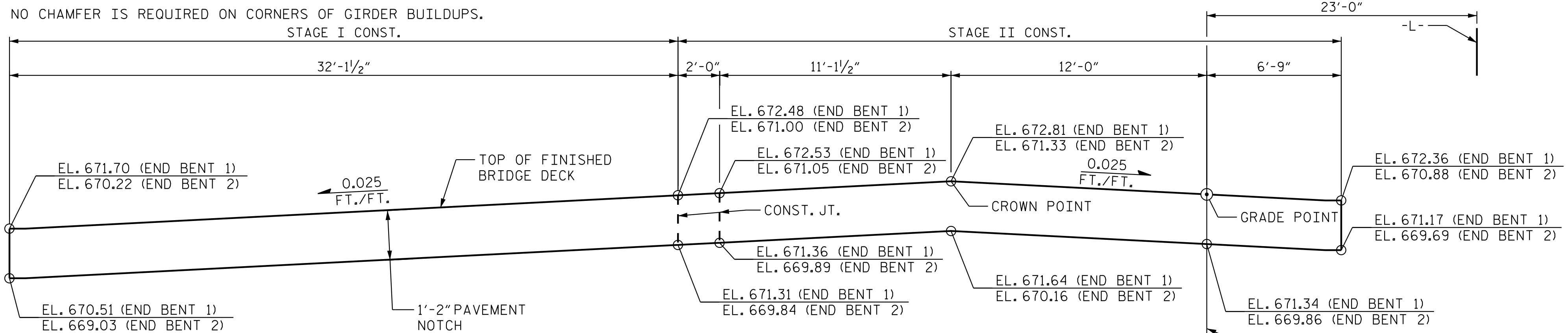


**NOTES:**  
 PROVIDE 1/4" HIGH BEAM BOLSTERS UPPER AT 4'-0" CTS. ATOP THE METAL STAY-IN-PLACE FORMS TO SUPPORT THE BOTTOM MAT OF 'A' BARS. WHEN USING REMOVABLE FORMS, PROVIDE CONTINUOUS HIGH CHAIRS FOR METAL DECK (C.H.C.M.) @ 4'-0" CTS. WITH A HEIGHT TO SUPPORT THE BOTTOM MAT OF 'A' BARS A CLEAR DISTANCE OF 2 1/2" ABOVE THE TOP OF THE REMOVABLE FORM.

LONGITUDINAL STEEL MAY BE SHIFTED SLIGHTLY, AS NECESSARY, TO AVOID INTERFERENCE WITH STIRRUPS IN PRESTRESSED CONCRETE GIRDERS.

