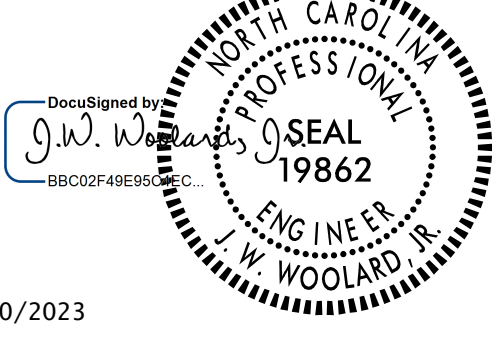
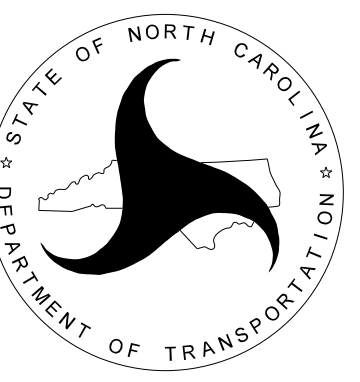
JJ) MAINTAIN EXISTING GUARDRAIL UNTIL PROPOSED FILL SLOPES ARE COMPLETED TO THE POINT THAT FIELD CONDITIONS NO LONGER MEET GUARDRAIL WARRANTS AS DIRECTED BY THE ENGINEER.

KK) MAINTAIN ALL EXISTING SIGNING ON PROJECT (WARNING, REGULATORY AND GUIDE SIGNS). WHERE CONSTRUCTION AFFECTS THE LOCATION OF A SIGN, RELOCATE AS NECESSARY, OR INSTALL REPLACEMENT SUCH THAT THE FUNCTION OF THE SIGN IS MAINTAINED AT ALL TIMES. DURING RELOCATION OF STOP SIGNS PROVIDE FLAGGERS WITH "FLAGGER AHEAD" (W20-7a) AND "BE PREPARED TO STOP" (W3-4) SIGNS AS NECESSARY TO MAINTAIN INTERSECTION TRAFFIC.

LL) DRAINAGE CONSTRUCTION SHOWN IN THE TRANSPORTATION MANAGEMENT PLAN IS A GENERAL REPRESENTATION OF WORK TO BE PERFORMED DURING A PARTICULAR PHASE OF CONSTRUCTION. REFER TO THE ROADWAY PLANS FOR DRAINAGE ITEMS. IN THE EVENT THERE IS A DISCREPANCY BETWEEN WHAT IS SHOWN IN THE TRANSPORTATION MANAGEMENT PLAN AND THE ROADWAY PLAN, THE ROADWAY PLAN SHALL GOVERN OVER THE TRANSPORTATION MANAGEMENT PLAN.

MM) IN THE EVENT OF FALLING DEBRIS OR WATER USE LAW ENFORCEMENT TO DIRECT TRAFFIC BELOW THE BRIDGE.

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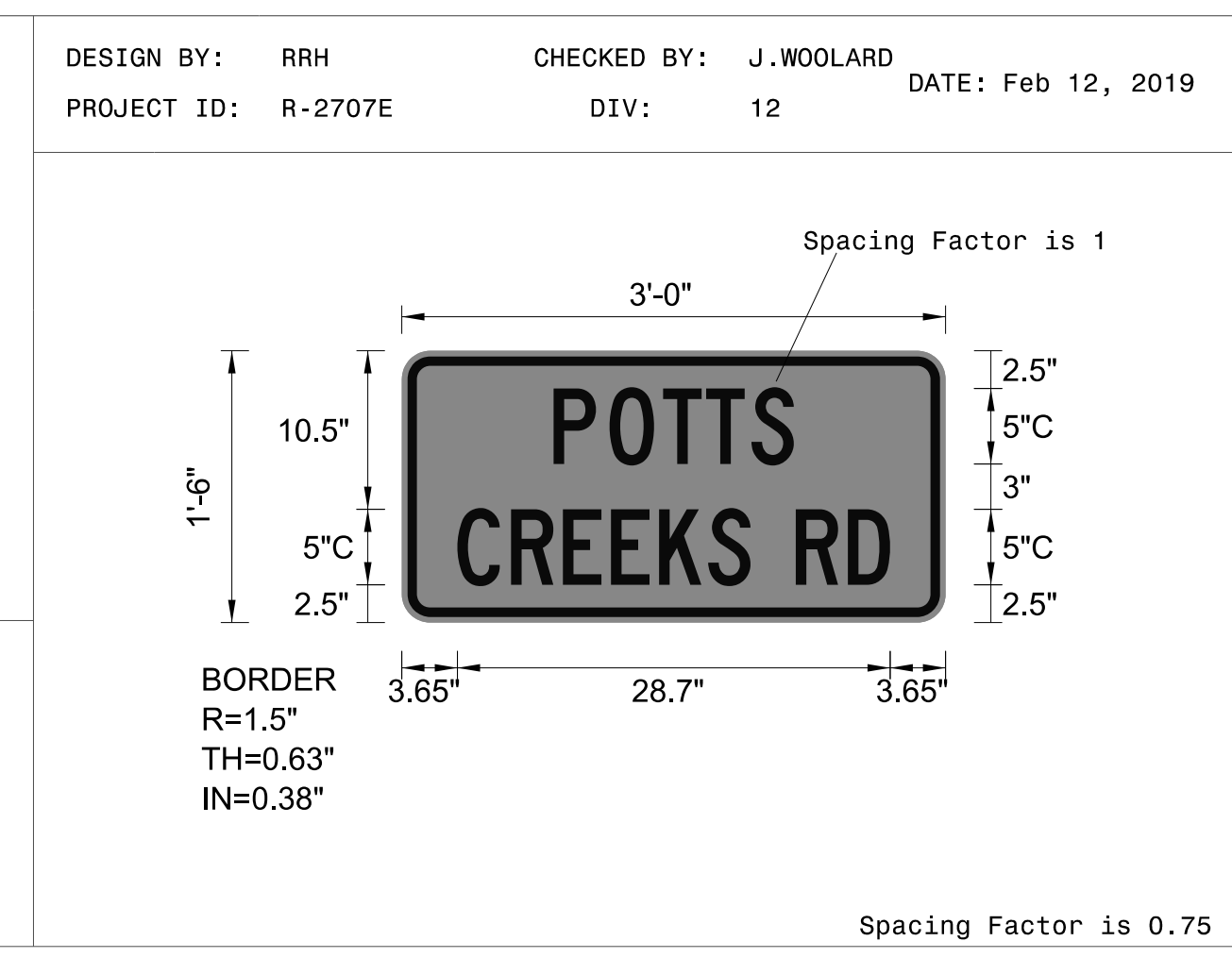
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SIGN NUMBER: SD-1
 TYPE: STATIONARY
 QUANTITY: SEE PLANS
 SIGN WIDTH: 36"
 HEIGHT: 18"
 TOTAL AREA: 4.5 Sq.Ft.
 BORDER TYPE: RECESSED
 RADII: 1.5"
 WIDTH: 0.63"
 RECESS: 0.38"
 NO. Z BARS:
 LENGTH:

BACKG COLOR: Fluorescent Orange
 COPY COLOR: Black

SYMBOL	X	Y	WID	HT

MAT'L: 0.080" (2.0MM) ALUMINUM



USE NOTES
 1. Legend and border shall be direct applied black non-reflective sheeting.
 2. Background shall be NC GRADE B Fluorescent Orange retroreflective sheeting.

LETTER POSITIONS

Letter spacings are to start of next letter

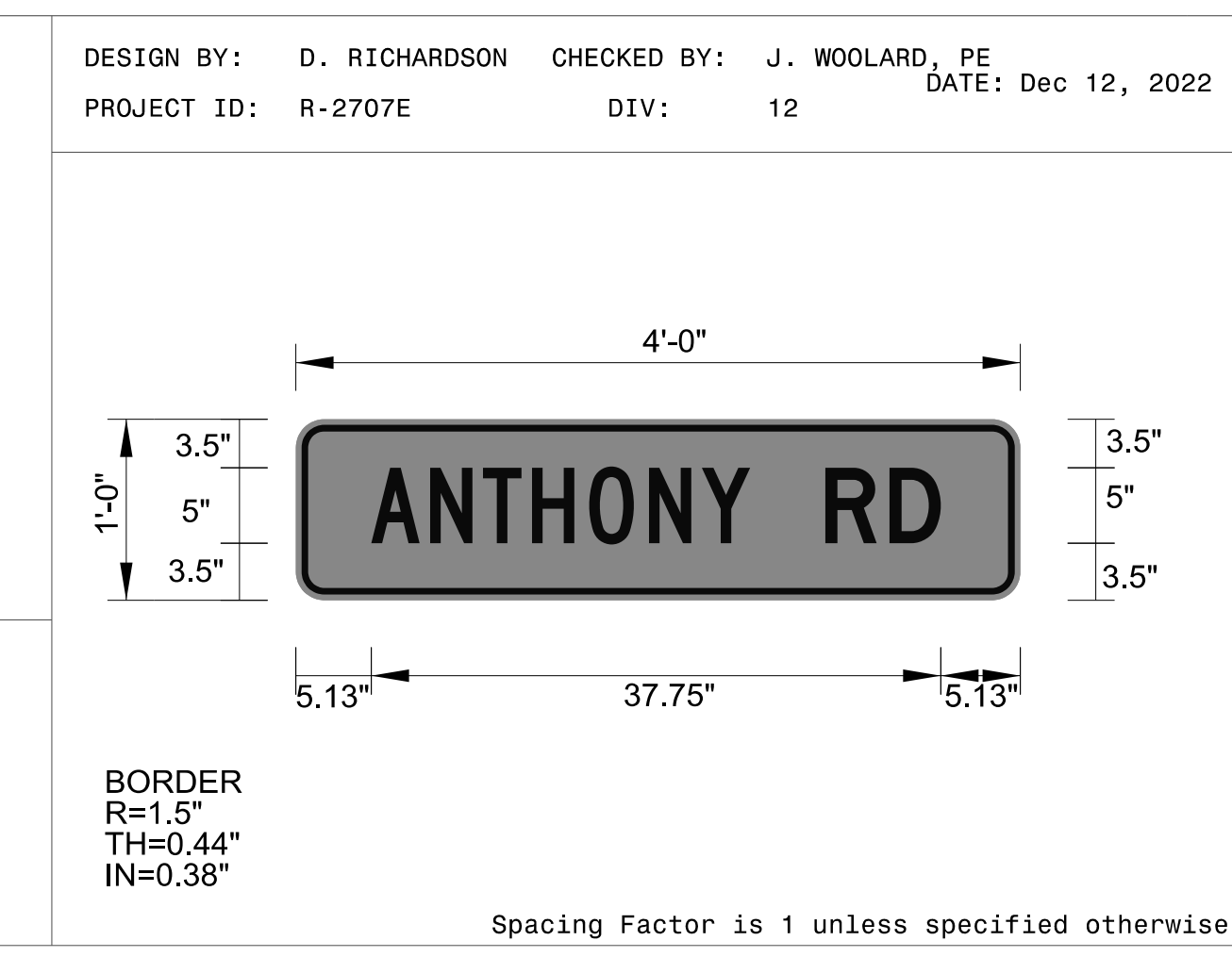
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10	3.7	3.5	2.9	3	2.8	10															C 2000	
																						15.9
	C	R	E	E	K	S		R	D												C 2000	
	3.7	3.5	3.4	3.2	3.2	3.2	2.8	3	3.4	2.8	3.7											28.7

SIGN NUMBER: SD-1
 TYPE: STATIONARY
 QUANTITY: SEE PLANS
 SIGN WIDTH: 4'-0"
 HEIGHT: 1'-0"
 TOTAL AREA: 4.0 Sq.Ft.
 BORDER TYPE: RECESSED
 RADII: 1.5"
 WIDTH: 0.44"
 RECESS: 0.38"
 NO. Z BARS:
 LENGTH:

BACKG COLOR: Fluorescent Orange
 COPY COLOR: Black

SYMBOL	X	Y	WID	HT

MAT'L: 0.063" (1.6MM) ALUMINUM



USE NOTES
 1. Legend and border shall be direct applied black non-reflective sheeting.
 2. Background shall be NC GRADE B Fluorescent Orange retroreflective sheeting.

LETTER POSITIONS

Letter spacings are to start of next letter

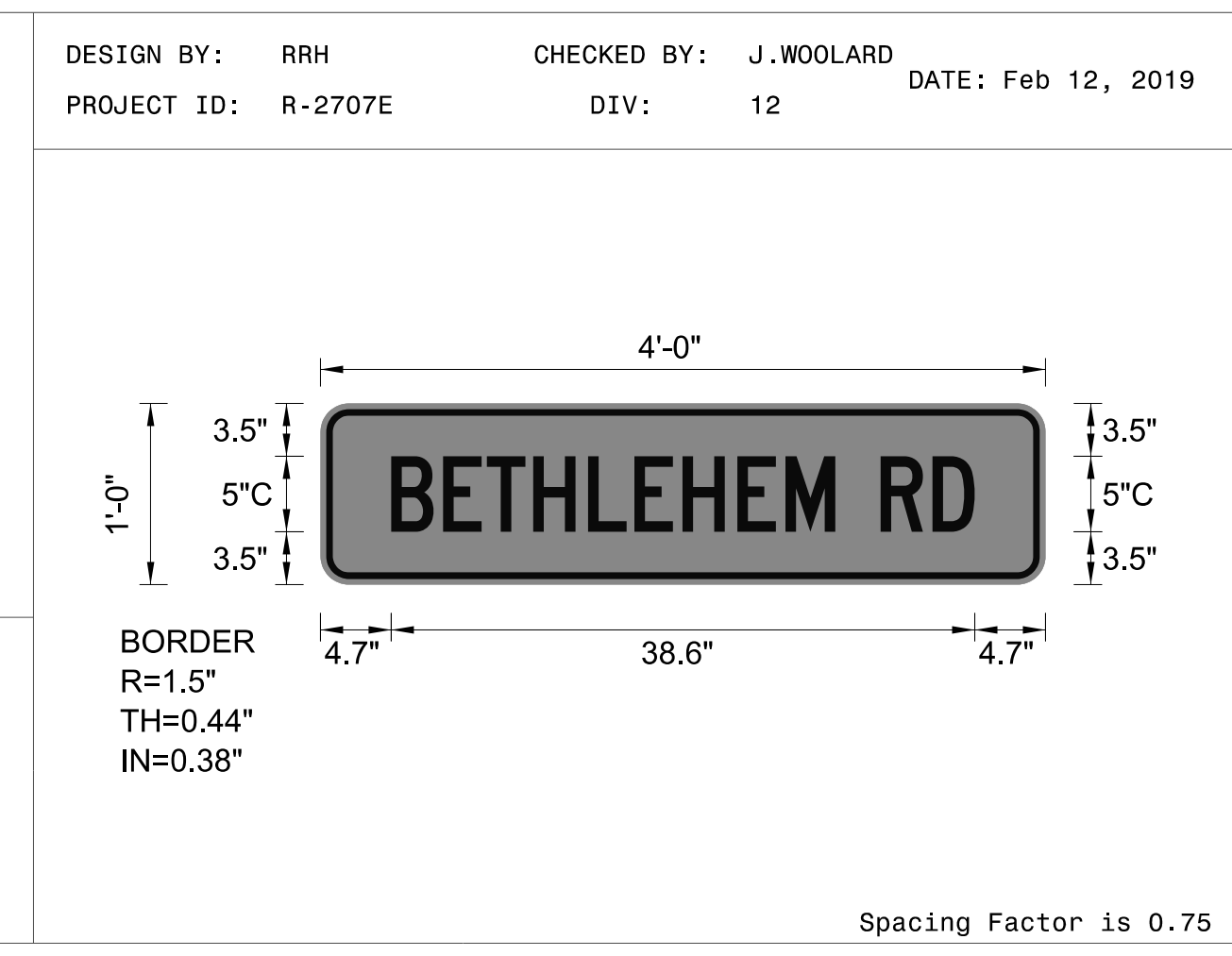
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	5.1	3.9	3.5	3.3	3.8	3.9	3.5	3.2	5	4.3	3.4	5.1										C 2000,D 2000
																						37.8

SIGN NUMBER: SD-2
 TYPE: STATIONARY
 QUANTITY: SEE PLANS
 SIGN WIDTH: 48"
 HEIGHT: 12"
 TOTAL AREA: 4.0 Sq.Ft.
 BORDER TYPE: RECESSED
 RADII: 1.5"
 WIDTH: 0.44"
 RECESS: 0.38"
 NO. Z BARS:
 LENGTH:

BACKG COLOR: Fluorescent Orange
 COPY COLOR: Black

SYMBOL	X	Y	WID	HT

MAT'L: 0.125" (3.2MM) ALUMINUM



USE NOTES
 1. Legend and border shall be direct applied black non-reflective sheeting.
 2. Background shall be NC GRADE B Fluorescent Orange retroreflective sheeting.

LETTER POSITIONS

Letter spacings are to start of next letter

	B	E	T	H	L	E	H	E	M	R	D										Series/Size Text Length	
	4.7	3.4	2.9	3.1	3.6	3.1	3.2	3.6	3.2	3.3	3	3.4	2.8	4.7								C 2000
																						38.6

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<p>Stantec Consulting Services Inc. 801 Jones Franklin Road Suite 300 Raleigh, NC 27606 Tel. 919.851.6866 Fax. 919.851.7024 www.stantec.com License No. F-0672</p>	<p>4/20/2023</p> <p>DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED</p>		<p>SPECIAL SIGN DESIGNS</p>
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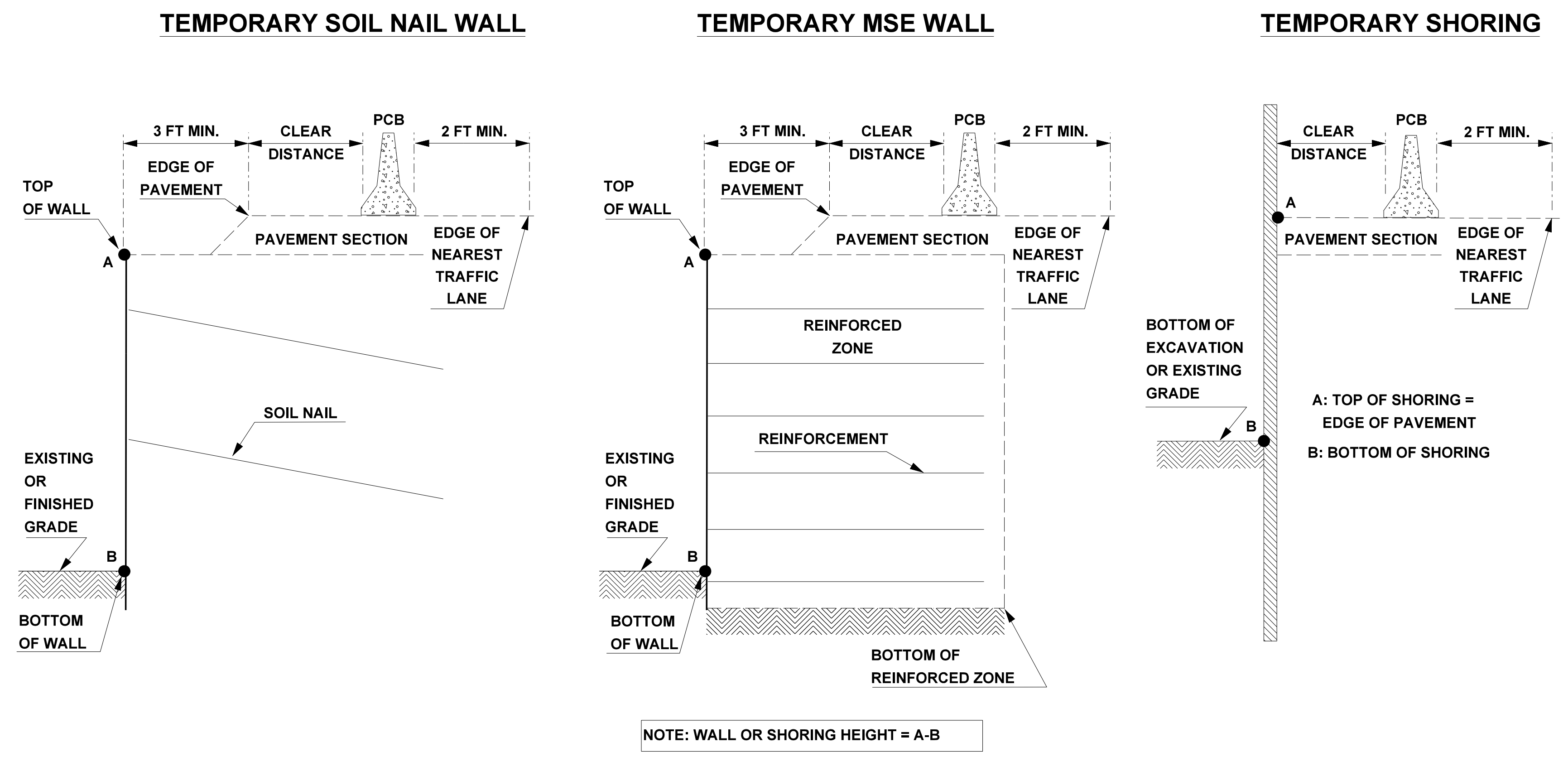


FIGURE A

NOTES

- 1- REFER TO THE TRAFFIC CONTROL PLANS FOR TEMPORARY SHORING LOCATIONS AND NOTES.
- 2- REFER TO THE "TEMPORARY SHORING" STANDARD PROVISION FOR INFORMATION ABOUT TEMPORARY SHORING AND PORTABLE CONCRETE BARRIER (PCB).
- 3- PCB IS REQUIRED IF TEMPORARY SHORING/WALL IS LOCATED WITHIN THE CLEAR ZONE IN ACCORDANCE WITH THE AASHTO ROADSIDE DESIGN GUIDE. DO NOT PLACE BARRIER DIRECTLY ON ANY SURFACE OTHER THAN ASPHALT OR CONCRETE. (CONTACT NCDOT PAVEMENT MANAGEMENT FOR APPLICABLE PAVEMENT DESIGN).
- 4- BASED ON THE CLEAR DISTANCE, OFFSET, DESIGN SPEED AND PAVEMENT TYPE, CHOOSE AN UNANCHORED OR ANCHORED PCB FROM THE TABLE SHOWN IN FIGURE B. CLEAR DISTANCE IS DEFINED AS SHOWN IN FIGURE A AND OFFSET IS DEFINED AS SHOWN IN FIGURE B.
- 5- AT THE CONTRACTOR'S OPTION OR IF THE MINIMUM REQUIRED CLEAR DISTANCE IS NOT AVAILABLE, SET PCB NEXT TO AND UP AGAINST THE TRAFFIC SIDE OF THE TEMPORARY SHORING/WALLS EXCEPT FOR BARRIER ABOVE TEMPORARY WALLS. PCB WITH THE MINIMUM REQUIRED CLEAR DISTANCE IS REQUIRED ABOVE TEMPORARY WALLS.
- 6- USE NCDOT PORTABLE CONCRETE BARRIER (PCB) IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1170.01 AND SECTION 1170 OF THE STANDARD SPECIFICATIONS.
- 7- SET PCB WITH A MINIMUM HORIZONTAL DISTANCE OF 2 FT BETWEEN THE FRONT FACE OF THE BARRIER AND THE EDGE OF THE NEAREST TRAFFIC LANE AS SHOWN IN FIGURE A UNLESS OTHERWISE SHOWN IN THE PLANS OR APPROVED BY THE ENGINEER.
- 8- FOR PCB ABOVE AND BEHIND TEMPORARY WALLS, PROVIDE A MINIMUM DISTANCE OF 3 FT BETWEEN THE EDGE OF PAVEMENT AND THE WALL FACE AS SHOWN IN FIGURE A. IF THIS MINIMUM REQUIRED DISTANCE IS NOT AVAILABLE, CONTACT THE ENGINEER.
- 9- TABLE SHOWN IN FIGURE B IS BASED ON NCDOT RESEARCH PROJECT NO. 2005-010 WITH VEHICLE TYPE USED FOR NCHRP 350 CRASH TESTS.

MINIMUM REQUIRED CLEAR DISTANCE, inches

Barrier Type	Pavement Type	Offset * ft	Design Speed, mph					
			<30	31-40	41-50	51-60	61-70	71-80
Unanchored PCB	Asphalt	<8	24	26	29	32	36	40
		8-14	26	28	31	35	38	42
		14-20	27	29	34	36	39	43
		20-26	28	31	35	38	40	44
		26-32	29	32	36	39	42	45
		32-38	30	34	38	41	43	46
		38-44	31	34	41	43	45	48
	Concrete	44-50	31	35	41	43	46	49
		50-56	32	36	42	44	47	50
		>56	32	36	42	45	47	51
		<8	17	18	21	22	25	26
		8-14	19	20	23	25	26	29
		14-20	22	22	24	26	28	31
		20-26	23	24	26	27	30	34
Anchored PCB	Asphalt	All Offsets	24 for All Design Speeds					
		Concrete (including bridge approach slabs)	All Offsets	12 for All Design Speeds				

* See Figure Below

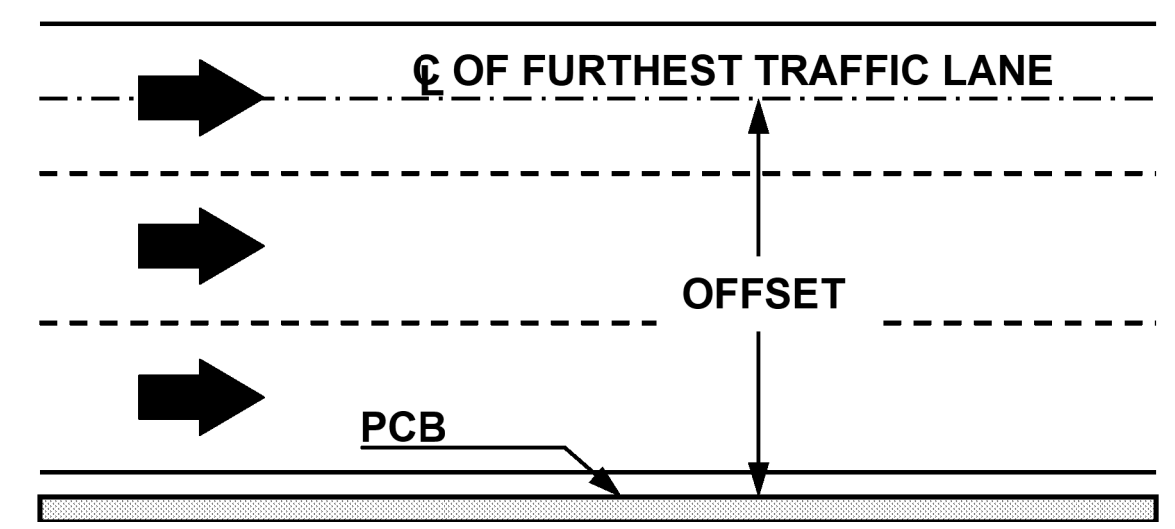
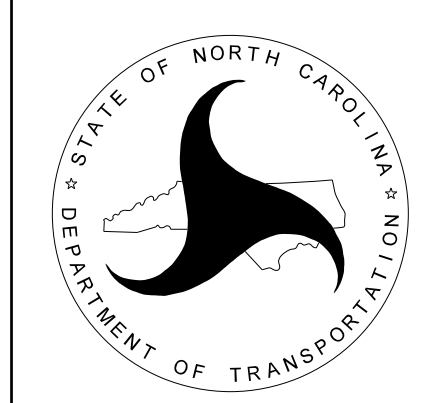


FIGURE B

DETAIL PROVIDED BY NCDOT



PORTABLE CONCRETE BARRIER AT TEMPORARY SHORING LOCATIONS

TEMPORARY SHORING LOCATION NO. 01 SEE SHEETS TMP-18-19
 ESTIMATED QUANTITY = 4323.0 SF

-L- STA. 897+00, 28.0' LT TO -L- STA. 908+00, 28.0' LT
 LENGTH=1100.0' AVERAGE HEIGHT = 3.93' MAX HEIGHT = 5.52'

- FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.
- BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OR SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.
- DESIGN TEMPORARY SHORING FROM -L- STATION 897+00, 28' LT TO STATION 908+00, 28' LT FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:

UNIT WEIGHT, (γ) = 120 LB/CF
 FRICTION ANGLE, (ϕ) = 30 DEGREES
 COHESION, c = 0 LB/SF
 GROUNDWATER ELEVATION = N/A
- AT THE CONTRACTOR'S OPTION, USE STANDARD TEMPORARY SHORING FOR TEMPORARY SHORING FROM -L- STATION 897+00, 28' LT TO STATION 908+00, 28' LT. SEE GEOTECHNICAL STANDARD DETAIL NO. 1801.01 FOR STANDARD TEMPORARY SHORING.

TEMPORARY SHORING LOCATION NO. 02 SEE SHEETS TMP-24D
 ESTIMATED QUANTITY = 336.0 SF

-L- STA. 892+23, 9.0' LT TO -L- STA. 892+65, 9.0' LT
 LENGTH=42.0' AVERAGE HEIGHT = 8.00' MAX HEIGHT = 8.00'

- FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.
- BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OR SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.
- DESIGN TEMPORARY SHORING FROM -L- STATION 892+23, 9' LT TO STATION 892+65, 9' LT FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:

UNIT WEIGHT, (γ) = 120 LB/CF
 FRICTION ANGLE, (ϕ) = 30 DEGREES
 COHESION, c = 0 LB/SF
 GROUNDWATER ELEVATION = N/A
- AT THE CONTRACTOR'S OPTION, USE STANDARD TEMPORARY SHORING FOR TEMPORARY SHORING FROM -L- STATION 892+23, 9' LT TO STATION 892+65, 9' LT. SEE GEOTECHNICAL STANDARD DETAIL NO. 1801.01 FOR STANDARD TEMPORARY SHORING.

TEMPORARY SHORING LOCATION NO. 03 SEE SHEETS TMP-24D
 ESTIMATED QUANTITY = 336.0 SF

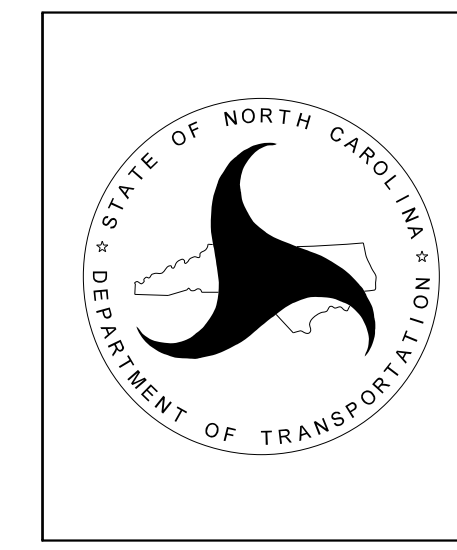
-L- STA. 892+23, 9.0' RT TO -L- STA. 892+65, 9.0' RT
 LENGTH=42.0' AVERAGE HEIGHT = 8.00' MAX HEIGHT = 8.00'

- FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.
- BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OR SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.
- DESIGN TEMPORARY SHORING FROM -L- STATION 892+23, 9' RT TO STATION 892+65, 9' RT FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:

UNIT WEIGHT, (γ) = 120 LB/CF
 FRICTION ANGLE, (ϕ) = 30 DEGREES
 COHESION, c = 0 LB/SF
 GROUNDWATER ELEVATION = N/A
- AT THE CONTRACTOR'S OPTION, USE STANDARD TEMPORARY SHORING FOR TEMPORARY SHORING FROM -L- STATION 892+23, 9' RT TO STATION 892+65, 9' RT. SEE GEOTECHNICAL STANDARD DETAIL NO. 1801.01 FOR STANDARD TEMPORARY SHORING.

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THE TEMPORARY SHORING NOTES SHOWN ON THIS SHEET WERE PROVIDED THROUGH A SEALED DOCUMENT FROM THE GEOTECHNICAL ENGINEER. THE DOCUMENT WAS SUBMITTED TO STANTEC CONSULTING ON MARCH 7, 2023 AND SEALED BY A PROFESSIONAL ENGINEER, (JEREMY R. HAMM), LICENSE #039779.



TEMPORARY SHORING NOTES

PHASING

PHASE I (TMP-4 THRU TMP-24)

PRIOR TO ANY WORK OPERATIONS, INSTALL WORK ZONE ADVANCE WARNING SIGNS ON -L- US 74 AND ALL -Y- LINES, PER ROADWAY STANDARD DRAWING 1101.01, SHEET 3 OF 3.

STEP 1 (SEE TMP-4 THRU TMP-16)

A) (-SR6-, -Y41-)

- USING RSD 1101.02, SHEET 1 OF 14 AS NECESSARY, BEGIN CONSTRUCTION OF -SR6- AS SHOWN ON TMP-4.

- USING RSD 1101.03, SHEET 1 OF 9 AND TMP-4, PERFORM THE FOLLOWING:

- 1- INSTALL AND COVER DETOUR ROUTE SIGNS AS SHOWN ON TMP-5
- 2- UNCOVER DETOUR ROUTE SIGNS, PLACE TRAFFIC ONTO DETOUR ROUTE, AND CLOSE -Y41- (POTTS CREEK RD).
- 3- CONSTRUCT -Y41- POTTS CREEK RD UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE.
- 4- PLACE TEMPORARY PAVMENT MARKINGS IN THE FINAL PATTERN.
- 5- COVER/REMOVE ALL DETOUR SIGNS AND OPEN -Y41- AND -SR6- TO TRAFFIC SIMULTANEOUSLY AS SHOWN ON TMP-8 AND TMP-9.

B) (-SR7-, -Y46-)

- 1- USING RSD 1101.02, SHEET 1 OF 14 AS NECESSARY, BEGIN CONSTRUCTION OF -SR7- FROM 16+00± TO 56+00± AND 57+00± TO 82+00±.
- 2- USING RSD 1101.02, SHEET 1 OF 14 AS NECESSARY, COMPLETE -SR7- FROM 57+00± TO 82+00± AND CONSTRUCT -Y46- AS SHOWN ON TMP-4.
- 3- COMPLETE CONSTRUCTION OF -SR7- FROM 16+00± TO 56+00±
- 4- SHIFT TRAFFIC TO PATTERN SHOWN ON TMP-9 THRU TMP-11.

C) (-L-, -RABT1-, -RABT2-, -SR6-, -SR7-, -SR8-, -SR9-, -SR10-, RAMPS)

- USING RSD 1101.02, SHEET 3 OF 14, BEGIN CONSTRUCTION OF -L- UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE AS FOLLOWS (WEDGE EXISTING PAVEMENT AS NEEDED TO MAINTAIN TRAFFIC. REPLACE ANY EXISTING MARKINGS COVERED UP BY THE WEDGING):

- L- STA 851+00± TO 857+50± (OUTSIDE RT)
- L- STA 859+50± TO 876+50± (OUTSIDE RT)
- L- STA 881+20± TO 898+50± (OUTSIDE RT)
- L- STA 900+00± TO 932+90± (OUTSIDE RT)

- L- STA 851+00± TO 857+50± (OUTSIDE LT)
- L- STA 859+50± TO 896+50± (OUTSIDE LT)
- L- STA 909+00± TO 957+00± (OUTSIDE LT)

- L- STA 889+16± TO 896+74± (MEDIAN TEMP ASPHALT PAD)
- L- STA 893+37± TO 898+96± (MEDIAN TEMP PVMT)
- L- STA 899+40± TO 912+32± (MEDIAN TEMP PVMT)
- L- STA 899+57± TO 909+00± (MEDIAN TEMP ASPHALT PAD)

NOTE: CONTRACTOR IS RESPONSIBLE FOR MAINTAINING ALL EXISTING ACCESSES ON EXISTING BETHLEHEM ROAD. PRIOR TO BEGINNING CONSTRUCTION ON -SR10-, THE CONTRACTOR SHALL SUBMIT AND HAVE APPROVED BY THE ENGINEER A PLAN TO MAINTAIN ALL EXISTING ACCESS ON EXISTING BETHLEHEM ROAD.

- AWAY FROM TRAFFIC, BEGIN CONSTRUCTION OF THE FOLLOWING AS SHOWN:

- RABT1-, -Y42RPC-, -Y42RPD-, -SR9-, -SR10-, -RABT2-, -Y42RPA-, -Y42RPB-, -SR6-, AND -SR7- (SEE TMP-8, TMP-9, AND TMP-10)

- USING RSD 1101.02, SHEET 1 OF 14, CONSTRUCT THE FOLLOWING:

- PVTENT2- (SEE TMP-6)
- Y9A- 17+50± TO 29+37± (SEE TMP-14,15)
- SR8- 15+50± TO 36+16± (SEE TMP-12,13)

- USING RSD 1101.03, SHEET 1 OF 9 AND TMP-14 AND TMP-15, PERFORM THE FOLLOWING:

- 1- INSTALL AND COVER DETOUR ROUTE SIGNS AS SHOWN ON TMP-16.
- 2- UNCOVER DETOUR ROUTE SIGNS, PLACE TRAFFIC ONTO DETOUR ROUTE, AND CLOSE -SR9A- (ANTHONY RD).
- 3- CONSTRUCT -SR9A- FROM 10+18± TO 17+50± UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE. (SEE TMP-14)
- 4- PLACE TEMPORARY PAVMENT MARKINGS IN THE FINAL PATTERN.
- 5- COVER/REMOVE ALL DETOUR SIGNS AND OPEN -SR9A- TO TRAFFIC.

PHASE I (CONT'D)

STEP 2 (SEE TMP-17 THRU TMP-19)

-L-

- A- USING LAW ENFORCEMENT TO DIRECT TRAFFIC, DEACTIVATE SIGNAL AT BETHLEHEM RD INTERSECTION.
- B- USING RSD 1101.02, SHEET 3 OF 14, INSTALL PCB ALONG US 74 EB MEDIAN.
- C- USING RSD 1101.02, SHEET 3 OF 14, INSTALL PCB ALONG US 74 WB MEDIAN.
- D- USING RSD 1101.02, SHEET 3 OF 14, RESTRIPE US 74 WB AS SHOWN ON TMP-18,19 AND SHIFT TRAFFIC TO NEW PATTERN.
- E- USING RSD 1101.02, SHEET 3 OF 14, INSTALL PCB ALONG US 74 WB OUTSIDE.
- F- BEHIND BARRIER, INSTALL TEMPORARY SHORING 01 AND CONSTRUCT -L- WB AND -Y42RPA- AS SHOWN.

-Y43-, -SR9-, -PVTENT1-

- A- USING RSD 1101.03, SHEET 1 OF 9 AND TMP-17, CLOSE ANTHONY RD TO TRAFFIC.
- B- USING RSD 1101.02, SHEET 1 OF 14 AS NECESSARY, CONSTRUCT -SR9-, -Y43-, AND -PVTENT1- AS SHOWN ON TMP-17 AND TMP18. WEDGE -Y43 TIE IN TO EXISTING MIDWAY LAKE ROAD AS NEEDED TO MAINTAIN TRAFFIC.

STEP 3 (SEE TMP-20 THRU TMP-24E)

A) -Y42RPC-

- 1- WITH MIDWAY LAKE TRAFFIC SHIFTED TO -SR9-, USING RSD 1101.02, SHEET 3 OF 14, CONSTRUCT FROM -L- 876+50± TO 881+20± AND -Y42RPC- FROM 10+00± TO 24+00±.

-SR10-

- 1- INSTALL AND COVER DETOUR ROUTE SIGNS AS SHOWN ON TMP-23.
- 2- UNCOVER DETOUR ROUTE SIGNS, PLACE TRAFFIC ONTO DETOUR ROUTE, AND CLOSE BETHLEHEM RD.
- 3- CONSTRUCT -SR10- FROM 22+00± TO 28+50± AND -Y42RPD- FROM 20+50± TO 22+50± UP TO, BUT NOT INCLUDING, THE FINAL LAYER OF SURFACE COURSE.
- 4- PLACE TEMPORARY PAVEMENT MARKINGS IN THE FINAL PATTERN.
- 5- COVER/REMOVE ALL DETOUR SIGNS AND OPEN -SR10- TO TRAFFIC FROM 14+00± TO 28+50±.

-L-

- 1- COMPLETE ALL WORK BEGUN IN PHASE I, STEP 1C.

B) COMPLETE THE FOLLOWING UP TO AND INCLUDING THE FINAL LAYER OF SURFACE COURSE:

- RABT 1-, -RABT 2-, -Y42-, -Y42RPA-, -Y42RPB-, -Y42RPC-, AND -Y42RPD- (FROM THE BACK OF THE GORE TO THE TERMINUS), -SR6-, -SR7-, -SR9-, AND -SR10-.

C) USING RSD 1101.03, SHEET 7 OF 9 NIGHTLY, DETOUR EACH DIRECTION US 74 TRAFFIC UP AND DOWN PROPOSED RAMPS. (SEE ICT FOR TIME RESTRICTIONS) CONSTRUCT -L- UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE AS FOLLOWS (SEE TMP-24, 24A, 24B):

- L- FROM STA. 884+50± TO 896+50±
- L- RIGHT FROM STA. 896+50± TO 898+50±

(NOTE: PLACE TEMPORARY MARKINGS AND REOPEN TRAFFIC ON -L- [US 74] AT THE END OF EACH NIGHTLY WORK PERIOD. REPEAT THIS STEP UNTIL CONSTRUCTION IS COMPLETE.)

D) -L- (SEE TMP-24C, 24D, 24E)

- 1- USING RSD 1101.02, SHEET 3 OF 14, REMOVE PCB ALONG US 74 WB OUTSIDE.
- 2- USING RSD 1101.02, SHEET 3 OF 14, RESTRIPE US 74 WB AS SHOWN ON TMP-20,21,22 AND SHIFT TRAFFIC TO NEW PATTERN.
- 3- USING RSD 1101.02, SHEET 3 OF 14, INSTALL PCB ALONG US 74 WB MEDIAN.
- 4- USING RSD 1101.02, SHEET 3 OF 14, REMOVE PCB ALONG US 74 EB MEDIAN.

