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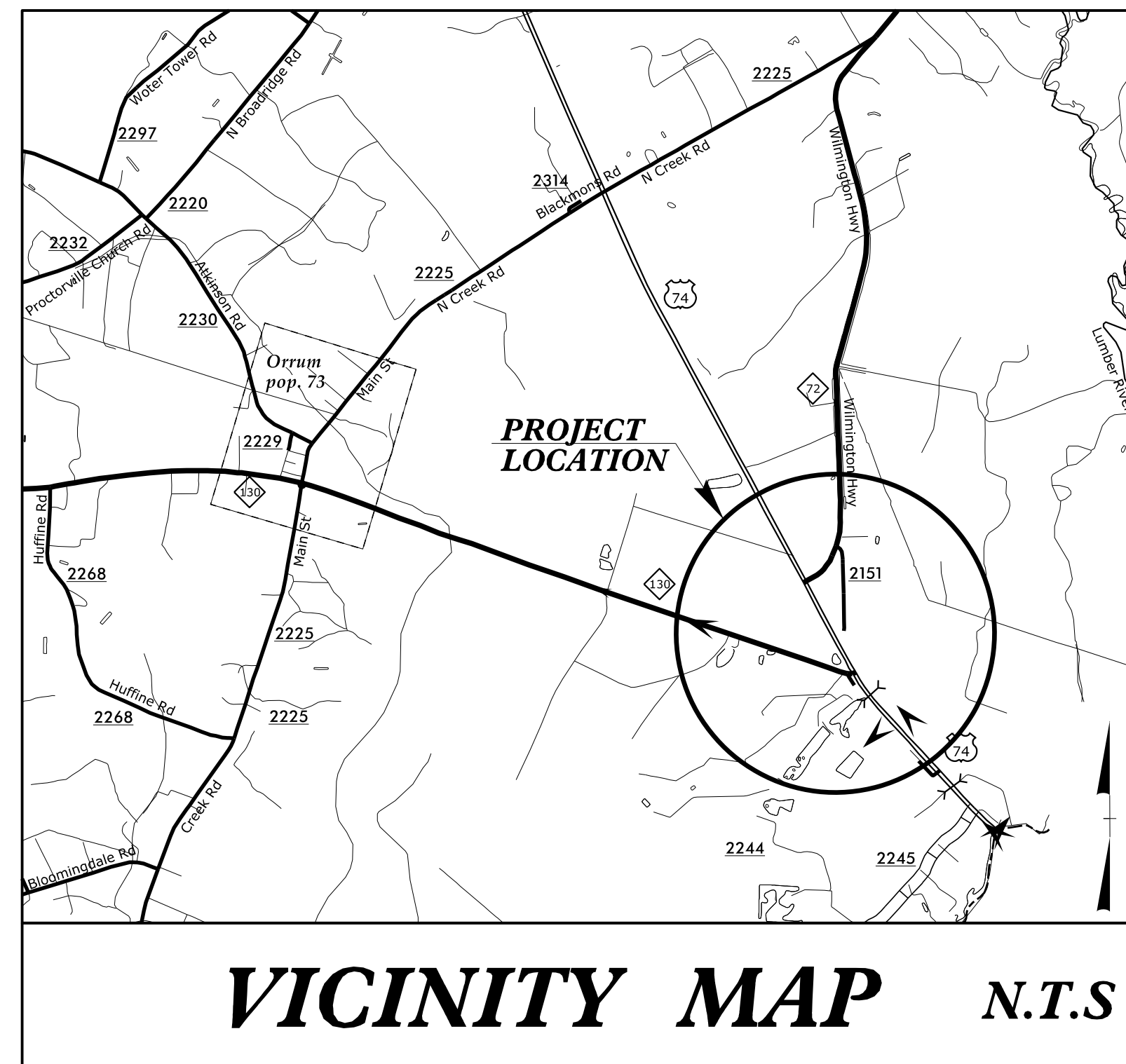
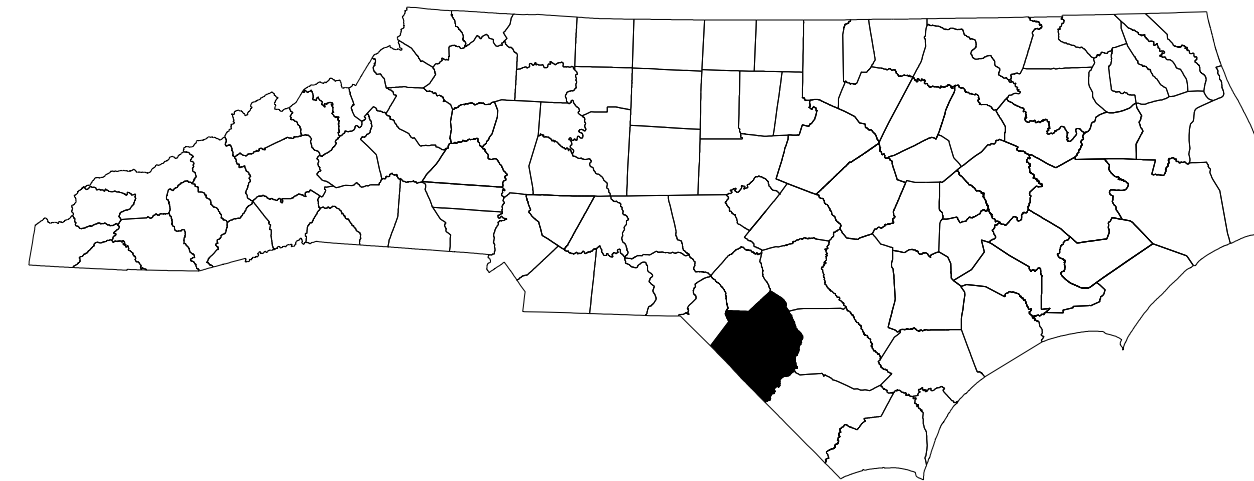
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STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

TRANSPORTATION MANAGEMENT PLAN

ROBESON COUNTY

LOCATION: US 74 AT NC 72 /NC 130
CONVERT INTERSECTION TO INTERCHANGE



INDEX OF SHEETS

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SHEET NO.
TMP-1

R-5751

TIP PROJECT:

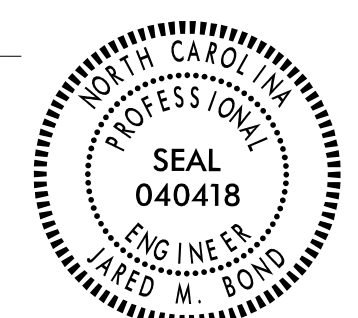
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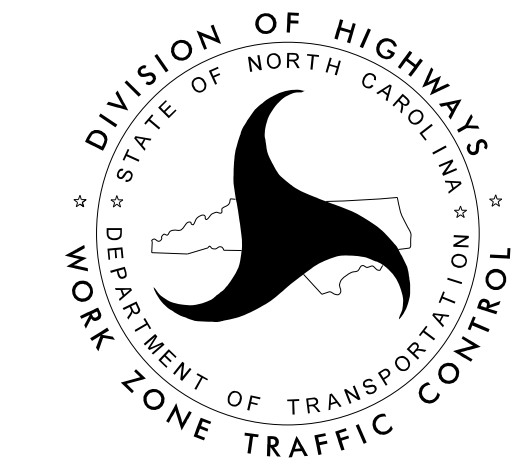
PLANS PREPARED BY:

JARED M. BOND, P.E.
PROJECT ENGINEER

NIKI AVERGINOS, E.I.
PROJECT DESIGN ENGINEER

NCDOT CONTACTS:

SPENCER JENNINGS
PROJECT DESIGN ENGINEER



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ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS SHOWN IN "ROADWAY STANDARD DRAWINGS" - N.C. DEPARTMENT OF TRANSPORTATION - RALEIGH, N.C., DATED JANUARY 2018 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.01	WORK ZONE WARNING SIGNS
1101.02	TEMPORARY LANE CLOSURES
1101.03	TEMPORARY ROAD CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW BOARDS
1130.01	DRUMS
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGERS
1160.01	TEMPORARY CRASH CUSHION
1165.01	TRUCK MOUNTED ATTENUATOR
1170.01	PORTABLE CONCRETE BARRIER
1180.01	SKINNY - DRUMS
1205.01	PAVEMENT MARKINGS - LINE TYPES AND OFFSETS
1205.02	PAVEMENT MARKINGS - TWO LANE AND MULTILANE ROADWAYS
1205.03	PAVEMENT MARKINGS - EXITS AND ENTRANCE RAMPS
1205.04	PAVEMENT MARKINGS - INTERSECTIONS
1205.05	PAVEMENT MARKINGS - TURN LANES
1205.08	PAVEMENT MARKINGS - SYMBOLS AND WORD MESSAGES
1205.09	PAVEMENT MARKINGS - PAINTED ISLANDS
1205.12	PAVEMENT MARKINGS - BRIDGES
1205.14	PAVEMENT MARKINGS - ROUNDABOUTS
1250.01	RAISED PAVEMENT MARKERS - INSTALLATION SPACING
1251.01	RAISED PAVEMENT MARKERS - (PERMANENT AND TEMPORARY)

LEGEND

GENERAL

- DIRECTION OF TRAFFIC FLOW
- DIRECTION OF PEDESTRIAN TRAFFIC FLOW
- EXIST. PVMT.
- NORTH ARROW
- PROPOSED PVMT.
- TEMP. SHORING (LOCATION PURPOSES ONLY)

- WORK AREA
- CONTINUED CONSTRUCTION
- REMOVAL
- PAVEMENT WEDGING
- TEMPORARY PAVEMENT

PAVEMENT MARKINGS

- EXISTING LINES
- TEMPORARY LINES

TRAFFIC CONTROL DEVICES

- BARRICADE (TYPE III)
- CONE
- DRUM SKINNY DRUM TUBULAR MARKER
- TEMPORARY CRASH CUSHION
- FLASHING ARROW BOARD
- FLAGGER
- LAW ENFORCEMENT
- TRUCK MOUNTED ATTENUATOR (TMA)
- CHANGEABLE MESSAGE SIGN

TEMPORARY SIGNING

- PORTABLE SIGN
- STATIONARY SIGN
- STATIONARY OR PORTABLE SIGN

PAVEMENT MARKERS

- CRYSTAL/CRYSTAL
- CRYSTAL/RED
- YELLOW/YELLOW

PAVEMENT MARKING SYMBOLS

- PAVEMENT MARKING SYMBOLS

6" WORK ZONE PERFORMANCE		12" WORK ZONE PERFORMANCE	
P20	WHITE EDGELINE (6", PAINT)	P50	WHITE GORELINE (12", PAINT)
P21	WHITE SOLID LANE LINE (6", PAINT)		
P22	10' WHITE SKIP (6", PAINT)		
P23	3'-9' WHITE MINISKIP (6", PAINT)		
P24	2'-6' WHITE MINISKIP (6", PAINT)		
P30	YELLOW EDGELINE (6", PAINT)		
P40	WHITE GORELINE (12", PAINT)		
		PAVEMENT MARKING SYMBOLS	
		P70	LEFT TURN ARROW
		P71	RIGHT TURN ARROW
		P72	STRAIGHT ARROW
		P79	MERGE ARROW

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MANAGEMENT STRATEGY

PHASE I:

JACK AND BORE THE STRUCTURAL PLATE AND WELDED STEEL PIPES UNDER -L-. USING LANE CLOSURES, CONSTRUCT INSIDE SHOULDER AND TEMPORARY PAVEMENT ALONG -L-. BEHIND ANCHORED PCB, BEGIN CONSTRUCTION OF MEDIAN BENT FOR STRUCTURE IN -L- MEDIAN.