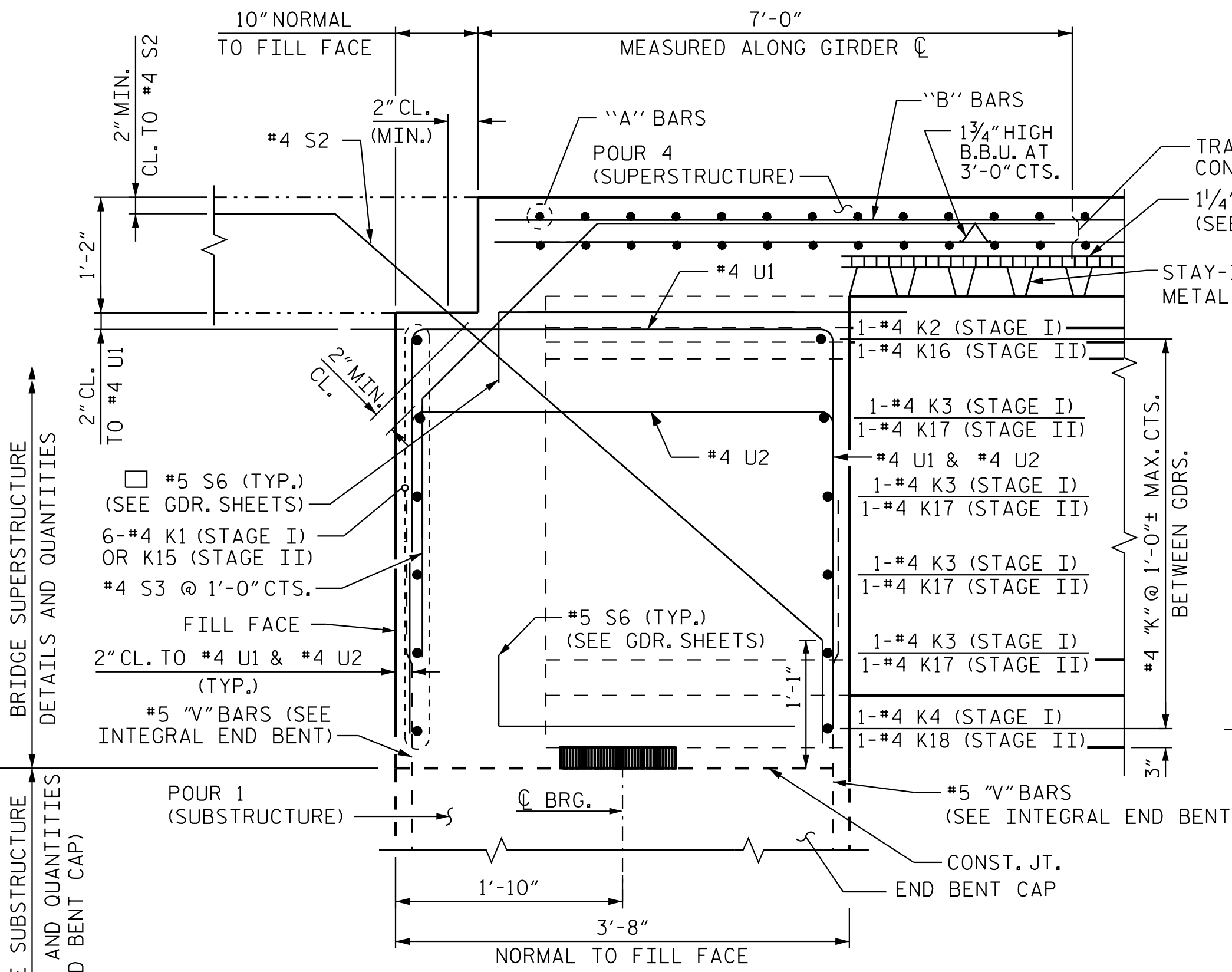
NO CHAMFER IS REQUIRED ON CORNERS OF GIRDER BUILDUPS.

PREVIOUSLY CAST CONCRETE IN A CONTINUOUS UNIT SHALL HAVE ATTAINED A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI BEFORE ADDITIONAL CONCRETE IS CAST IN THE UNIT.