D) (CONT'D)

- 5- BEHIND BARRIER, CONSTRUCT -L- MEDIAN INCLUDING TEMPORARY SHORING FOR BRIDGE MEDIAN BENT AS SHOWN ON TMP-21, 22.
- 6- CONSTRUCT PROPOSED -Y42- BRIDGE OVER US 74. USE RSD 1101.03, SHEET 7 OF 9, TO DETOUR EACH DIRECTION US 74 TRAFFIC UP AND DOWN PROPOSED RAMPS WHEN HANGING PROPOSED GIRDERS. (SEE ICT FOR TIME RESTRICTIONS)

PHASE II (TMP-25 - 32)

STEP 1)

- A) USING RSD 1101.02, SHEET 3 OF 14, INSTALL PAVEMENT MARKINGS AS SHOWN ON TMP-25 THRU 30 AND SHIFT TRAFFIC TO PATTERN SHOWN. INSTALL FINAL PAVEMENT MARKINGS (SEE FINAL PAVEMENT MARKING PLAN) ON -Y42-, -Y42RPA-, -Y42RPB-, -Y42RPC-, -Y42RPD-, -SR6-, -SR7-, -SR9-, AND -SR10-.

- B) USING RSD 1101.02, SHEET 3 OF 14, INSTALL PCB ON US 74 EB MEDIAN AS SHOWN ON TMP-27,28.

C) USING RSD 1101.02, SHEET 3 OF 14, CONSTRUCT THE FOLLOWING:

- L- STA 898+50± TO 909+00± (EB MEDIAN)

STEP 2) (-L-, -SR8-)

- 1- CLOSE HARMON RD AS SHOWN ON TMP-31.
- 2- USING RSD 1101.02, SHEET 3 OF 14, CONSTRUCT -L- STA 932+90± TO 955+00± (EB OUTSIDE) (SEE TMP-31 AND TMP-32)
 - AWAY FROM TRAFFIC, CONSTRUCT -SR8- FROM 10+75± TO 17+50± UP TO, BUT NOT INCLUDING, THE FINAL LAYER OF SURFACE COURSE.
 - AWAY FROM TRAFFIC, COMPLETE CONSTRUCTION OF -SR8- FROM 17+50± TO 35+75±
- 3- AWAY FROM TRAFFIC, INSTALL TEMPORARY PAVEMENT MARKINGS.
- 4- OPEN NEWLY CONSTRUCTED -SR8- TO TRAFFIC.

PHASE III (TMP-33 THRU TMP-41)

STEP 1 (SEE TMP-33)

- 1- USING LAW ENFORCEMENT TO DIRECT TRAFFIC, DEACTIVATE EXISTING SIGNAL AT LONGBRANCH RD.
- 2- USING RSD RSD 1101.02, SHEET 3 OF 14, CONSTRUCT -L- FROM 857+50± TO 859+50± UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE
 - AWAY FROM TRAFFIC, CONSTRUCT CUL DE SAC ON -SR11- AND SHIFT -SR6- TRAFFIC TO INCLUDE -PVTENT2-

STEP 2 (SEE TMP- 34 THRU TMP-41)

- A- USING RSD 1101.02, SHEET 3 OF 14, SHIFT TRAFFIC TO PATTERN SHOWN.
- B- USING RSD 1101.02, SHEET 3 OF 14, CONSTRUCT MEDIAN WIDENING ON -L- UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE AS SHOWN AND REMOVE BARRIER AS NEEDED. WEDGE EXISTING -L- AS NEEDED TO BRING IT UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE.

PHASE IV (NOT SHOWN)

STEP 1

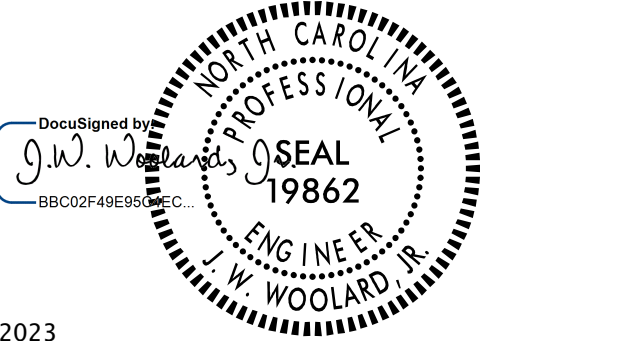
- 1- USING RSD 1101.02, SHEET 3 OF 14, REMOVE ANY REMAINING PCB ON -L-
- 2- USING RSD 1101.02, SHEET 3 OF 14, INSTALL FINAL LAYER OF SURFACE COURSE ON -L- AND INSTALL FINAL PAVEMENT MARKINGS AND MARKERS.
 - USING BRSD 1101.02, SHEET 1 OF 14, INSTALL FINAL LAYER OF SURFACE COURSE ON ALL -Y- LINES AND INSTALL FINAL PAVEMENT MARKINGS AND MARKERS.
- 3- REMOVE ALL TRAFFIC CONTROL DEVICES AND OPEN ALL ROADS TO THE FINAL TRAFFIC PATTERN.

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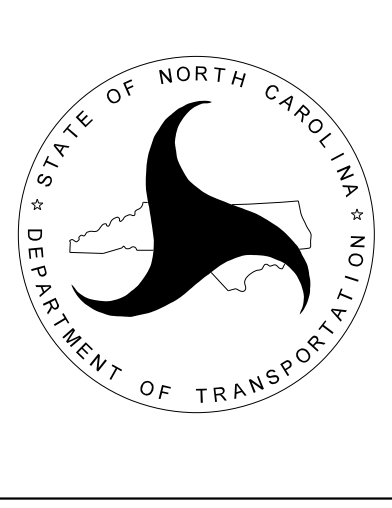


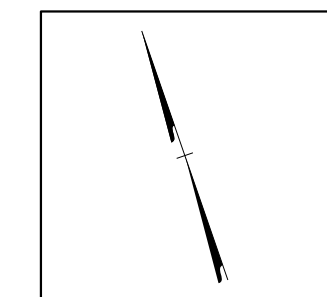
Stantec Consulting Services Inc.
801 Jones Franklin Road
Suite 300
Raleigh, NC 27606
Tel. 919.851.6866
Fax. 919.851.7024
www.stantec.com
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-SR6- CONSTRUCTION:

CONSTRUCT -SR6- IN PROGRESSIVE MANNER FROM STA. 12+00± TO STA. 28+00± AND TIE TO ANTHONY FARM RD

PROPERTIES ALONG -SR6- TO USE ANTHONY FARM RD TO ACCESS US 74 UNTIL ROUNDABOUTS AND RAMP CONSTRUCTION COMPLETE.

GENERAL NOTE:

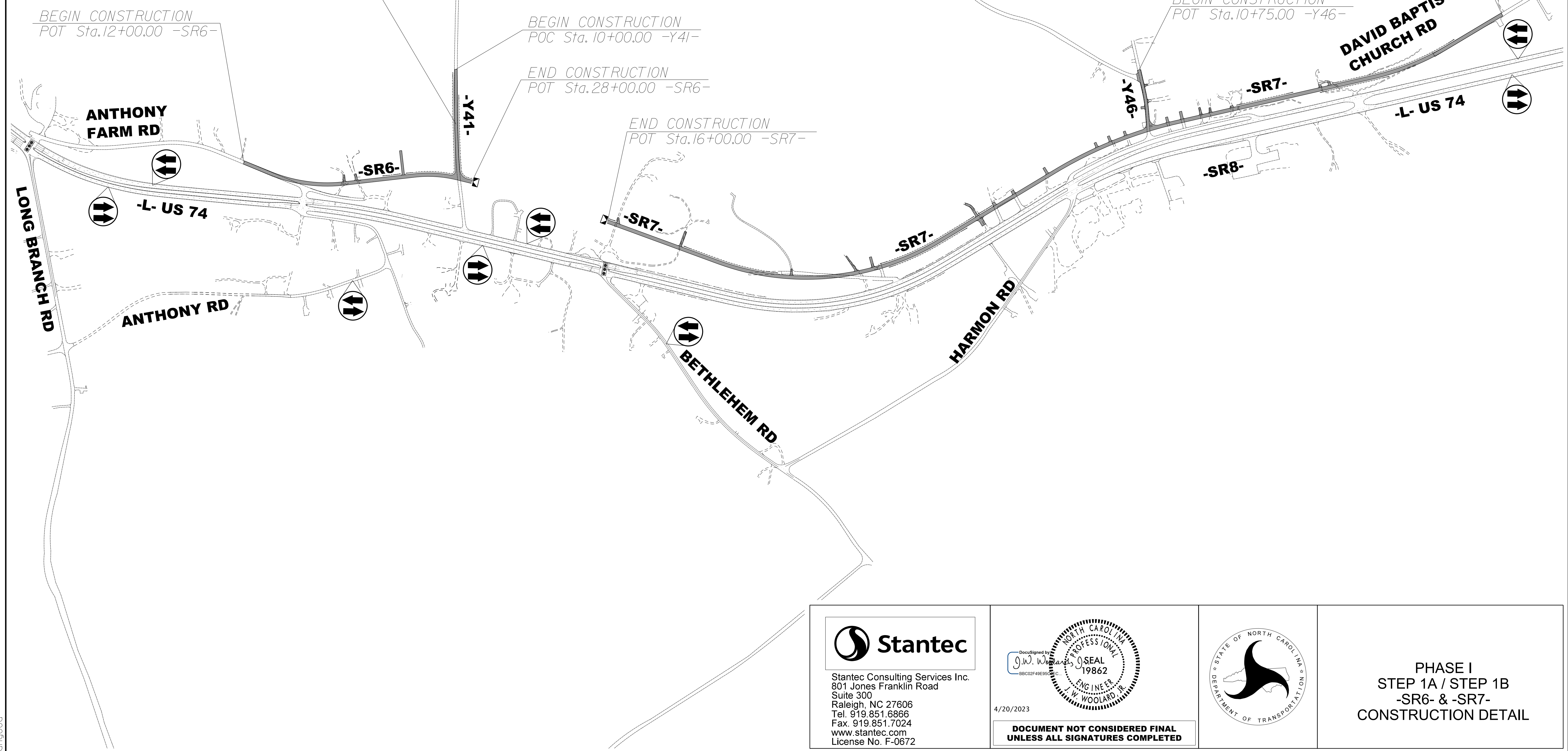
IN AREAS ALONG US 74 WHERE THERE ARE EXISTING DRIVEWAY ACCESSES, -L- OUTSIDE WIDENING MAY NOT BEGIN UNTIL THE SERVICE ROAD HAS BEEN CONSTRUCTED UP TO THAT POINT AND THERE IS AN ALTERNATE ACCESS TO US 74 FOR PROPERTIES.

-SR7- CONSTRUCTION:

CONSTRUCT -SR7- IN PROGRESSIVE MANNER FROM STA. 82+00± TO STA. 16+00± AND TIE TO DAVID BAPTIST CHURCH RD.

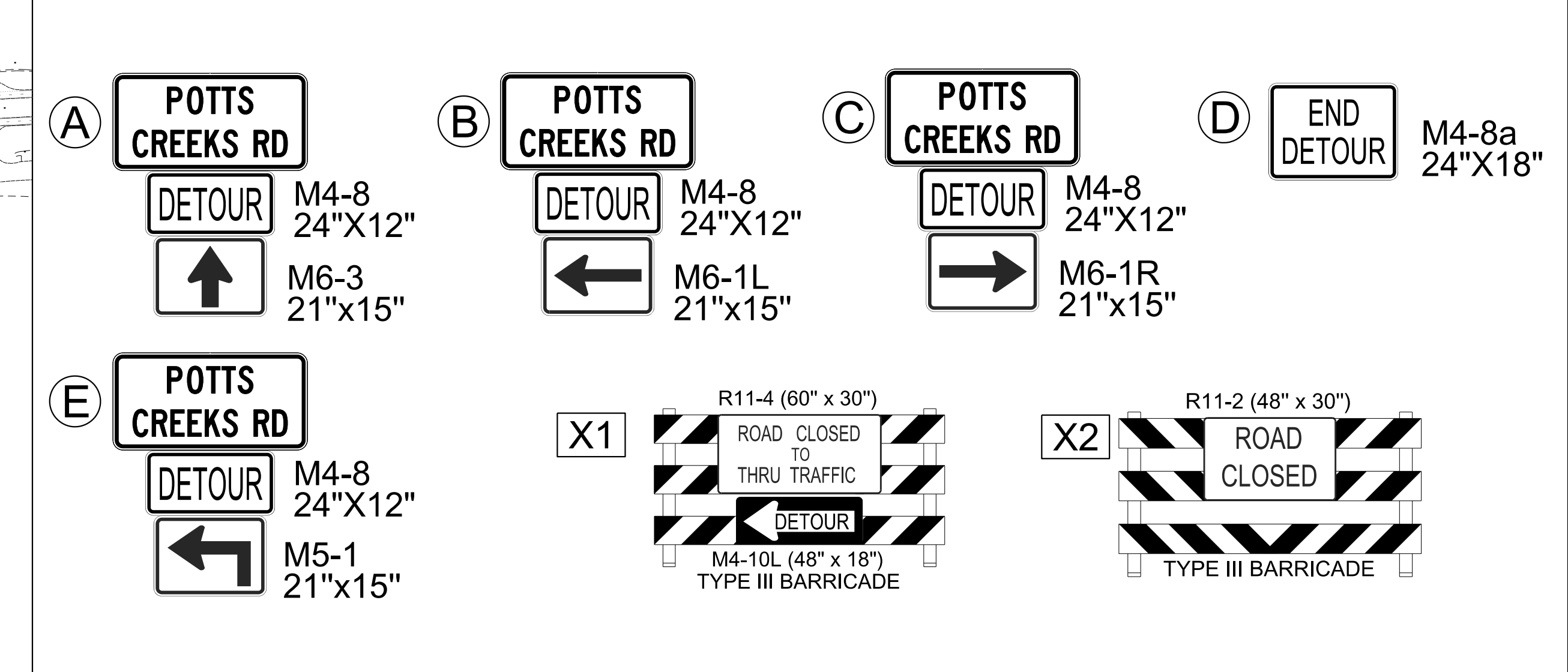
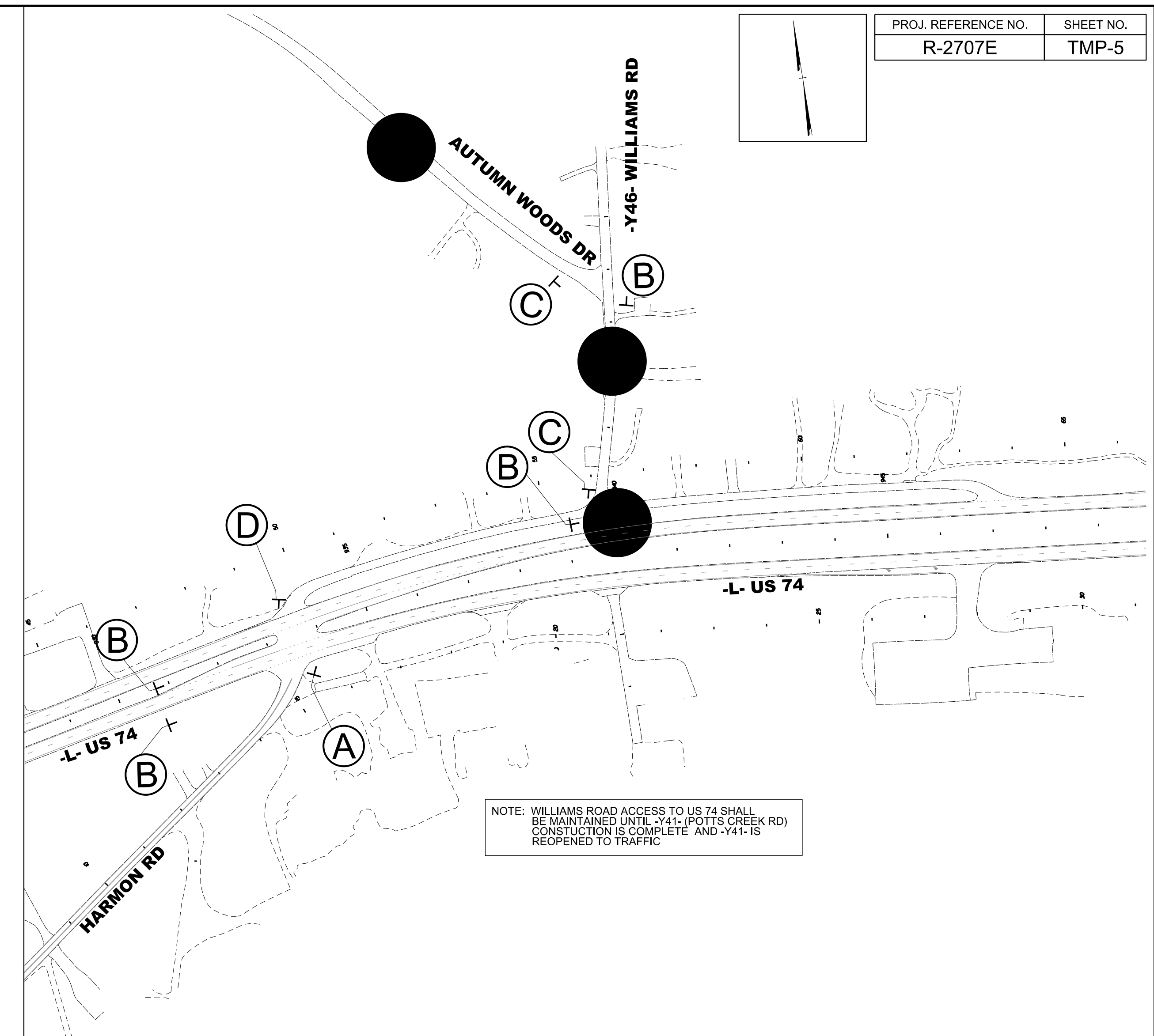
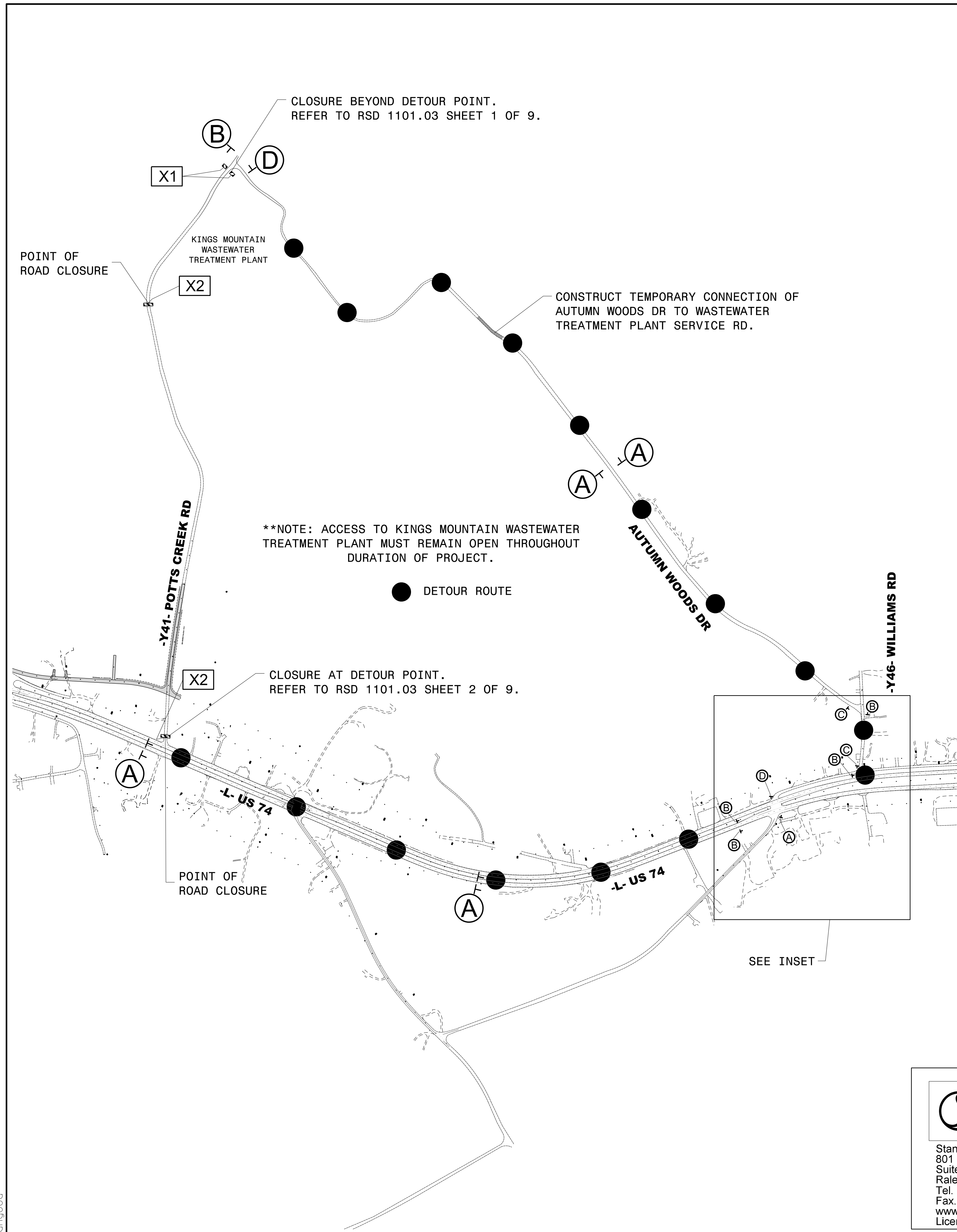
PROPERTIES ALONG -SR7- TO USE DAVID BAPTIST CHURCH RD TO ACCESS STONY POINT RD/US 74 BUS UNTIL ROUNDABOUTS AND RAMP CONSTRUCTION COMPLETE.

CONSTRUCT -Y41- UNDER ROAD CLOSURE. REFER TO TMP-5 FOR -Y41- POTTS CREEK ROAD CLOSURE AND DETOUR



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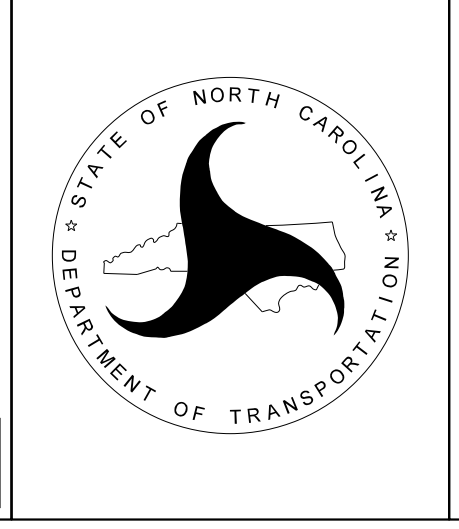
<p>Stantec Consulting Services Inc. 801 Jones Franklin Road Suite 300 Raleigh, NC 27606 Tel. 919.851.6866 Fax. 919.851.7024 www.stantec.com License No. F-0672</p>	<p>4/20/2023</p> <p>DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED</p>		<p>PHASE I STEP 1A / STEP 1B -SR6- & -SR7- CONSTRUCTION DETAIL</p>
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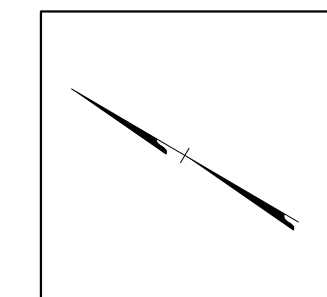
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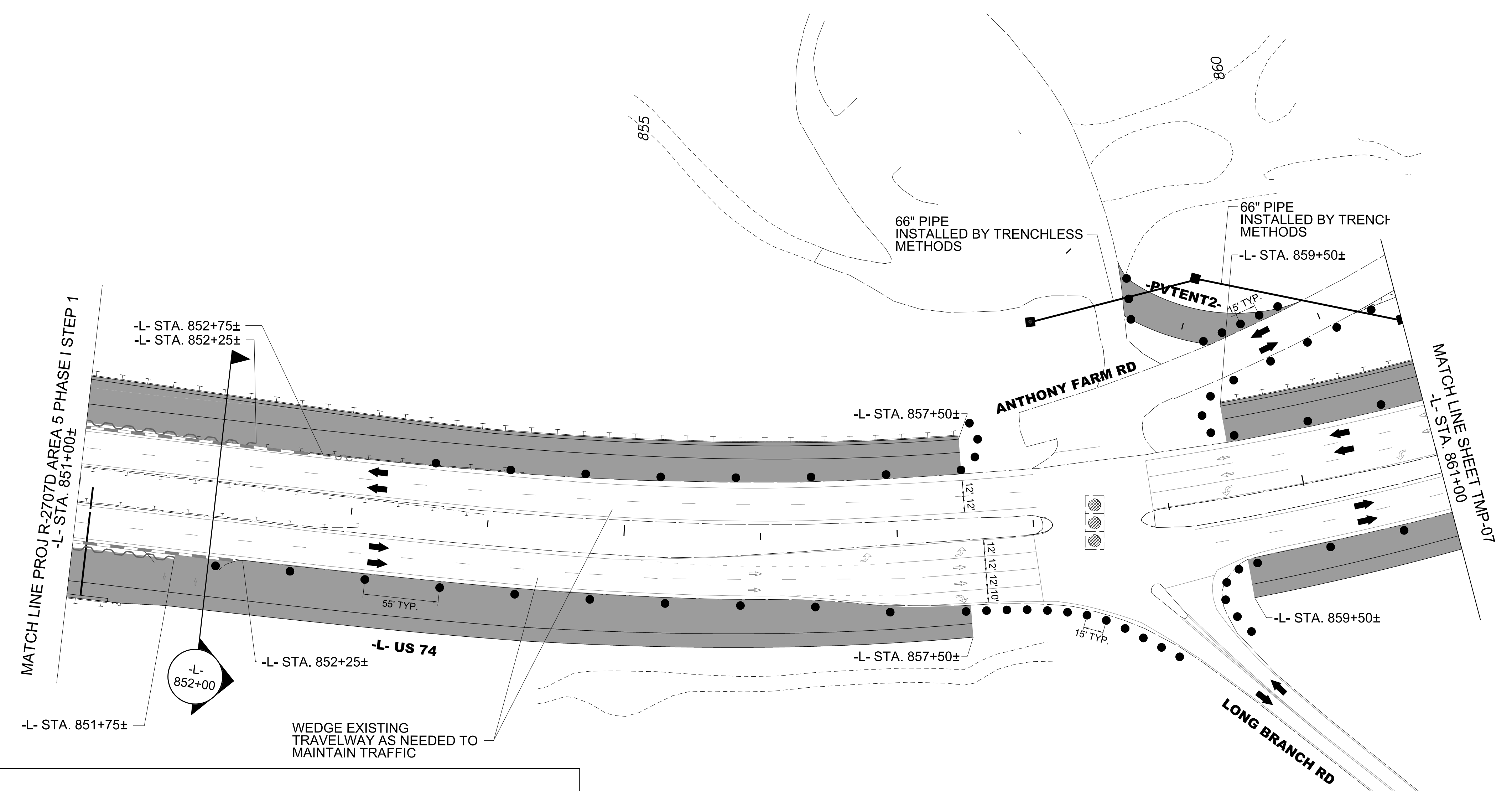
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**-Y41- POTTS CREEK RD
DETOUR ROUTE**



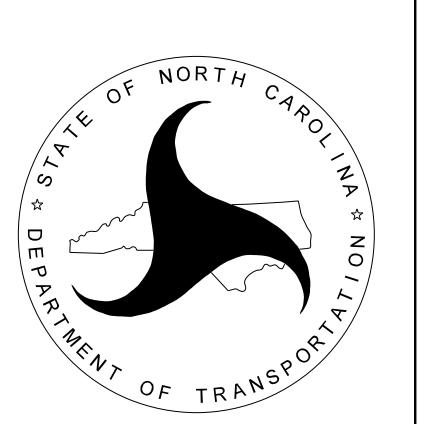
COORDINATE R-2707E CONSTRUCTION STAGING WITH R-2707D AREA 5 BRIDGES OVER BUFFALO CREEK STAGING



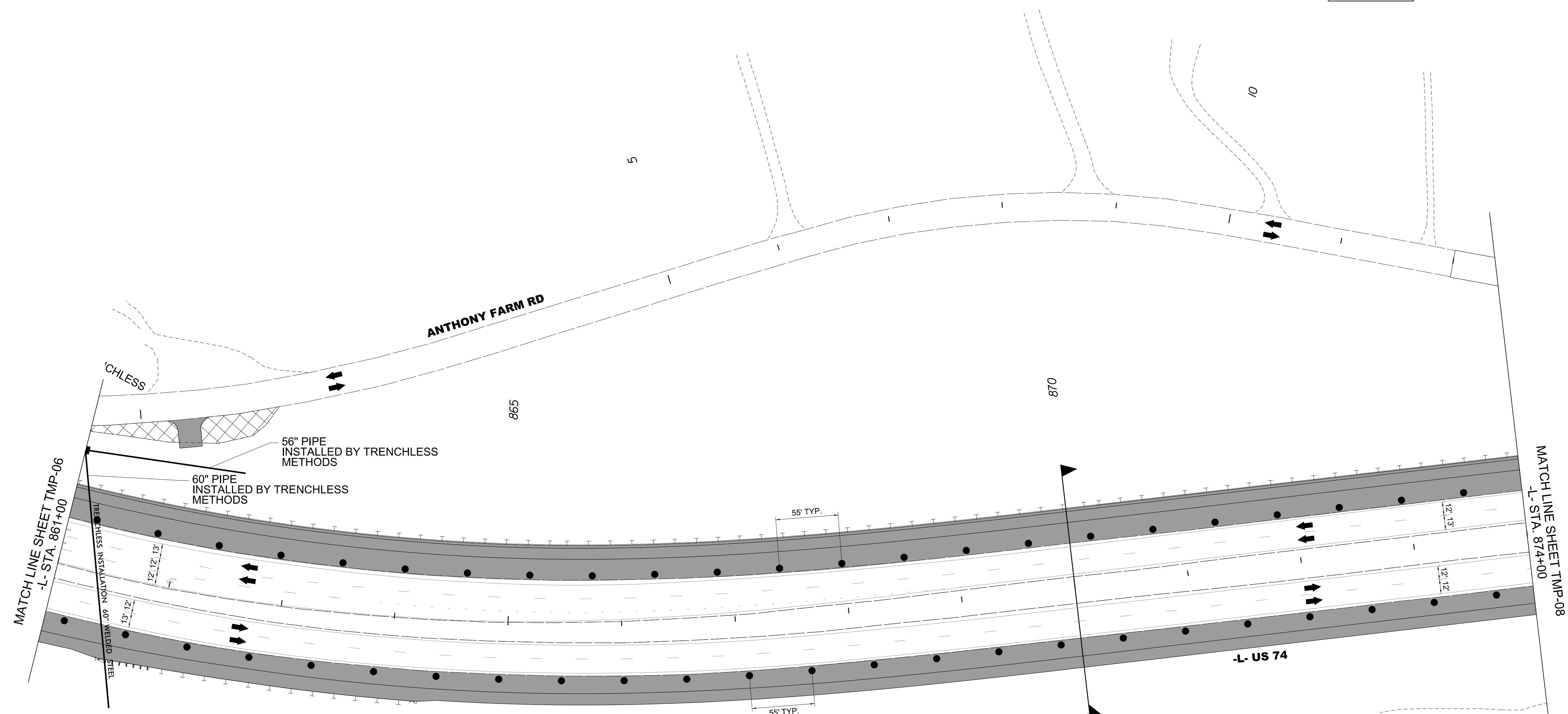
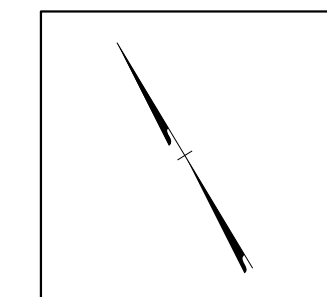
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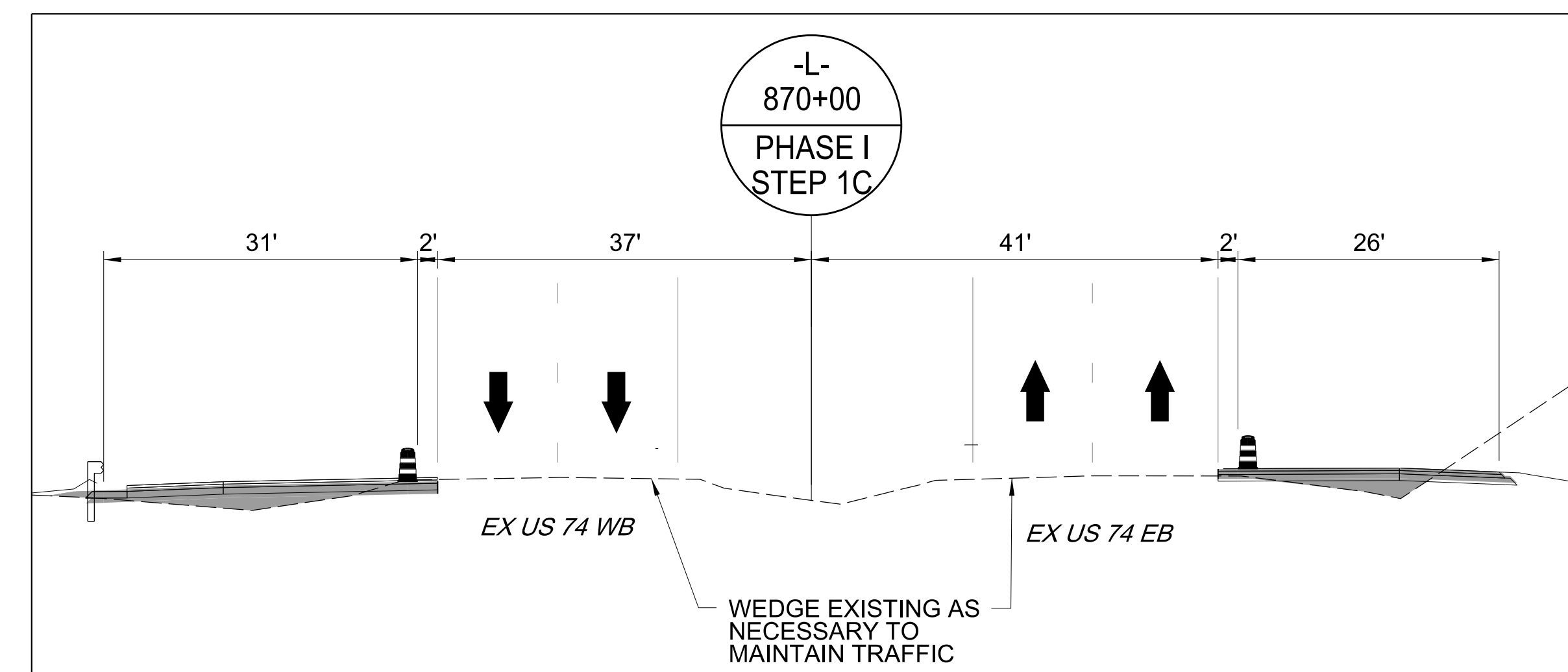


-L- US 74
 PHASE I
 STEP 1C



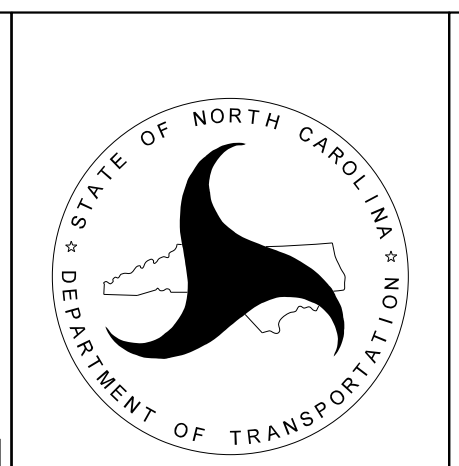
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-L- STA. 861+00

MATCH LINE SHEET TMP-08
-L- STA. 874+00



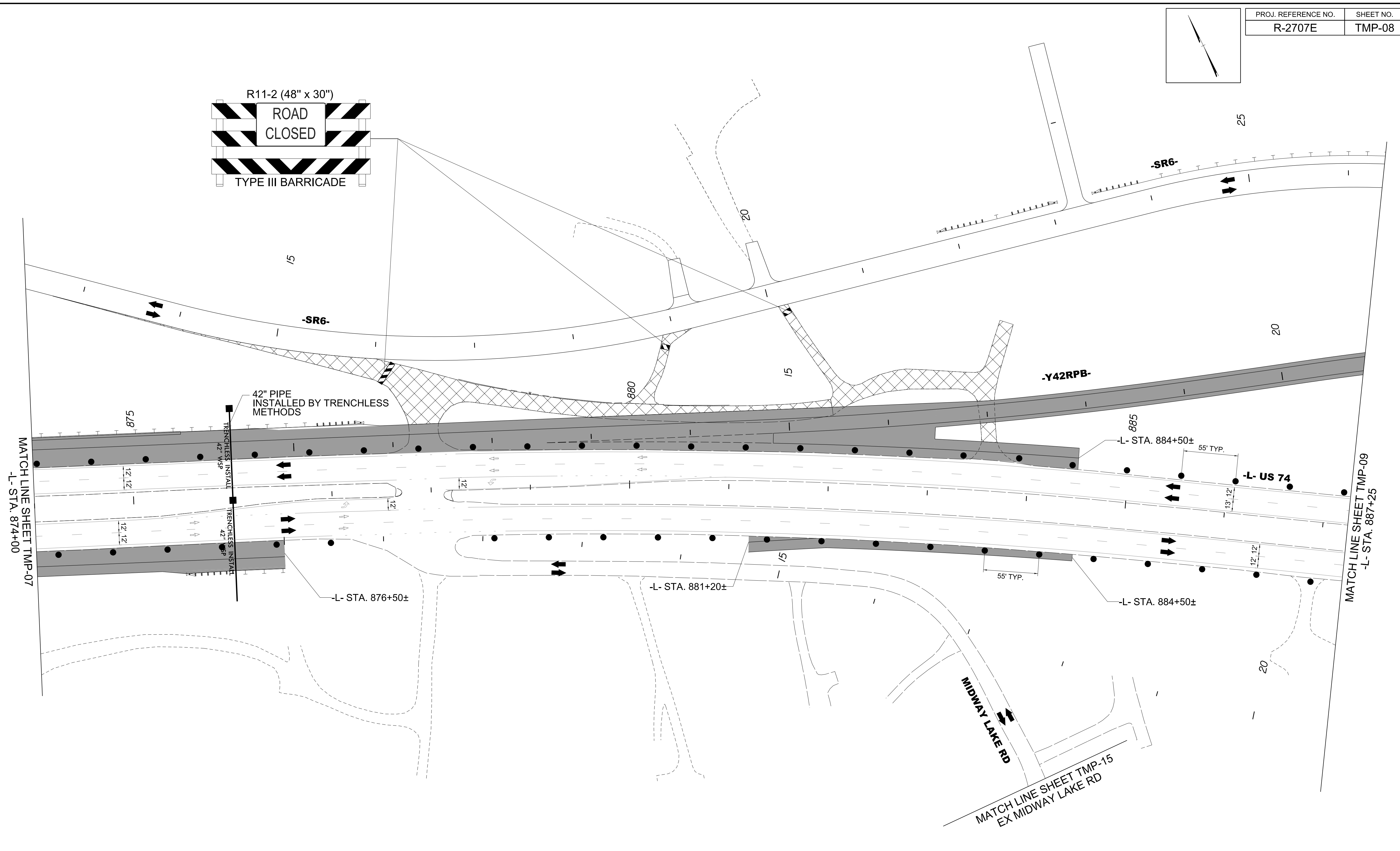
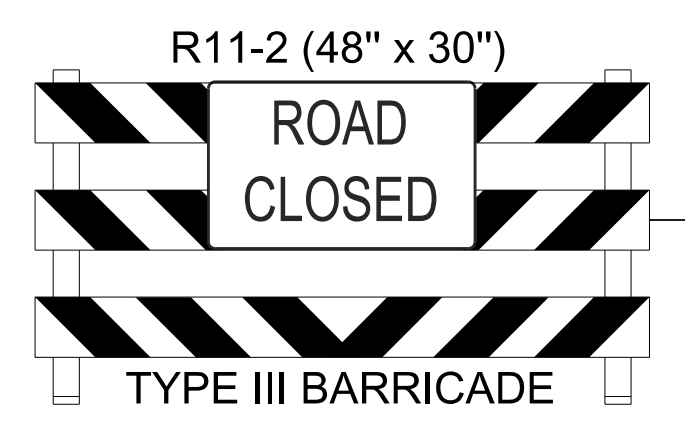
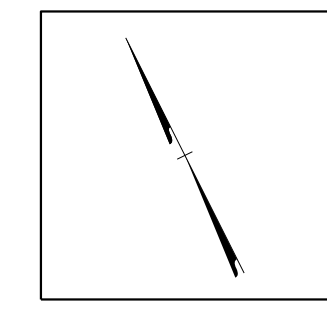
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**-L- US 74
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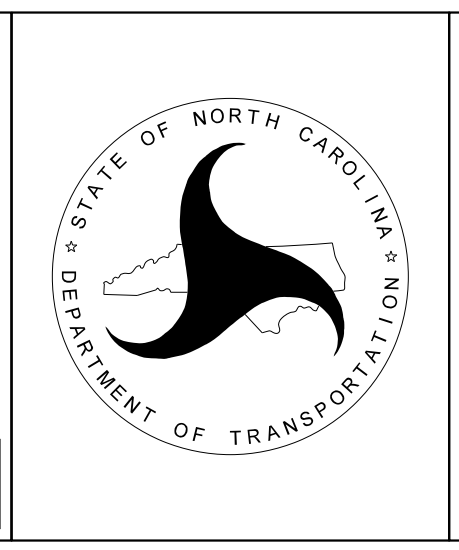