PHASE II:

WITH TRAFFIC SHIFTED TO THE INSIDE, INSTALL PCB ALONG -L-, BEGIN OUTSIDE CONSTRUCTION OF -L-, OFFLINE CONSTRUCTION OF -Y1A-, -Y1B-, -Y1C-, -Y1RPA-, -Y1RPB-, -Y1RPC-, -Y1RPD-, -RAB-, -RCD-, -SRA- AND -SRB-. USING AN OFF-SITE DETOUR, CONSTRUCT -SRA- TIE-IN. USING ALTERNATE LANE CLOSURES, WEDGE PAVEMENT ALONG -Y1A-, -Y1C- AND -SRB- UP TO PROPOSED GRADE. USING ROAD CLOSURES AND AN OFF-SITE DETOUR, COMPLETE CONSTRUCTION OF -Y1RPB-. USING AN OFF-SITE DETOUR, PLACE GIRDERS AND CONSTRUCT OVERHEAD STRUCTURE ALONG -Y1B-.

PHASE III:

WITH TRAFFIC SHIFTED TO THE OUTSIDE, COMPLETE CONSTRUCTION OF -L- MEDIAN, TEMPORARY PAVEMENT REMOVAL AND CABLE GUIDERAIL INSTALLATION.

PHASE IV:

PAVE THE FINAL LAYER OF SURFACE COURSE, PLACE THE FINAL PAVEMENT MARKINGS AND SHIFT TRAFFIC INTO THE FINAL PATTERNS.

GENERAL NOTES

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS, AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS OR RESULT IN DUPLICATE OR UNDESIRED OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING, OR REMOVAL OF DEVICES AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT EXCEPT WHEN OTHERWISE NOTED IN THE PLAN OR DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
US 74	EASTER TO OCTOBER 1ST 6:00 AM FRIDAY TO 10:00 PM SUNDAY

B) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS:

ROAD NAME	HOLIDAY
US 74	
	1. FOR ANY UNEXPECTED OCCURRENCE THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
	2. FOR NEW YEAR'S, BETWEEN THE HOURS OF 6:00 A.M. DECEMBER 31st TO 10:00 P.M. JANUARY 2ND. IF NEW YEAR'S DAY IS ON A FRIDAY, SATURDAY, SUNDAY, OR MONDAY THEN UNTIL 10:00 P.M. THE FOLLOWING TUESDAY.
	3. FOR EASTER, BETWEEN THE HOURS OF 6:00 A.M. THURSDAY AND 10:00 P.M. MONDAY.
	4. FOR MEMORIAL DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY TO 10:00 P.M. TUESDAY.
	5. FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 6:00 A.M. THE DAY BEFORE INDEPENDENCE DAY AND 10:00 P.M. THE DAY AFTER INDEPENDENCE DAY.

IF INDEPENDENCE DAY IS ON A FRIDAY, SATURDAY, SUNDAY OR MONDAY THEN BETWEEN THE HOURS OF 6:00 A.M. THE THURSDAY BEFORE INDEPENDENCE DAY AND 10:00 P.M. THE TUESDAY AFTER INDEPENDENCE DAY.

6. FOR LABOR DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY AND 10:00 P.M. TUESDAY.

7. FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 6:00 A.M. TUESDAY TO 10:00 P.M. MONDAY.

8. FOR CHRISTMAS, BETWEEN THE HOURS OF 6:00 A.M. THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 10:00 P.M. THE FOLLOWING TUESDAY AFTER THE WEEK OF CHRISTMAS.

C) DO NOT CLOSE ROADS AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS	OPERATION
US 74	MONDAY - SUNDAY 6:00 AM TO 10:00 PM	GIRDER PLACEMENT

D) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE HAULING OPERATION IS PROTECTED BY BARRIER OR GUARDRAIL OR AS DIRECTED BY THE ENGINEER.

LANE AND SHOULDER CLOSURE REQUIREMENTS

E) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED OR AS DIRECTED BY THE ENGINEER.

F) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 15 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR A LANE CLOSURE IS INSTALLED.

G) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

H) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS, OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.

I) DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY, RAMP, OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.

J) PROVIDE TRAFFIC CONTROL FOR APPROPRIATE LANE CLOSURES FOR SURVEYING DONE BY THE DEPARTMENT.

PAVEMENT EDGE DROP OFF REQUIREMENTS

K) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS AN EDGE OF PAVEMENT DROP-OFF AS FOLLOWS:



BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.

BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.



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GENERAL NOTES

- L) DO NOT EXCEED A DIFFERENCE OF 2 INCHES IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC FOR NOMINAL LIFTS OF 1.5 INCHES. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) IN ADVANCE AND A MINIMUM OF EVERY HALF MILE THROUGHOUT THE UNEVEN AREA.

TRAFFIC PATTERN ALTERATIONS

- M) NOTIFY THE ENGINEER THIRTY (30) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

- N) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 40 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.

- O) PROVIDE SIGNING AND DEVICES REQUIRED TO CLOSE THE ROAD ACCORDING TO THE ROADWAY STANDARD DRAWINGS AND TRAFFIC CONTROL PLANS.

PROVIDE AND INSTALL SIGNING REQUIRED FOR THE OFF-SITE DETOUR ROUTE AS SHOWN IN THE TRAFFIC CONTROL PLANS.

- P) COVER OR REMOVE ALL SIGNS AND DEVICES REQUIRED TO CLOSE THE ROAD WHEN ROAD CLOSURE IS NOT IN OPERATION.

COVER OR REMOVE ALL SIGNS REQUIRED FOR THE OFF-SITE DETOUR WHEN THE DETOUR IS NOT IN OPERATION.

- Q) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

- R) INSTALL BLACK ON ORANGE "DIP" SIGNS (W8-2) AND/OR "BUMP" SIGNS (W8-1) IN ADVANCE OF THE UNEVEN AREA, OR AS DIRECTED BY THE ENGINEER.

TRAFFIC BARRIER

- S) INSTALL TEMPORARY BARRIER ACCORDING TO THE TRANSPORTATION MANAGEMENT PLANS A MAXIMUM OF TWO (2) WEEKS PRIOR TO BEGINNING WORK IN ANY LOCATION. ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION PROCEED IN A CONTINUOUS MANNER TO COMPLETE THE PROPOSED WORK IN THAT LOCATION UNLESS OTHERWISE STATED IN THE TRANSPORTATION MANAGEMENT PLANS OR AS DIRECTED BY THE ENGINEER.

DO NOT PLACE BARRIER DIRECTLY ON ANY SURFACE OTHER THAN ASPHALT OR CONCRETE.

ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION AND NO WORK IS PERFORMED BEHIND THE TEMPORARY BARRIER FOR A PERIOD LONGER THAN TWO (2) MONTHS, REMOVE / RESET TEMPORARY BARRIER AT NO COST TO THE DEPARTMENT UNLESS OTHERWISE STATED IN THE TRANSPORTATION MANAGEMENT PLANS, TEMPORARY BARRIER IS PROTECTING A HAZARD, OR AS DIRECTED BY THE ENGINEER.

INSTALL TEMPORARY BARRIER WITH THE TRAFFIC FLOW BEGINNING WITH THE UPSTREAM SIDE OF TRAFFIC. REMOVE TEMPORARY BARRIER AGAINST THE TRAFFIC FLOW BEGINNING WITH THE DOWNSTREAM SIDE OF TRAFFIC.

INSTALL AND SPACE DRUMS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH) TO CLOSE OR KEEP THE SECTION OF THE ROADWAY CLOSED UNTIL THE TEMPORARY BARRIER CAN BE PLACED OR AFTER THE TEMPORARY BARRIER IS REMOVED.

- T) PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER AT ALL TIMES DURING THE INSTALLATION AND REMOVAL OF THE BARRIER BY EITHER A TRUCK MOUNTED ATTENUATOR (MAXIMUM 72 HOURS) OR A TEMPORARY CRASH CUSHION.

PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER FROM ONCOMING TRAFFIC AT ALL TIMES BY A TEMPORARY CRASH CUSHION UNLESS THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER IS OFFSET FROM ONCOMING TRAFFIC AS FOLLOWS OR AS SHOWN IN THE PLANS: (SEE ALSO 1101.05)

POSTED SPEED LIMIT	MINIMUM OFFSET
40 OR LESS	15 FT
45 - 50	20 FT
55	25 FT
60 MPH or HIGHER	30 FT

TRAFFIC CONTROL DEVICES

- U) WHEN LANE CLOSURES ARE NOT IN EFFECT SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER IN FEET THAN TWICE THE POSTED SPEED LIMIT (MPH) EXCEPT, 10 FT ON-CENTER IN RADII, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY. REFER TO STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES SECTIONS 1130 (DRUMS), 1135 (CONES) AND 1180 (SKINNY DRUMS) FOR ADDITIONAL REQUIREMENTS.

- V) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY.

- W) PLACE ADDITIONAL SETS OF THREE CHANNELIZING DEVICES PERPENDICULAR TO THE EDGE OF TRAVELWAY ON 500 FT CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC.