**APPROACH SLAB PAVEMENT NOTCH DETAIL**

ELEVATIONS ARE ALONG FILL FACE OF END BENT  
 DIMENSIONS ARE NORMAL TO -L-

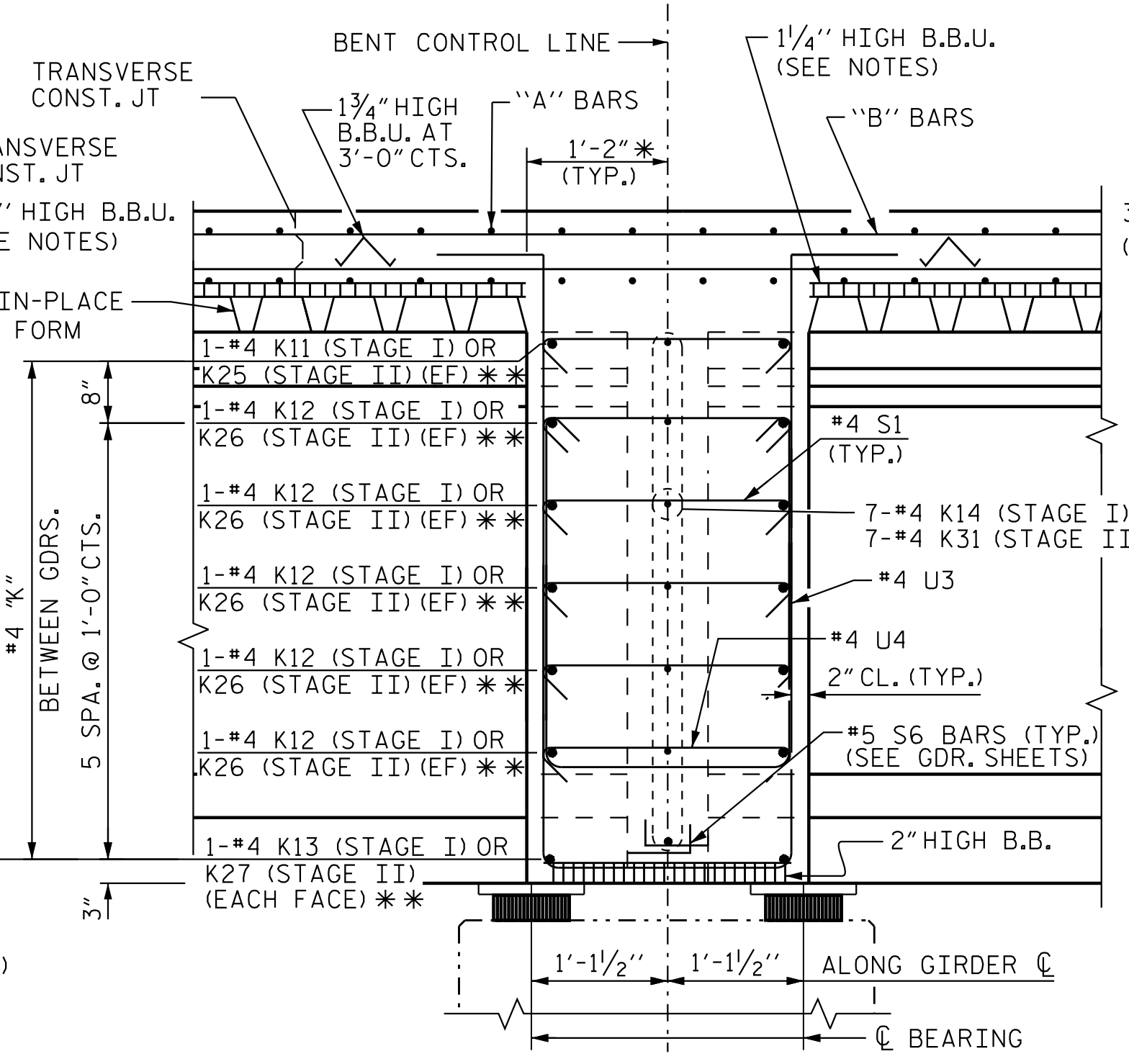


**SECTION B-B**

SECTION THRU INTEGRAL END BENT DIAPHRAGM  
 SEE 'PLAN OF SPANS - END BENT DIAPHRAGMS', SHEET 7 OF 8

#4 S2 & S3 BARS MAY BE REPOSITIONED AS NECESSARY TO CLEAR SLAB REINFORCING STEEL AND GIRDERS.

☐ #5 S6 MAY BE FIELD BENT TO CLEAR 1'-2" DEEP PAVEMENT NOTCH.