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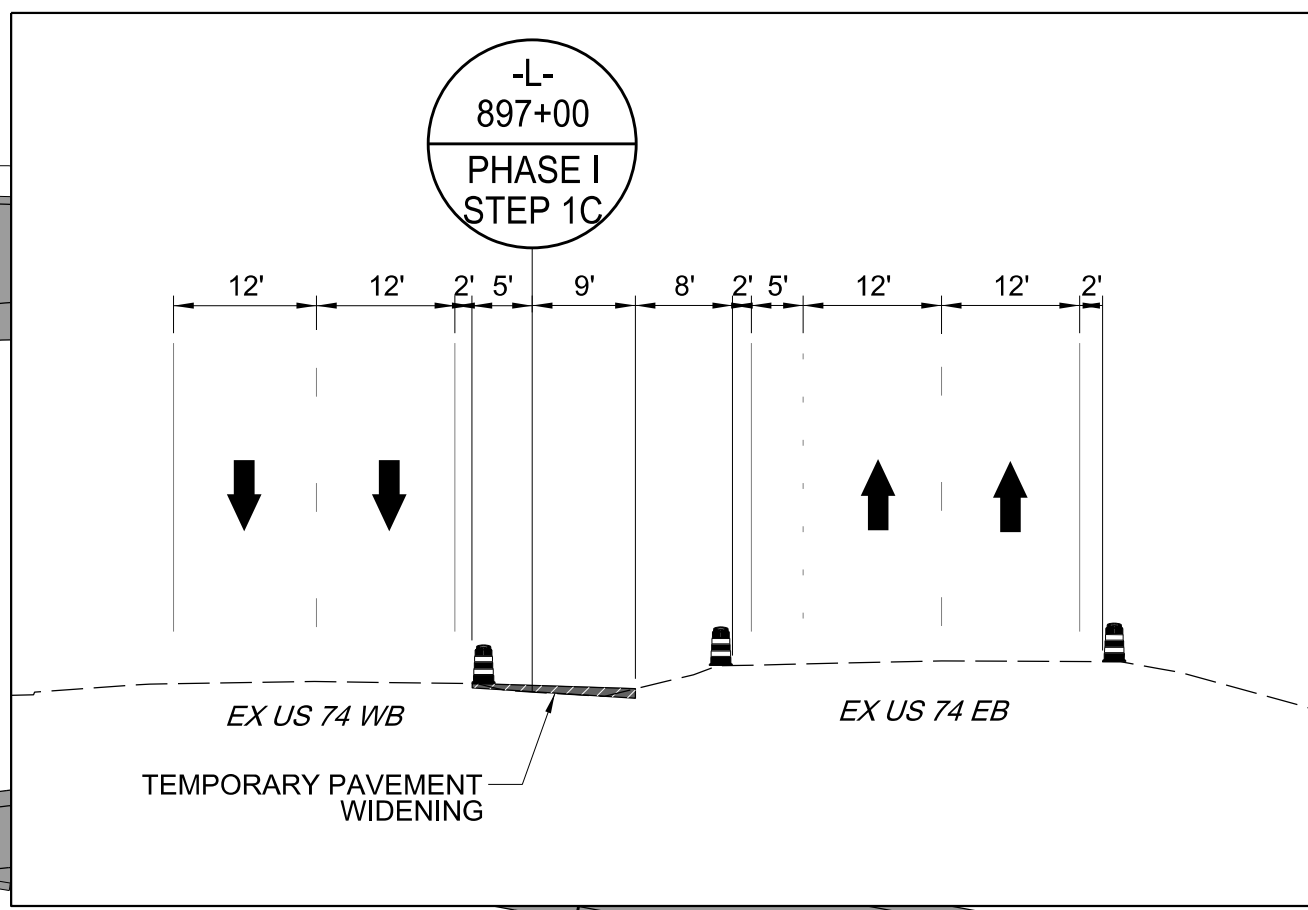
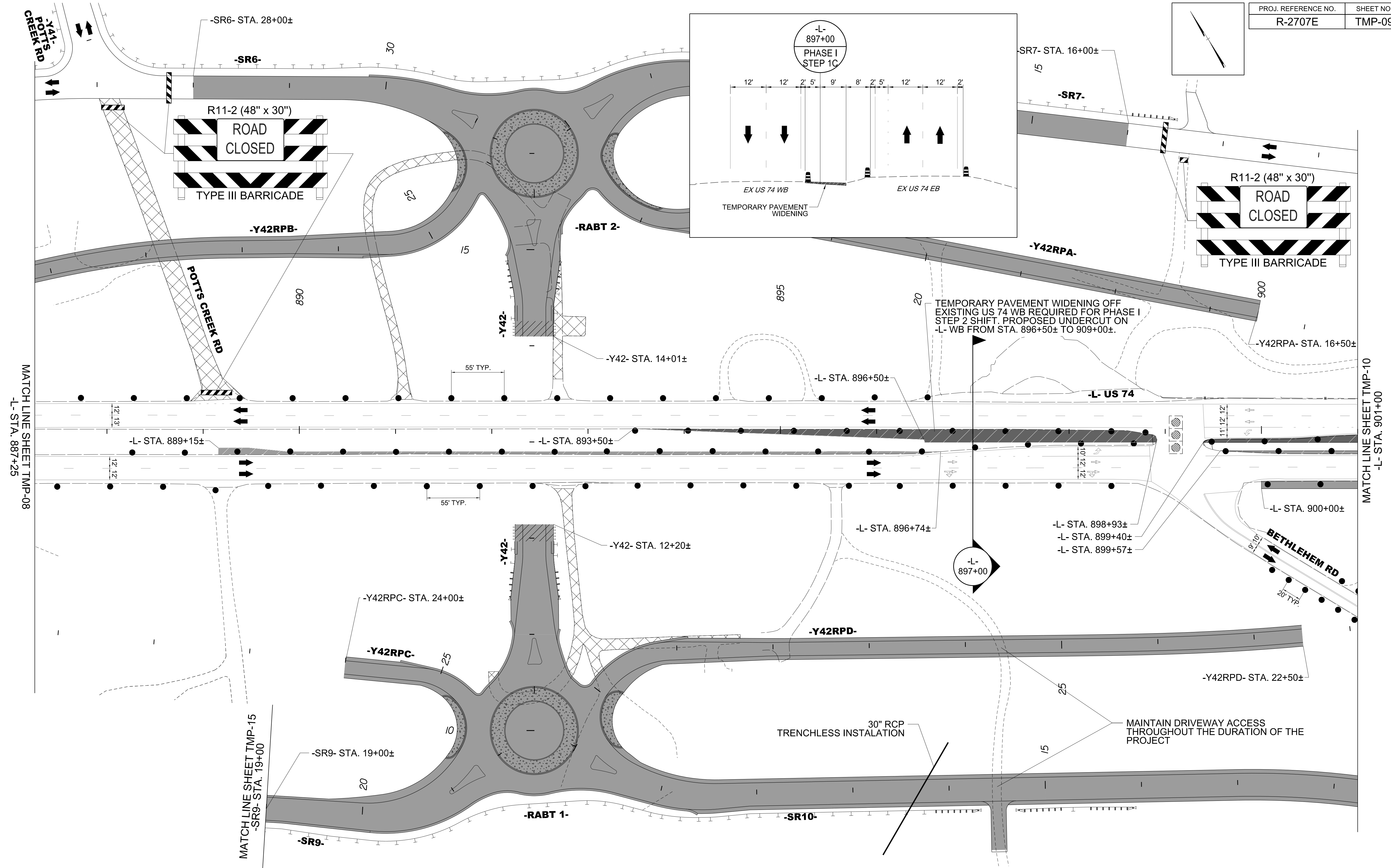
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-L- US 74
 PHASE I
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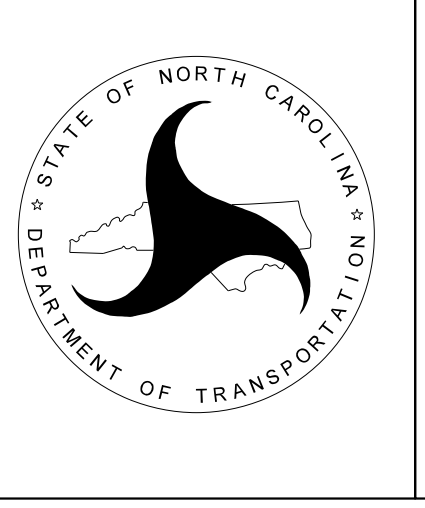
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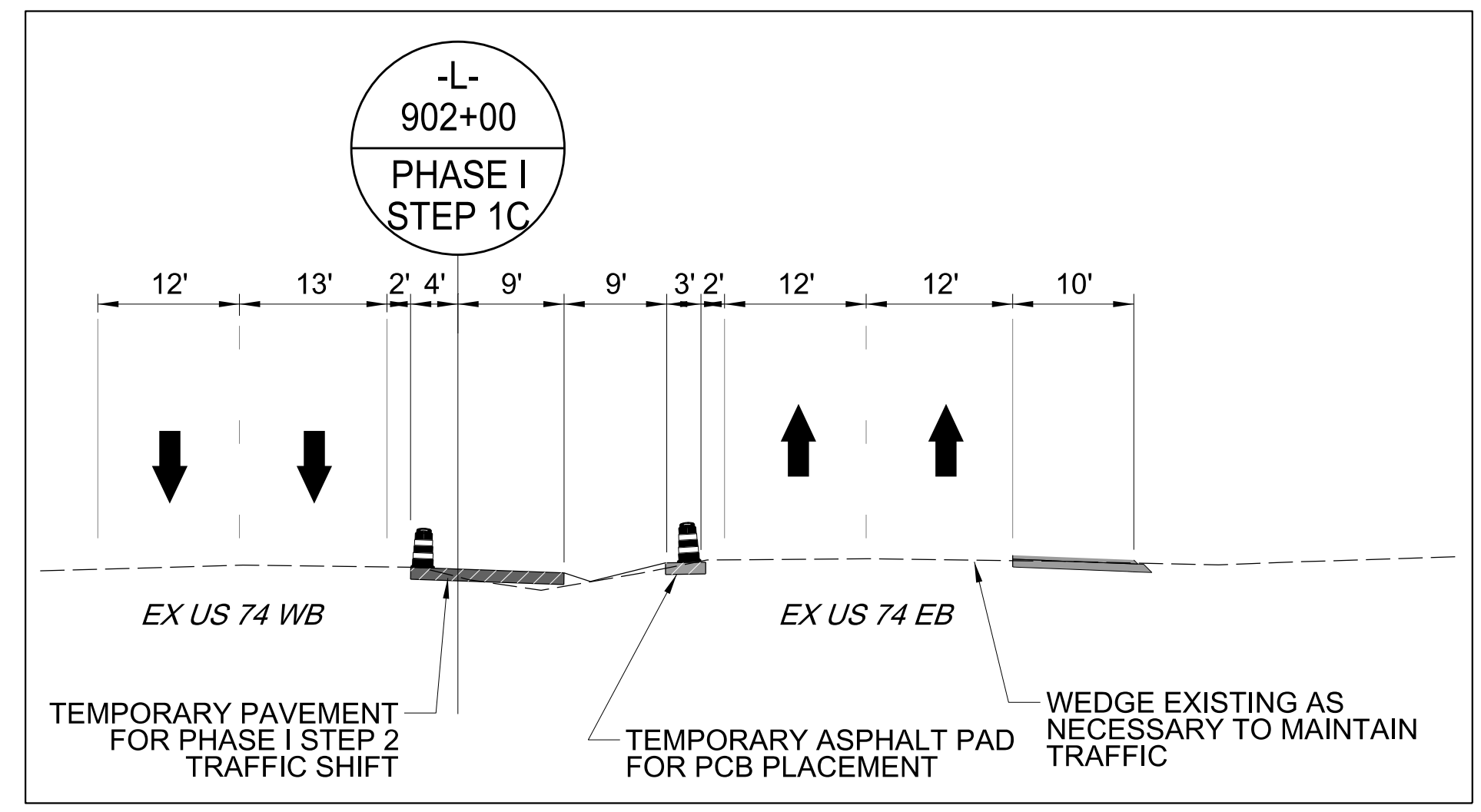
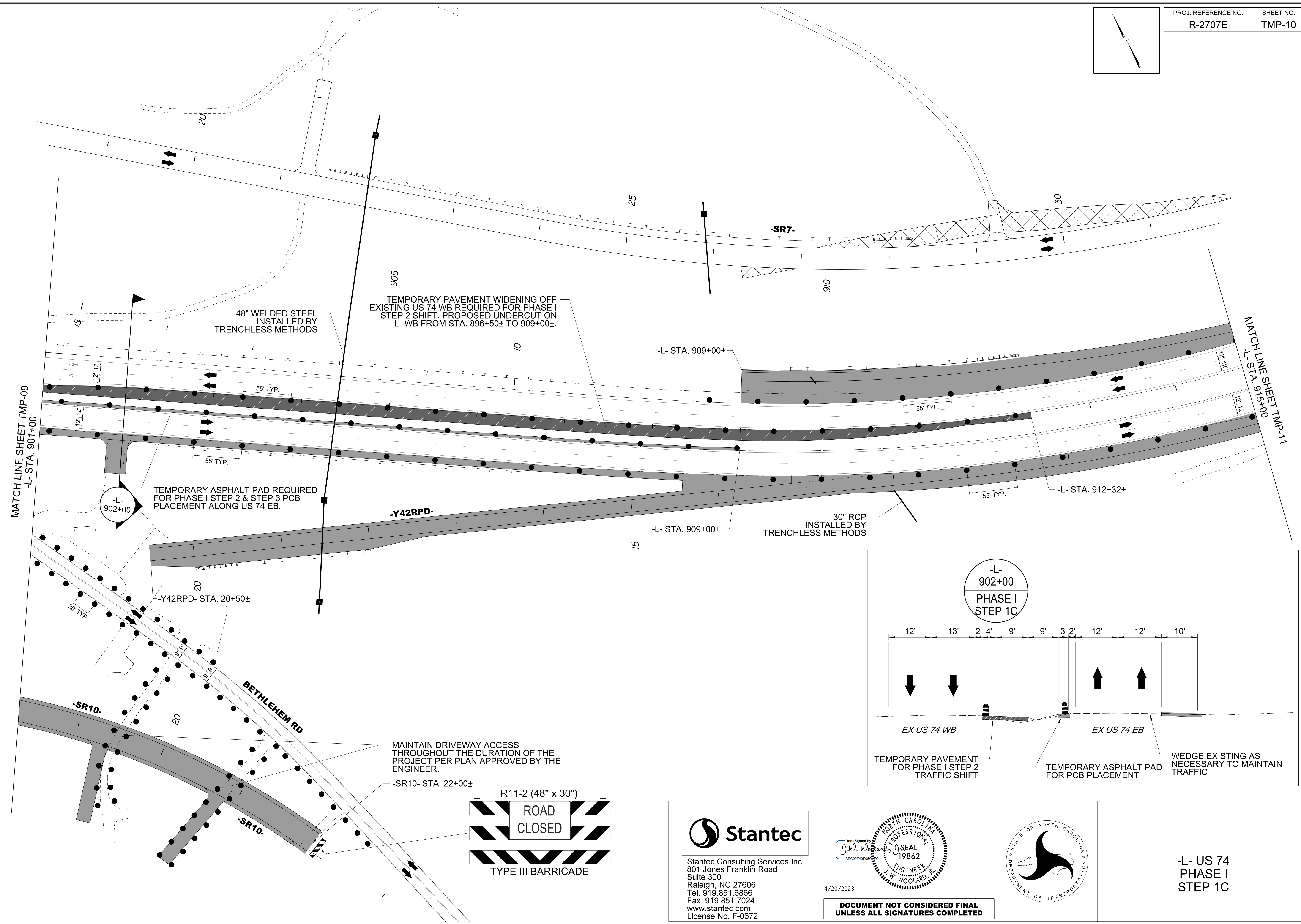
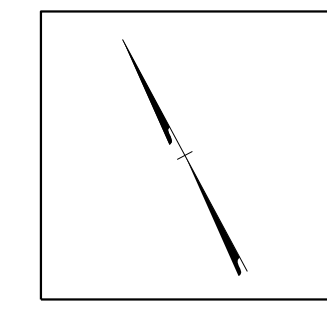
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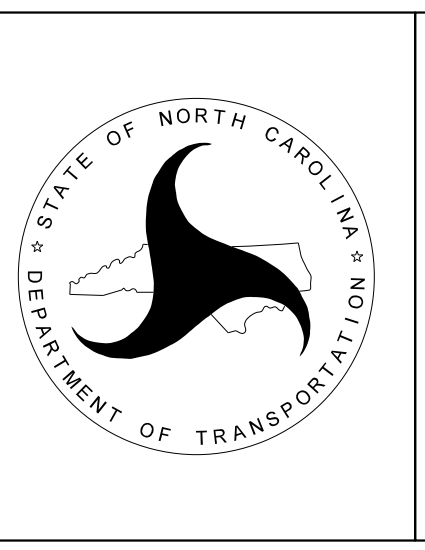


**-L- US 74
PHASE I
STEP 1C**



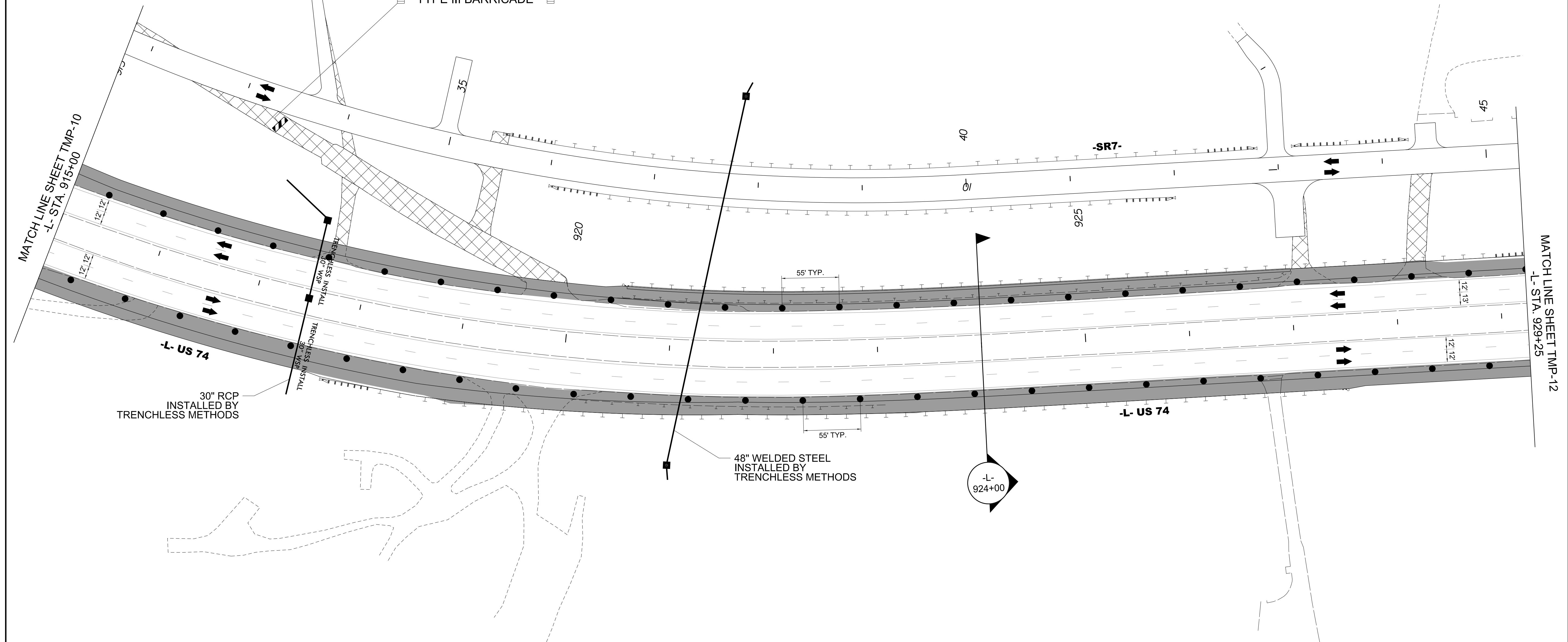
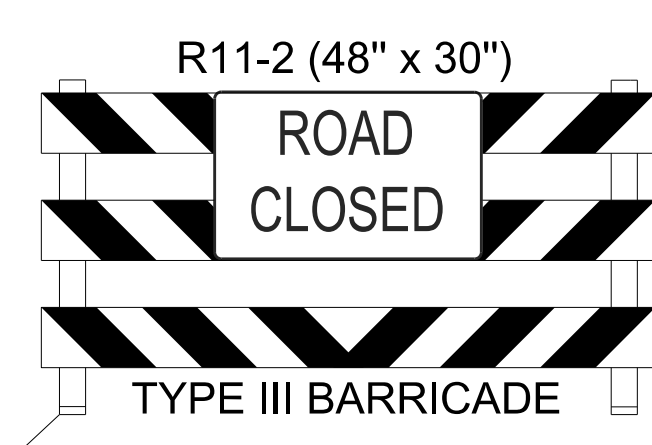
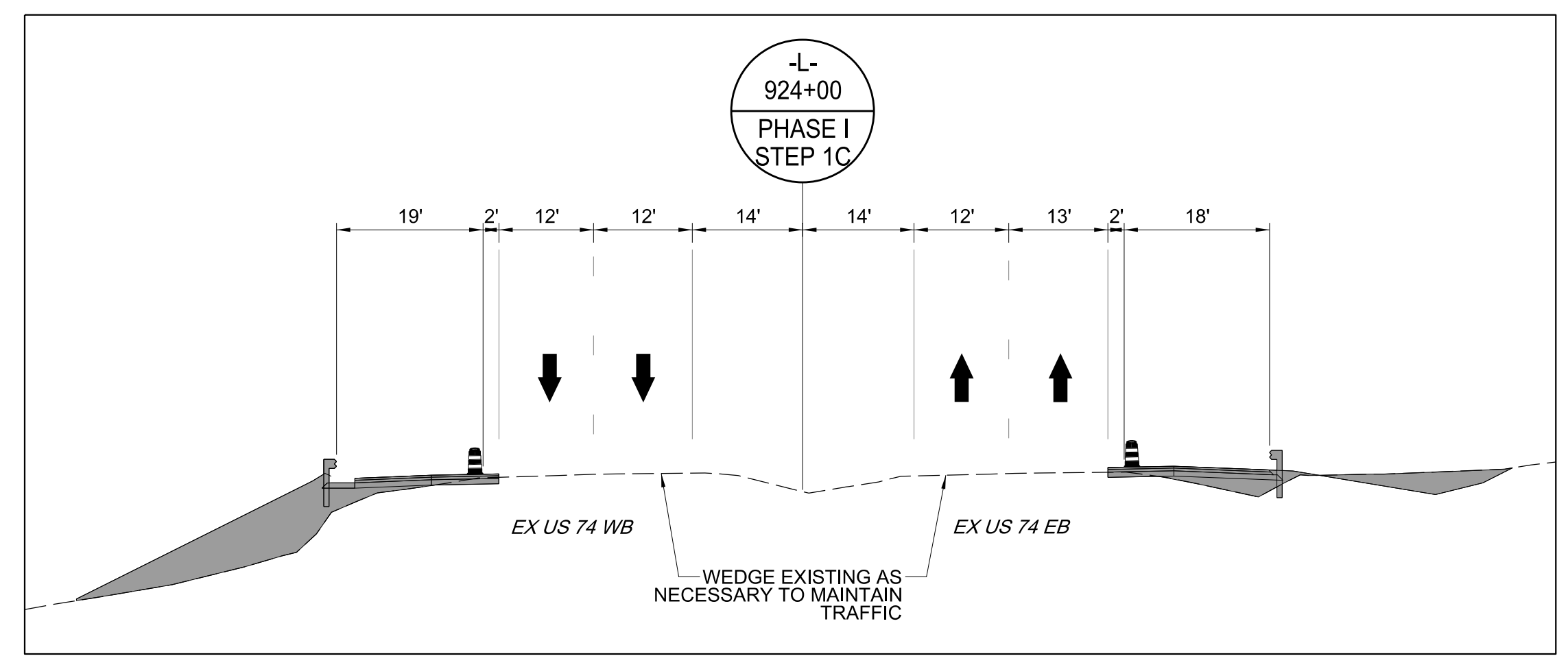
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-L- US 74 PHASE I STEP 1C

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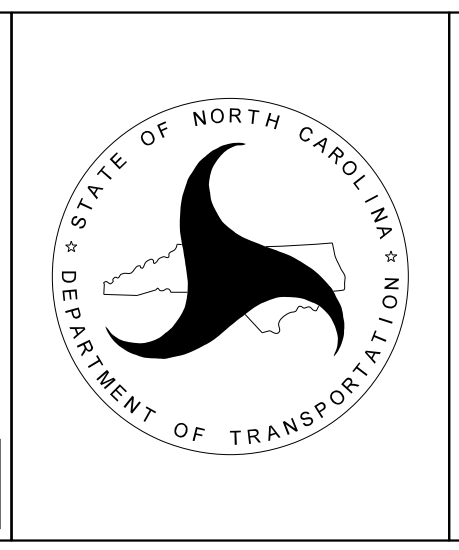


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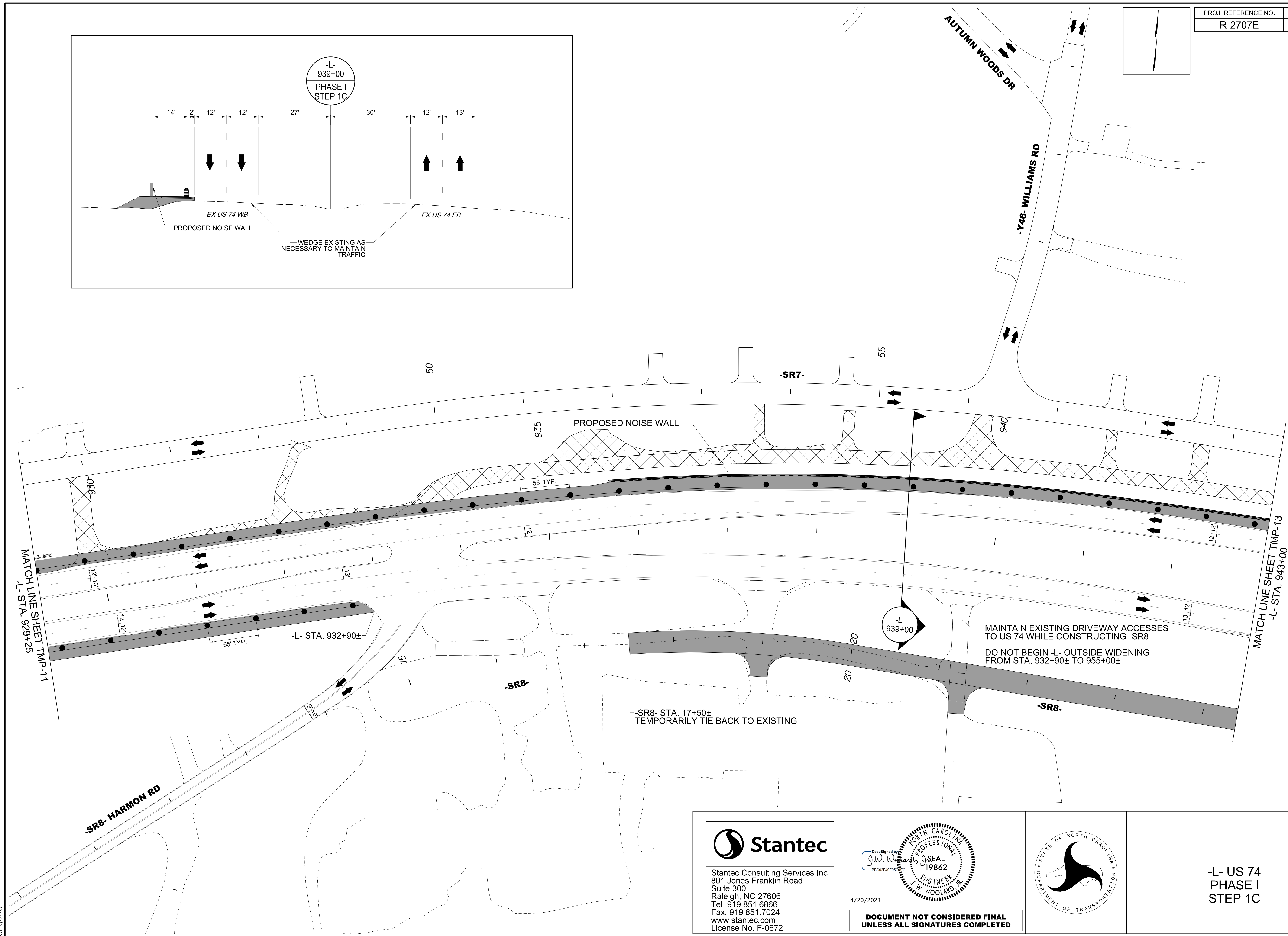
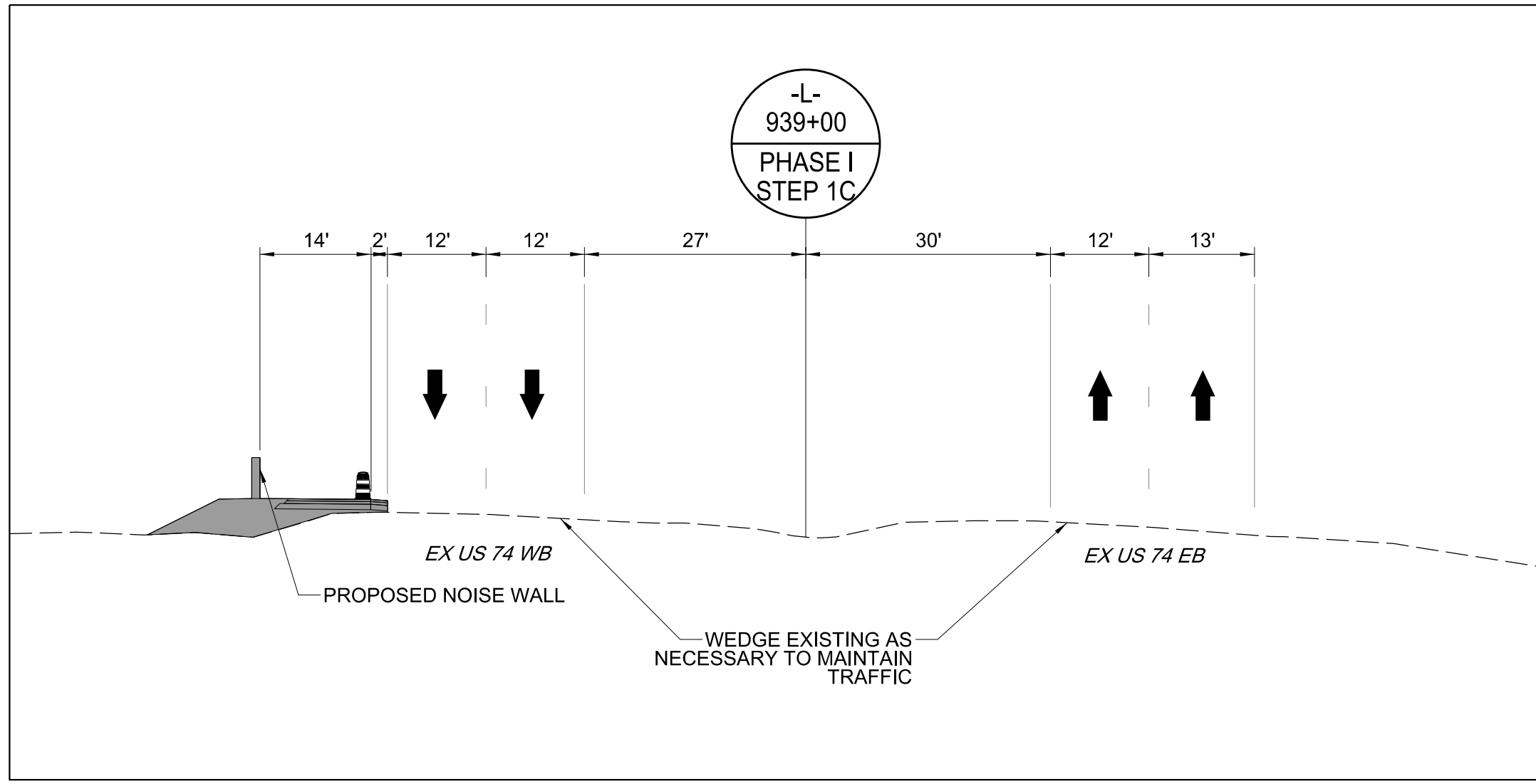
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PHASE I
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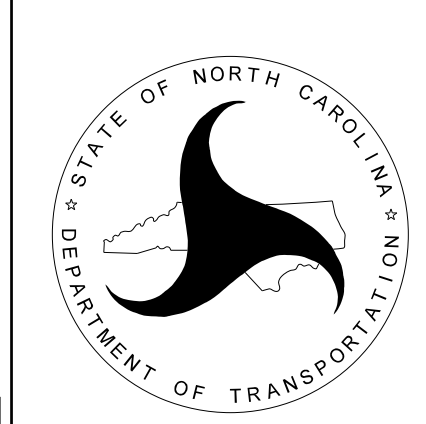
MAINTAIN EXISTING DRIVEWAY ACCESSES TO US 74 WHILE CONSTRUCTING -SR8-
DO NOT BEGIN -L- OUTSIDE WIDENING FROM STA. 932+90± TO 955+00±

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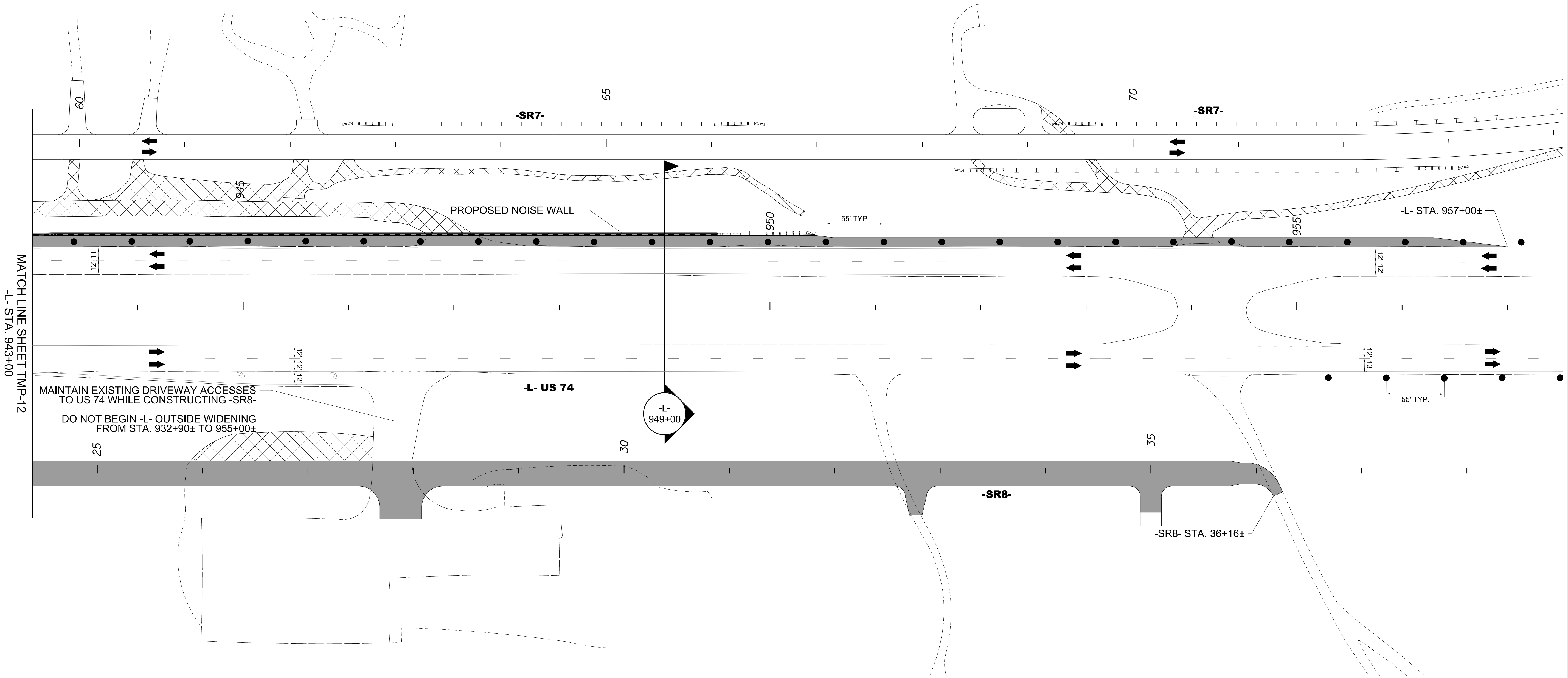
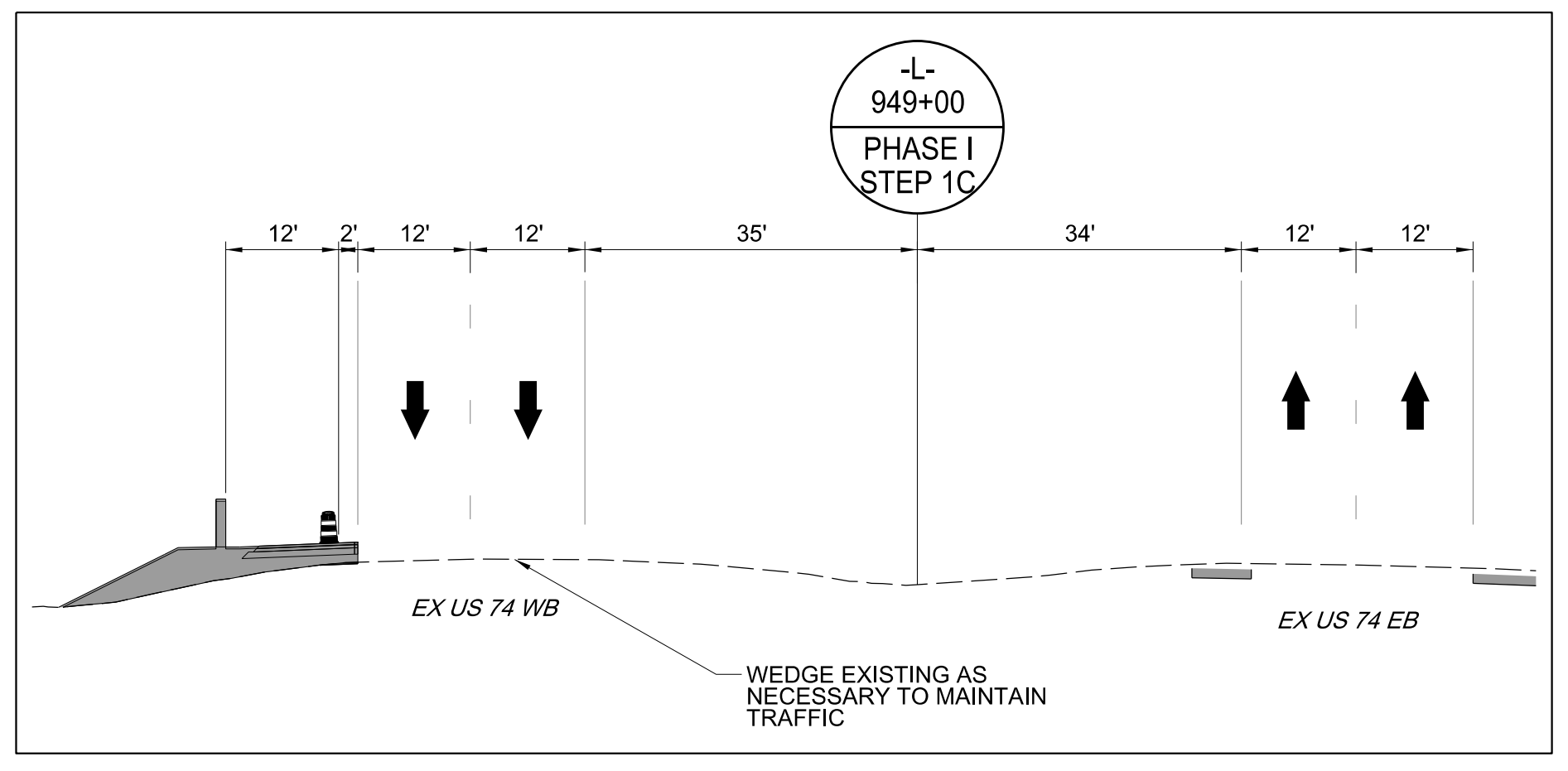
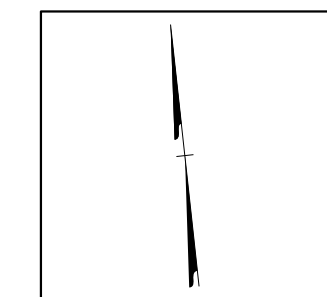
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-L- US 74
PHASE I
STEP 1C



MATCH LINE SHEET TMP-12
-L- STA. 943+00

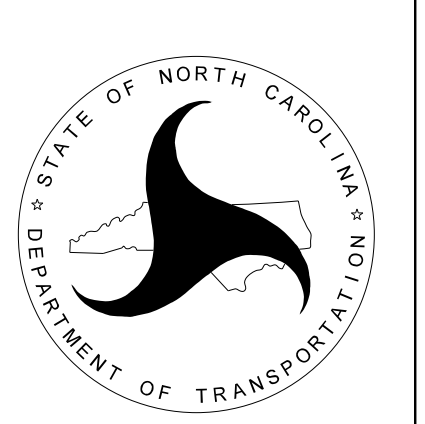
MAINTAIN EXISTING DRIVEWAY ACCESSSES TO US 74 WHILE CONSTRUCTING -SR8-
DO NOT BEGIN -L- OUTSIDE WIDENING FROM STA. 932+90± TO 955+00±