PAVEMENT MARKINGS AND MARKERS

- X) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:

ROAD NAME	MARKING	MARKER
SOUTH CREEK RD.	PAINT	PERMANENT RAISED
ALL OTHER ROADS/BRIDGES	WORKZONE PERFORMANCE	TEMPORARY RAISED

- Y) PLACE ONE APPLICATION OF PAINT FOR TEMPORARY TRAFFIC PATTERNS. PLACE A SECOND APPLICATION OF PAINT SIX (6) MONTHS AFTER THE INITIAL APPLICATION AND EVERY SIX MONTHS AS DIRECTED BY THE ENGINEER.

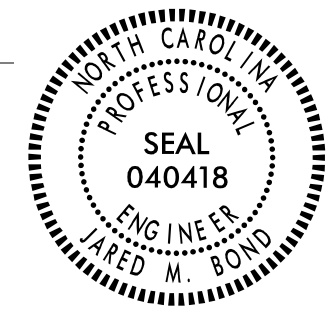
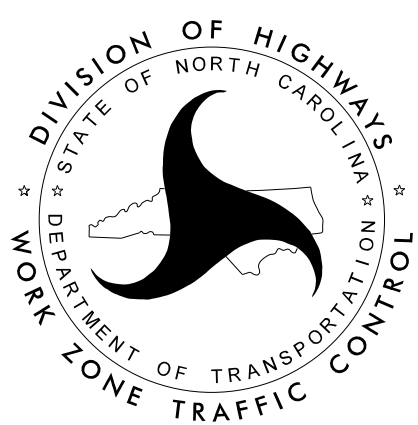
- Z) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.

- AA) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS BY THE END OF EACH DAY'S OPERATION.

- BB) TRACE THE PROPOSED MONOLITHIC ISLAND LOCATIONS WITH THE PROPER COLOR PAVEMENT MARKINGS PRIOR TO INSTALLATION. PLACE DRUMS TO DELINEATE ANY PROPOSED MONOLITHIC ISLANDS AFTER REMOVAL AND/OR BEFORE INSTALLATION.

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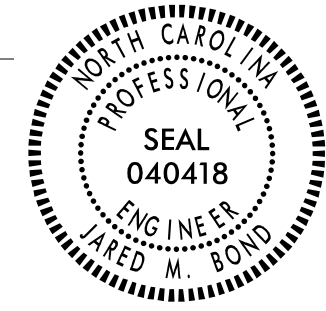
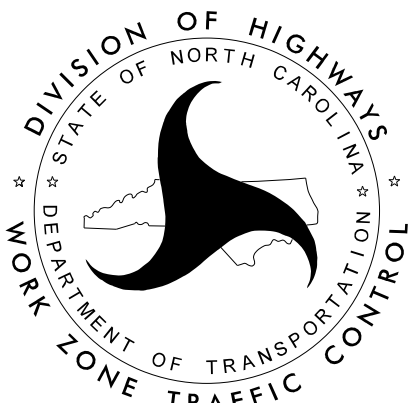
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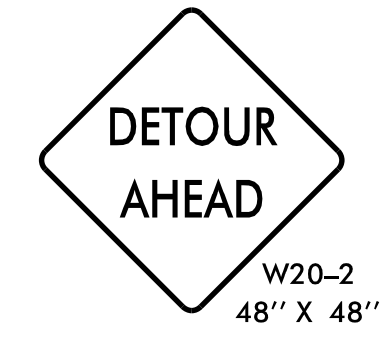
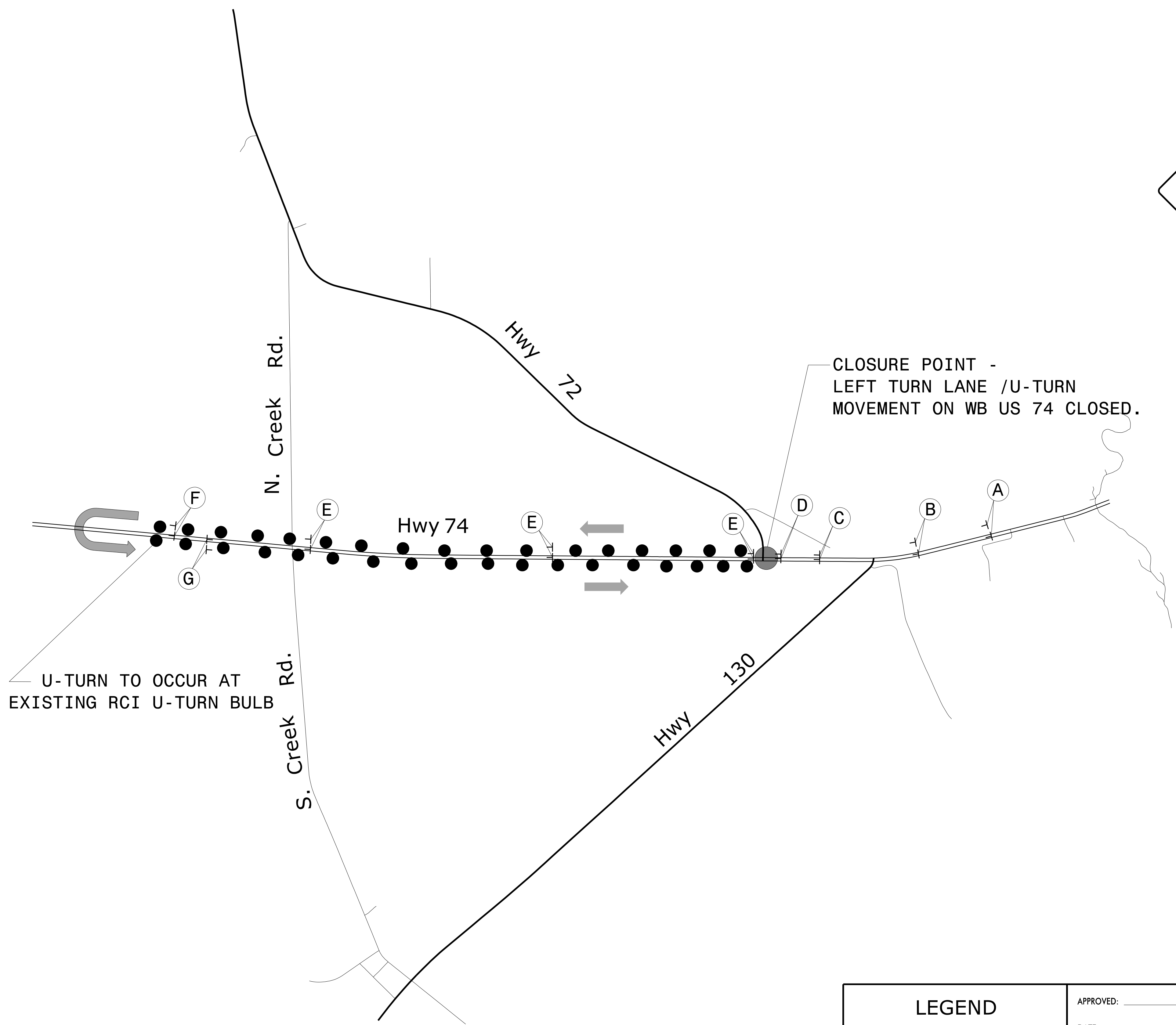
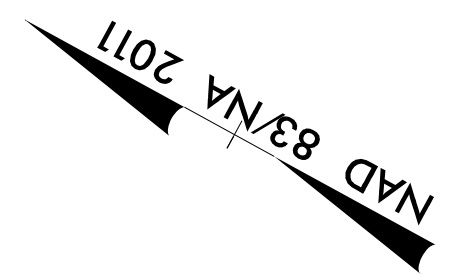
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- CC) LAW ENFORCEMENT MAY BE USED TO MAINTAIN TRAFFIC THROUGH THE WORK AREA AND/OR INTERSECTIONS AS DIRECTED BY THE ENGINEER.
- DD) CONSTRUCT ALL PAVEMENT (TEMPORARY AND/OR PROPOSED) TO MAINTAIN DRAINAGE AND NOT POND WATER IN THE TRAFFIC LANES.
- EE) PLACE TRAFFIC BACK INTO EXISTING PATTERN AT THE END OF EACH WORK PERIOD.
- FF) THE CONTRACTOR SHALL INSTALL ONE CMS IN EACH DIRECTION OF US 74 TO INFORM AND ALERT THE TRAVELING PUBLIC TO TRAFFIC CONDITIONS. THESE CMS SHALL NOT BE UTILIZED FOR TRAFFIC CONTROL OPERATIONS DURING CONSTRUCTION.

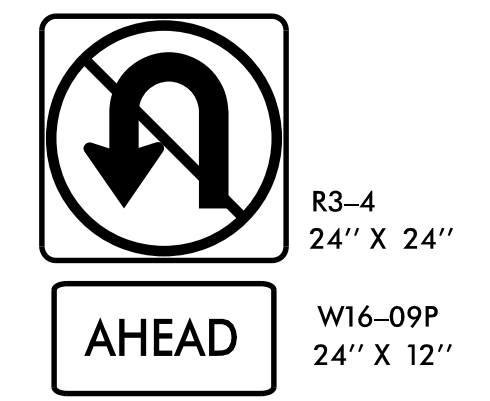
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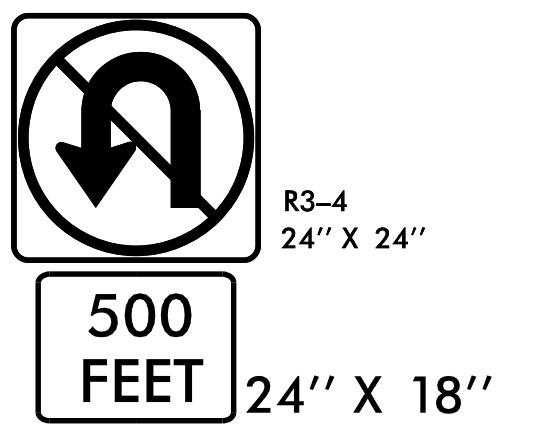
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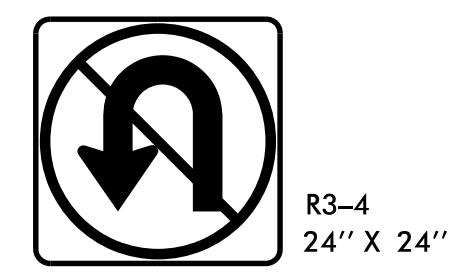
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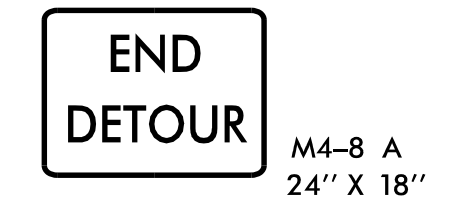
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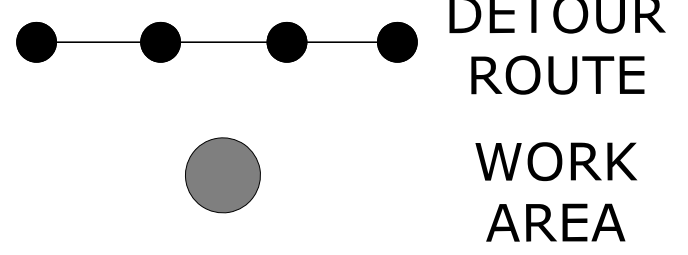