**SECTION A-A**

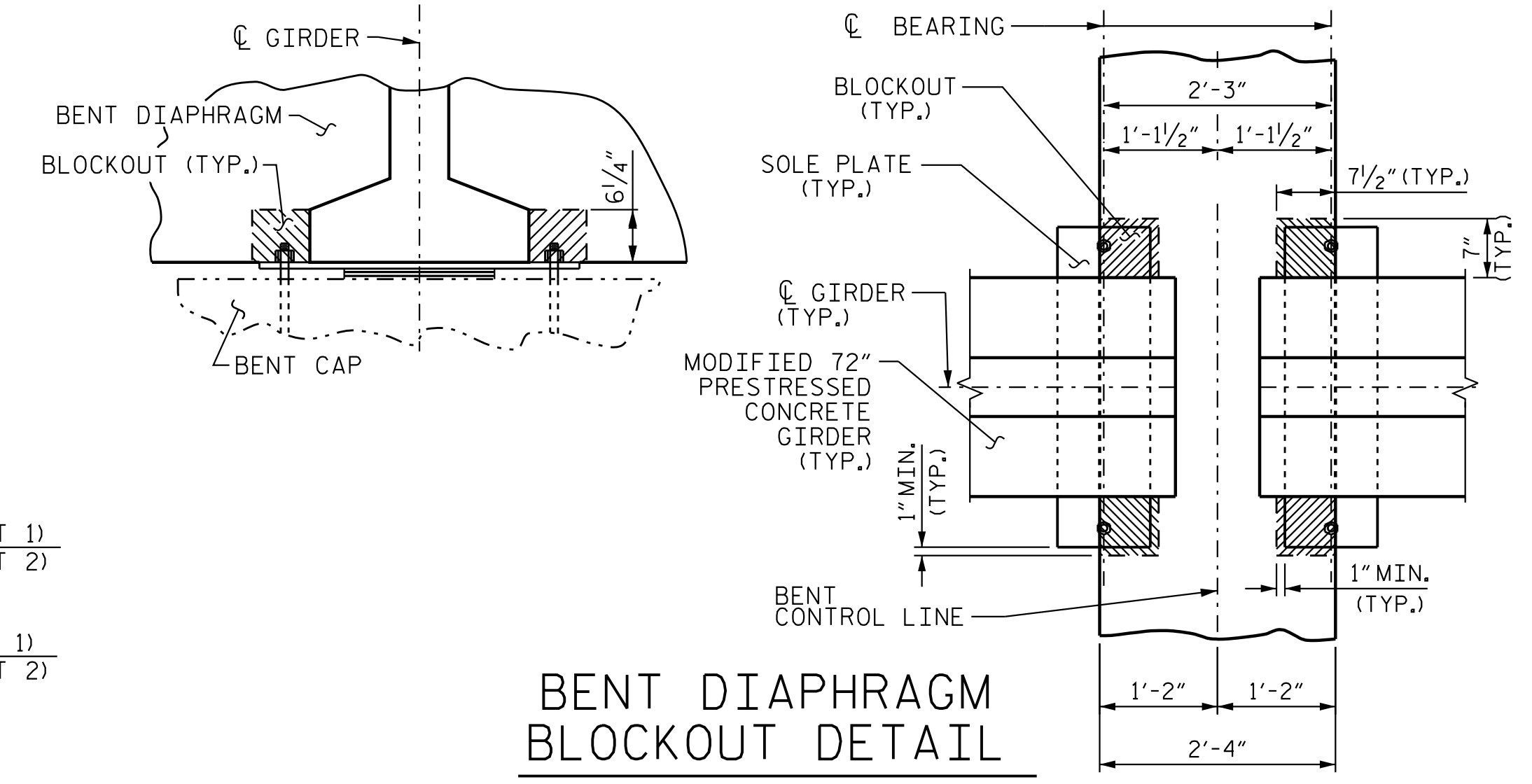
SECTION THRU DIAPHRAGM @ INTERIOR BENT  
 SEE 'PLAN OF SPANS - INTERIOR BENT DIAPHRAGMS', SHEET 8 OF 8

(EF) DENOTES EACH FACE

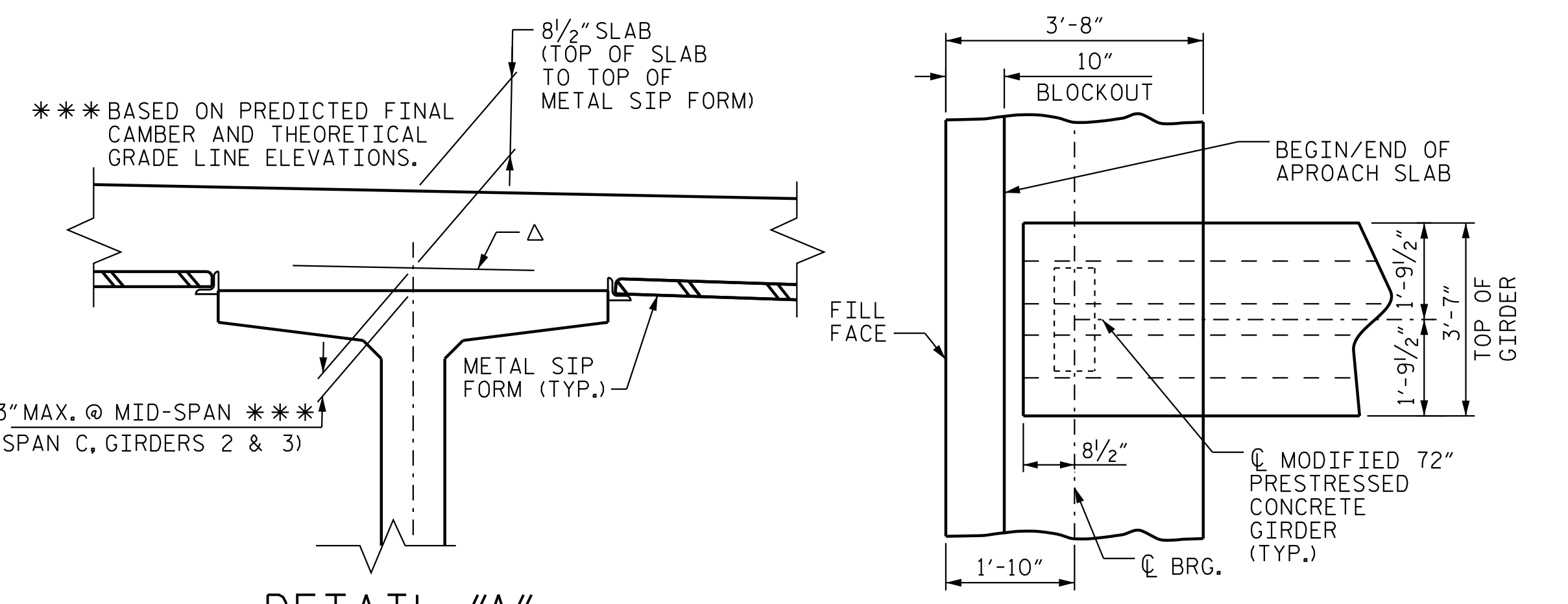
\* NORMAL TO C BENT  
 \*\* TYP. EA. BAY, EXCEPT FOR STAGE II CLOSURE BAY.