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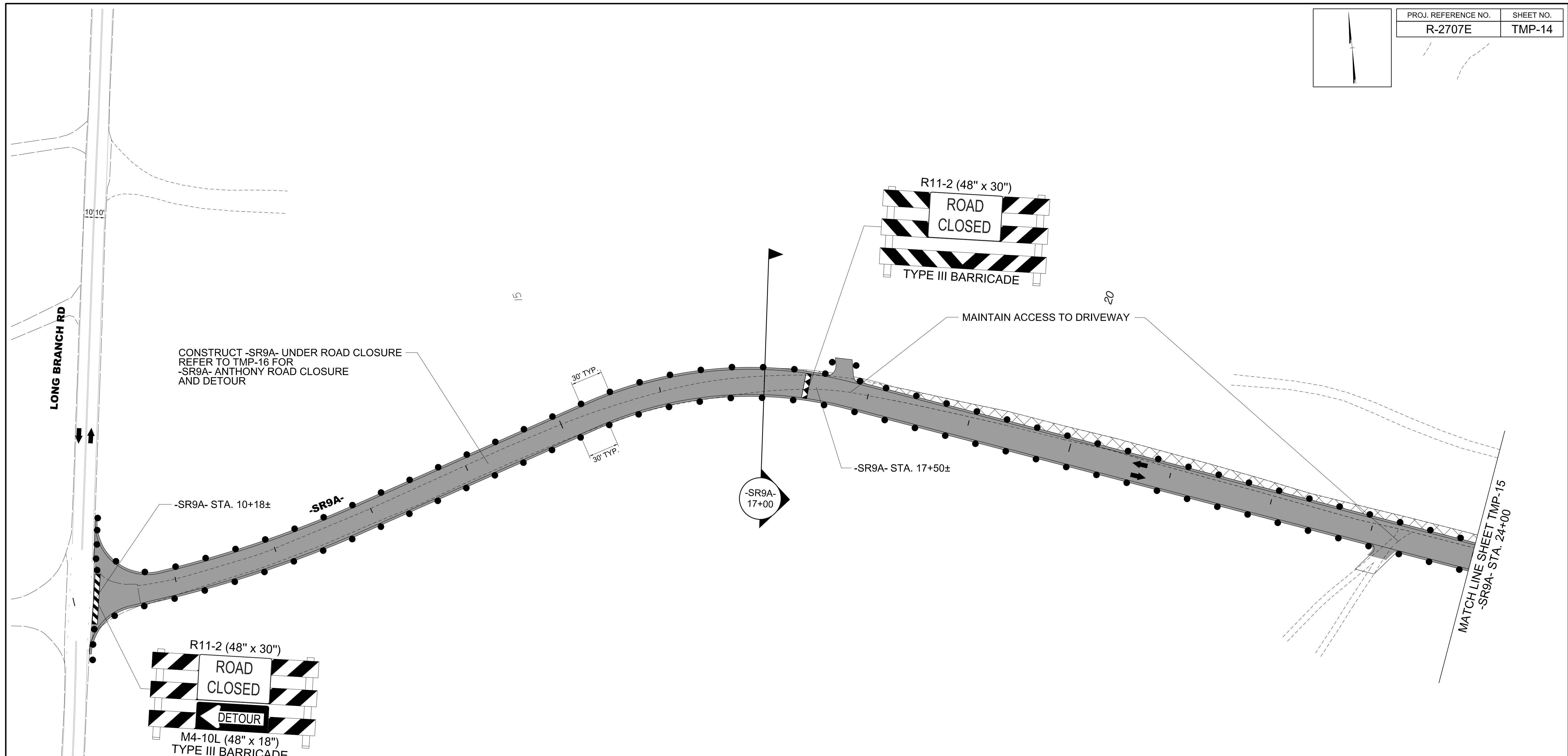
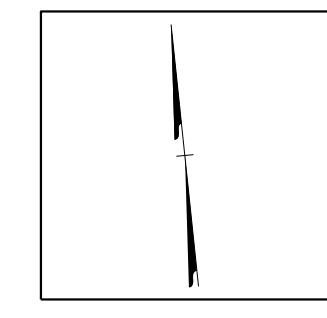
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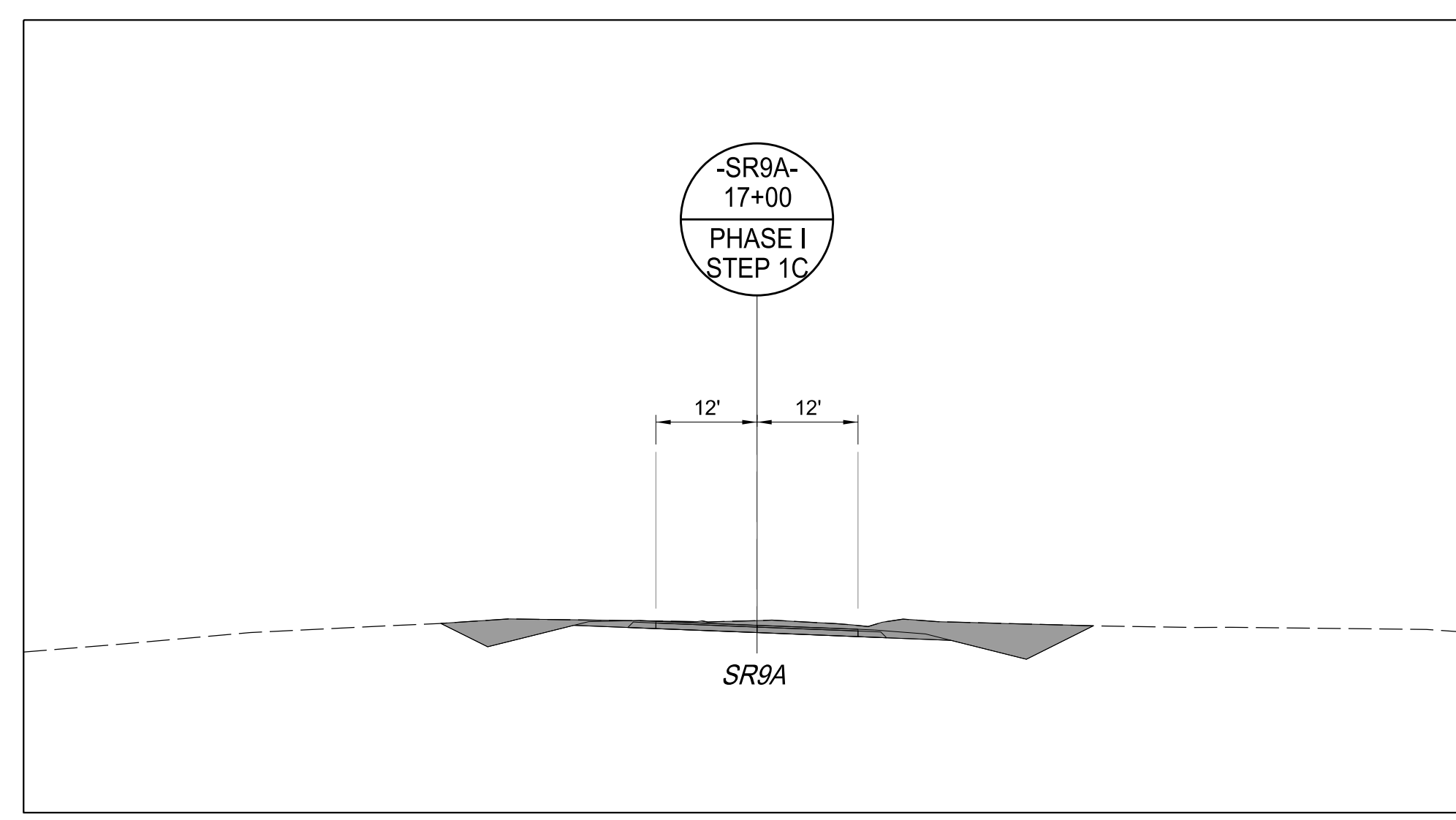
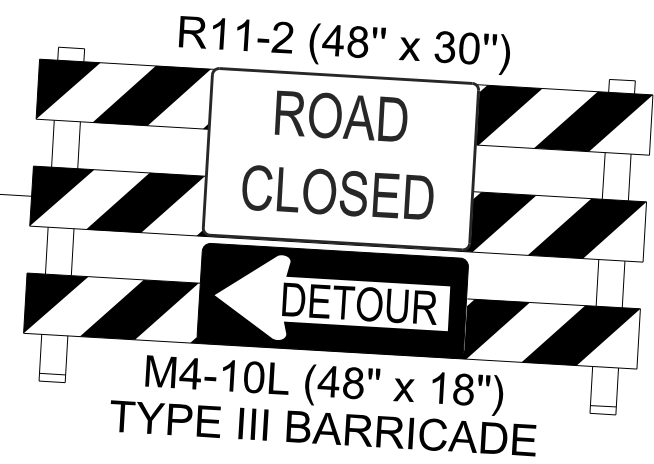
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-L- US 74
PHASE I
STEP 1C



CONSTRUCT -SR9A- UNDER ROAD CLOSURE
REFER TO TMP-16 FOR
-SR9A- ANTHONY ROAD CLOSURE
AND DETOUR

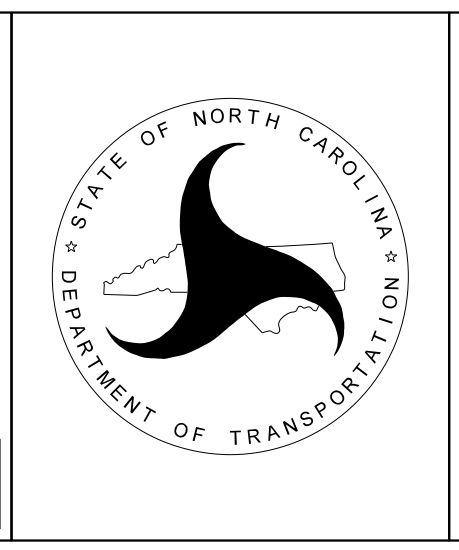


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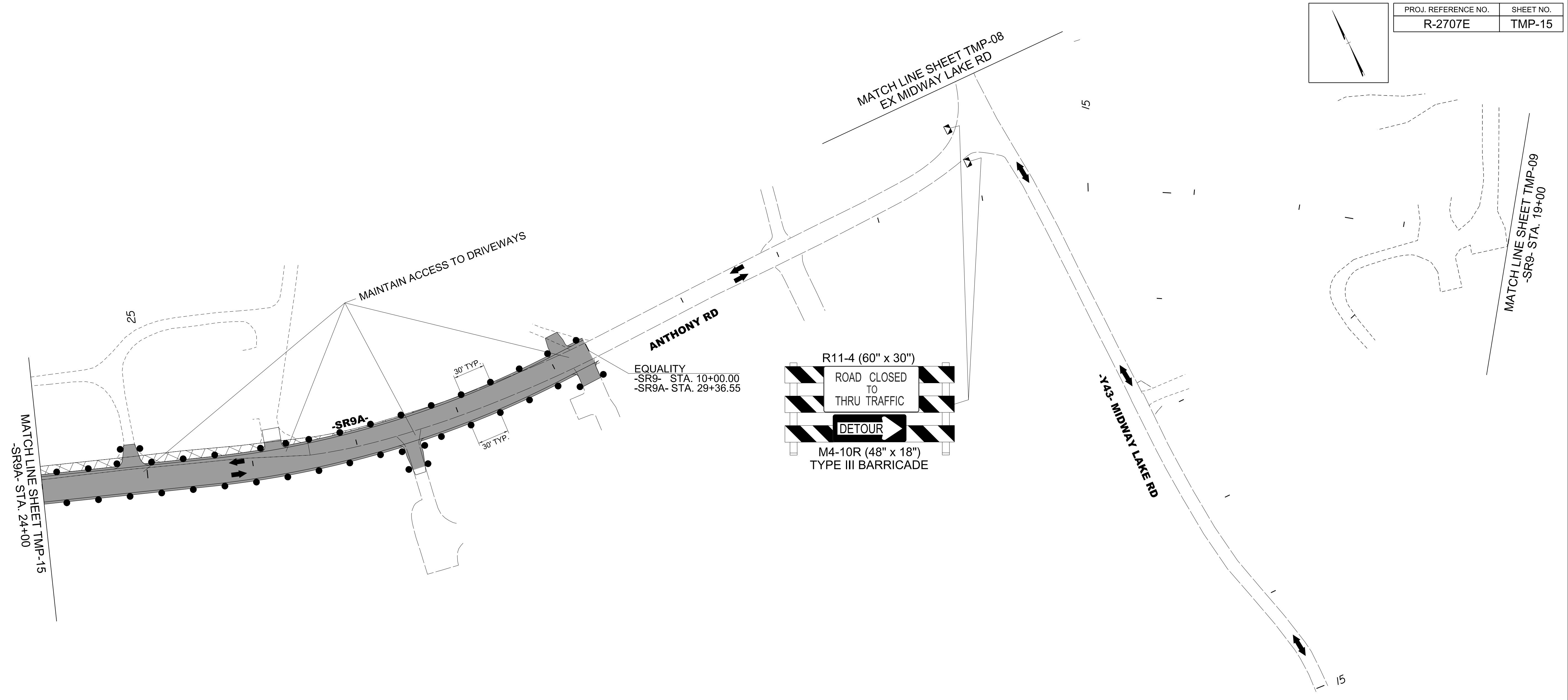
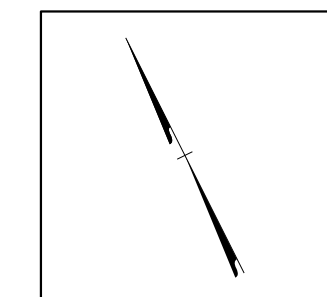
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**-L- US 74
PHASE I
STEP 1C**

PROJ. REFERENCE NO.	SHEET NO.
R-2707E	TMP-15



EQUALITY
 -SR9- STA. 10+00.00
 -SR9A- STA. 29+36.55

R11-4 (60" x 30")
 ROAD CLOSED
 TO
 THRU TRAFFIC
 M4-10R (48" x 18")
 TYPE III BARRICADE

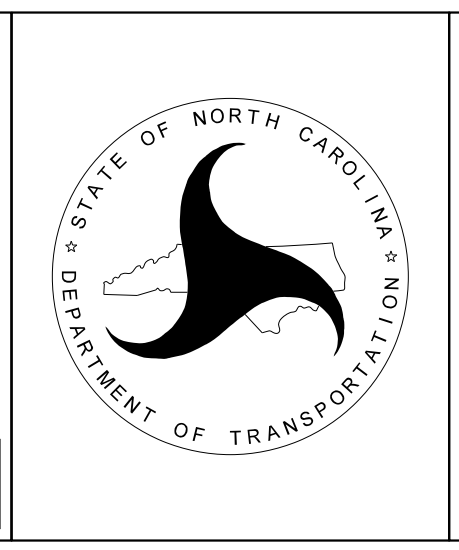
DETOUR

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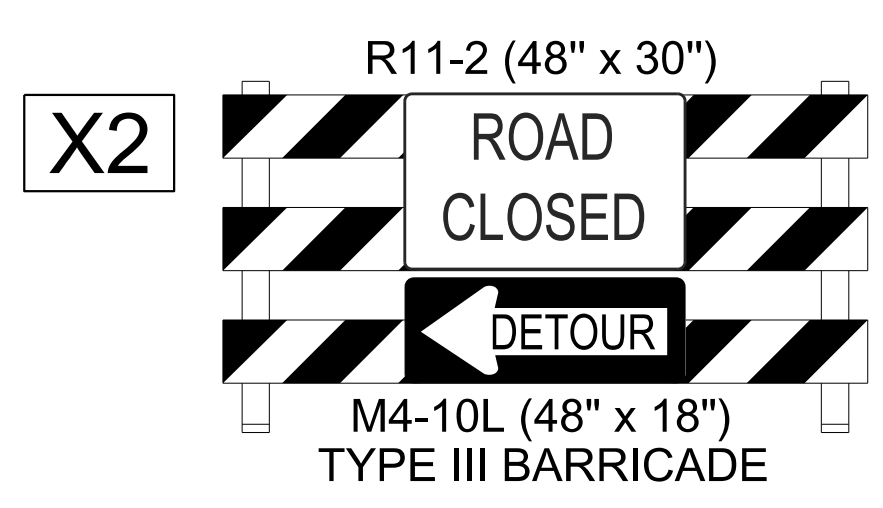
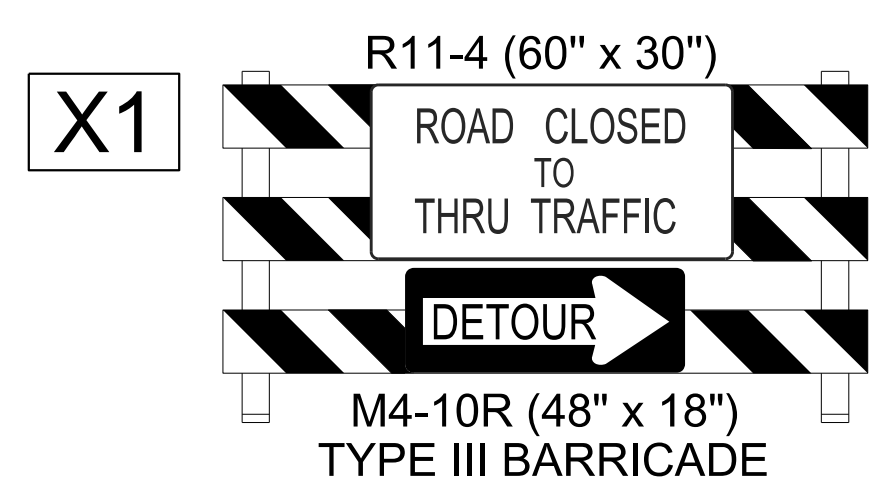
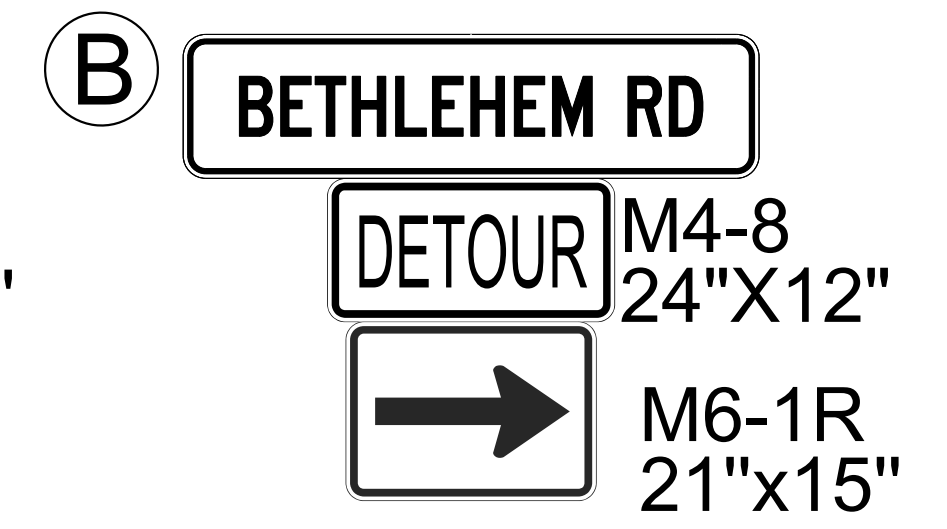
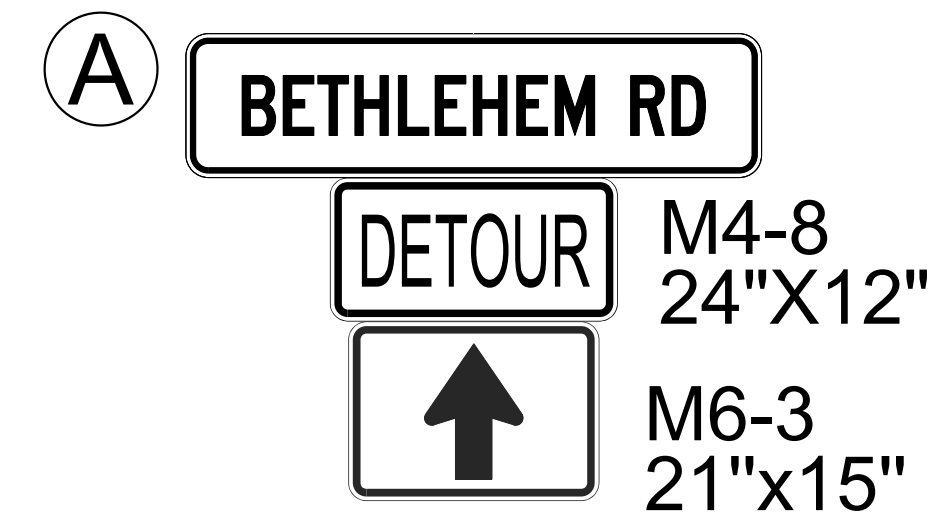
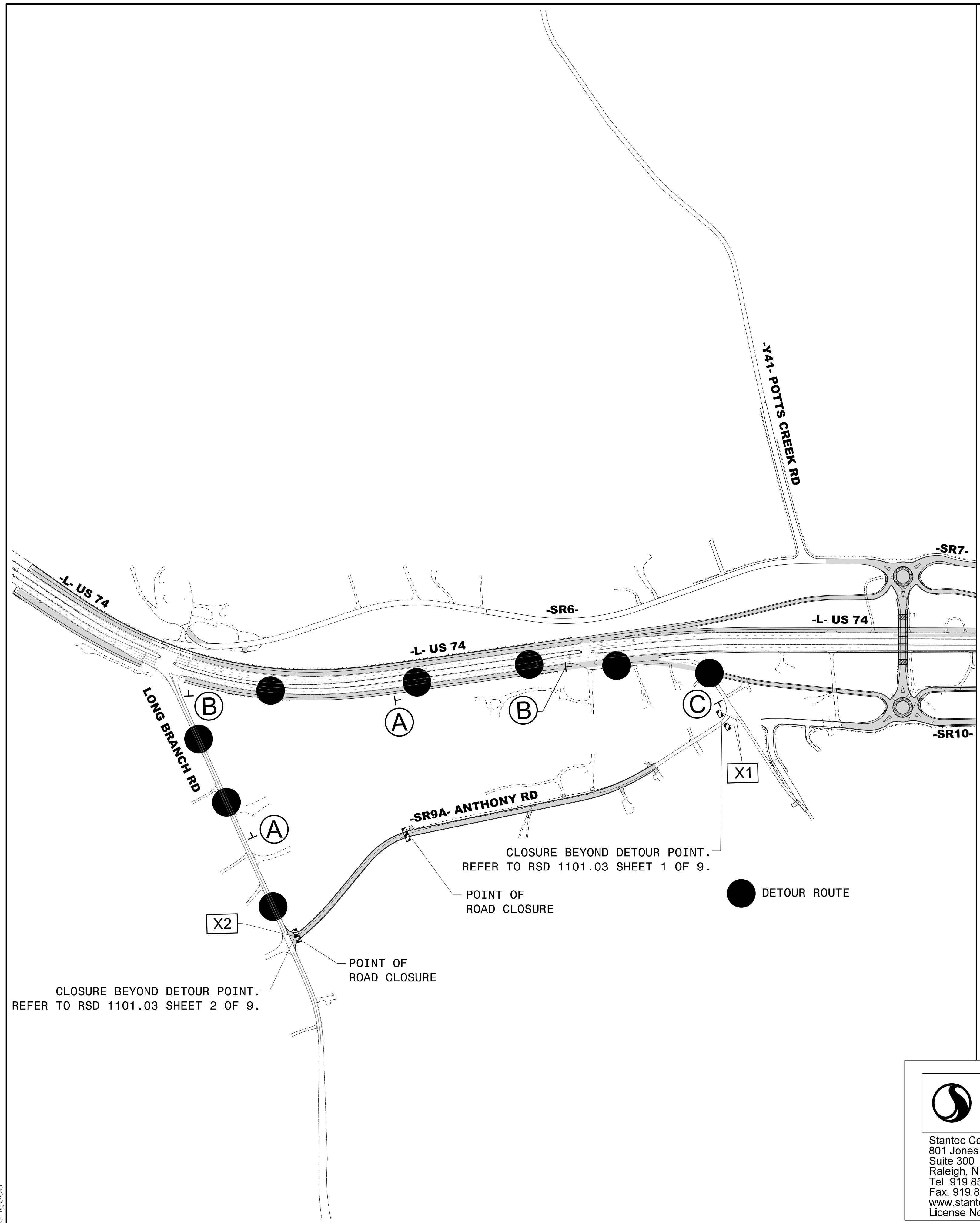
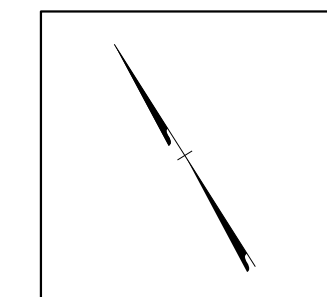
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-L- US 74
 PHASE I
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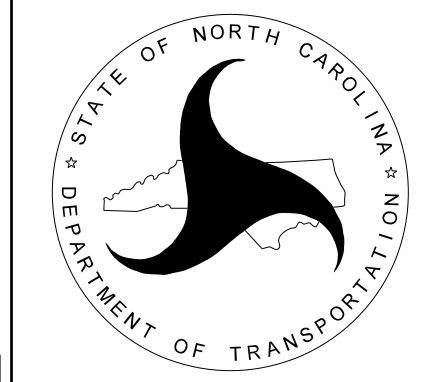
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REFER TO RSD 1101.03 SHEET 2 OF 9.

CLOSURE BEYOND DETOUR POINT.
REFER TO RSD 1101.03 SHEET 1 OF 9.

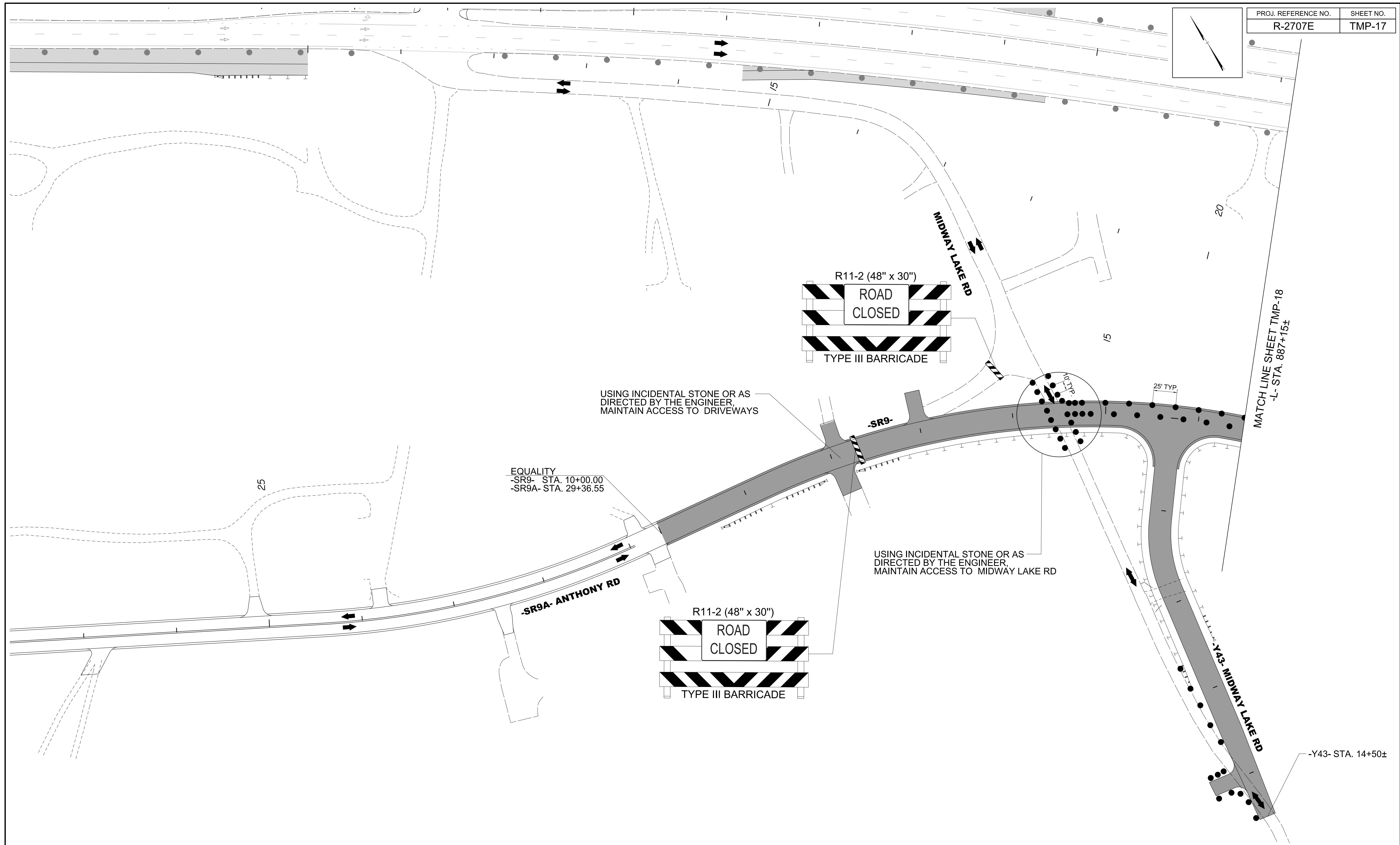
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-SR9A- ANTHONY RD
DETOUR ROUTE



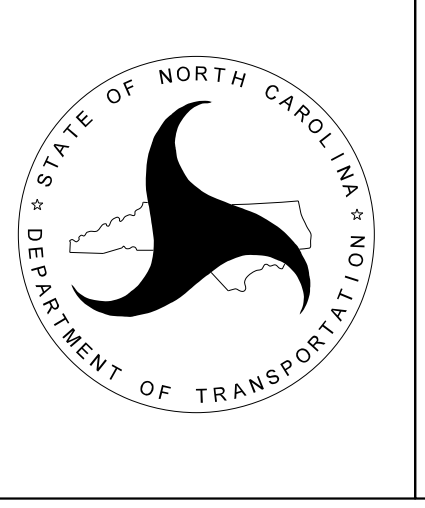
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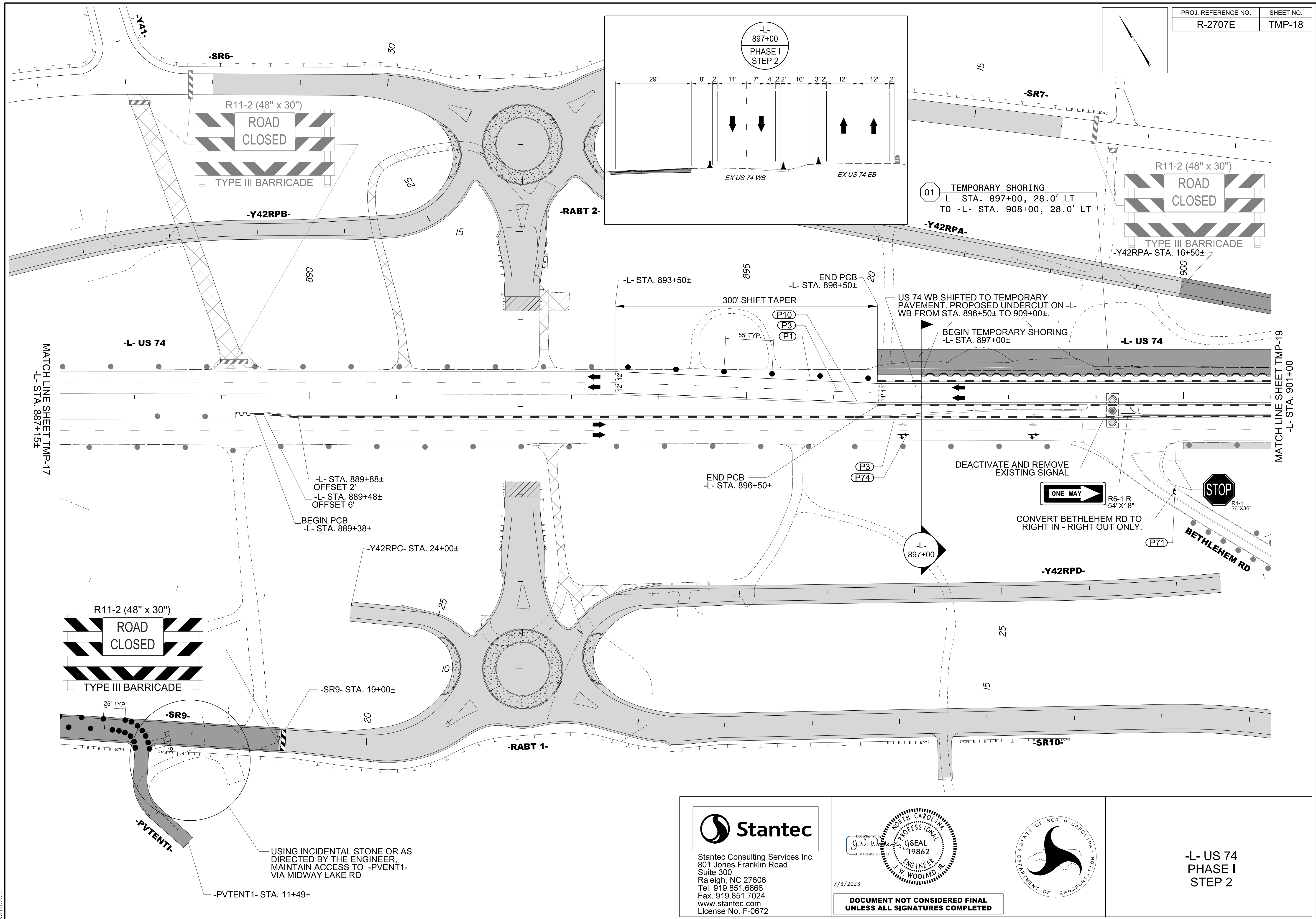
DocuSigned by:
 J.W. Woolard
 BB02F49E83C
 7/3/2023

PROFESSIONAL SEAL
 19862
 ENGINEER
 J. W. WOOLARD, P.E.

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-L- US 74
PHASE I
STEP 2



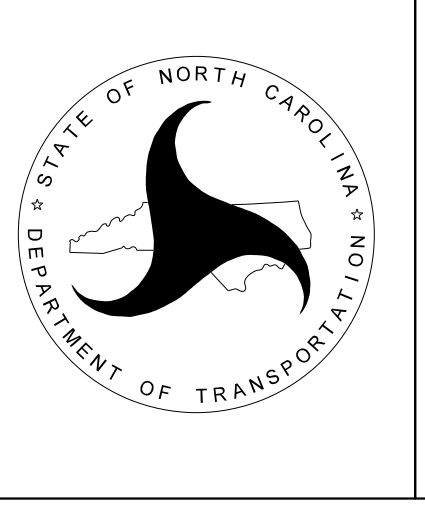
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MATCH LINE SHEET TMP-19
-L- STA. 901+00

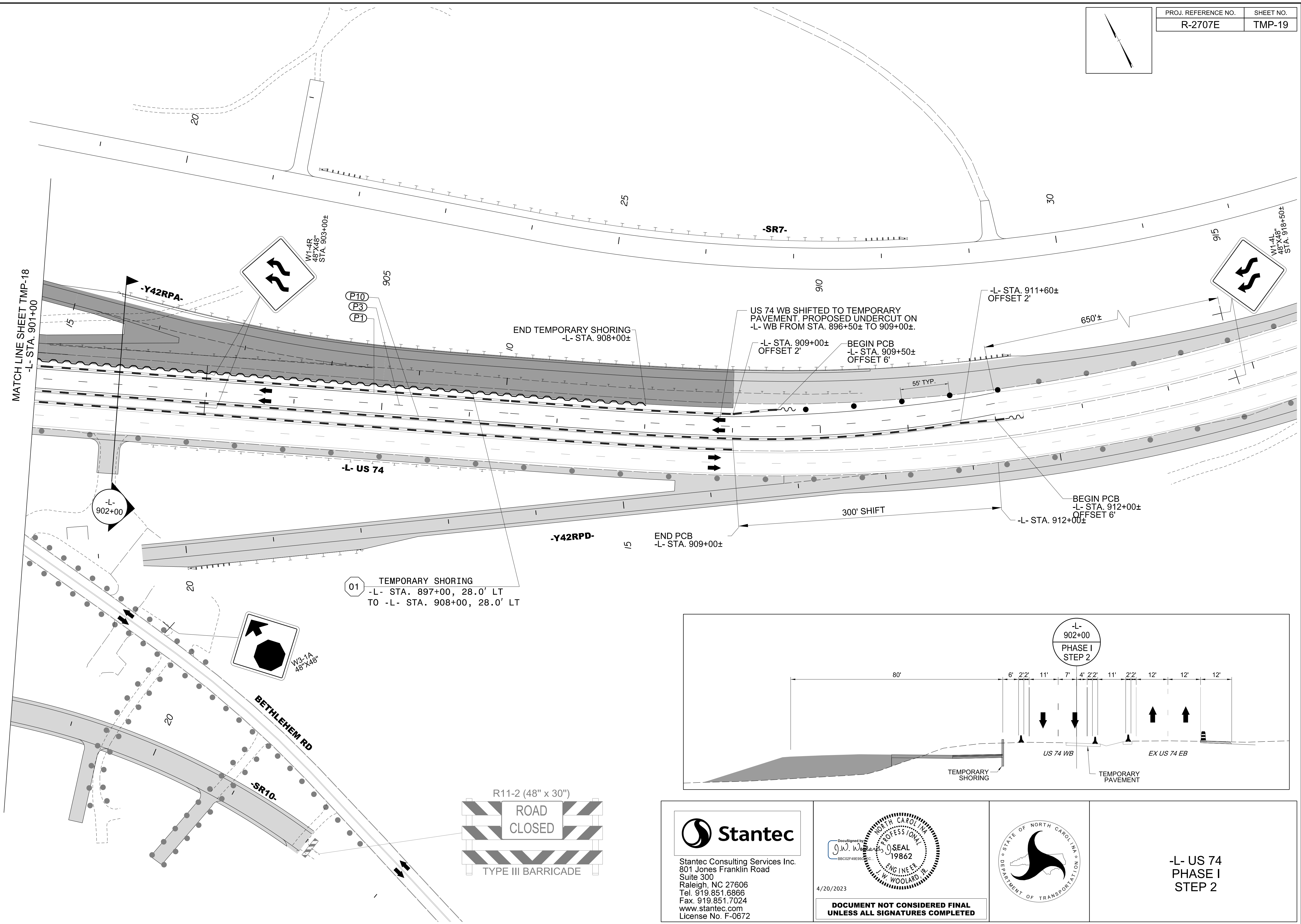
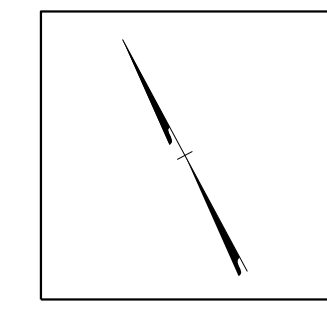
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 PHASE I
 STEP 2**



MATCH LINE SHEET TMP-18
-L- STA. 901+00

-L- 902+00

01 TEMPORARY SHORING
-L- STA. 897+00, 28.0' LT
TO -L- STA. 908+00, 28.0' LT

END PCB
-L- STA. 909+00±

US 74 WB SHIFTED TO TEMPORARY PAVEMENT, PROPOSED UNDERCUT ON -L- WB FROM STA. 896+50± TO 909+00±.