(G)



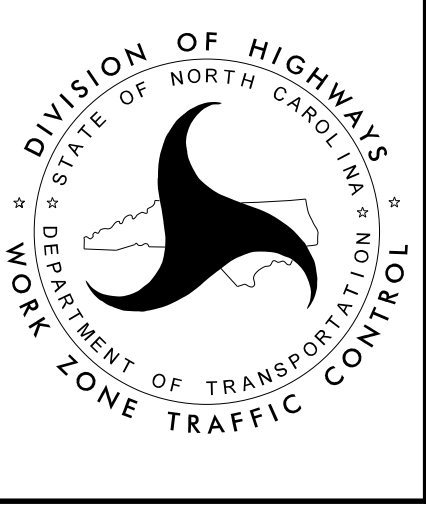
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LEGEND



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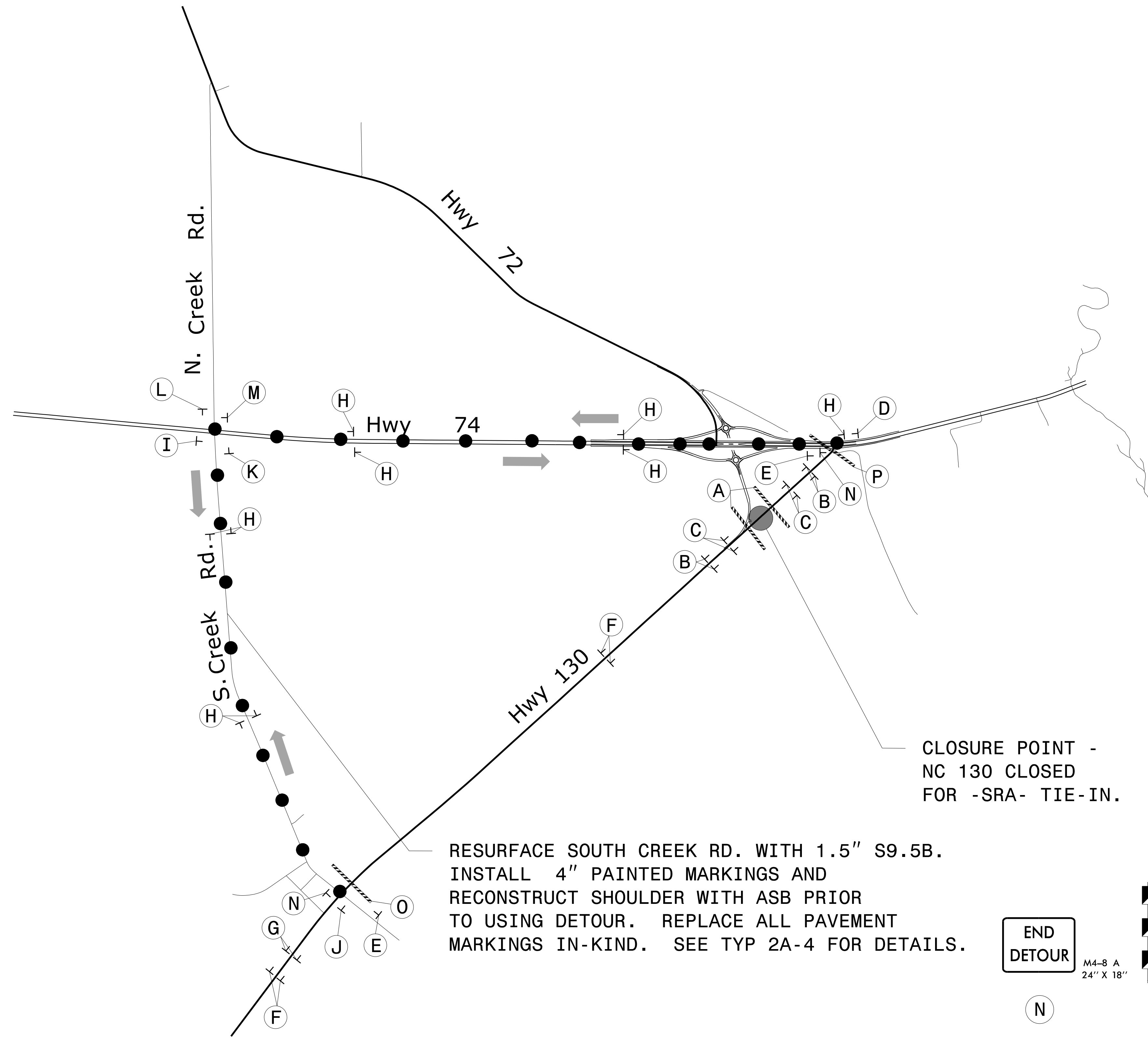
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US 74 WB U-TURN
DETOUR ROUTE

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R11-2
48" x 30"

TYPE III BARRICADE(S)

(A) ROAD CLOSED AHEAD W20-3 48" X 48"

(B) ROAD CLOSED 1000 FT W20-3 48" X 48"

(C) ROAD CLOSED 500 FT W20-3 48" X 48"

(D) ROAD CLOSED AHEAD W20-3 48" X 48"

(E) ROAD CLOSED AHEAD W20-3 48" X 48"

(F) ROAD CLOSED AHEAD W20-3 48" X 48"

(G) DETOUR AHEAD W20-2 48" X 48"

(H) DETOUR M4-8 24" X 12"

(I) DETOUR M4-8 24" X 12"

(J) DETOUR M4-8 24" X 12"

(K) DETOUR M4-8 24" X 12"

(L) DETOUR M4-8 24" X 12"

(M) DETOUR M4-8 24" X 12"

(N) END DETOUR M4-8 A 24" X 18"

(O) TYPE III BARRICADE R11-3 60" x 30"

(P) TYPE III BARRICADE R11-4 60" x 30"

SP-4L 42" X 12"

SP-4R 42" X 12"

M1-5

M3-2 24" X 12"

M3-4 24" X 12"

M4-8 24" X 12"

M6-1 21" X 15"

M6-3 21" X 15"

M6-1 L 21" X 15"

M6-1 21" X 15"

M6-3 21" X 15"

M6-1 L 21" X 15"

M4-10L 48" x 18"

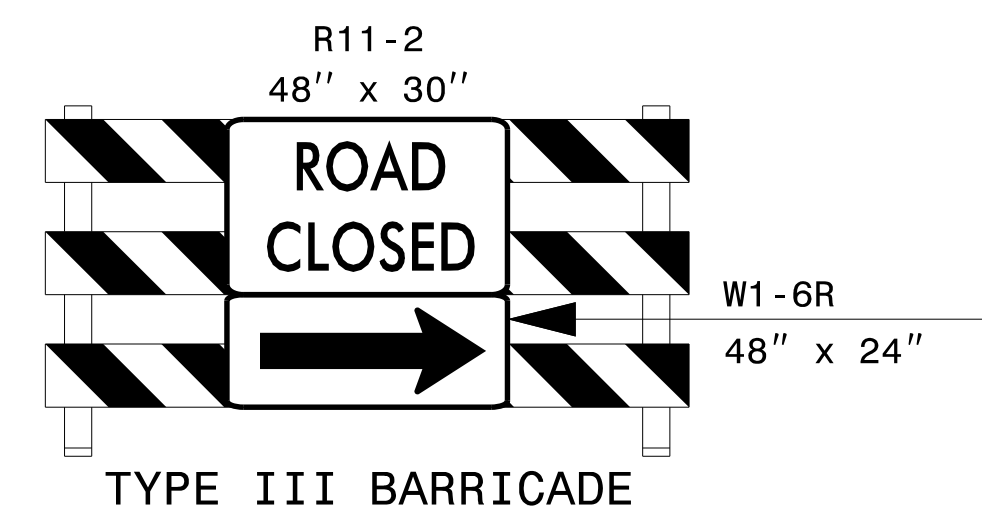
M4-10R 48" x 18"