**BOTTOM OF OVERHANG ELEV.  
 @ OUTSIDE EDGE OF SUPERSTR.**

OVERHANG	END BENT	ELEV.
LEFT SIDE	1	670.66
RIGHT SIDE	1	671.32
LEFT SIDE	2	669.23
RIGHT SIDE	2	669.89

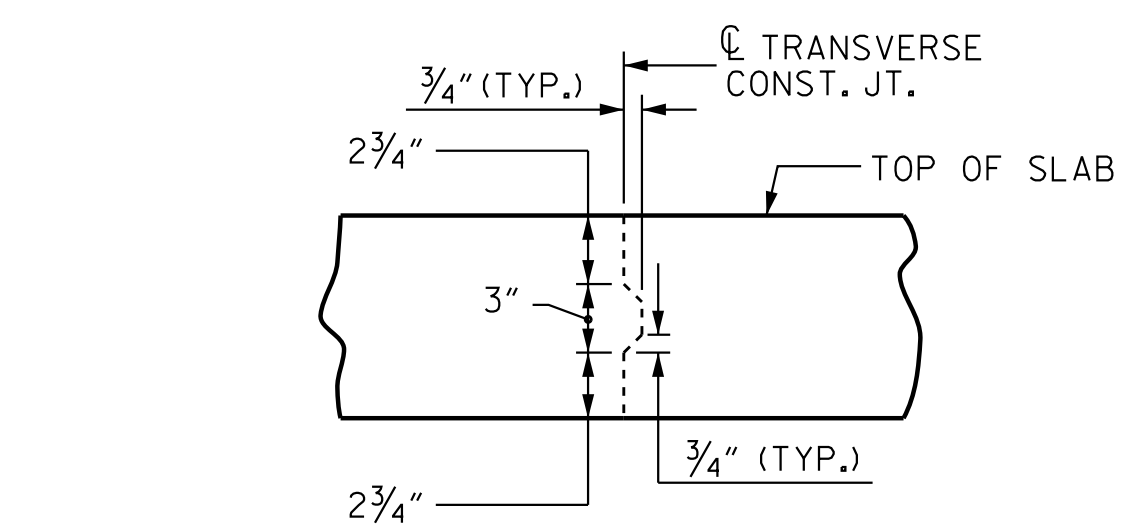


**BENT DIAPHRAGM BLOCKOUT DETAIL**



**PLAN OF INTEGRAL END BENT**

\*\*\* BASED ON PREDICTED FINAL CAMBER AND THEORETICAL GRADE LINE ELEVATIONS.



**TRANSVERSE CONSTRUCTION JOINT DETAIL**

NOTE: REINFORCING STEEL IN SLAB NOT SHOWN. LONGITUDINAL REINFORCING STEEL SHALL BE CONTINUOUS THRU JOINT

PROJECT NO. R-2707D  
CLEVELAND COUNTY  
 STATION: 849+00.00 -L-

SHEET 5 OF 5



STATE OF NORTH CAROLINA  
 DEPARTMENT OF TRANSPORTATION  
 RALEIGH  
 SUPERSTRUCTURE  
 TYPICAL SECTION  
 DETAILS  
 (LEFT LANE)

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S5-12
1			3			TOTAL SHEETS
2			4			56

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

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5/9/2023 2:07:46 PM jhogenbush