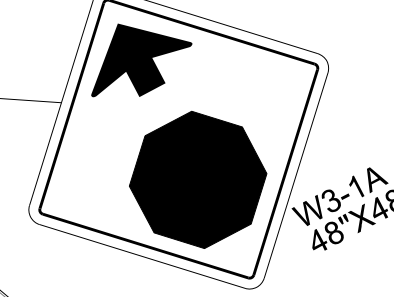
-L- STA. 909+00± OFFSET 2'

BEGIN PCB
-L- STA. 909+50± OFFSET 6'

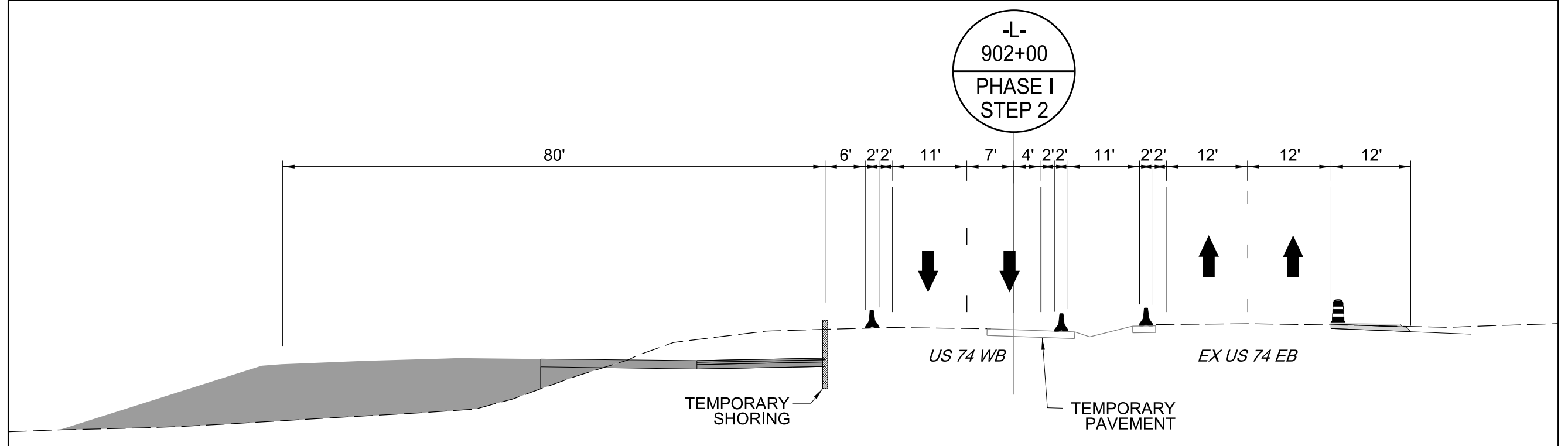
-L- STA. 911+60± OFFSET 2'

BEGIN PCB
-L- STA. 912+00± OFFSET 6'

-L- STA. 912+00±



BETHLEHEM RD



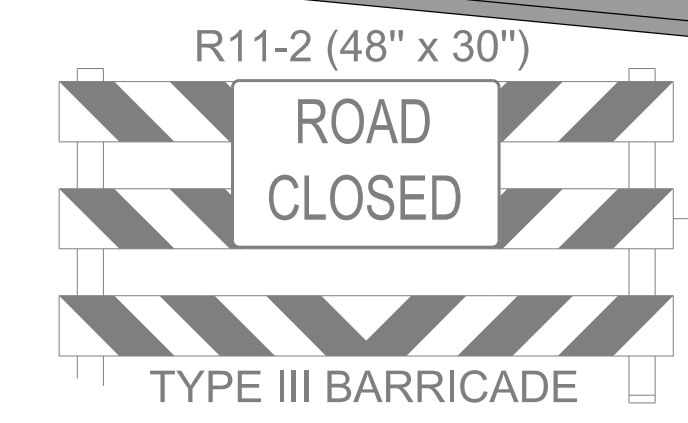
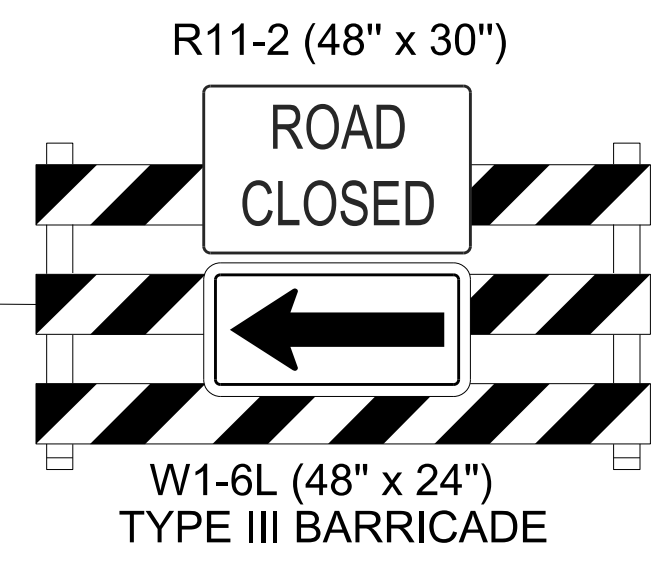
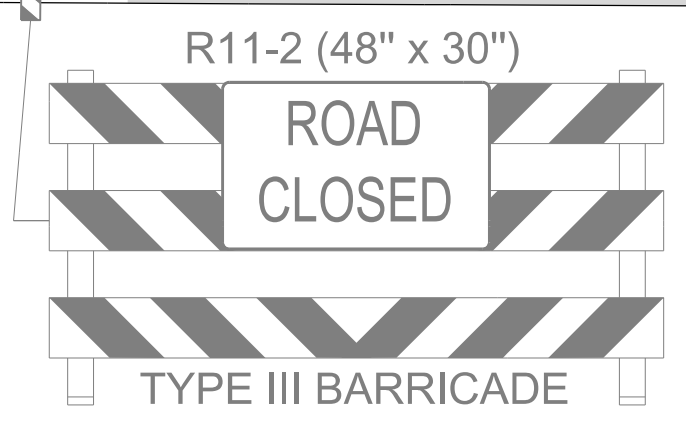
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-L- US 74
PHASE I
STEP 2

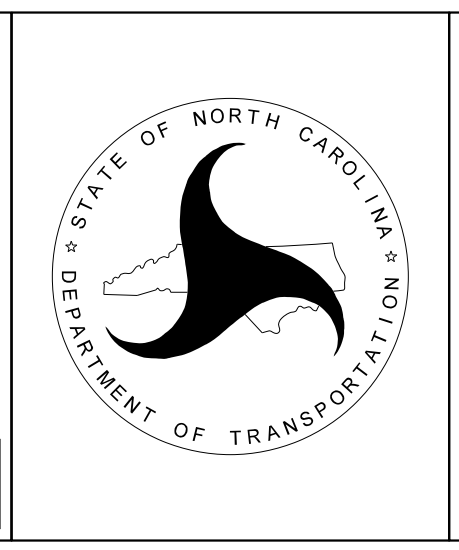


EQUALITY
 -SR9- STA. 10+00.00
 -SR9A- STA. 29+36.55

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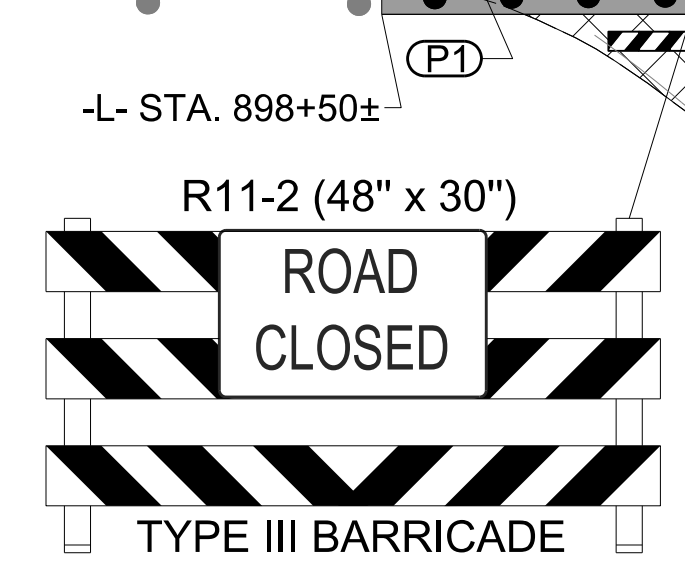
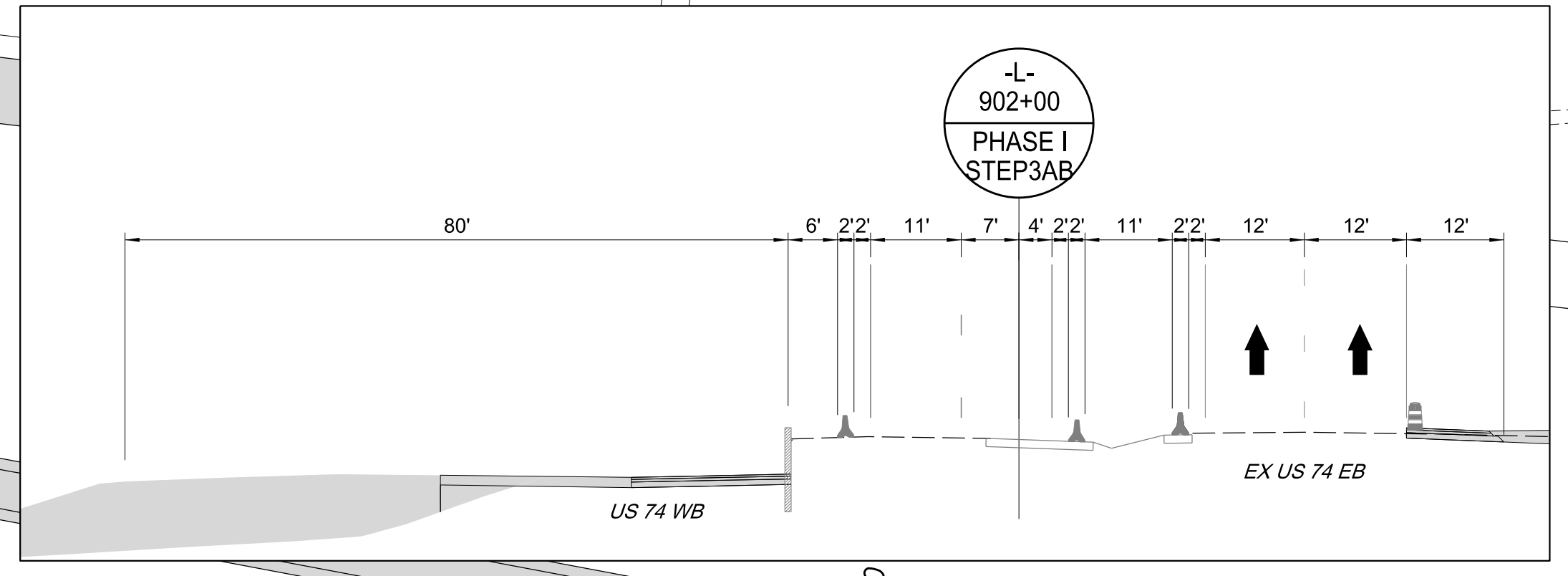
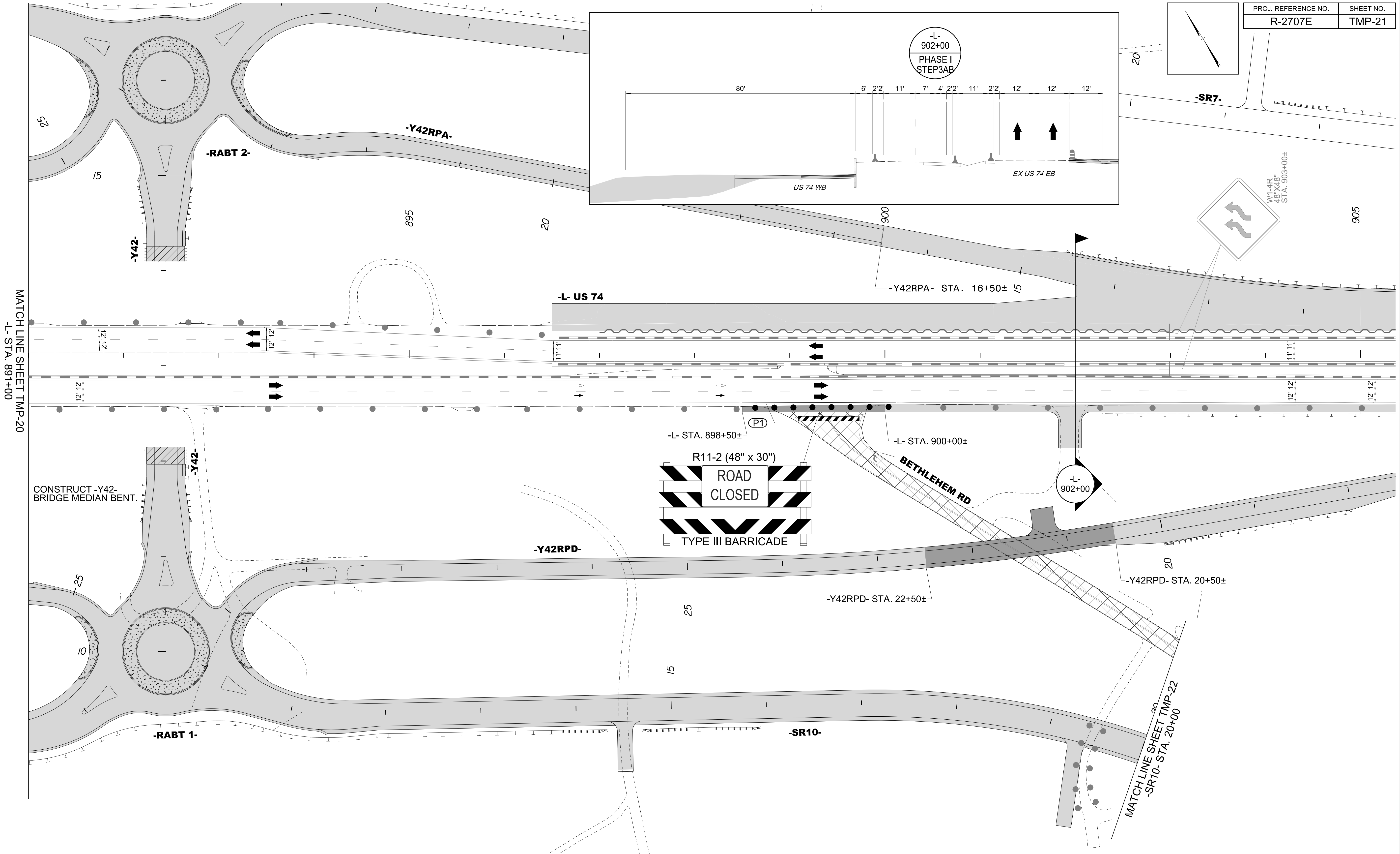
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**-L- US 74
 PHASE I
 STEP 3AB**

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MATCH LINE SHEET TMP-21
 -L- STA. 891+00



MATCH LINE SHEET TMP-20
-L- STA. 891+00

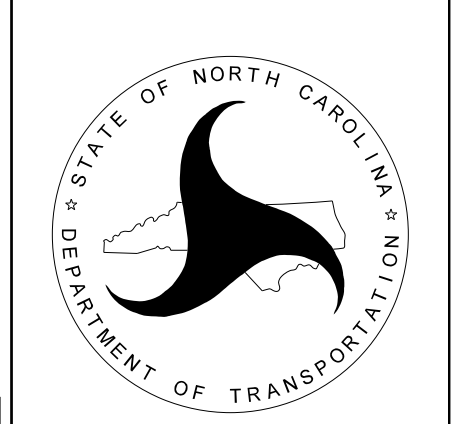
CONSTRUCT -Y42-
BRIDGE MEDIAN BENT.

MATCH LINE SHEET TMP-22
-SR10- STA. 20+00

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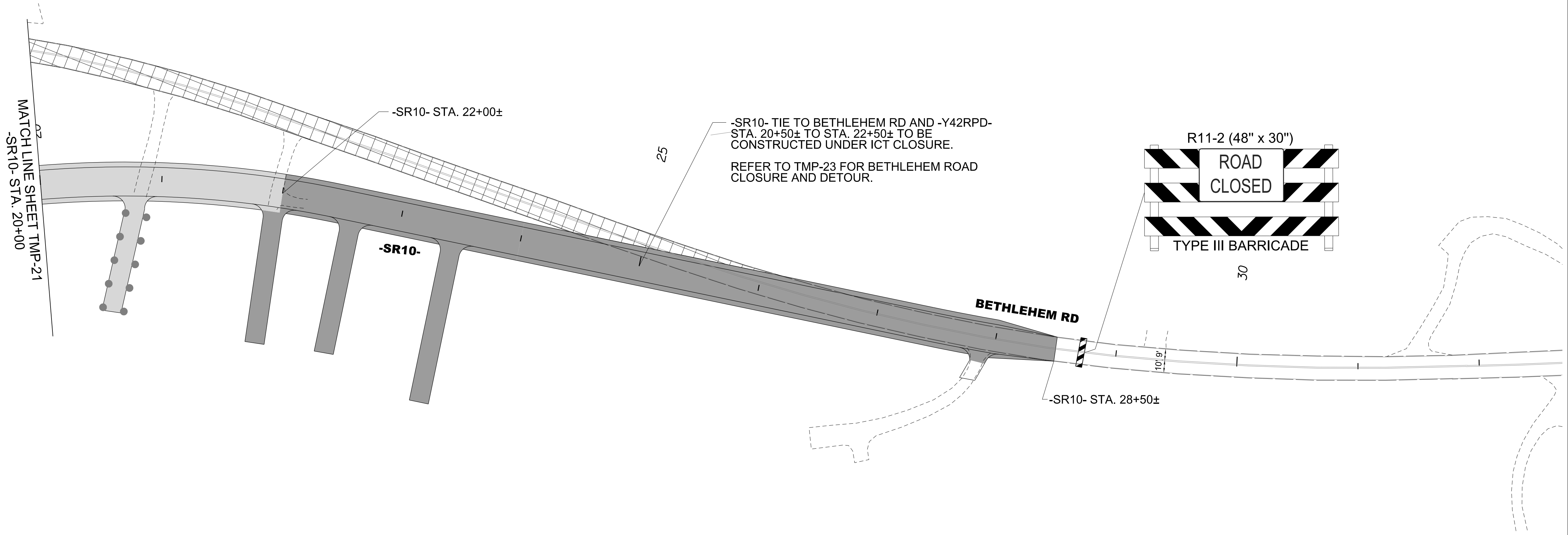
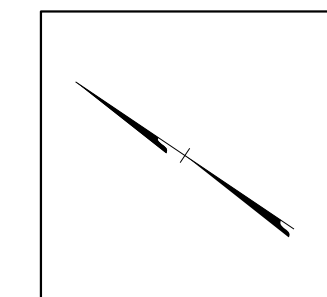
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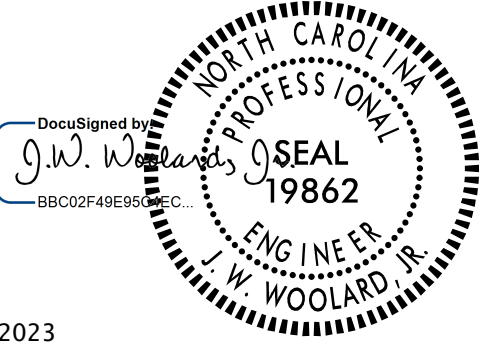
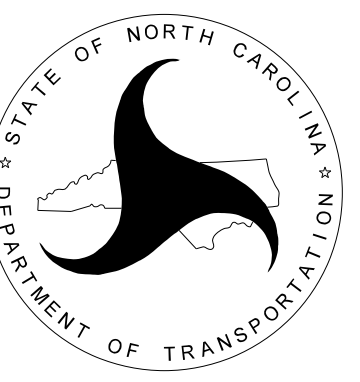


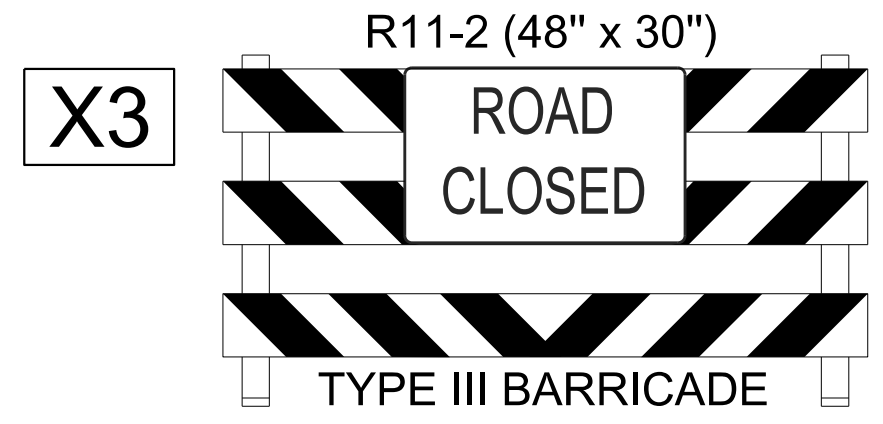
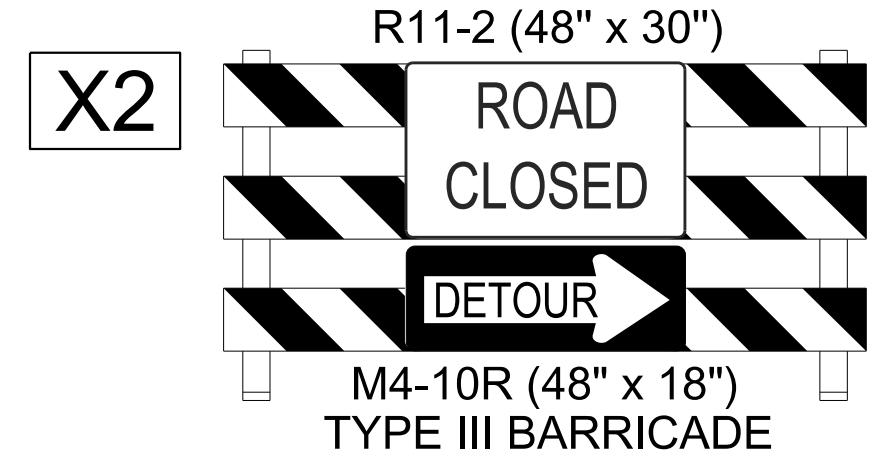
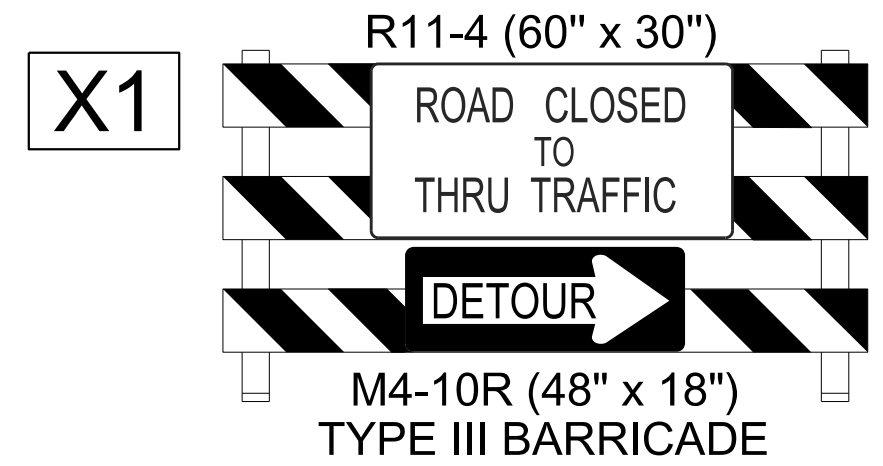
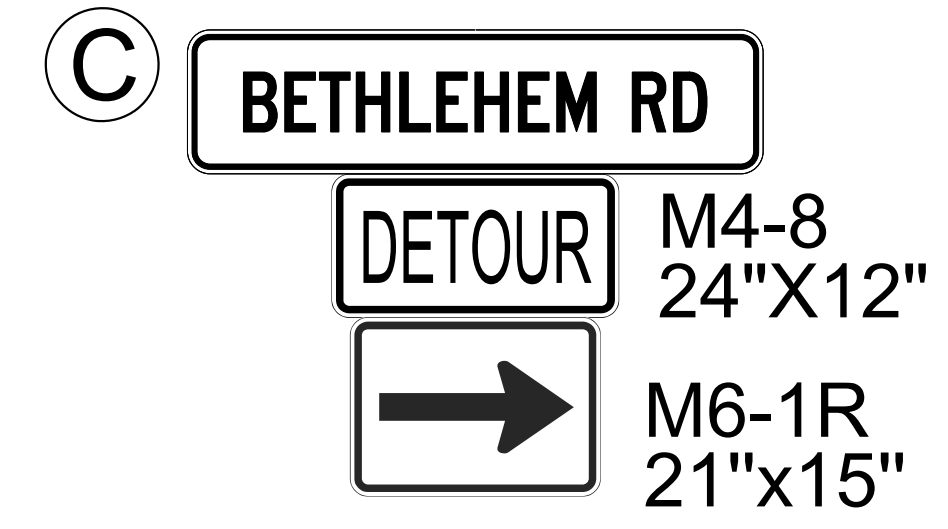
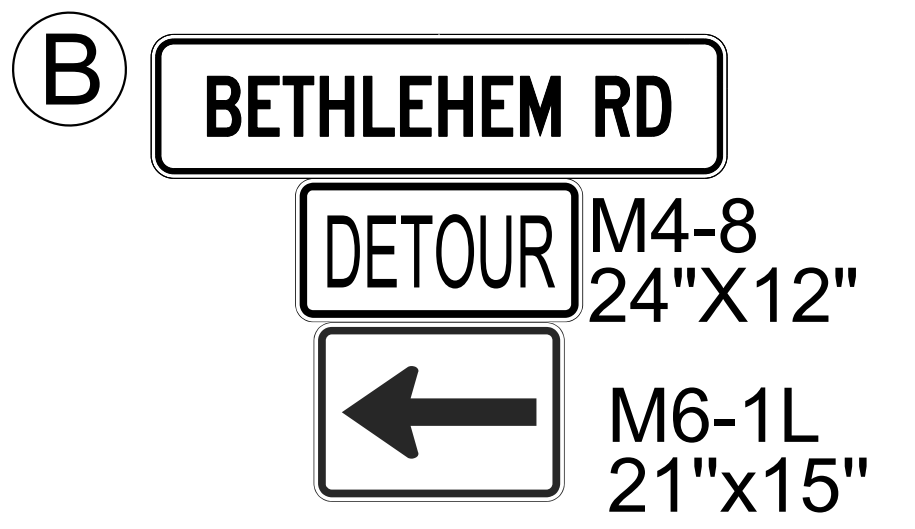
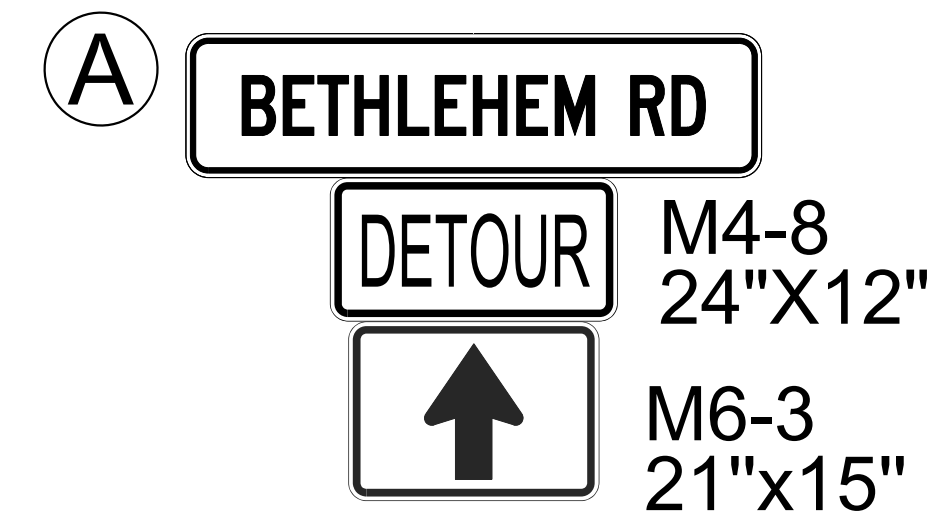
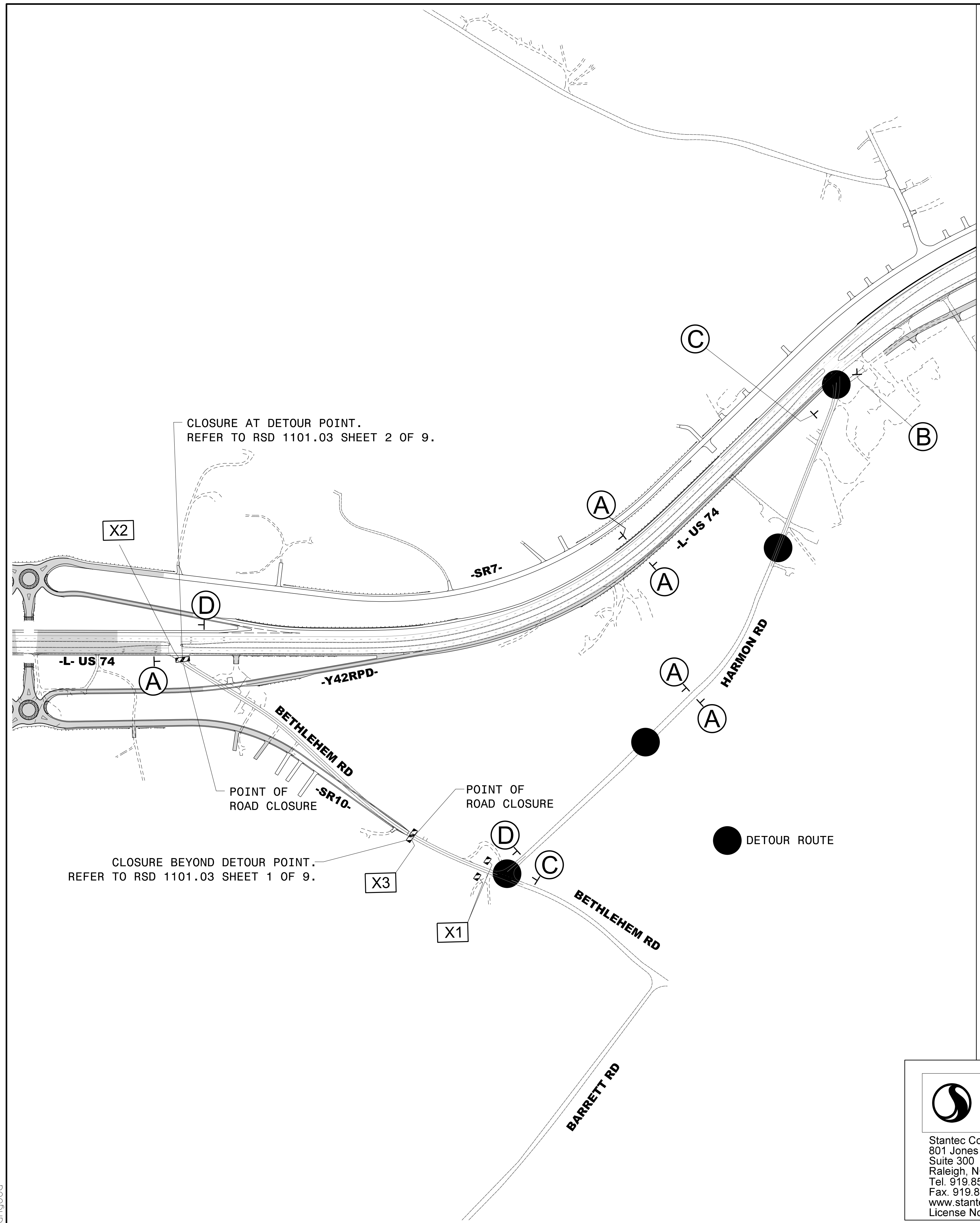
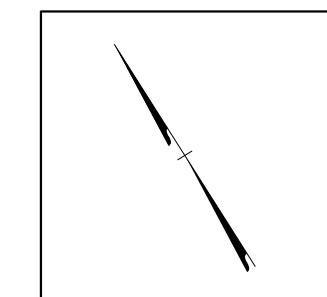
-L- US 74
PHASE I
STEP 3AB

PROJ. REFERENCE NO.	SHEET NO.
R-2707E	TMP-22



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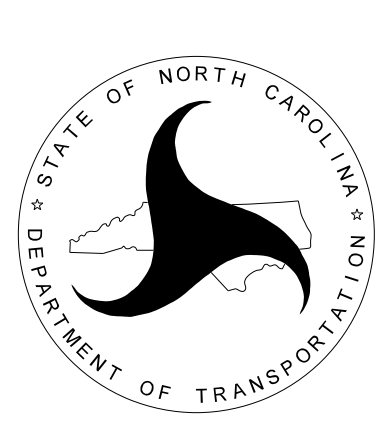
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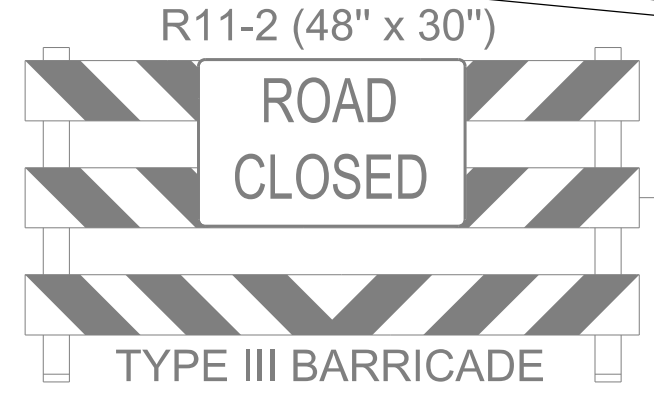
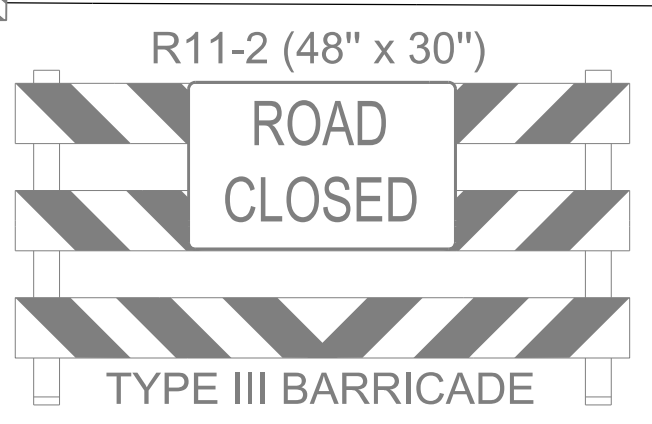
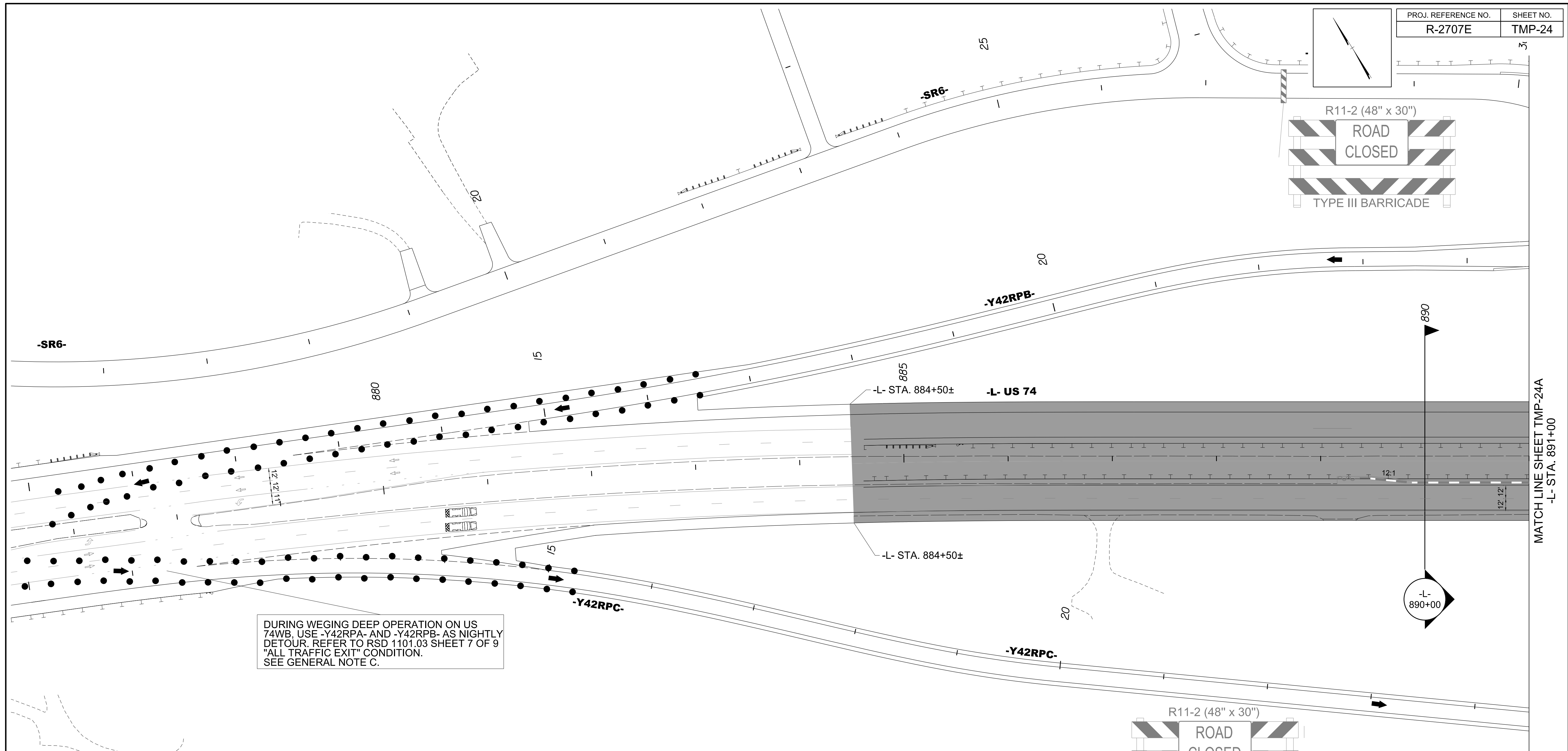
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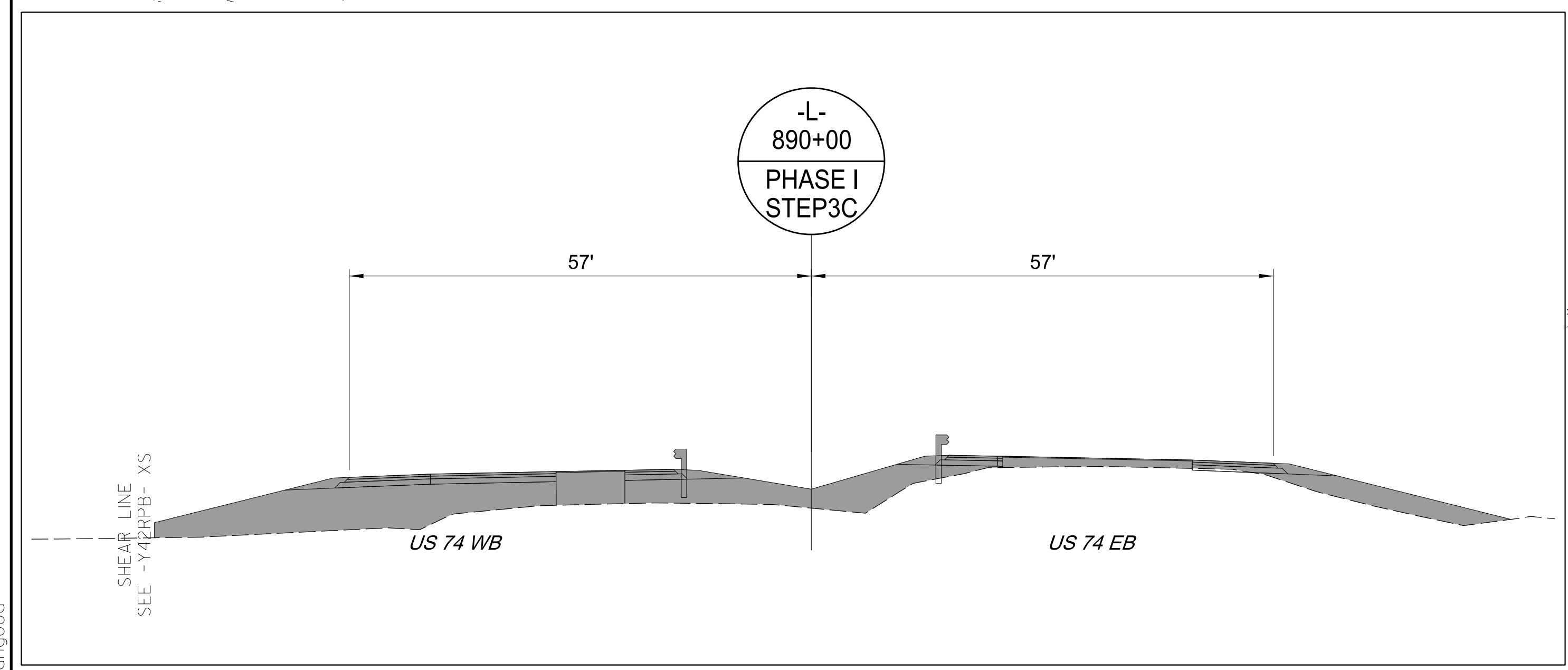


**-SR10- BETHLEHEM RD
DETOUR ROUTE**



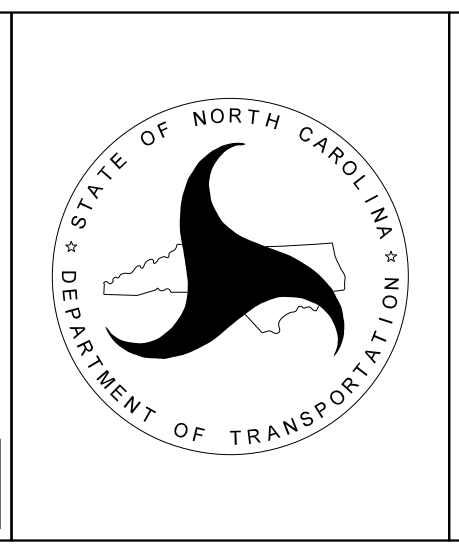
DURING WEGING DEEP OPERATION ON US 74WB, USE -Y42RPA- AND -Y42RPB- AS NIGHTLY DETOUR. REFER TO RSD 1101.03 SHEET 7 OF 9 "ALL TRAFFIC EXIT" CONDITION. SEE GENERAL NOTE C.