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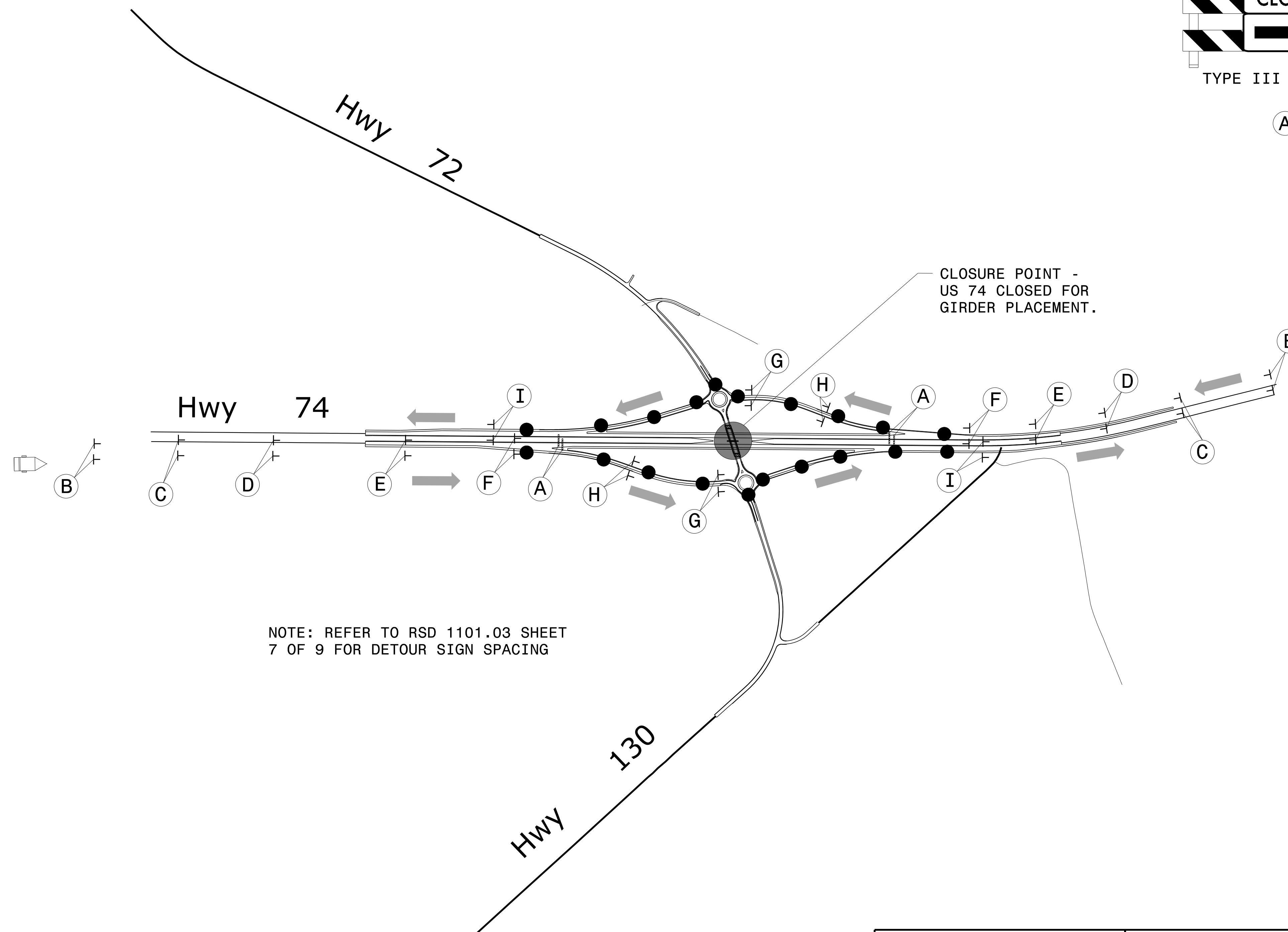
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 8521 SIX FORKS ROAD, SUITE 400
 RALEIGH, NC 27615
 NC FIRM LICENSE No: F-0493

NAD 83/NA 2011

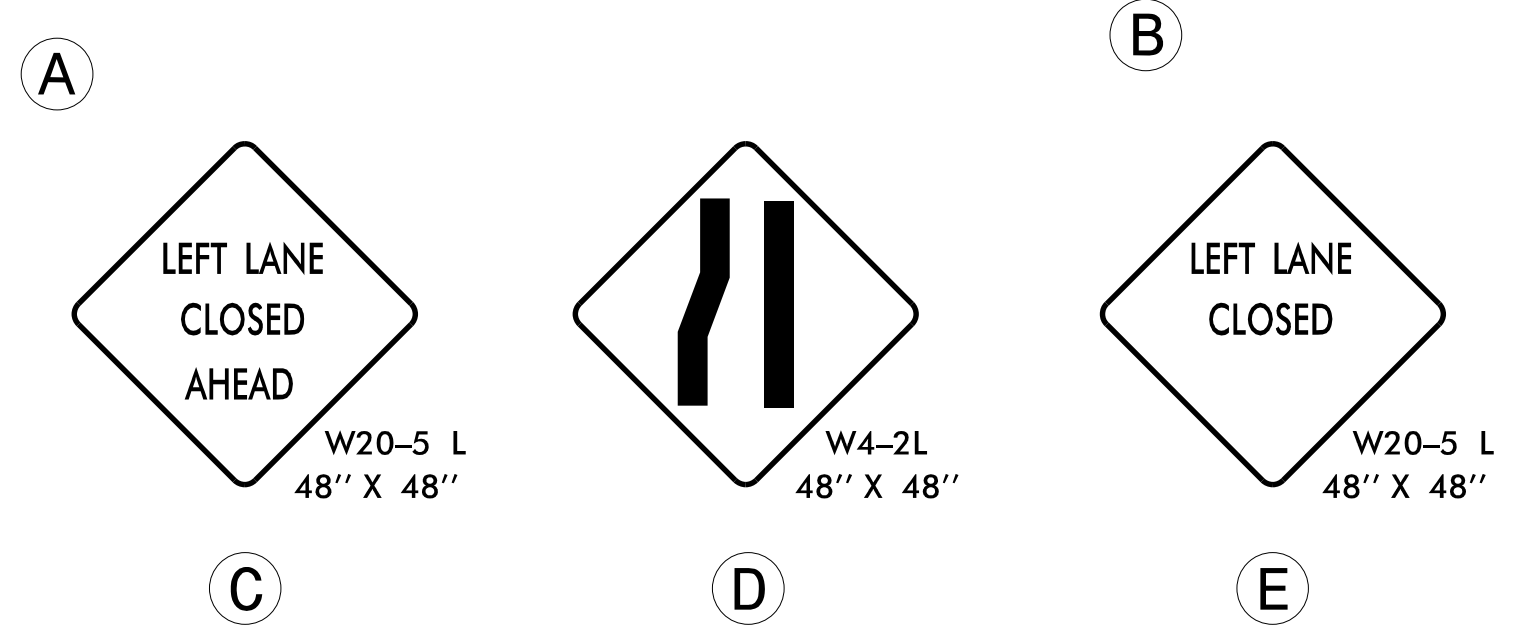


ALL TRAFFIC
USE NEXT EXIT

G21-65
132" X 54"



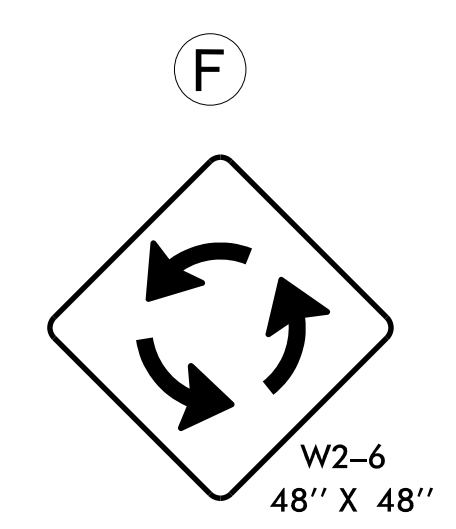
NOTE: REFER TO RSD 1101.03 SHEET
7 OF 9 FOR DETOUR SIGN SPACING



ALL TRAFFIC
→

SP 03355
48" X 48"

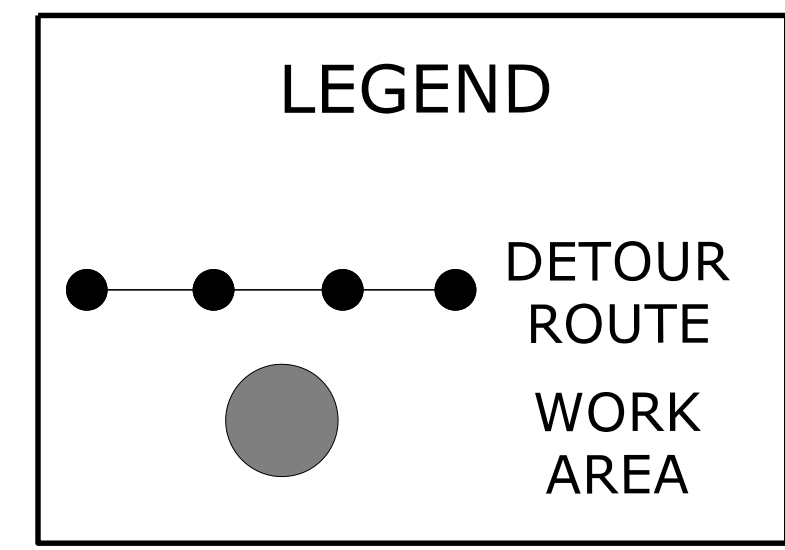
DETOUR



END
DETOUR

M4-8 A
24" X 18"

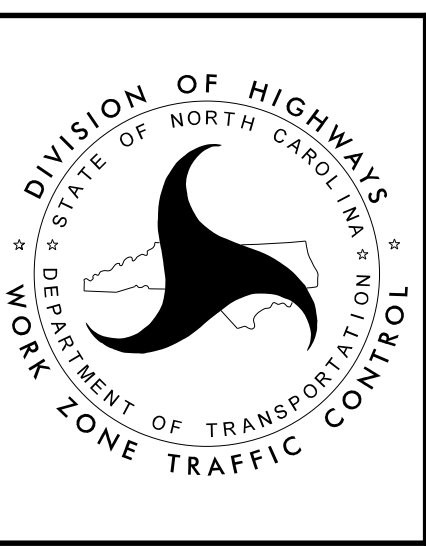
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8521 SIX FORKS ROAD, SUITE 400
RALEIGH, NC 27615
NC FIRM LICENSE No: F-0493



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DATE: _____

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**US 74 DETOUR FOR
GIRDER PLACEMENT**