MATCH LINE SHEET TMP-24A
-L- STA. 891+00



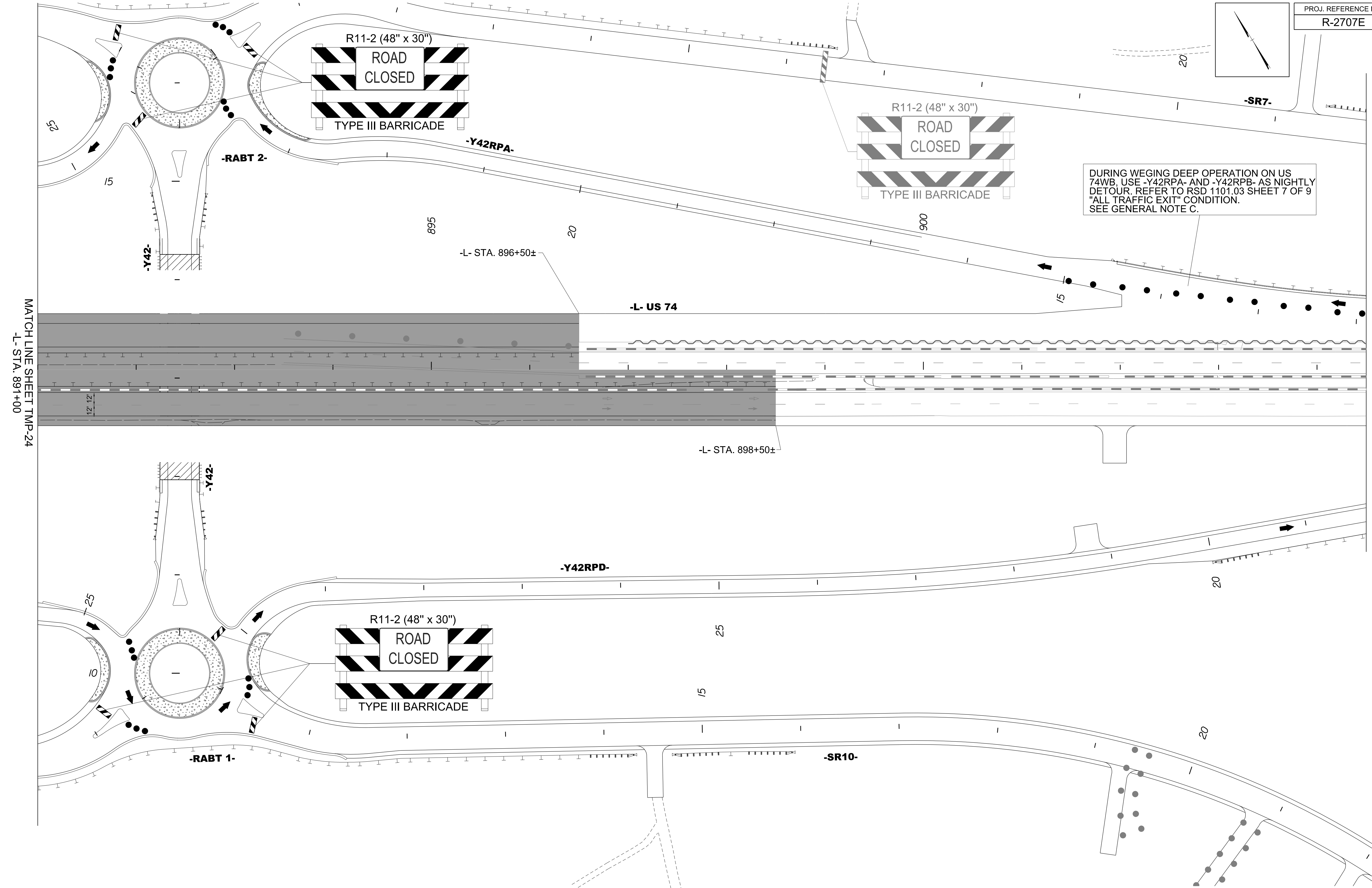
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**-L- US 74
 PHASE I
 STEP 3C**

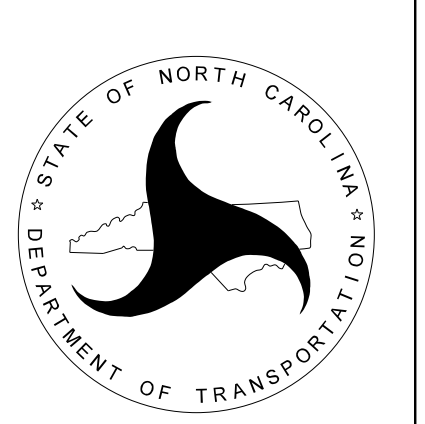
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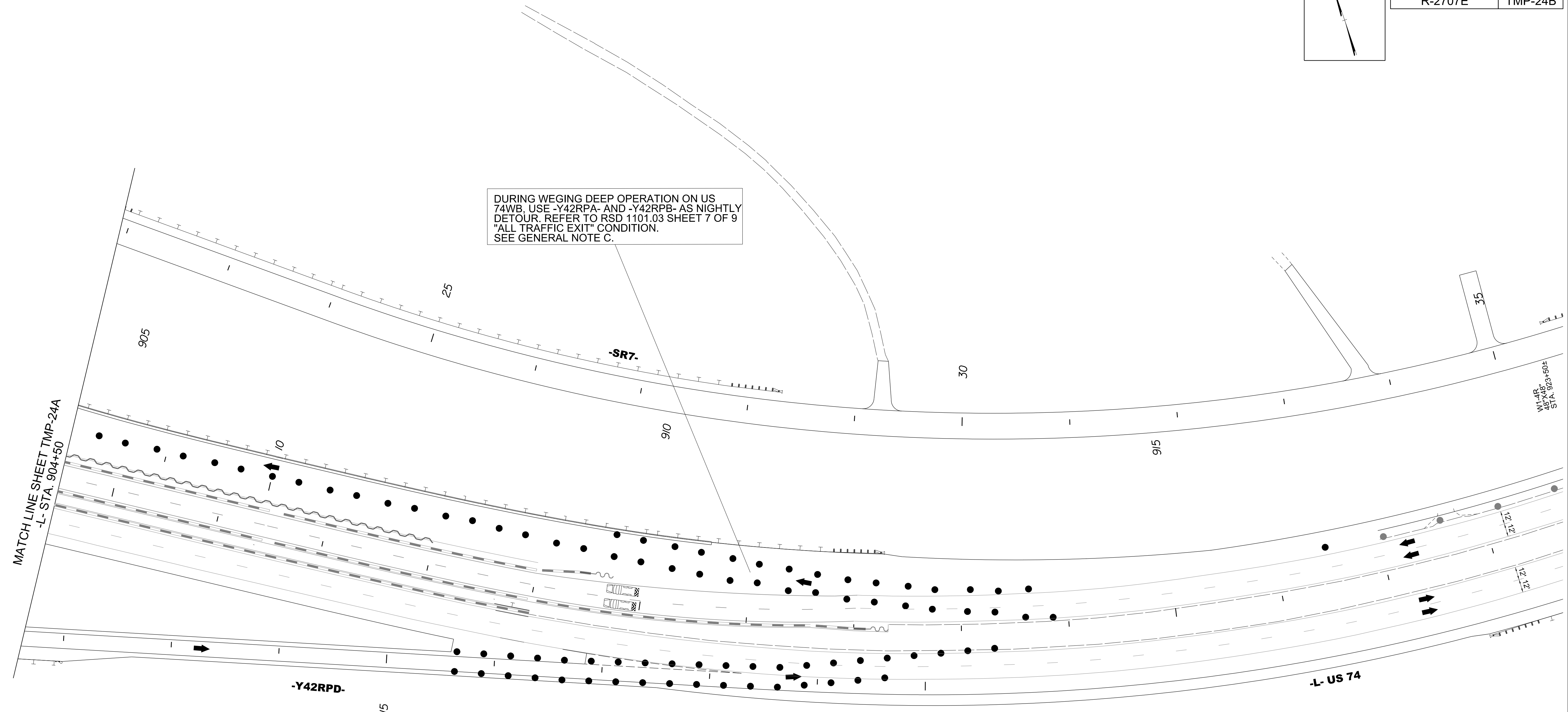
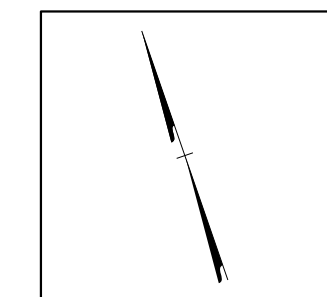
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**-L- US 74
PHASE I
STEP 3C**



MATCHLINE SHEET TMP-24A
-L- STA. 904+50

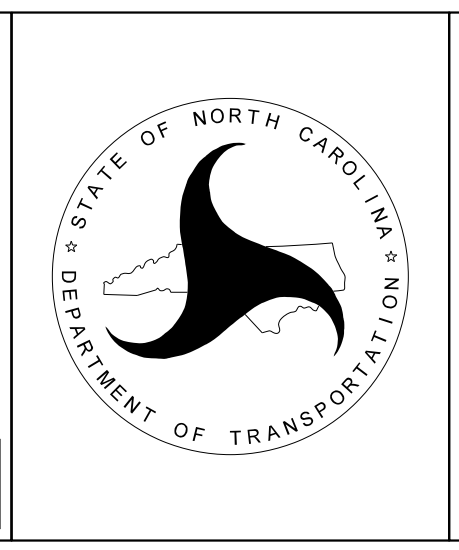
W/AR
48'X45'
STA. 923+50±

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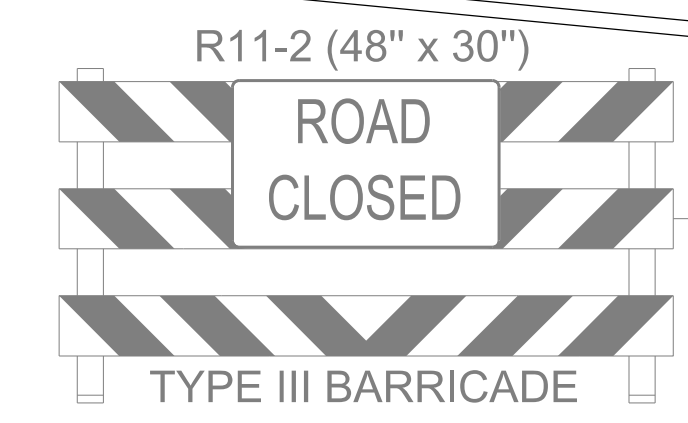
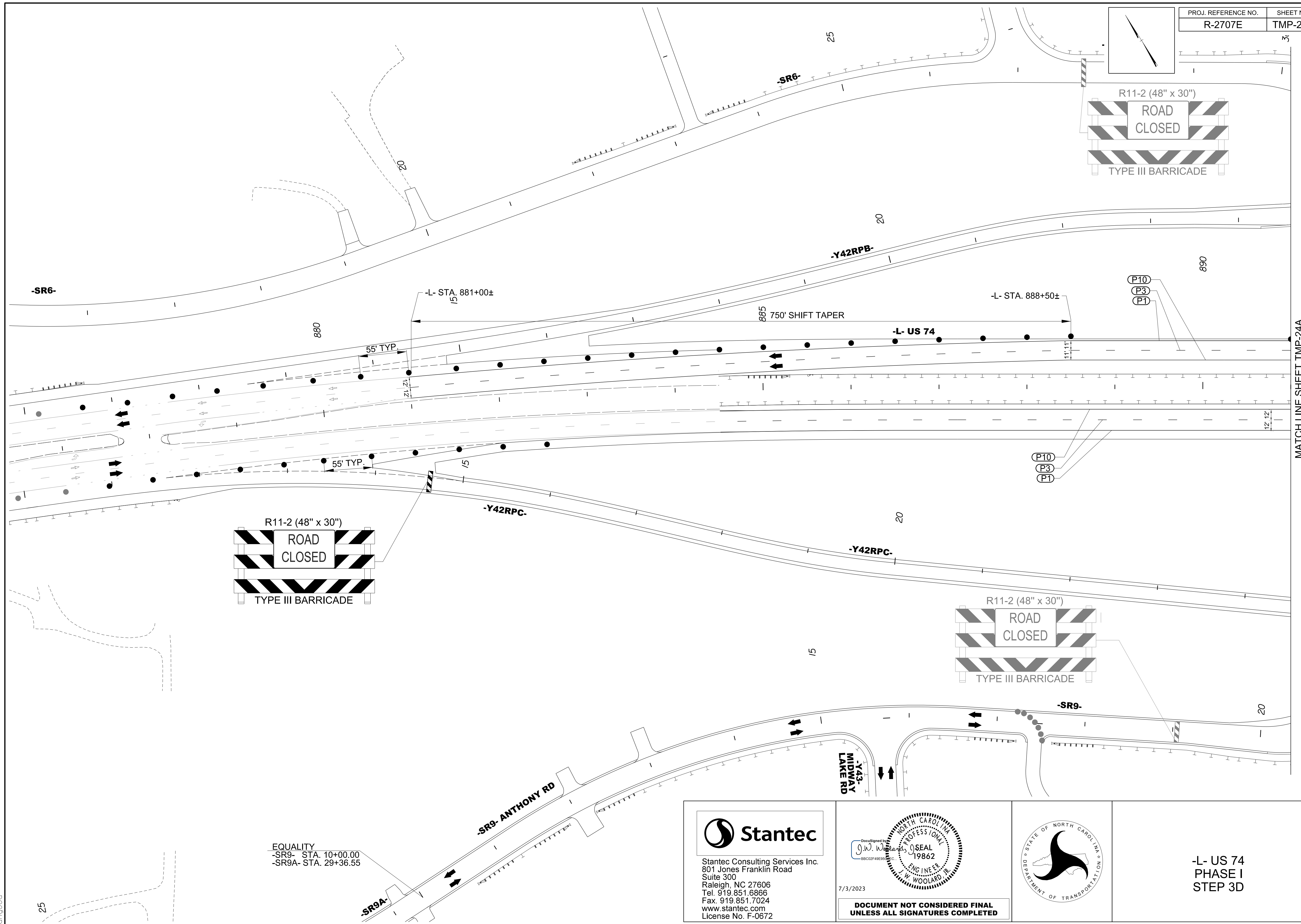
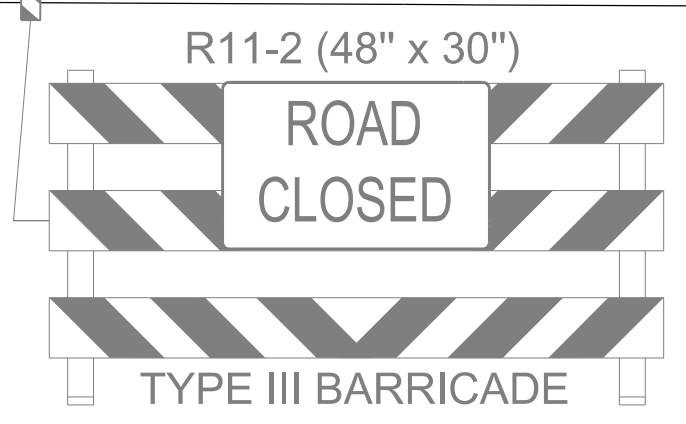
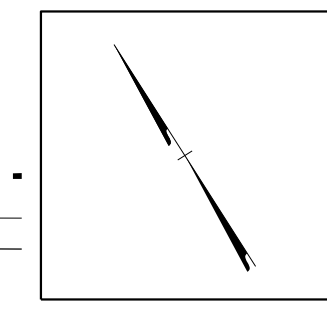
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PHASE I
STEP 3C

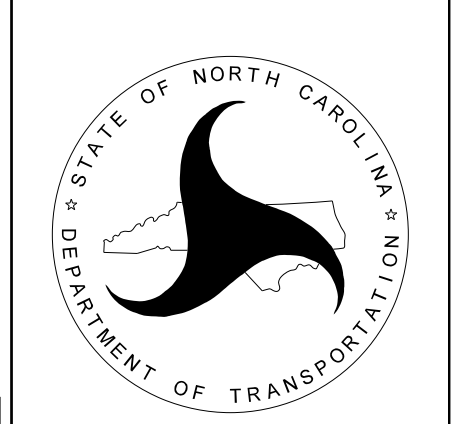


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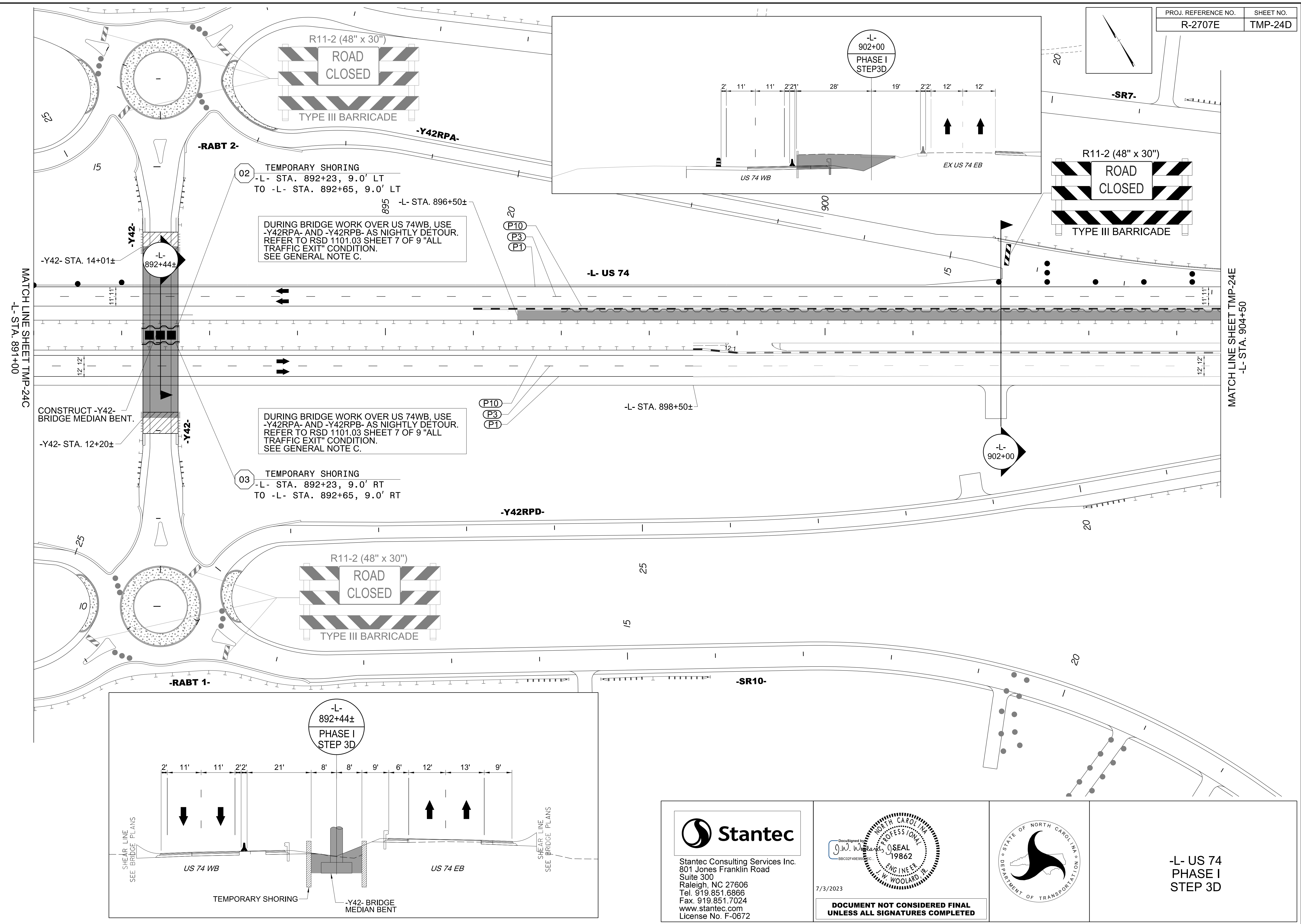
EQUALITY
 -SR9- STA. 10+00.00
 -SR9A- STA. 29+36.55

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 19862
 ENGINEER
 W. WOOLARD, R.
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-L- US 74
 PHASE I
 STEP 3D

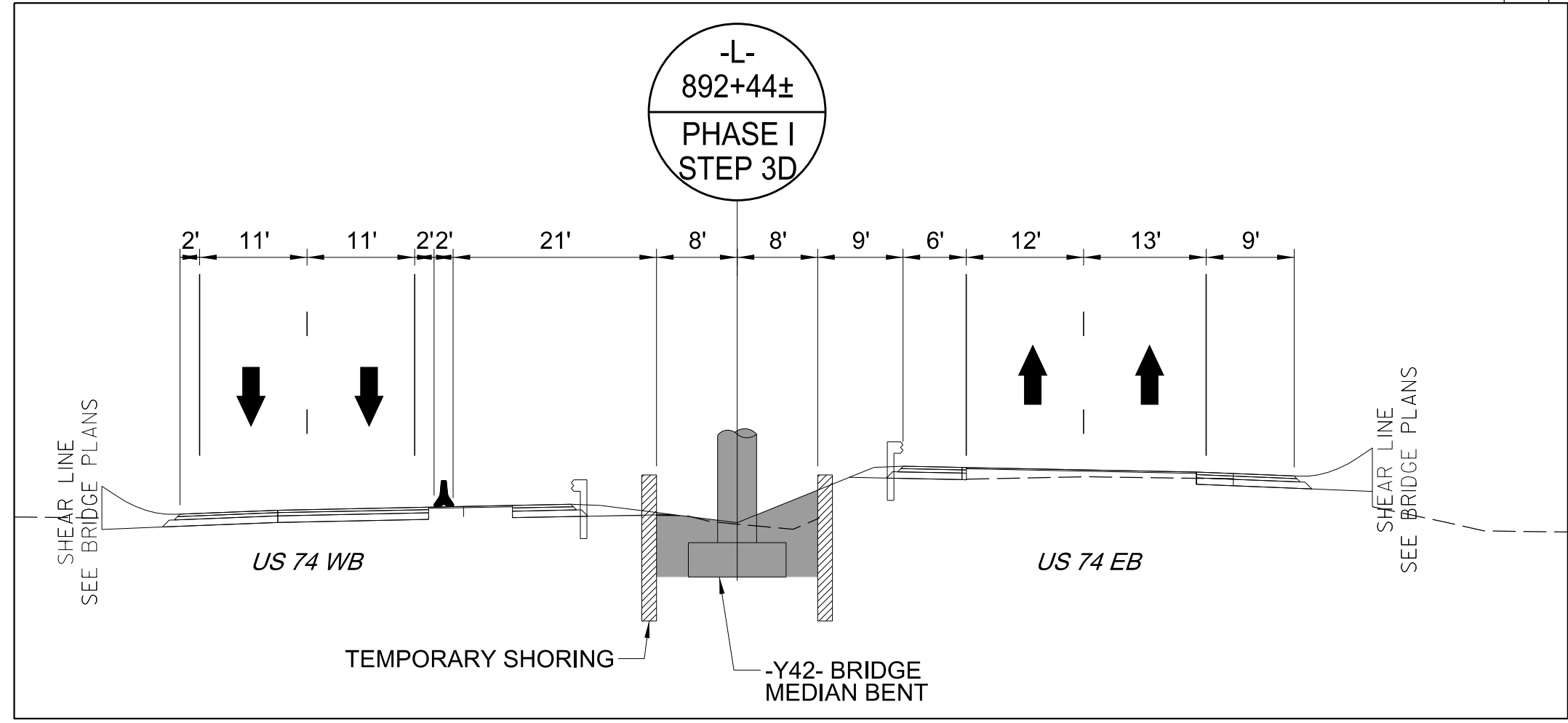
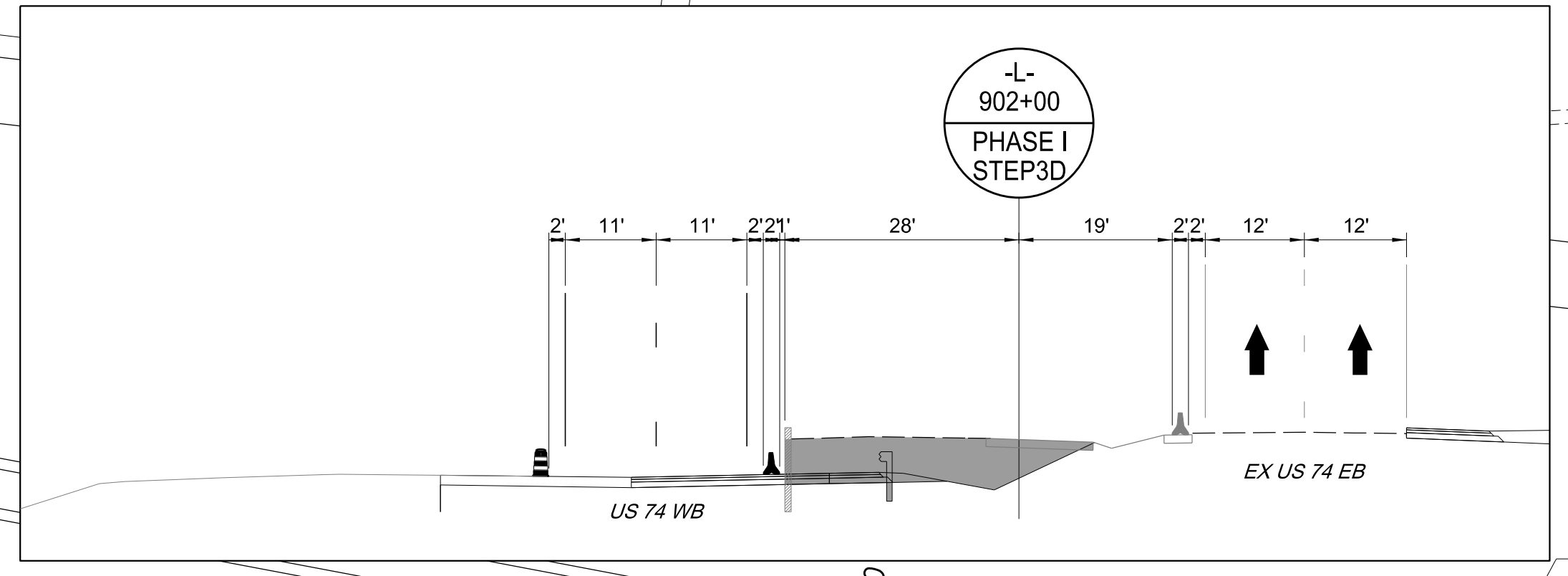


02 TEMPORARY SHORING
 -L- STA. 892+23, 9.0' LT
 TO -L- STA. 892+65, 9.0' LT

DURING BRIDGE WORK OVER US 74WB, USE -Y42RPA- AND -Y42RPB- AS NIGHTLY DETOUR. REFER TO RSD 1101.03 SHEET 7 OF 9 "ALL TRAFFIC EXIT" CONDITION. SEE GENERAL NOTE C.

03 TEMPORARY SHORING
 -L- STA. 892+23, 9.0' RT
 TO -L- STA. 892+65, 9.0' RT

DURING BRIDGE WORK OVER US 74WB, USE -Y42RPA- AND -Y42RPB- AS NIGHTLY DETOUR. REFER TO RSD 1101.03 SHEET 7 OF 9 "ALL TRAFFIC EXIT" CONDITION. SEE GENERAL NOTE C.

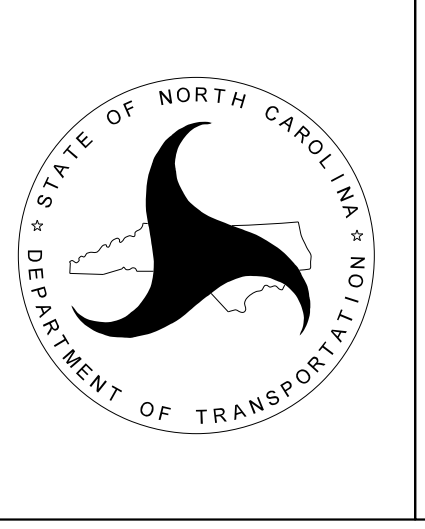


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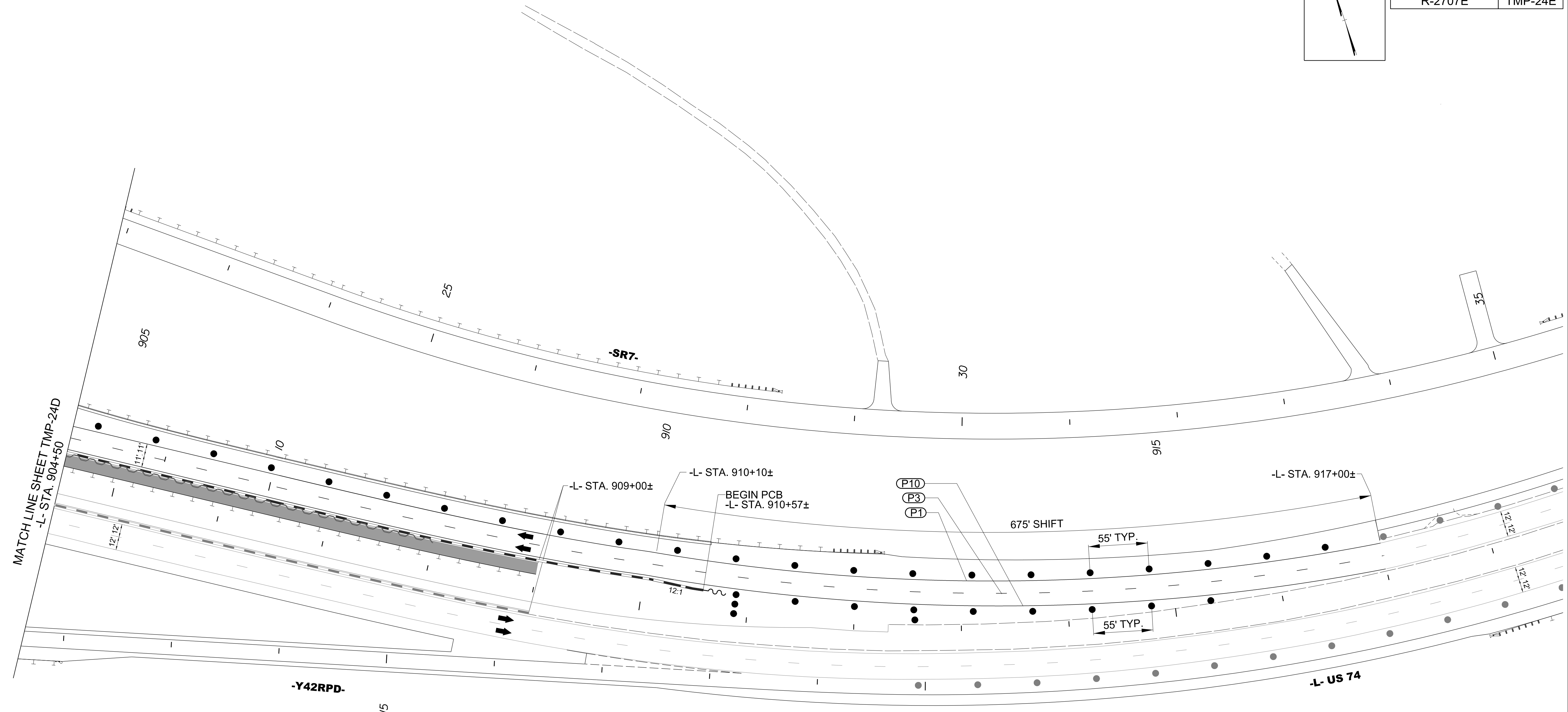
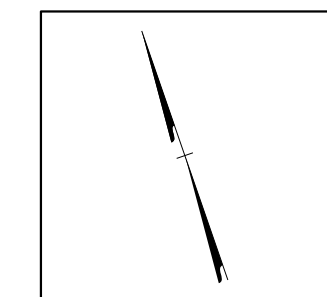
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**-L- US 74
 PHASE I
 STEP 3D**

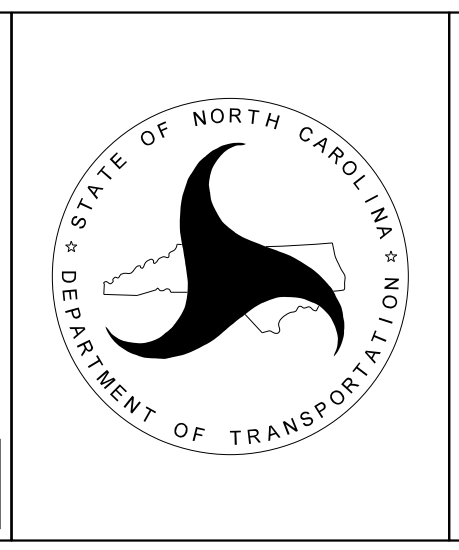
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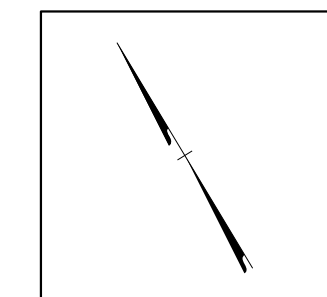
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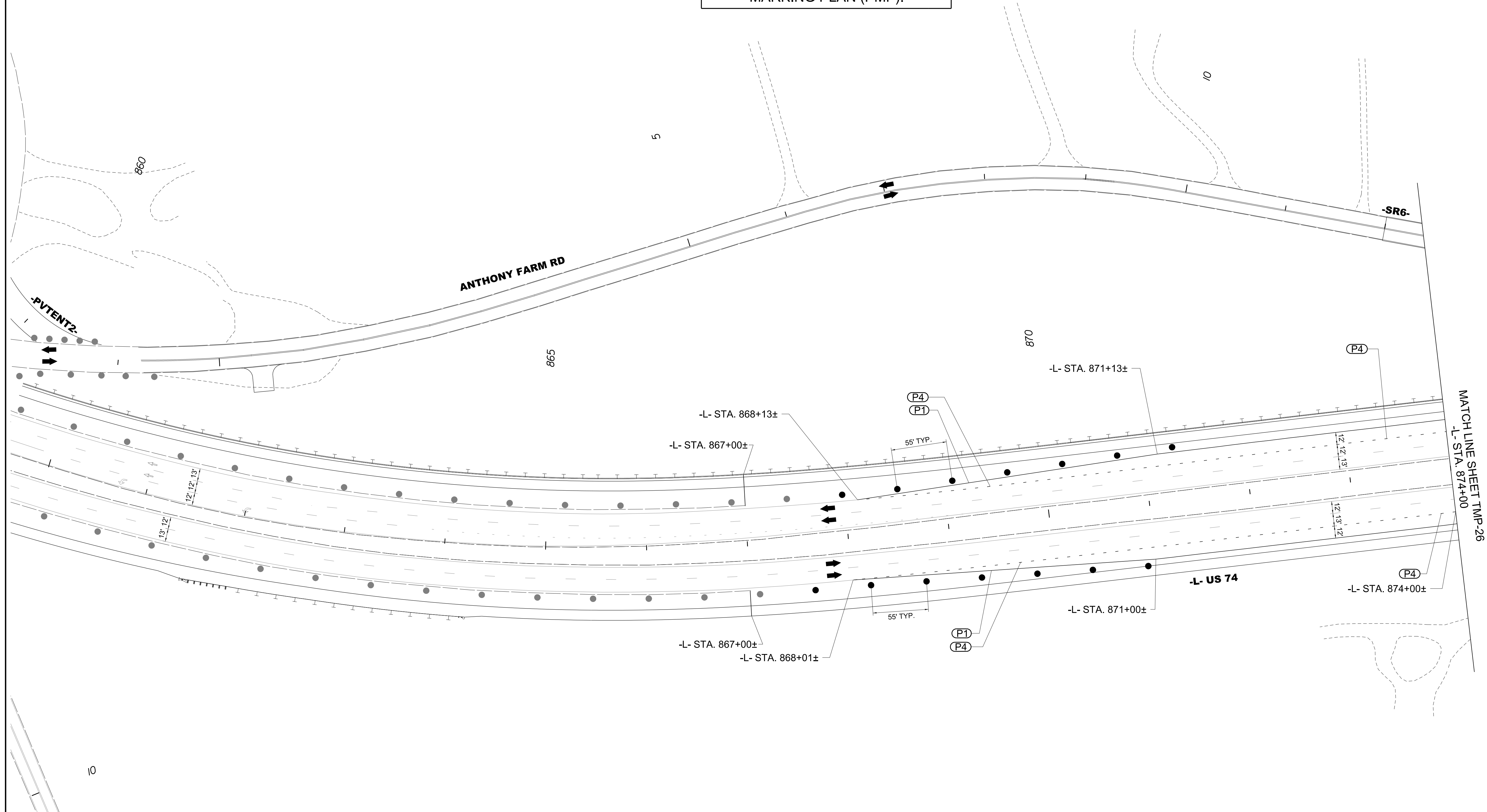
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-L- US 74
 PHASE I
 STEP 3D



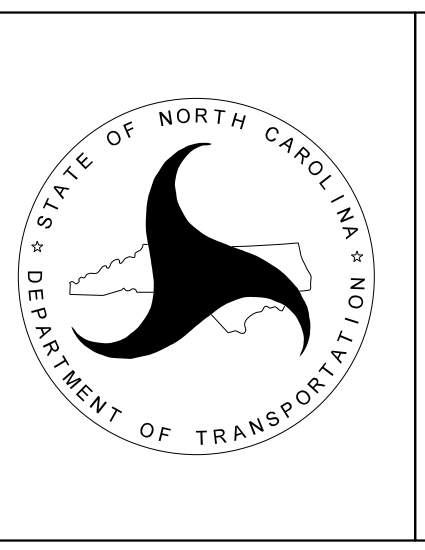
FOR UNSCHEDULED PAVEMENT MARKINGS, SEE FINAL PAVEMENT MARKING PLAN (PMP).



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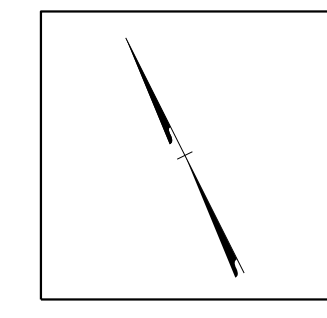
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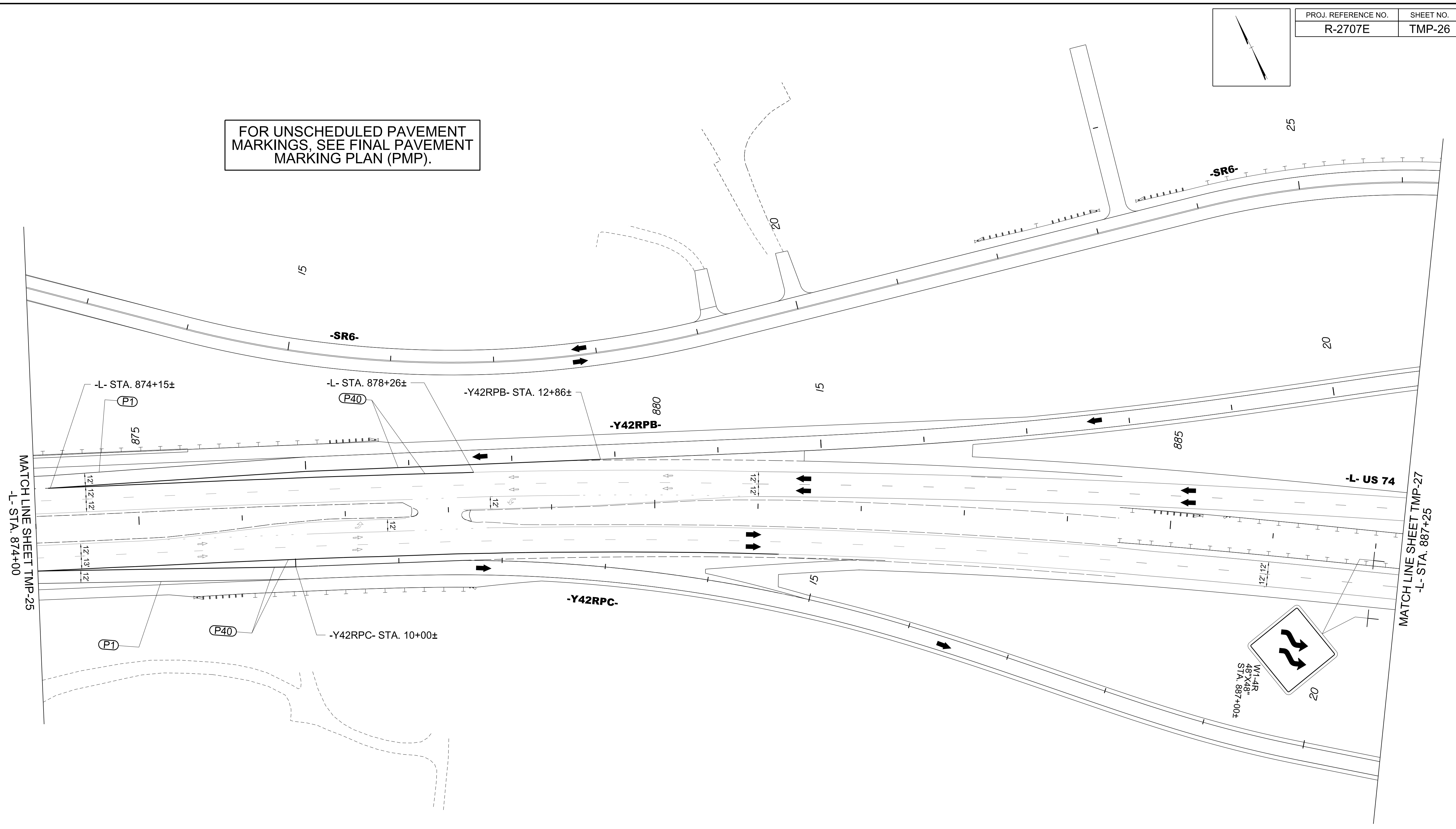


-L- US 74
 PHASE II
 STEP 1

PROJ. REFERENCE NO.	SHEET NO.
R-2707E	TMP-26



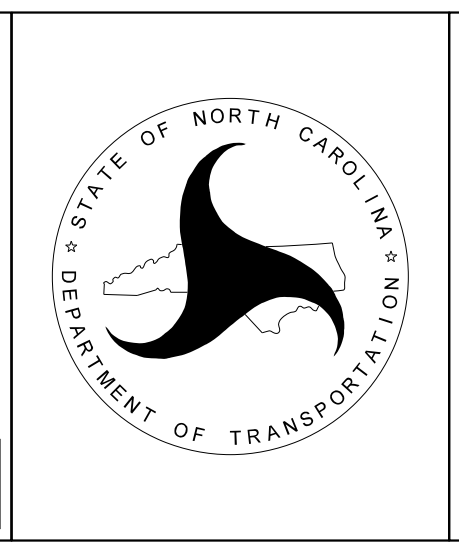
FOR UNSCHEDULED PAVEMENT MARKINGS, SEE FINAL PAVEMENT MARKING PLAN (PMP).



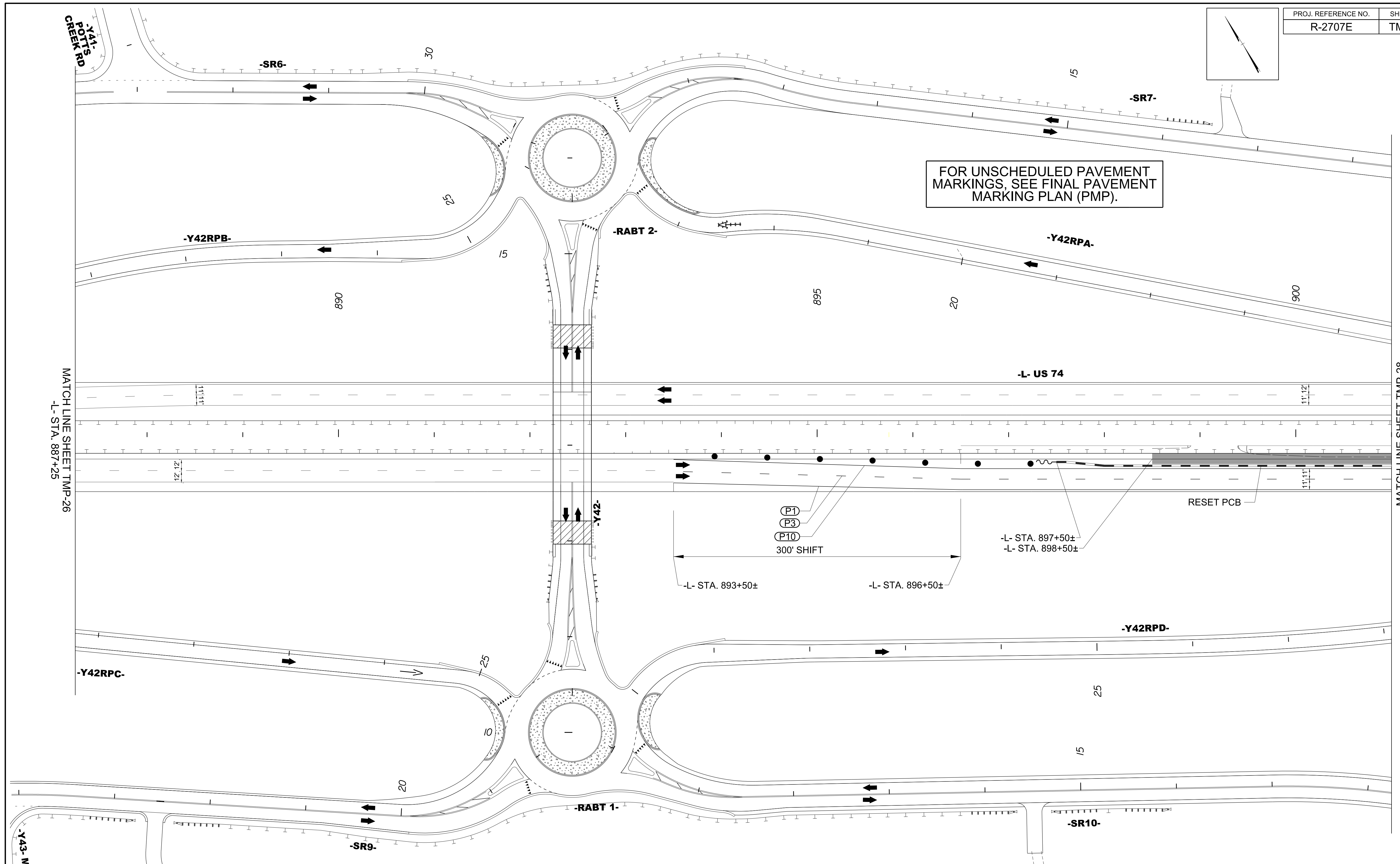
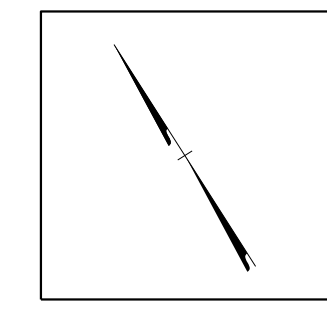
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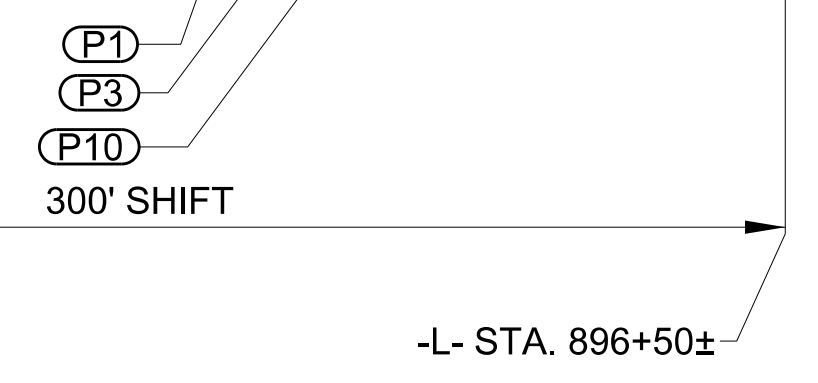
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**-L- US 74
 PHASE II
 STEP 1**



FOR UNSCHEDULED PAVEMENT MARKINGS, SEE FINAL PAVEMENT MARKING PLAN (PMP).