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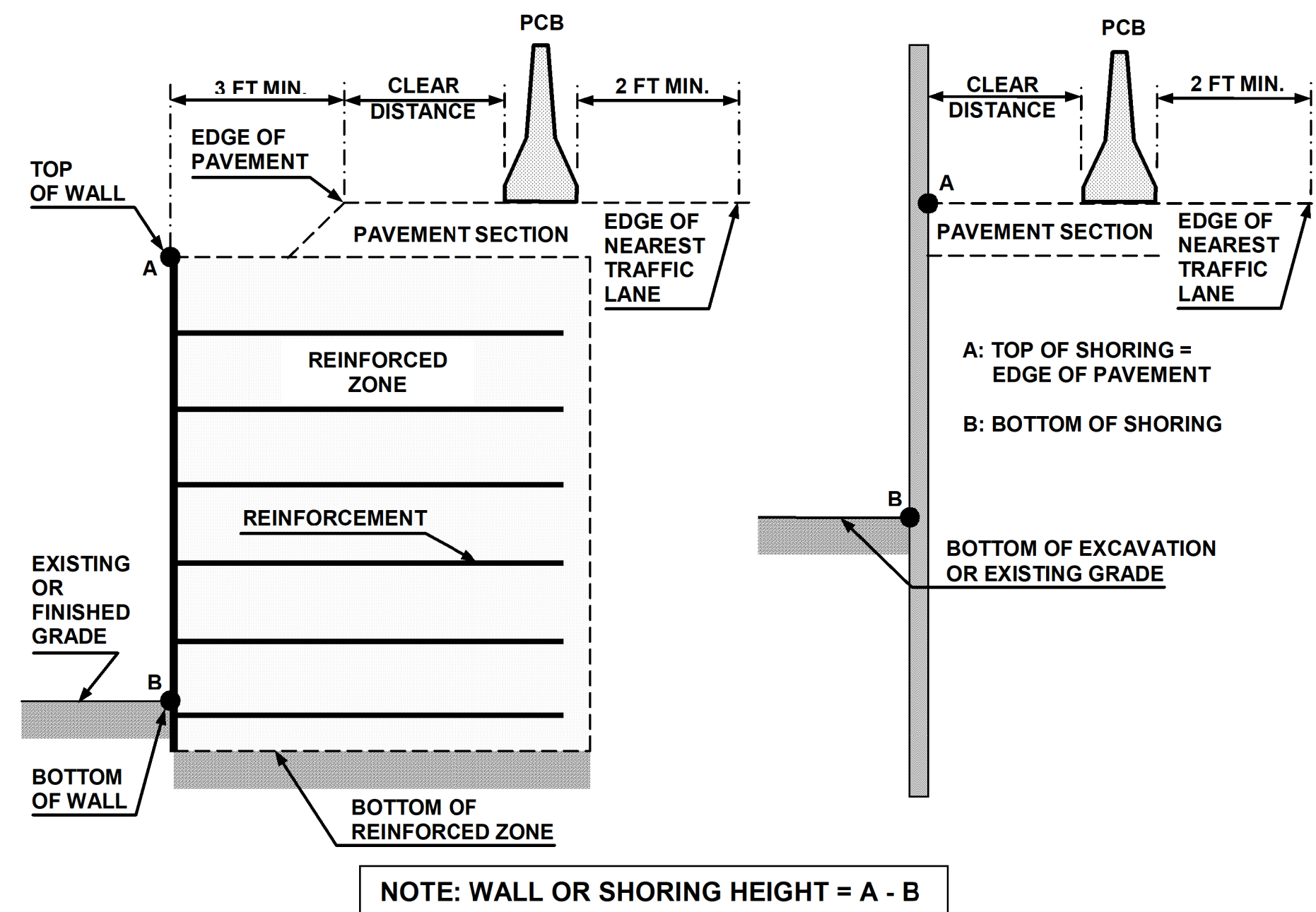


FIGURE A

NOTES

- 1- REFER TO THE TRAFFIC CONTROL PLANS FOR TEMPORARY SHORING LOCATIONS AND NOTES.
- 2- REFER TO THE "TEMPORARY SHORING" PROJECT SPECIAL PROVISION FOR INFORMATION ABOUT TEMPORARY SHORING AND PORTABLE CONCRETE BARRIER (PCB).
- 3- PCB IS REQUIRED IF TEMPORARY SHORING IS LOCATED WITHIN THE CLEAR ZONE IN ACCORDANCE WITH THE AASHTO ROADSIDE DESIGN GUIDE. DO NOT PLACE BARRIER DIRECTLY ON ANY SURFACE OTHER THAN ASPHALT OR CONCRETE. (CONTACT NCDOT PAVEMENT MANAGEMENT UNIT FOR APPLICABLE PAVEMENT DESIGN).
- 4- BASED ON THE CLEAR DISTANCE, OFFSET, DESIGN SPEED AND PAVEMENT TYPE, CHOOSE AN UNANCHORED OR ANCHORED PCB FROM THE TABLE SHOWN IN FIGURE B. CLEAR DISTANCE IS DEFINED AS SHOWN IN FIGURE A AND OFFSET IS DEFINED AS SHOWN IN FIGURE B.
- 5- AT THE CONTRACTOR'S OPTION OR IF THE MINIMUM REQUIRED CLEAR DISTANCE IS NOT AVAILABLE, SET PCB NEXT TO AND UP AGAINST THE TRAFFIC SIDE OF THE TEMPORARY SHORING EXCEPT FOR BARRIER ABOVE TEMPORARY WALLS. PCB WITH THE MINIMUM REQUIRED CLEAR DISTANCE IS REQUIRED ABOVE TEMPORARY WALLS.
- 6- USE NCDOT PORTABLE CONCRETE BARRIER (PCB) IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1170.01 AND SECTION 1170 OF THE STANDARD SPECIFICATIONS.
- 7- PCB REQUIREMENTS FOR TEMPORARY WALLS APPLY TO TEMPORARY MECHANICALLY STABILIZED EARTH (MSE) WALLS AND TEMPORARY SOIL NAIL WALLS.
- 8- SET PCB WITH A MINIMUM HORIZONTAL DISTANCE OF 2 FT BETWEEN THE FRONT FACE OF THE BARRIER AND THE EDGE OF THE NEAREST TRAFFIC LANE AS SHOWN IN FIGURE A UNLESS OTHERWISE SHOWN IN THE PLANS AND OR AS APPROVED BY THE ENGINEER.
- 9- FOR PCB ABOVE AND BEHIND TEMPORARY WALLS, PROVIDE A MINIMUM DISTANCE OF 3 FT BETWEEN THE EDGE OF PAVEMENT AND THE WALL FACE AS SHOWN IN FIGURE A. IF THESE MINIMUM REQUIRED DISTANCES ARE NOT AVAILABLE, CONTACT THE ENGINEER.
- 10- TABLE SHOWN IN FIGURE B IS BASED ON NCDOT RESEARCH PROJECT NO. 2005-010 WITH VEHICLE TYPE USED FOR NCHRP 350 CRASH TESTS. BARRIER DEFLECTIONS AND RESULTING MINIMUM REQUIRED CLEAR DISTANCES MIGHT VARY SIGNIFICANTLY FOR LARGER HEAVIER VEHICLES, RUNS OF BARRIER LESS THAN 200 FT IN LENGTH AND WET OR DRY PAVEMENT.

MINIMUM REQUIRED CLEAR DISTANCE, inches

Barrier Type	Pavement Type	Offset * ft	Design Speed, mph					
			<30	31-40	41-50	51-60	61-70	71-80
Unanchored PCB	Asphalt	<8	24	26	29	32	36	40
		8-14	26	28	31	35	38	42
		14-20	27	29	34	36	39	43
		20-26	28	31	35	38	40	44
		26-32	29	32	36	39	42	45
		32-38	30	34	38	41	43	46
		38-44	31	34	41	43	45	48
		44-50	31	35	41	43	46	49
		50-56	32	36	42	44	47	50
	>56	32	36	42	45	47	51	
	Concrete	<8	17	18	21	22	25	26
		8-14	19	20	23	25	26	29
		14-20	22	22	24	26	28	31
		20-26	23	24	26	27	30	34
		26-32	24	25	27	28	32	35
		32-38	24	26	27	30	33	36
		38-44	25	26	28	30	34	37
		44-50	26	26	28	32	35	37
50-56		26	26	28	32	35	38	
>56	26	27	29	32	36	38		
Anchored PCB	Asphalt	All Offsets	24 for All Design Speeds					
Anchored PCB	Concrete (including bridge approach slabs)	All Offsets	12 for All Design Speeds					

* See Figure Below

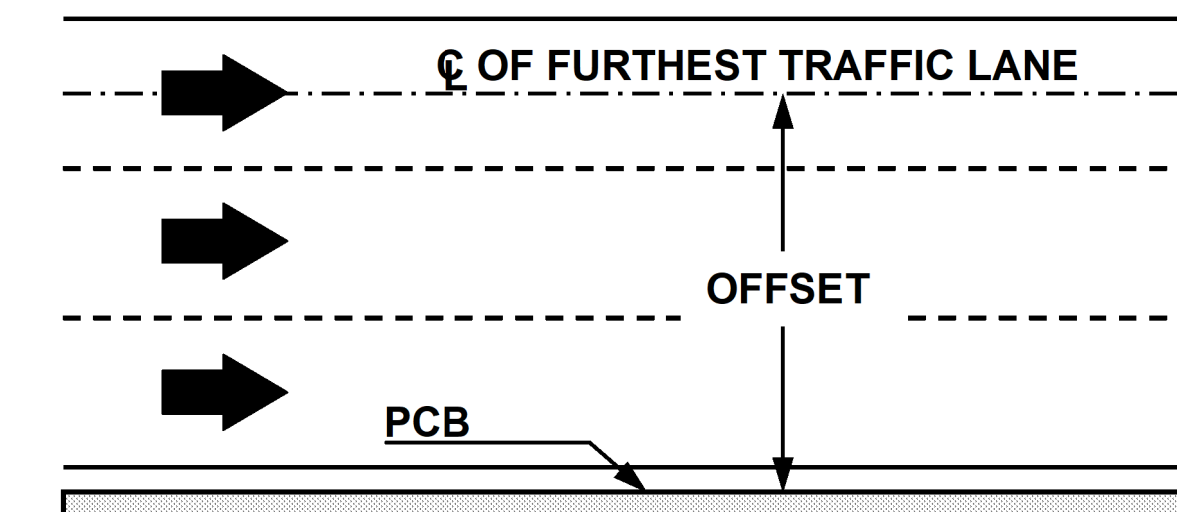


FIGURE B



APPROVED: _____	DATE: _____		
PORTABLE CONCRETE BARRIER AT TEMPORARY SHORING LOCATIONS			

TEMPORARY SHORING NOTES

SHORING LOCATION NO. 1

FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.

BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.

DESIGN TEMPORARY SHORING FROM STATION -L- 42+05±, 13 FT LT, TO STATION -L- 42+67±, 13 FT LT, FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:

UNIT WEIGHT = 120 PCF
 FRICTION ANGLE = 30
 DEGREES COHESION = 0 PSF
 GROUNDWATER ELEVATION = N/A

DO NOT USE A TEMPORARY WALL FOR TEMPORARY SHORING FROM STATION -L- 42+05±, 13 FT LT, TO STATION -L- 42+67±, 13 FT LT.

AT THE CONTRACTOR'S OPTION, USE STANDARD TEMPORARY SHORING FOR TEMPORARY SHORING FROM STATION -L- 42+05±, 13 FT LT, TO STATION -L- 42+67±, 13 FT LT. SEE GEOTECHNICAL STANDARD DETAIL NO. 1801.01 FOR STANDARD TEMPORARY SHORING.

SHORING LOCATION NO. 2

FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.

BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.