-L- STA. 897+50±
-L- STA. 898+50±

-L- STA. 893+50± -L- STA. 896+50±

RESET PCB

MATCH LINE SHEET TMP-26
-L- STA. 887+25

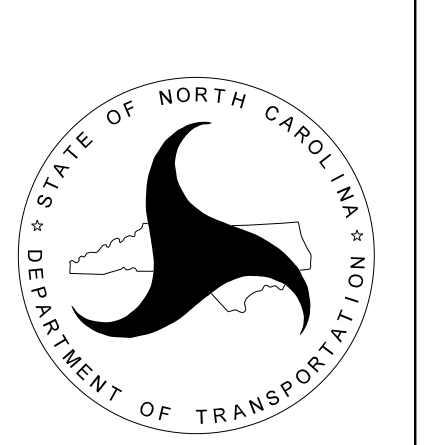
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-L- STA. 901+00

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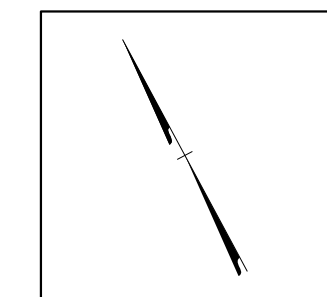
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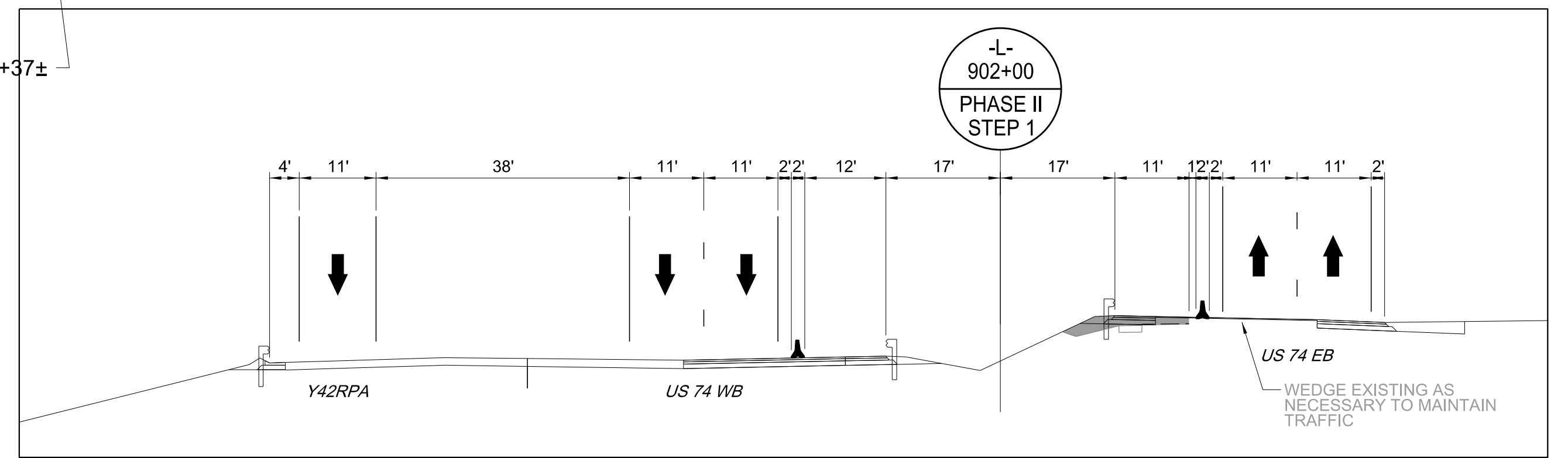
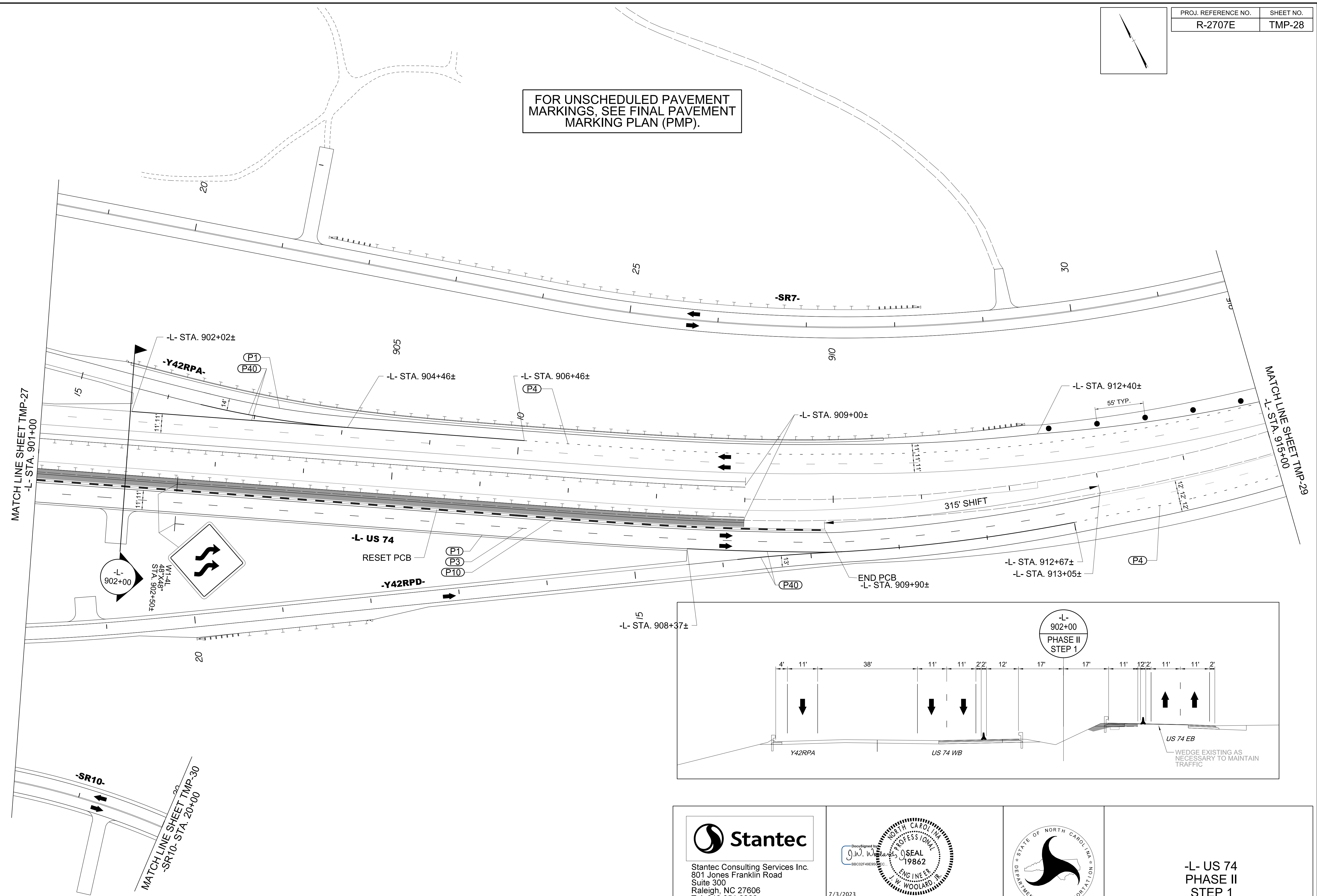
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-L- US 74
PHASE II
STEP 1



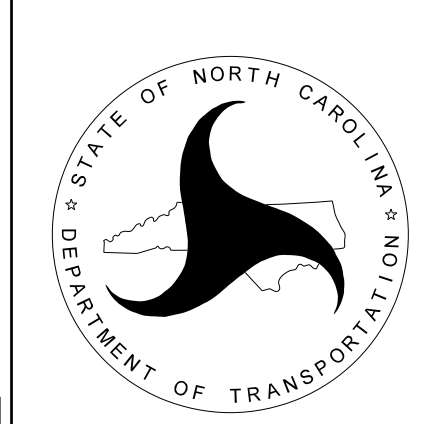
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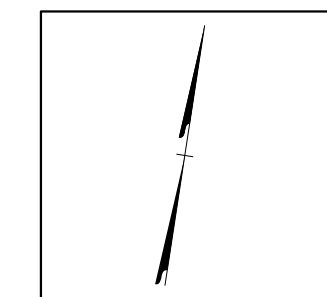
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DocuSigned by:
 J.W. Woolard
 PROFESSIONAL SEAL
 19862
 ENGINEER
 W. WOOLARD, R.
 7/3/2023

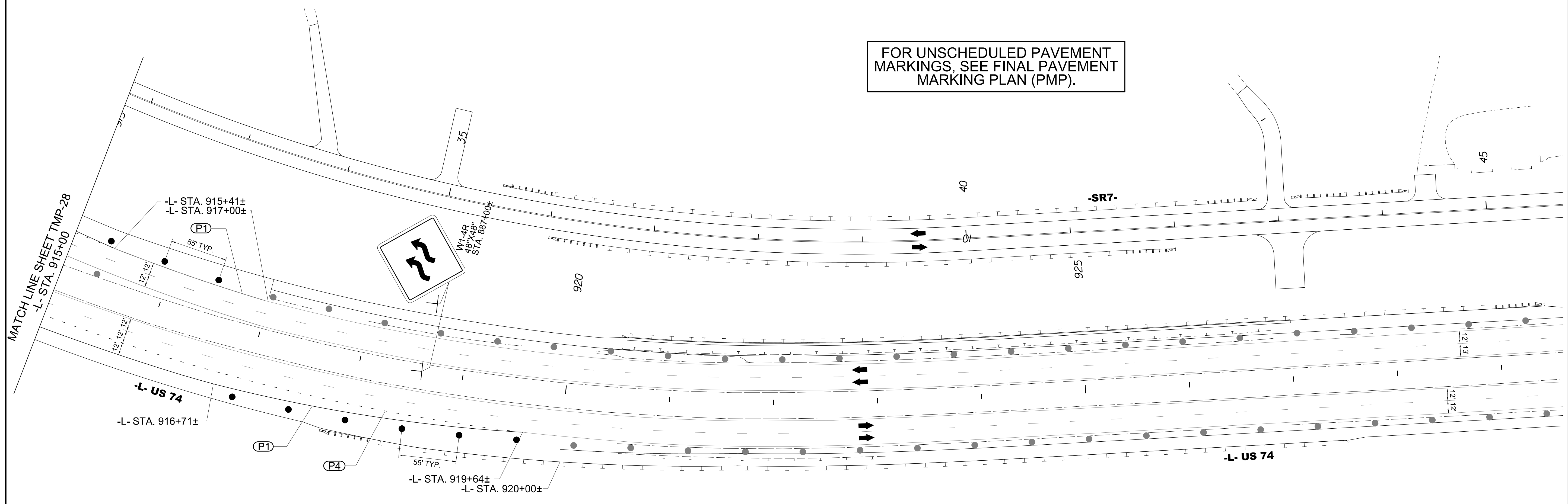


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 PHASE II
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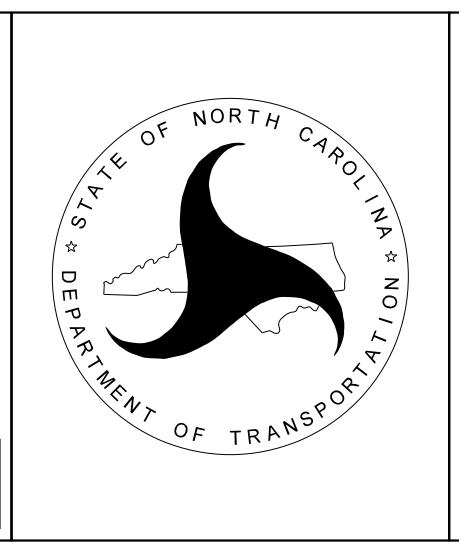


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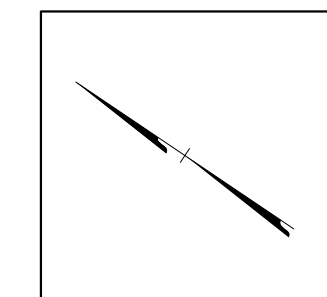
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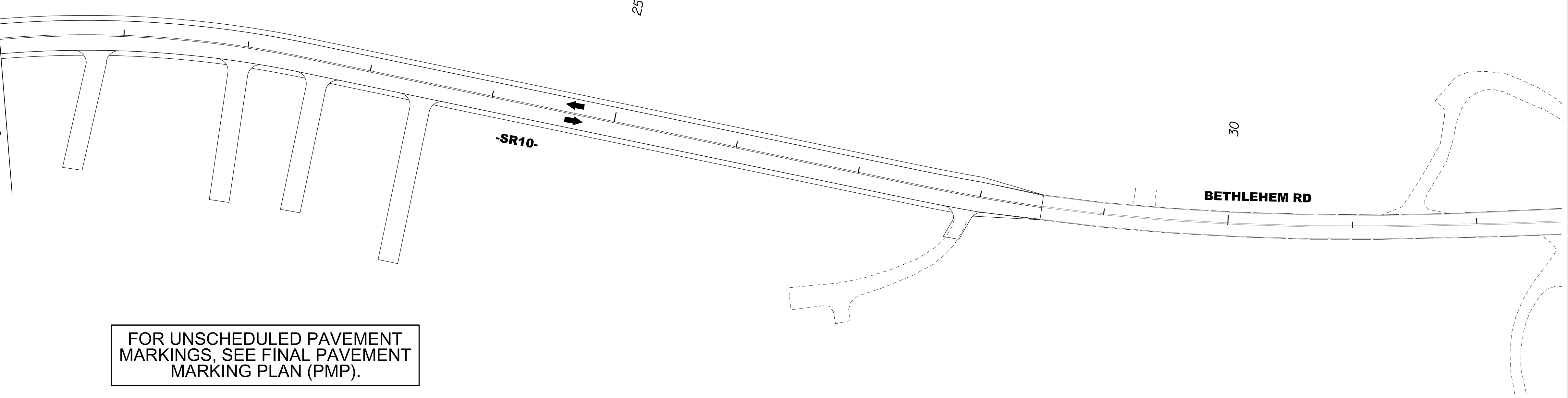


**-L- US 74
PHASE II
STEP 1**

PROJ. REFERENCE NO.	SHEET NO.
R-2707E	TMP-30



MATCH LINE SHEET TMP-28
-SR10- STA. 20+00



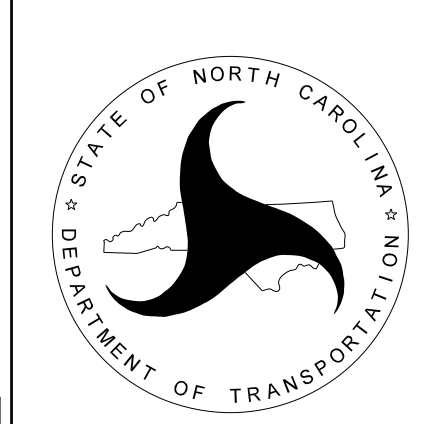
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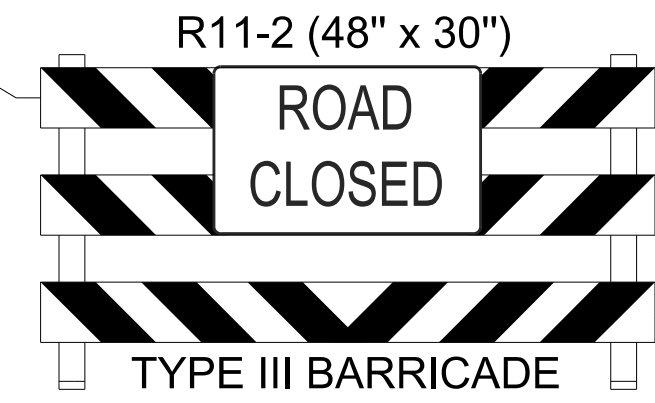
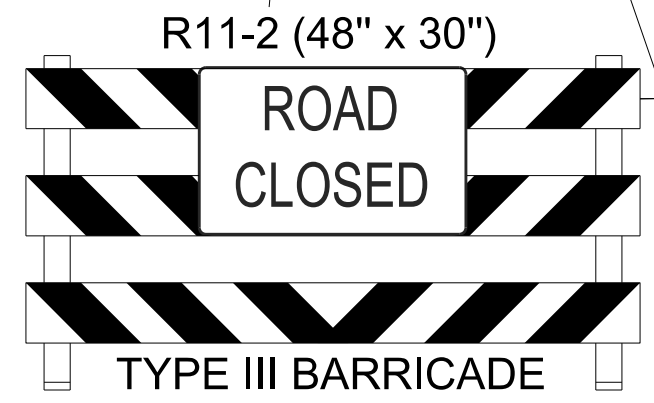
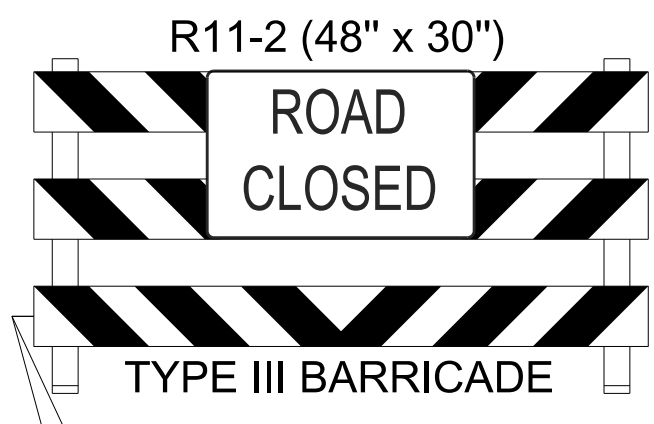
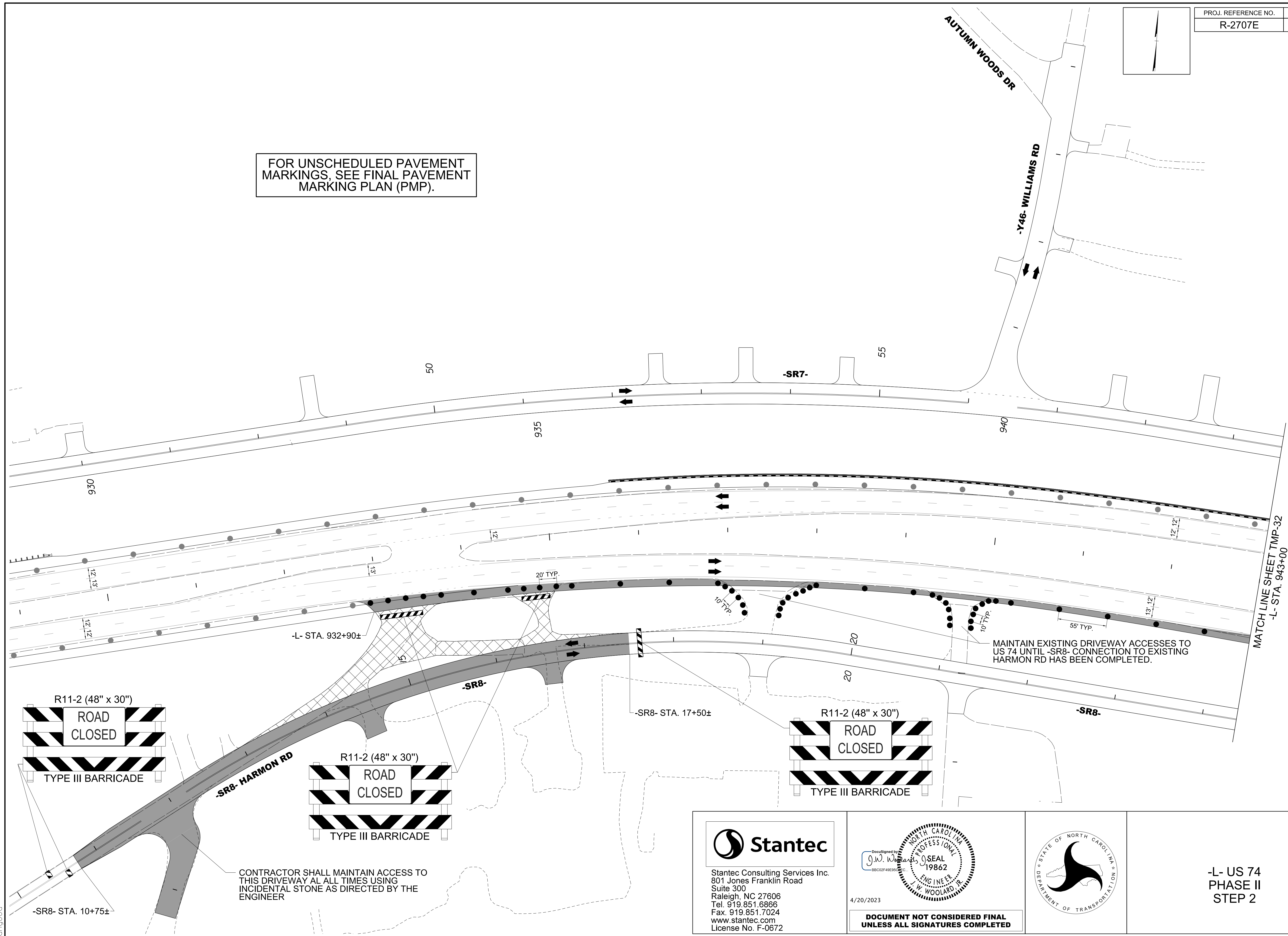
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PHASE II
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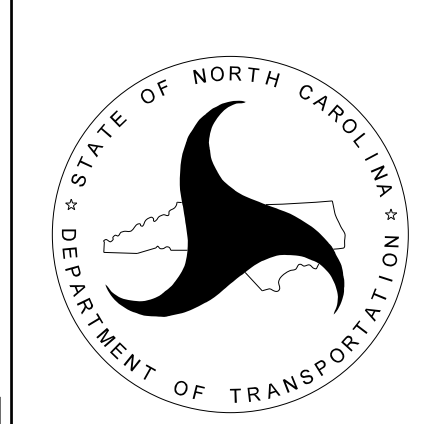


CONTRACTOR SHALL MAINTAIN ACCESS TO THIS DRIVEWAY AT ALL TIMES USING INCIDENTAL STONE AS DIRECTED BY THE ENGINEER

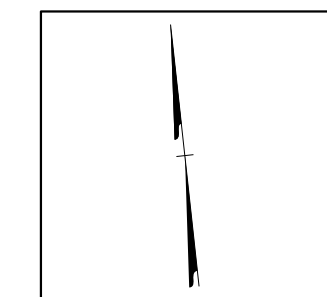
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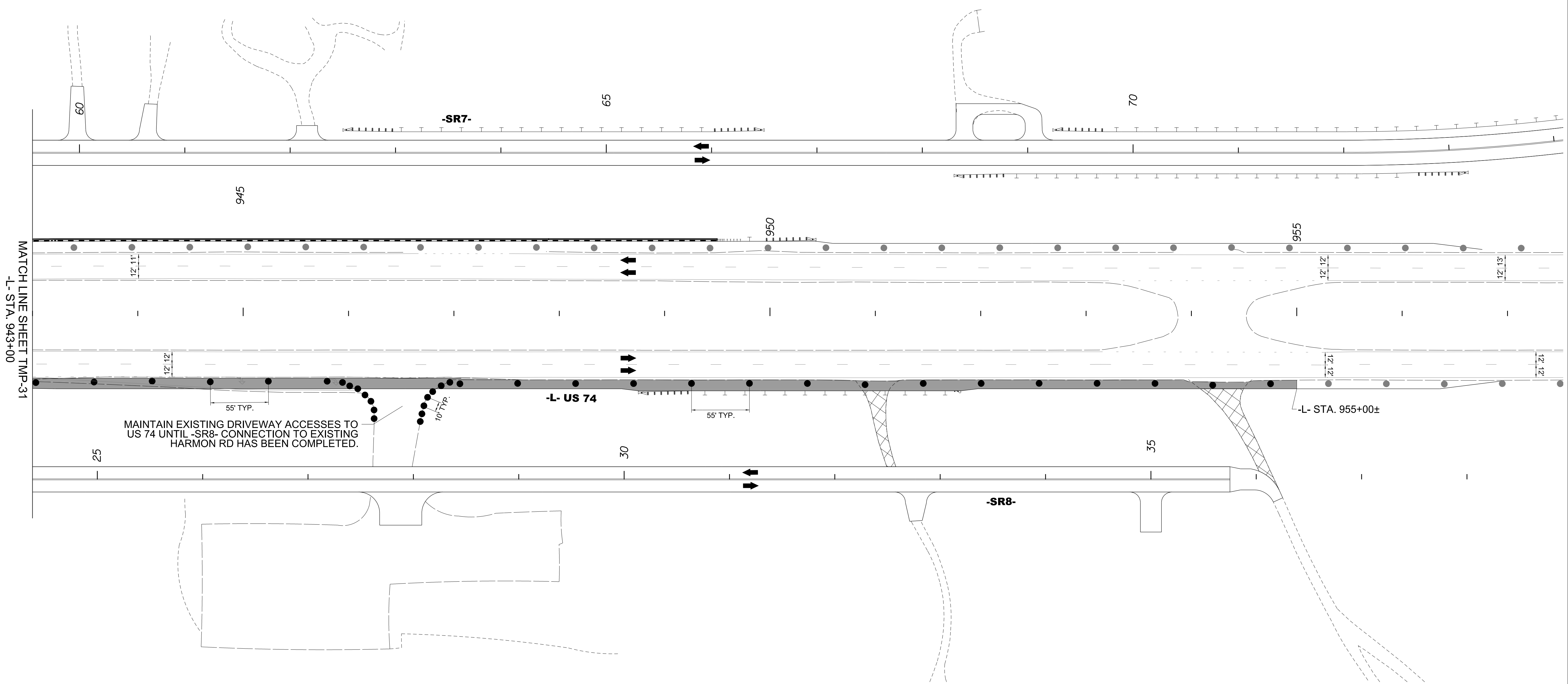
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 PHASE II
 STEP 2



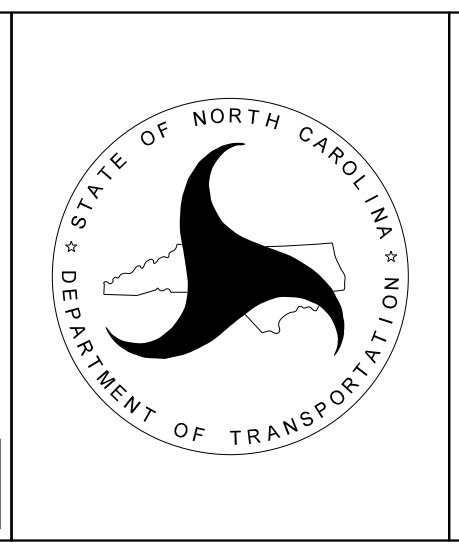
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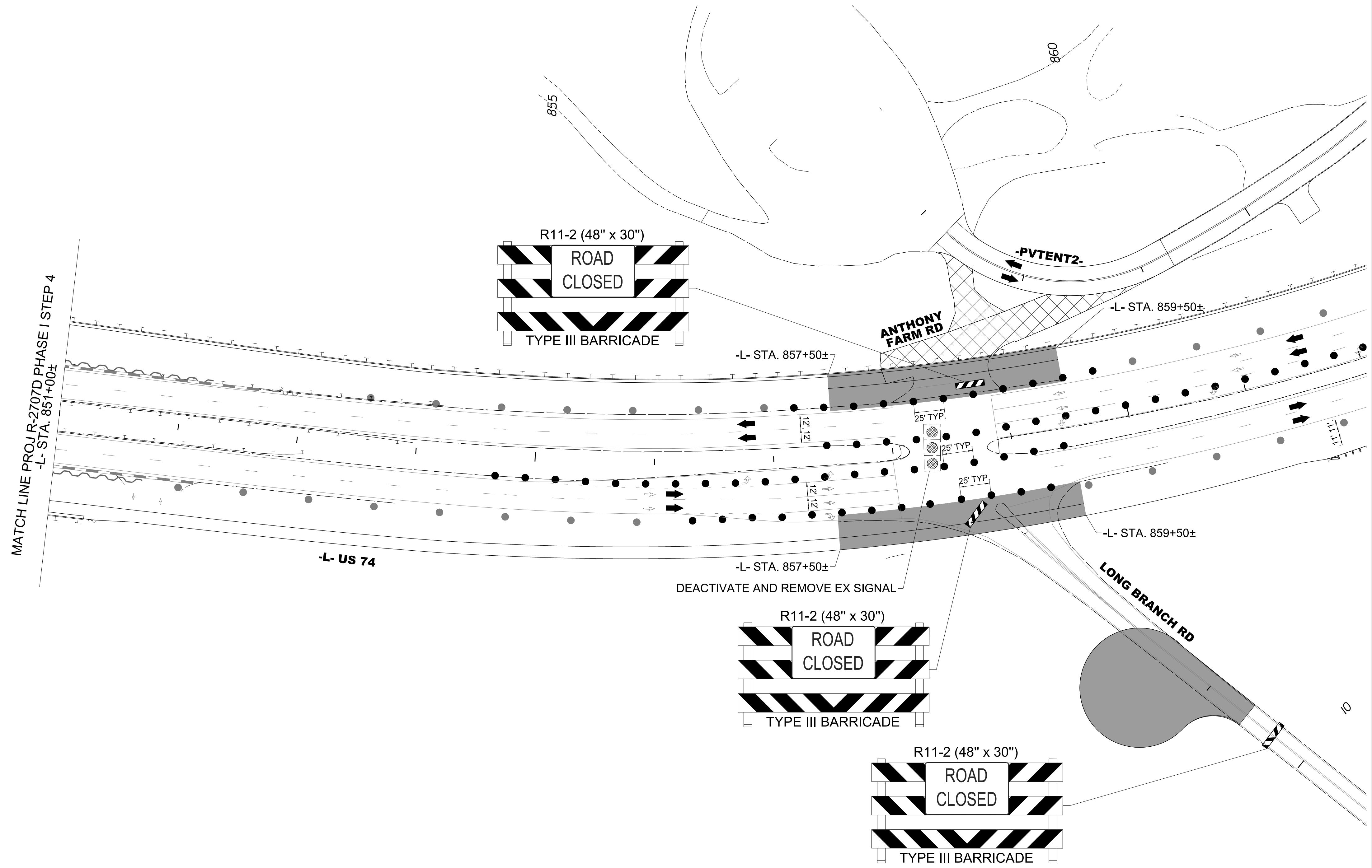
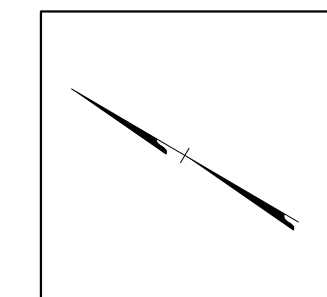
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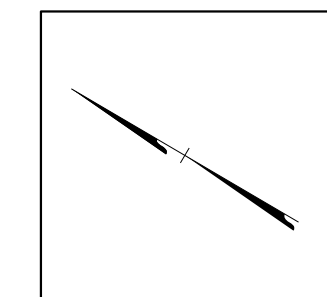


-L- US 74
 PHASE II
 STEP 2

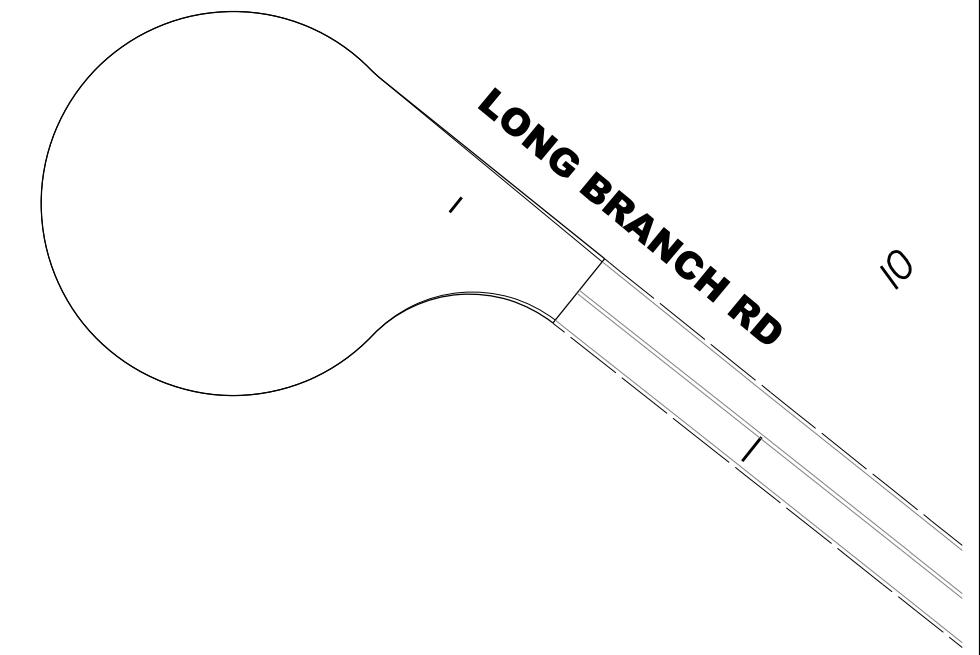
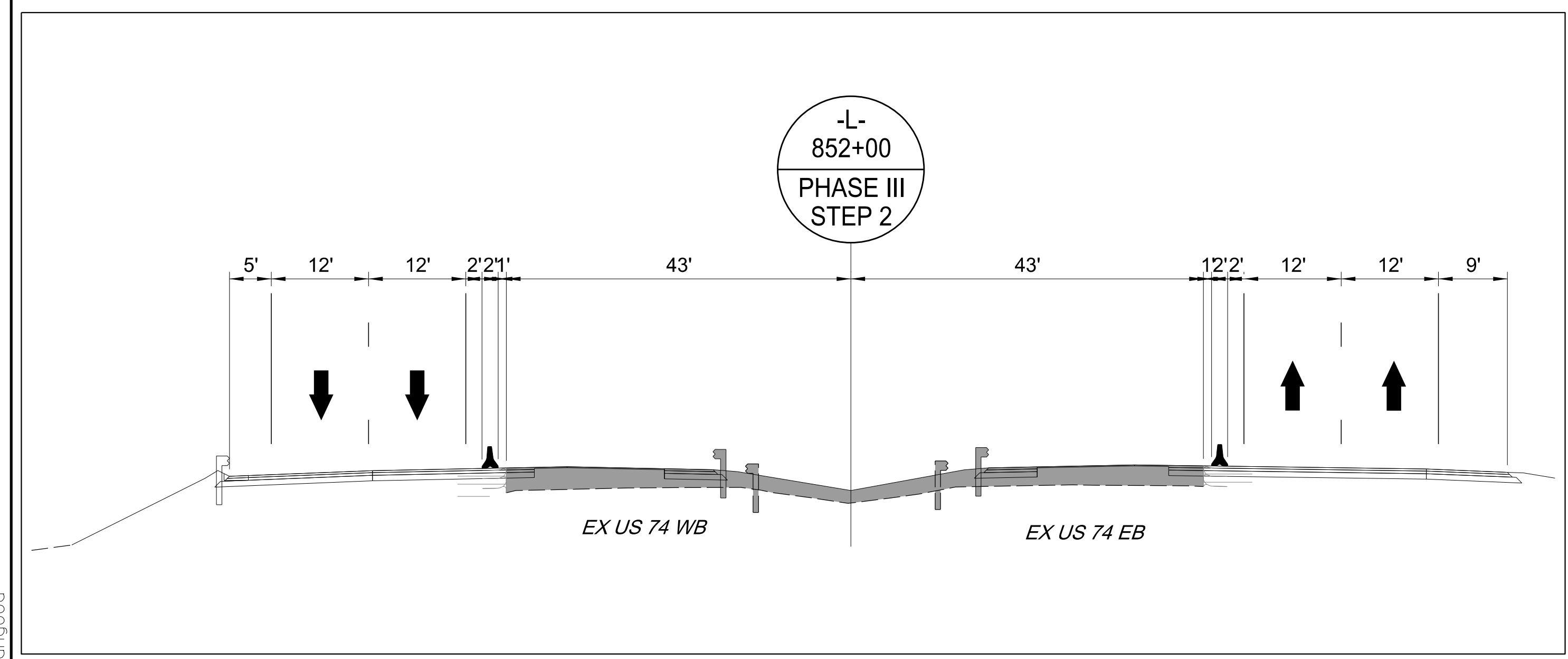
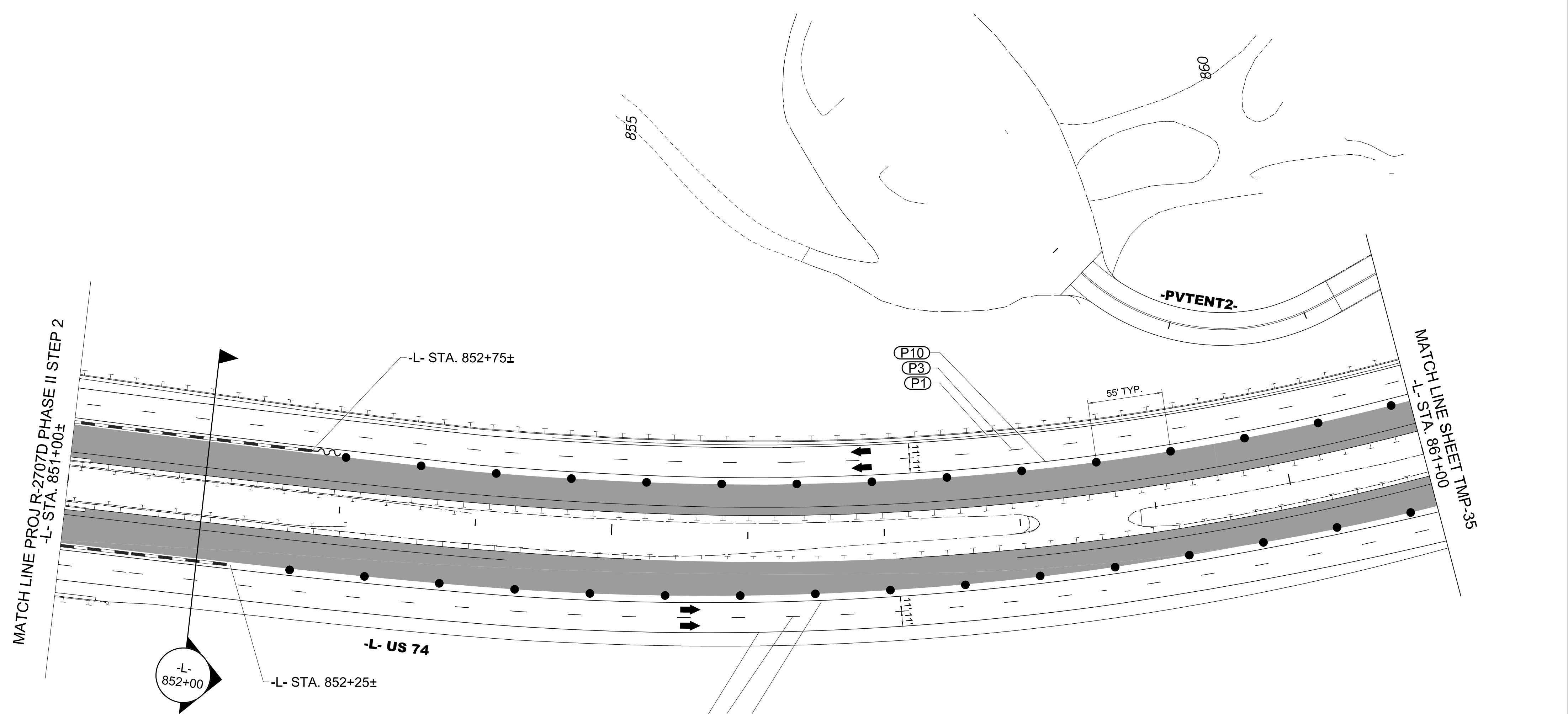