DESIGN TEMPORARY SHORING FROM STATION -L- 42+13±, 13 FT RT, TO STATION -L- 42+77±, 13 FT RT, FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:

UNIT WEIGHT = 120 PCF
 FRICTION ANGLE = 30
 DEGREES COHESION = 0 PSF
 GROUNDWATER ELEVATION = N/A

DO NOT USE A TEMPORARY WALL FOR TEMPORARY SHORING FROM STATION -L- 42+13±, 13 FT RT, TO STATION -L- 42+77±, 13 FT RT.

AT THE CONTRACTOR'S OPTION, USE STANDARD TEMPORARY SHORING FOR TEMPORARY SHORING FROM STATION -L- 42+13±, 13 FT RT, TO STATION -L- 42+77±, 13 FT RT. SEE GEOTECHNICAL STANDARD DETAIL NO. 1801.01 FOR STANDARD TEMPORARY SHORING.

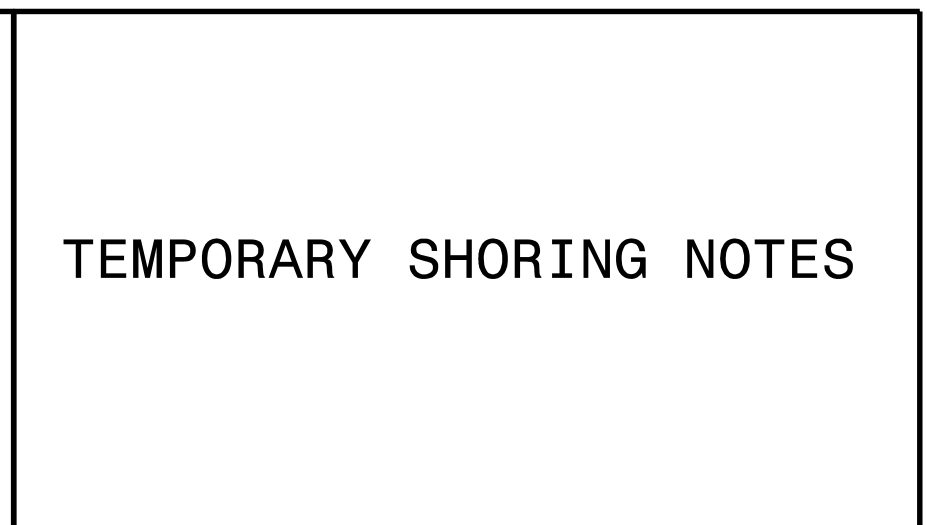
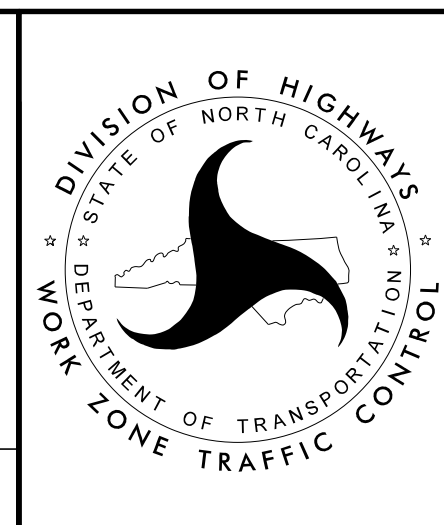
THE TEMPORARY SHORING NOTES SHOWN ON THIS SHEET WERE PROVIDED THROUGH A SEALED DOCUMENT FROM THE GEOTECHNICAL ENGINEERING UNIT. THE DOCUMENT WAS SUBMITTED TO THE WZTC SECTION ON SEPTEMBER 08, 2022 AND SEALED BY A PROFESSIONAL ENGINEER, THEIN TUN ZAN, LICENSE # 030943.

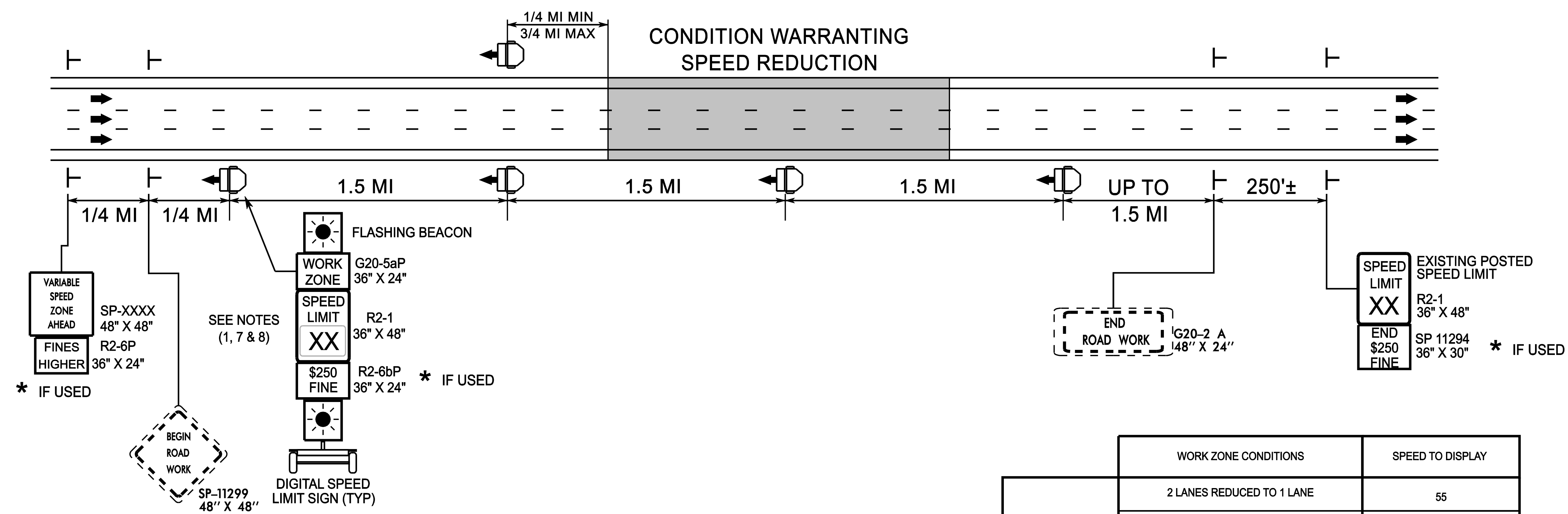
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 DATE: _____

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NOTES

- WITHIN 1/4 TO 3/4 MILE UPSTREAM OF CONDITION WARRANTING A SPEED REDUCTION, PLACE A DIGITAL SPEED LIMIT SIGN ON BOTH THE INSIDE AND OUTSIDE SHOULDERS, UNLESS DIRECTED OTHERWISE BY THE ENGINEER. AT ALL OTHER LOCATIONS DOWNSTREAM, PLACE A SINGLE DIGITAL SPEED LIMIT SIGN ON THE OUTSIDE SHOULDER.

IF SIGNS ARE NOT HIGHLY VISIBLE TO ALL MOTORISTS, SUPPLEMENTAL DIGITAL SPEED LIMIT SIGNS ARE PERMITTED TO BE INSTALLED ON THE MEDIAN SHOULDER.
- THE DIGITAL SPEED LIMIT SIGNS TAKE PRECEDENCE OVER EXISTING SPEED LIMIT SIGNS AND REMAIN UPRIGHT AND VISIBLE AT ALL TIMES. ALL EXISTING SPEED LIMIT SIGNS SHALL BE COVERED OR REMOVED FOR DURATION OF THE PROJECT.
- THE DIGITAL SPEED LIMITS SIGNS WILL BE INSTALLED (TRAILER MOUNTED OR STATIONARY MOUNTED) IN ADVANCE AND SPACED APPROXIMATELY 1.5 MILES THROUGHOUT THE ACTIVE WORK AREA, UNLESS DIRECTED OTHERWISE.
- NCDOT HAS SOLE AUTHORITY OF THE SPEED LIMITS DISPLAYED ON THE DIGITAL SPEED LIMIT SIGNS.
- THE WORK ZONE VARIABLE SPEED LIMIT AND THE \$250 SPEEDING PENALTY ARE SEPARATE ORDINANCES THAT MUST BE SIGNED BY THE STATE TRAFFIC ENGINEER TO BE VALID AND ENFORCEABLE. WITHOUT A SIGNED ORDINANCE, THE SPEED LIMIT ON A FACILITY SHALL REMAIN UNCHANGED.
- THE SPEED DISPLAYED SHALL BE THE LOWER OF THE EXISTING SPEED LIMIT OR THE SPEED IN THE WORK ZONE CONDITION CHART.
- THE SPEED LIMIT SHALL ONLY BE REDUCED IN AREAS MEETING WORK ZONE CONDITION WARRANTS. THE EXISTING SPEED LIMIT SHALL BE DISPLAYED ON ALL OTHER DIGITAL SPEED LIMIT SIGNS.
- THE BEACONS ON THE DIGITAL SPEED LIMIT SIGNS SHALL ONLY FLASH DURING TIMES THE SPEED IS REDUCED, AND REMAIN OFF AT ALL OTHER TIMES.

	WORK ZONE CONDITIONS	SPEED TO DISPLAY
LANE CLOSURES	2 LANES REDUCED TO 1 LANE	55
	3 LANES REDUCED TO 1 LANE	55
	3 LANES REDUCED TO 2 LANES	60
	4 LANES REDUCED TO 1 LANE	55
	4 LANES REDUCED TO 2 LANES	60
	4 LANES REDUCED TO 3 LANES	65
CONTINUOUS BARRIER (LENGTH OF BARRIER GREATER THAN 1 MILE)	1 OPEN LANE WITH CONTINUOUS BARRIER ON BOTH SHOULDERS	55
	1 OPEN LANE WITH CONTINUOUS BARRIER ON 1 SHOULDER	60
	3 OR 2 OPEN LANES WITH CONTINUOUS BARRIER ON BOTH SHOULDERS	60
	3 OR 2 OPEN LANES WITH CONTINUOUS BARRIER ON 1 SHOULDER	65
	4 OPEN LANES WITH BARRIER CONTINUOUS ON BOTH SHOULDERS	65
	4 OPEN LANES WITH BARRIER CONTINUOUS ON 1 SHOULDER	EXISTING
	UNEVEN LANES	60

* SEE NOTE 6

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PLAN PREPARED IN THE OFFICE OF: 8521 SIX FORKS ROAD, SUITE 400 RALEIGH, NC 27615 NC FIRM LICENSE No: F-0493	APPROVED: _____ DATE: _____	
	SEAL	

PHASING

PROJ. REFERENCE NO.	SHEET NO.
R-5751	TMP-3

NOTES:

'RSD' REFERS TO NCDOT ROADWAY STANDARD DRAWINGS.