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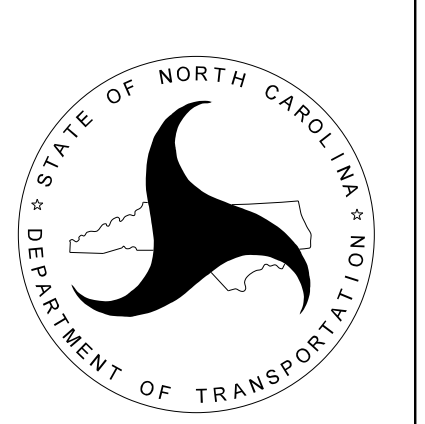
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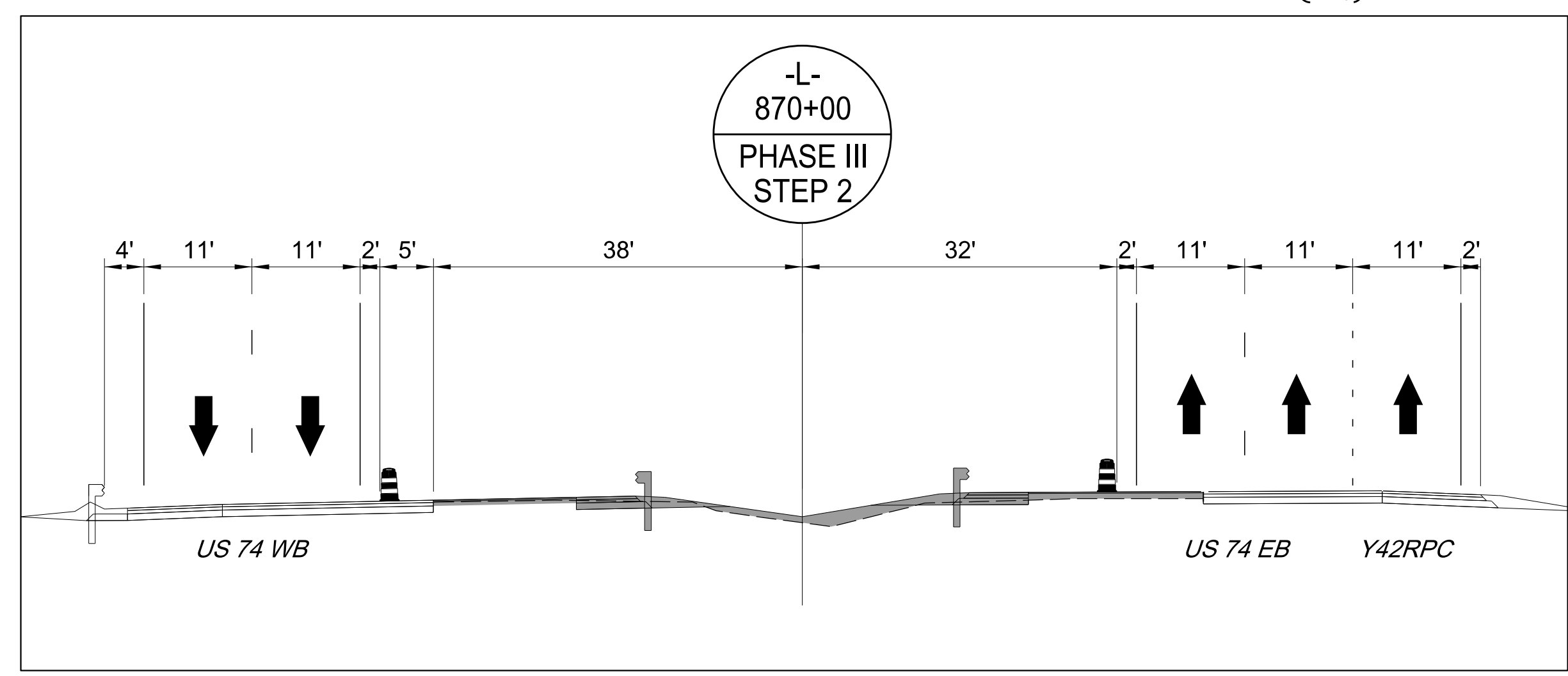
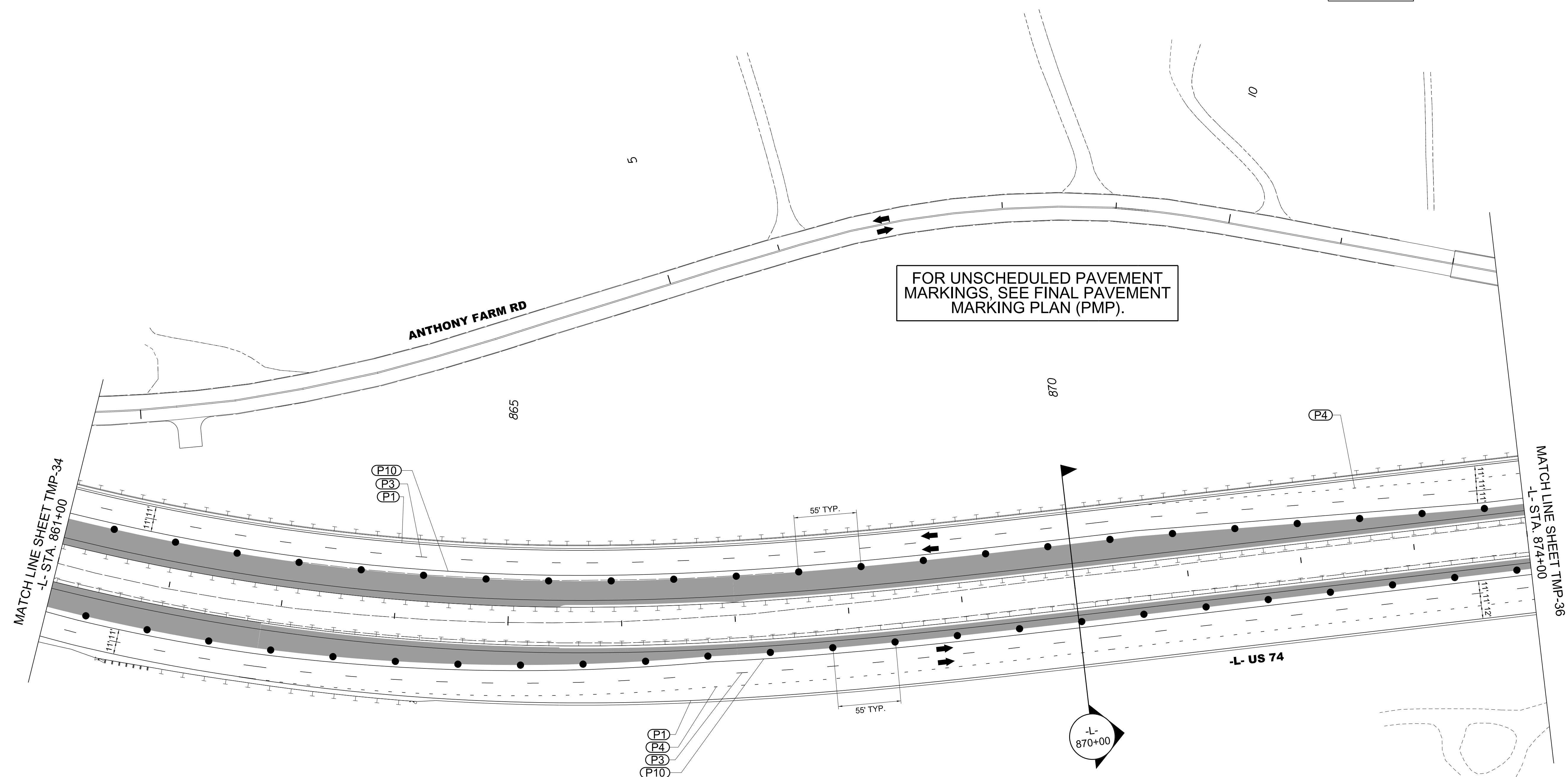
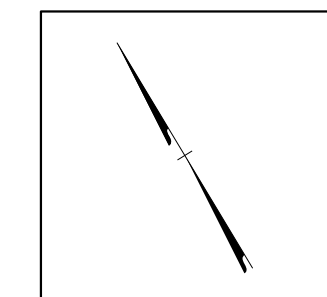
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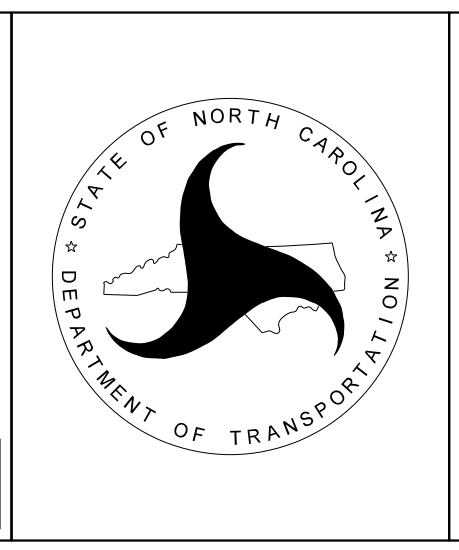


-L- US 74
PHASE III
STEP 2



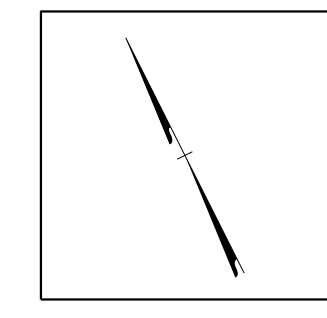
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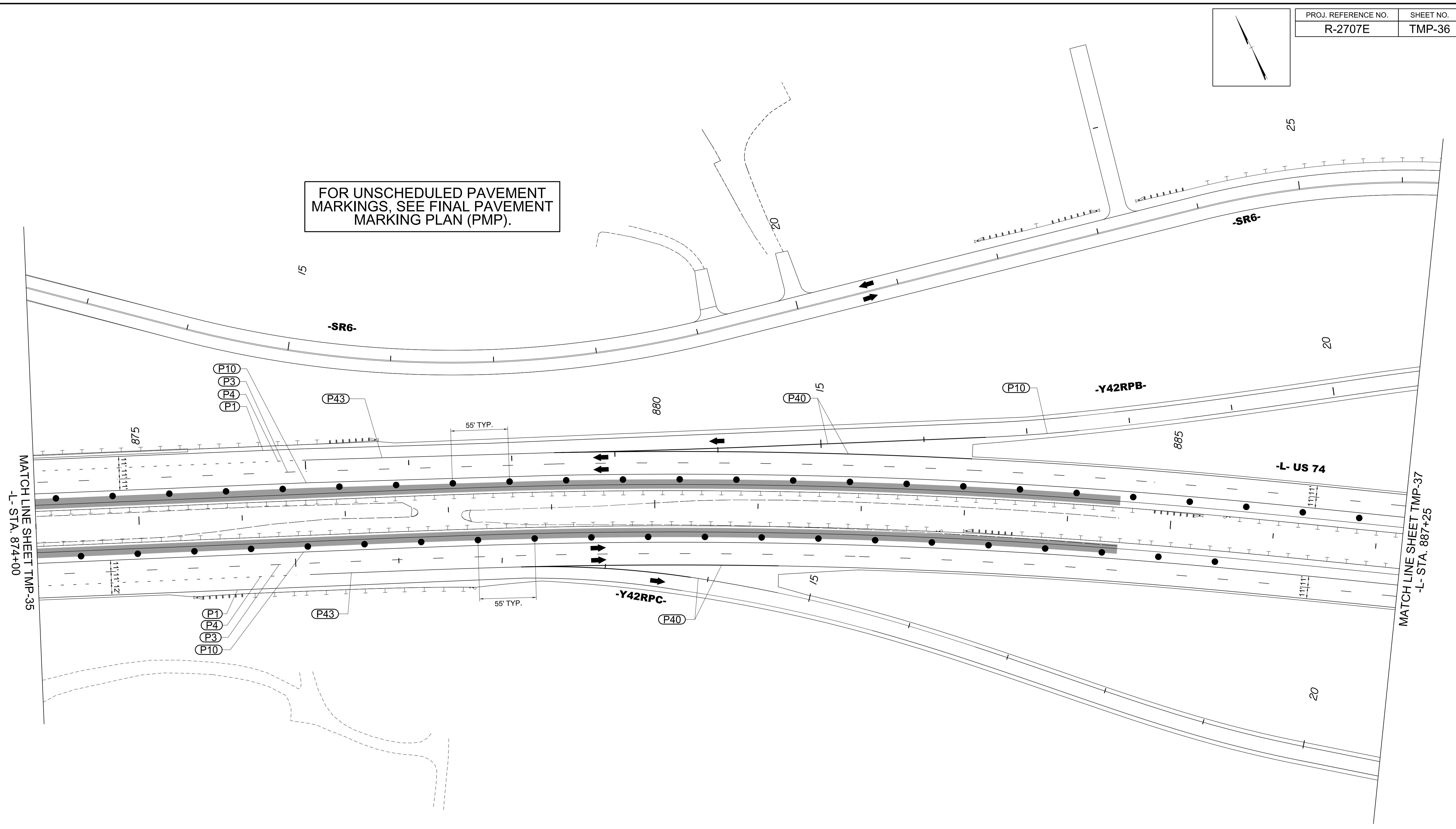


-L- US 74 PHASE III STEP 2

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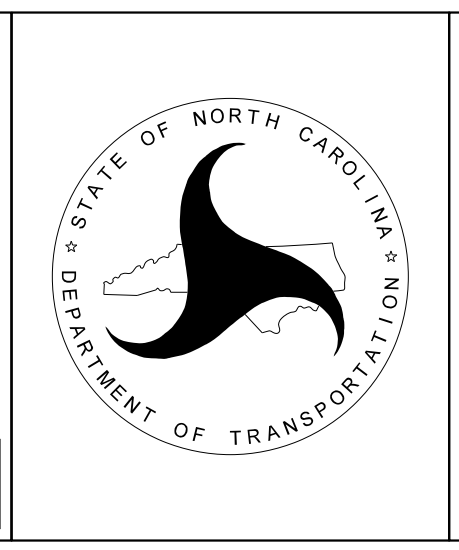


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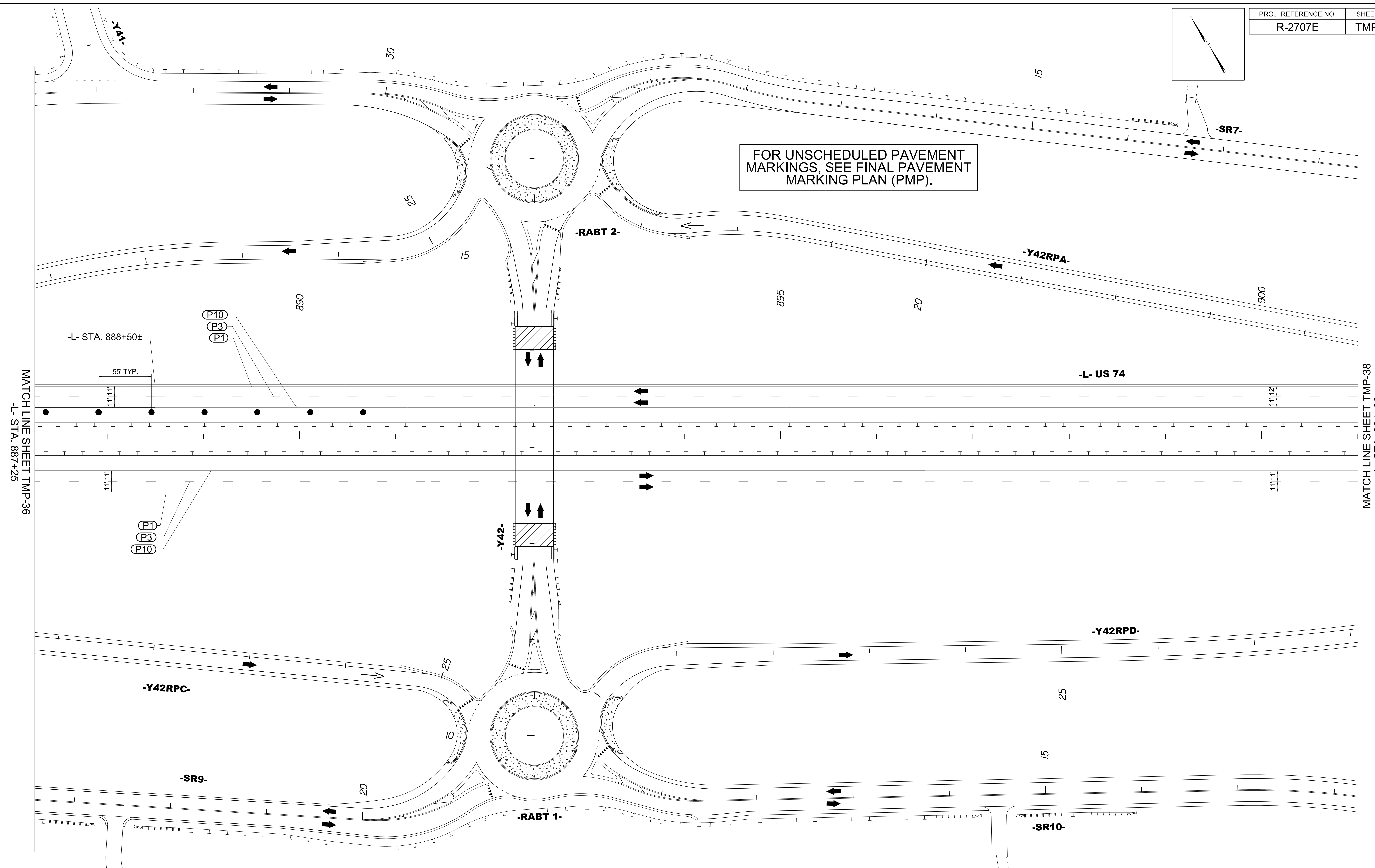
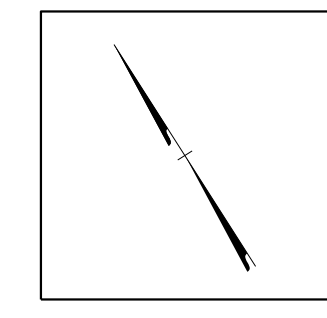
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 STEP 2



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MATCH LINE SHEET TMP-36
-L- STA. 887+25

MATCH LINE SHEET TMP-38
-L- STA. 901+00

-L- STA. 888+50±

55' TYP.

P10
P3
P1

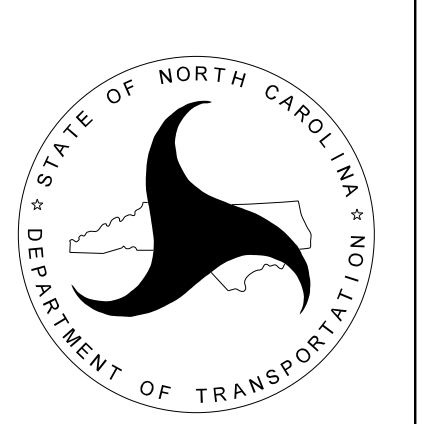
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P3
P10

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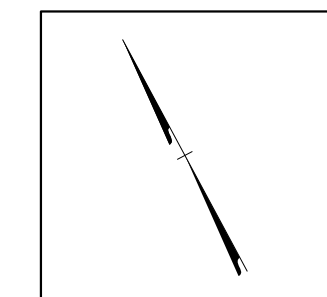
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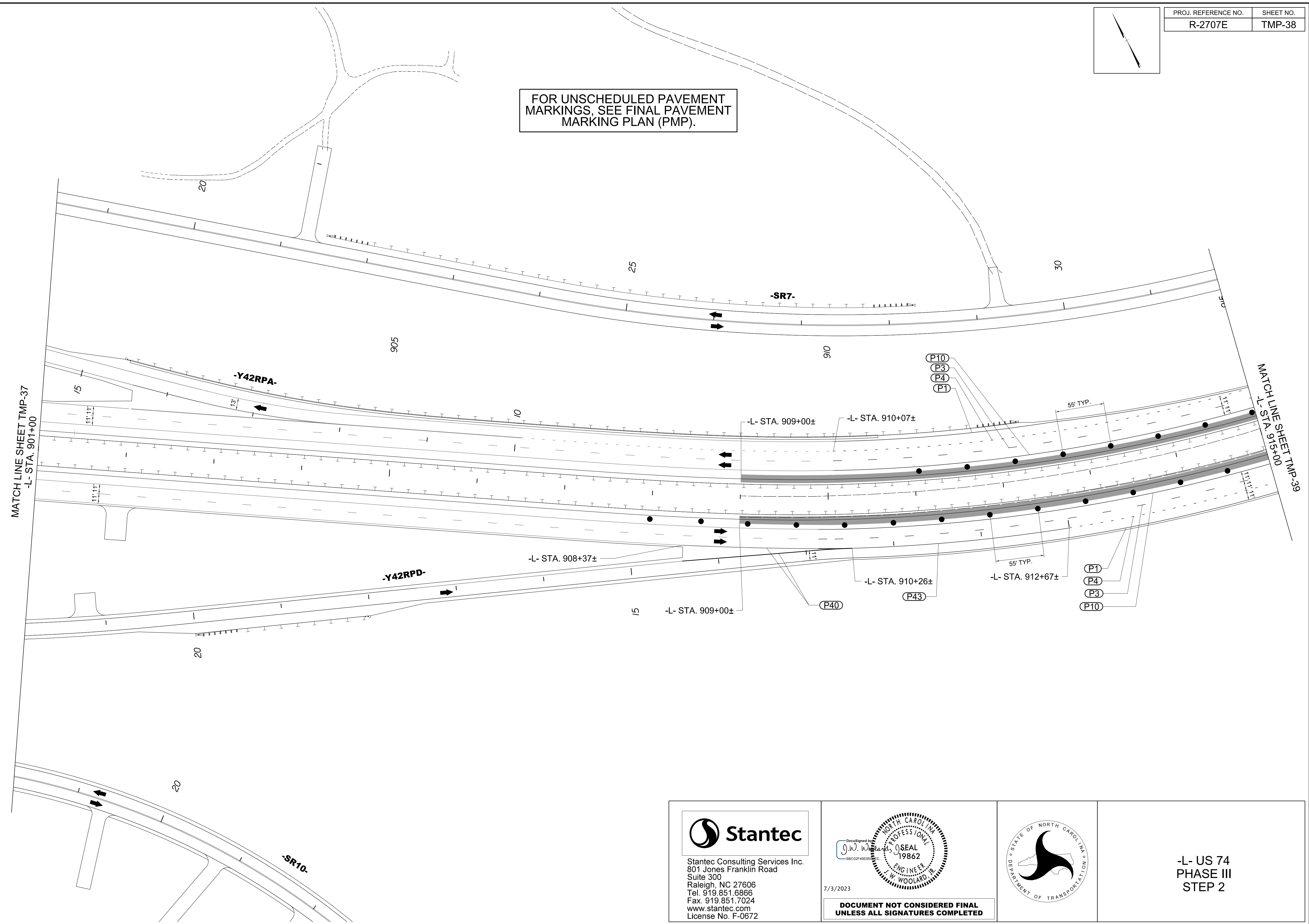
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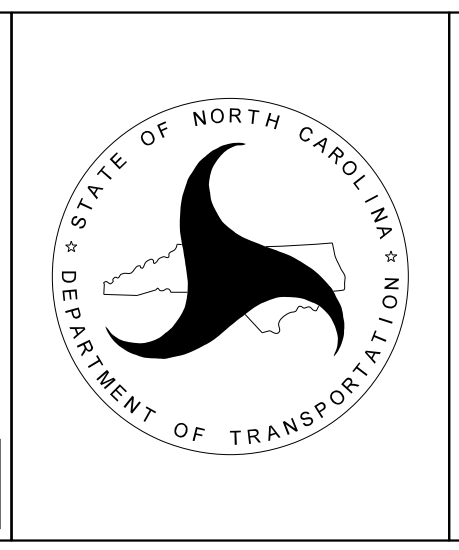


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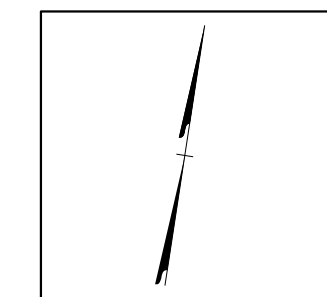
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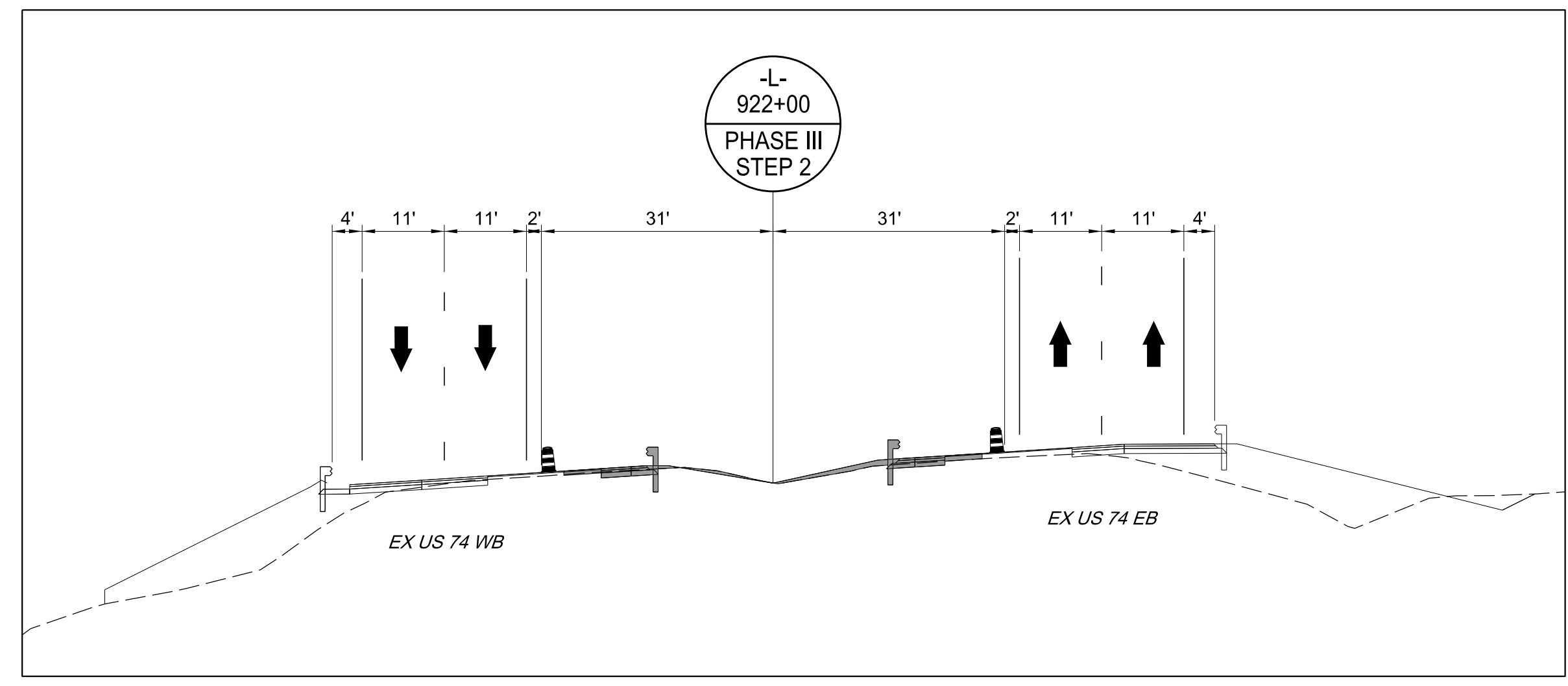
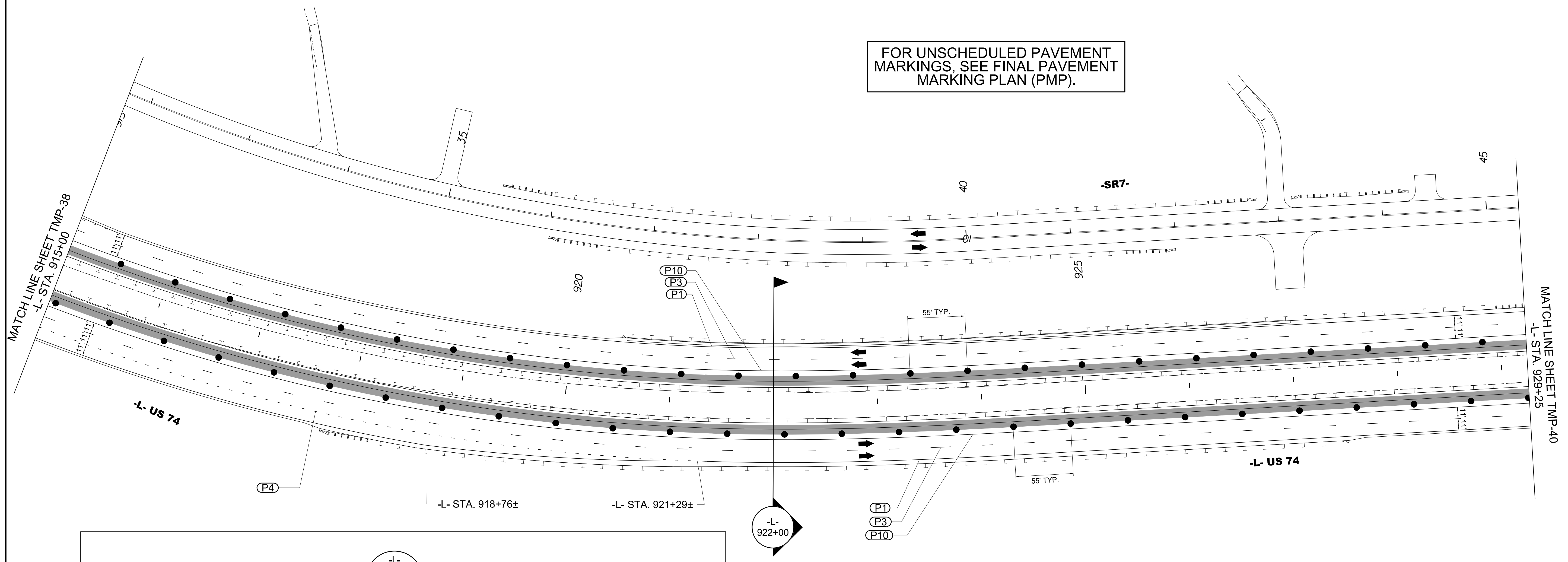
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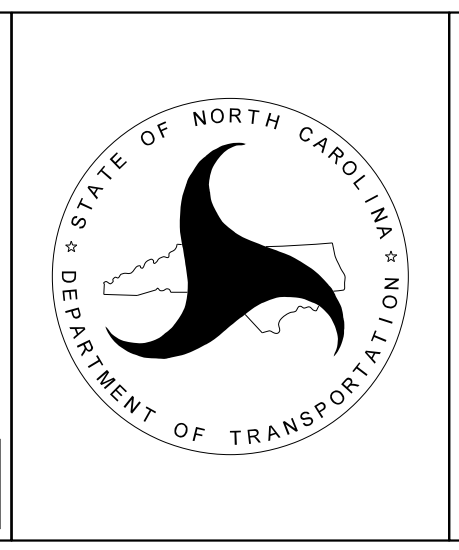
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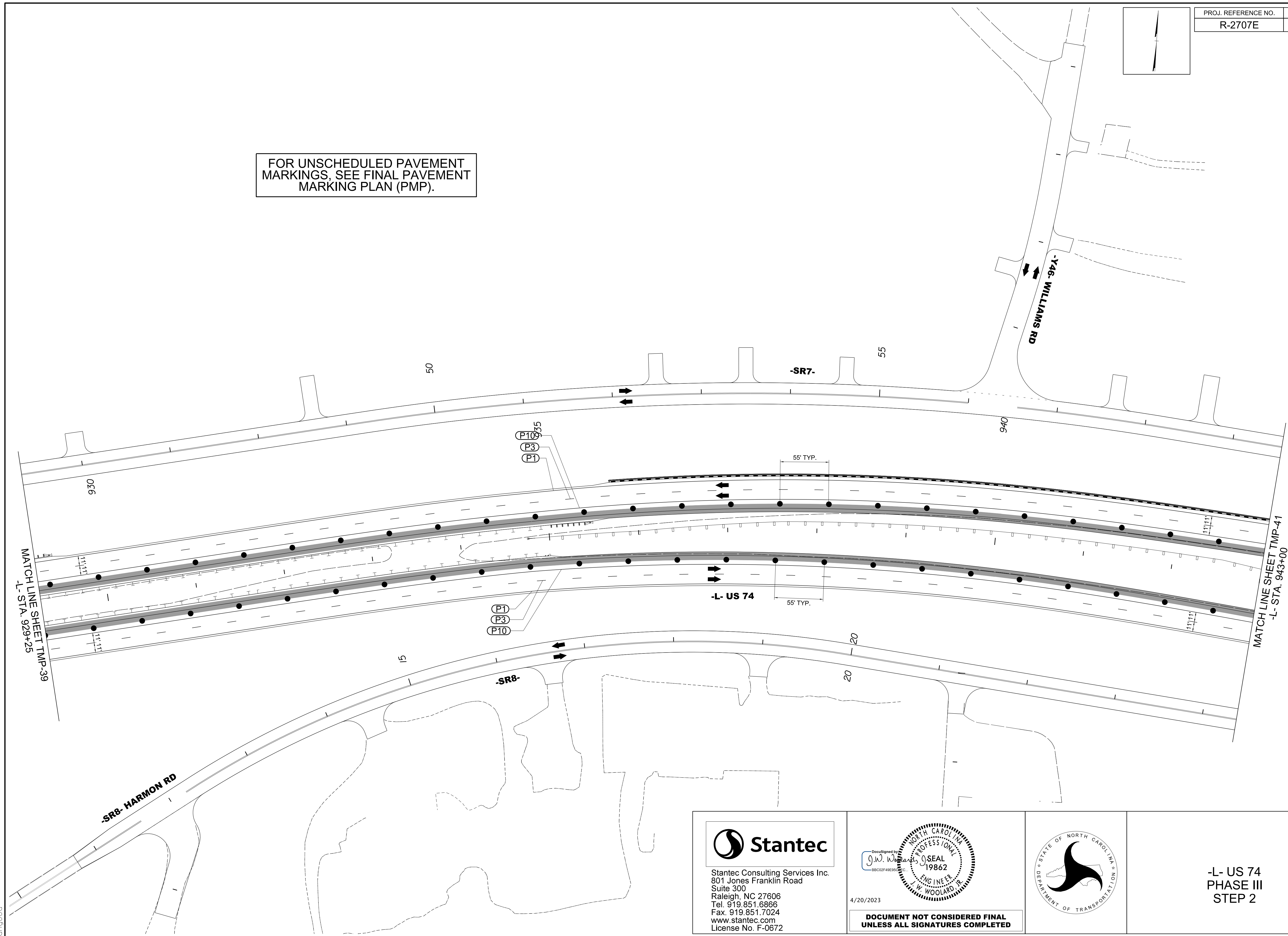
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**-L- US 74
 PHASE III
 STEP 2**

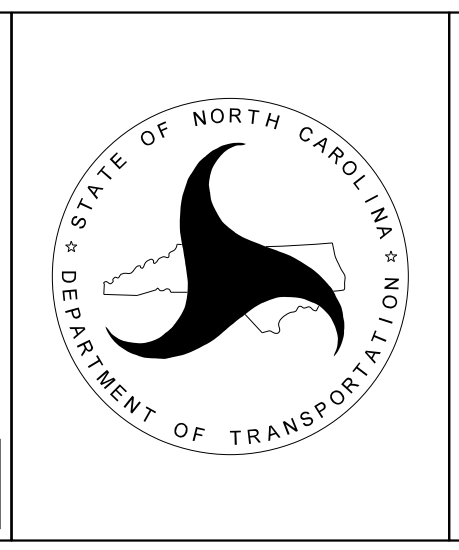
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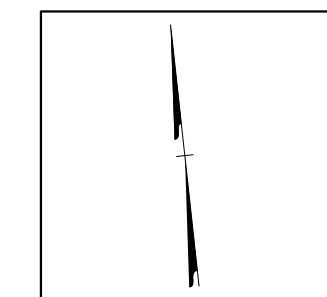
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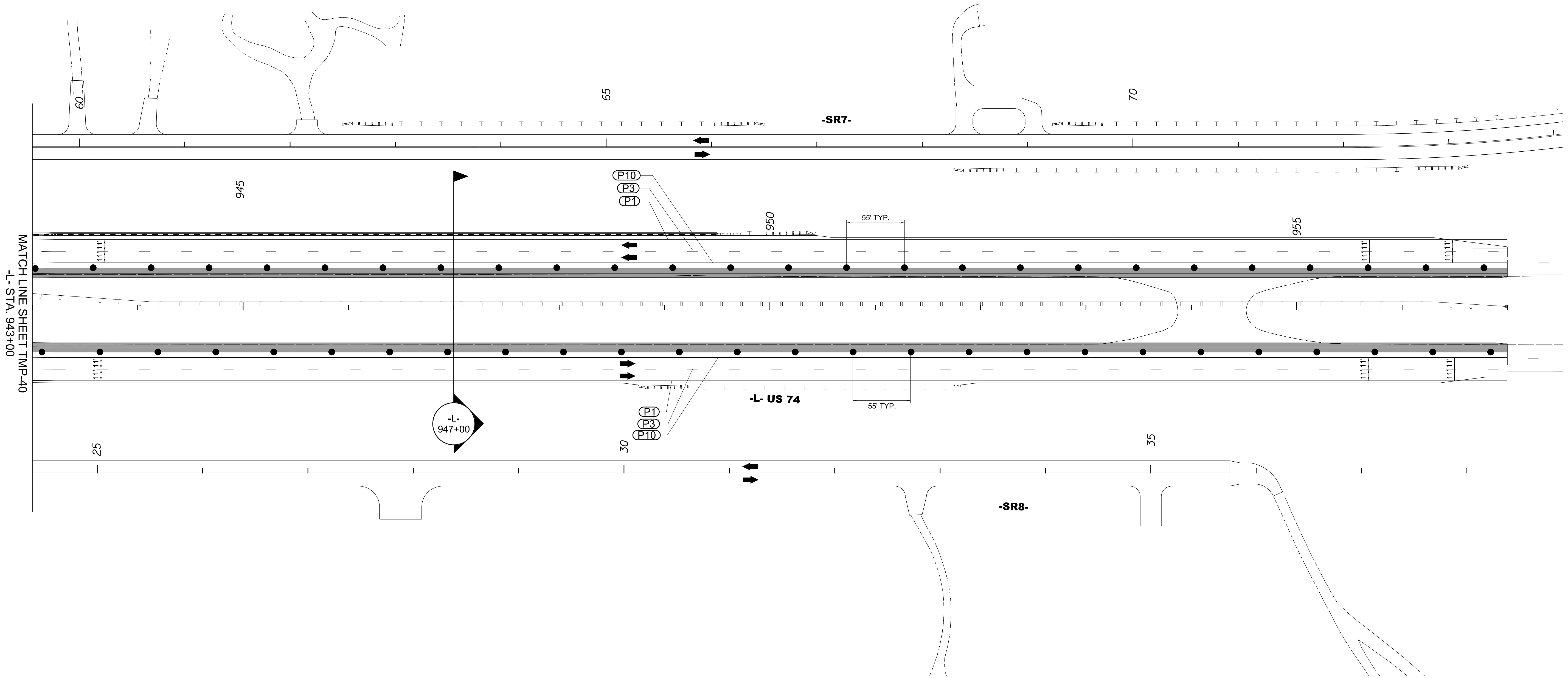
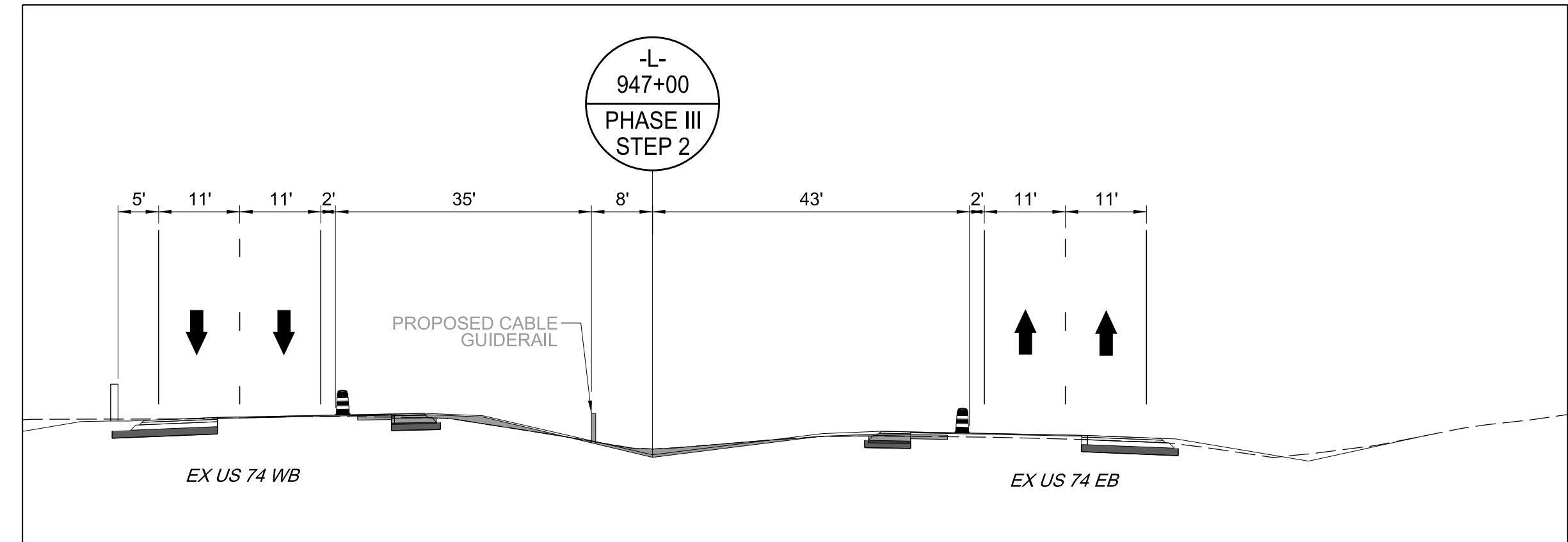
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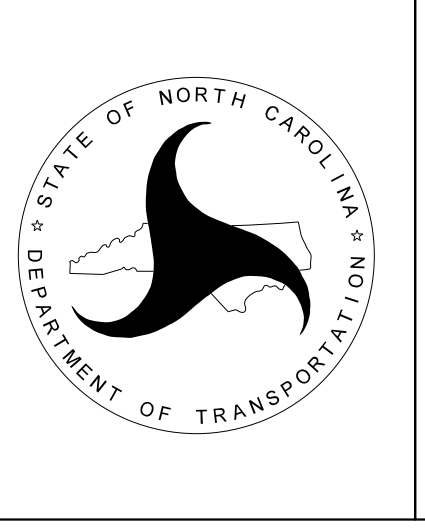
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