COMPLETE ANY PROPOSED OR TEMPORARY WIDENING IN SUCH A MATTER THAT PONDING OF WATER WILL NOT OCCUR IN THE TRAVEL LANE.

ALL PROPOSED ASPHALT ROADWAY CONSTRUCTION IS UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE UNLESS OTHERWISE NOTED.

PHASE I STEP 1:

USING RSD 1101.01 (SHEETS 1 AND 3 OF 3) AND USING LANE CLOSURES, PLACE ALL ADVANCE WARNING SIGNS ALONG -L- AND -Y- LINES.

INSTALL ONE (1) CHANGEABLE MESSAGE SIGN (CMS) IN EACH DIRECTION OF -L- (US-74) AS DIRECTED BY THE ENGINEER. THESE CHANGEABLE MESSAGE SIGNS ARE TO BE UTILIZED TO INFORM & ALERT THE TRAVELING PUBLIC TO TRAFFIC CONDITIONS AND MAY BE UTILIZED FOR TRAFFIC CONTROL OPERATIONS DURING CONSTRUCTION AS DIRECTED BY THE ENGINEER.

PHASE I STEP 2:

USING RSD 1101.02 (SHEET 4 OF 14) AND RSD 1101.03 (SHEETS 1 AND 2 OF 9) CLOSE DOWN THE WB LEFT TURN LANE FROM -L- STA 39+89± TO -L- STA 46+19±. USING AN OFFSITE DETOUR (SEE DETOUR SHEET TMP-2) ALONG -L- (US 74) AND RSD 1101.02 (SHEET 4 OF 14), JACK AND BORE THE STEEL WELDED PIPES UNDER -L- (US 74) AT -L- STA 24+18±, -L- STA 24+75±, -L- STA 35+42±, -L- STA 40+90±, -L- STA 44+51±, AND -L- STA 54+50±.

PHASE I STEP 3:

WITH TRAFFIC IN EXISTING PATTERN USING RSD 1101.02 (SHEET 4 OF 14) AND RSD 1101.04 (SHEET 1 OF 1), LANE CLOSURES, AND KEEPING AWAY FROM TRAFFIC, CONSTRUCT THE INSIDE SHOULDER, WEDGING, AND TEMPORARY PAVEMENT ALONG -L- (US 74).

-L- STA 13+50± TO -L- STA 74+05±

PHASE I STEP 4:

MAINTAINING THE OFFSITE DETOUR (SEE DETOUR SHEET TMP-2) CONSTRUCT THE MEDIAN BENT FOR STRUCTURE BEHIND PORTABLE CONCRETE BARRIER AND USING TEMPORARY SHORING.

-L- STA 40+41± TO -L- STA 44+42±

PHASE II STEP 1:

USING RSD 1101.02 (SHEET 4 OF 14) PLACE TEMPORARY PAVEMENT MARKINGS AND SHIFT TRAFFIC TOWARDS THE MEDIAN ALONG US 74 (-L-). USING RSD 1101.04 (SHEET 1 OF 1) AND RSD 1101.05 (SHEET 2 OF 2) WHEN APPLICABLE, INSTALL PORTABLE CONCRETE BARRIER, CRASH CUSHIONS, AND CONSTRUCT OUTSIDE WIDENING ALONG -L-, -Y1RPA-, -Y1RPB-, -Y1RPC-, -Y1RPD-, -RAB-, -RCD-, AND -Y1B- (NC 72/NC 130) EXCLUDING THE STRUCTURE.

-L- STA 16+00± TO -L- STA 74+36±	-Y1RPD- STA 14+84± TO -Y1RPD- STA 22+30±
-Y1RPA- STA 14+89± TO -Y1RPA- STA 26+68±	-Y1B- STA 35+59± TO -Y1B- STA 37+06±
-Y1RPB- STA 14+84± TO -Y1RPB- STA 21+00±	-Y1B- STA 38+94± TO -Y1B- STA 40+40±
-Y1RPB- STA 21+50± TO -Y1RPB- STA 22+58±	-RAB- STA 10+00± TO -RAB- STA 14+71±
-Y1RPC- STA 14+85± TO -Y1RPC- STA 27+26±	-RCD- STA 10+00± TO -RCD- STA 14+71±

ONCE CONSTRUCTION OF -Y1RPA-, -Y1RPC-, -Y1RPD-, -RAB-, -RCD- AND WIDENING ALONG -L- IS COMPLETE, INSTALL ROUNDABOUT SIGNING ACCORDING TO THE PROPOSED SIGNING PLANS.

USING RSD 1101.02 (SHEET 1 OF 14), LANE CLOSURES, WEDGING, AND KEEPING AWAY FROM TRAFFIC CONSTRUCT -Y1A- (NC 130), -Y1C (NC 72), AND -SRA- UP TO THE EDGE AND ELEVATION OF EXISTING ROADWAY WHILE MAINTAINING ACCESS.

-Y1A- STA 16+00± TO -Y1A- STA 34+09
 -Y1C- STA 41+90± TO -Y1C- STA 53+50±
 -SRA- STA 10+00± TO -SRA- STA 12+13±

INTERMEDIATE CONTRACT TIME (ICT): COMPLETE THE WORK REQUIRED IN PHASE II STEP 2 FOR THE -SRA- TIE-IN IN SEVEN (7) CONSECUTIVE DAYS.

PHASE II STEP 2:

USING ROAD CLOSURES AND AN OFF-SITE DETOUR WITH RSD 1101.03 (SHEETS 1 AND 2 OF 9) ALONG US 74 AND SOUTH CREEK RD., CONSTRUCT -SRA- (SEE DETOUR SHEET TMP-2A). USING RSD 1101.02 (SHEET 1 OF 14) AND LANE CLOSURES COMPLETE THE -Y1A-, -Y1C-, AND -SRB- CONSTRUCTION AND TIE-INS.

PRIOR TO DETOURING TRAFFIC ON SOUTH CREEK RD., RESURFACE SOUTH CREEK RD. BETWEEN NC 130 AND US 74 WITH 1.5" S9.5B, 4" PAINTED MARKINGS, AND RECONSTRUCT THE SHOULDER WITH ASB USING RSD 1101.02 (SHEET 1 OF 14). REPLACE ALL PAVEMENT MARKINGS IN-KIND.

-SRA- STA 11+31± TO -SRA- STA 13+50±	-Y1C- STA 51+69± TO -Y1C- STA 51+90±
-Y1A- STA 8+00± TO -Y1A- STA 20+93±	-Y1C- STA 52+20± TO -Y1C- STA 59+00±
-Y1C- STA 49+69± TO -Y1C- STA 50+12±	-SRB- STA 11+80± TO -SRB- STA 14+00±

INTERMEDIATE CONTRACT TIME (ICT): COMPLETE THE WORK REQUIRED IN PHASE II STEP 3 IN FORTY-FIVE (45) CONSECUTIVE DAYS.

PHASE II STEP 3:

USING AN OFF-SITE DETOUR ALONG NC 72 AND SR 2225 (NORTH CREEK RD) WITH USING RSD 1101.03 (SHEETS 1 OF 9) AND ROAD CLOSURES, COMPLETE CONSTRUCTION OF -Y1RPB- (SEE DETOUR SHEET TMP-2B).

-Y1RPB- STA 21+17± TO -Y1RPB- STA 21+43±

PHASE II STEP 4:

USING RSD 1101.03 (SHEET 7 OF 9), ROAD CLOSURES ALONG -L- (US 74), AND ROAD CLOSURE RESTRICTIONS PLACE -L- NB TRAFFIC ON A DETOUR ALONG -Y1RPC-, -RCD-, AND -Y1RPD- AND PLACE -L- SB TRAFFIC ON A DETOUR ALONG -Y1RPB-, -RAB-, AND -Y1RPA-. PLACE THE GIRDERS ALONG -Y1B- (NC 72) (SEE DETOUR SHEET TMP-2C). AWAY FROM TRAFFIC, COMPLETE BRIDGE CONSTRUCTION.

-Y1B- STA 37+06± TO -Y1B- STA 38+94±

PHASE III:

USING RSD 1101.02 (SHEET 4 OF 14) PLACE TEMPORARY PAVEMENT MARKINGS AND SHIFT TRAFFIC TOWARDS THE OUTSIDE SHOULDER ALONG US 74 (-L-). COMPLETE MEDIAN CONSTRUCTION AND REMOVE TEMPORARY PAVEMENT.

-L- STA 13+50± TO -L- STA 74+36±

PHASE IV:

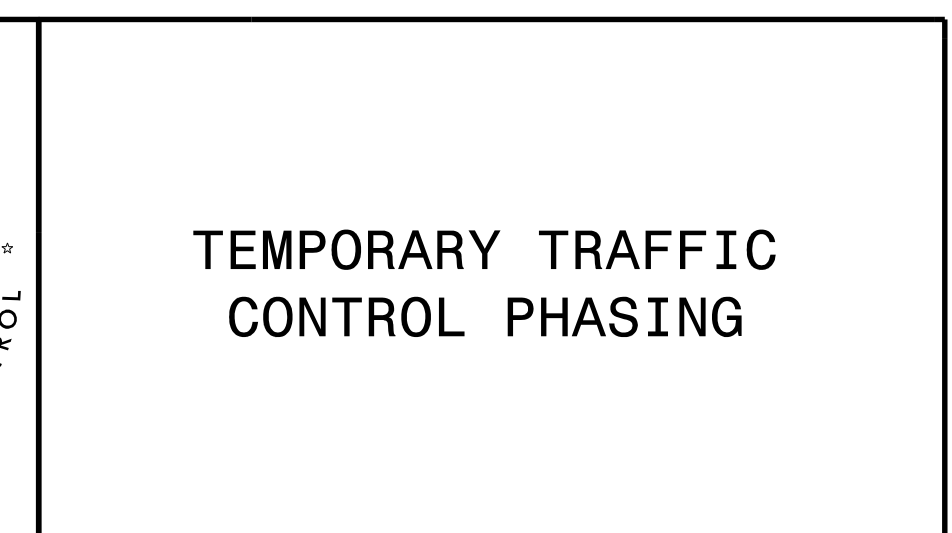
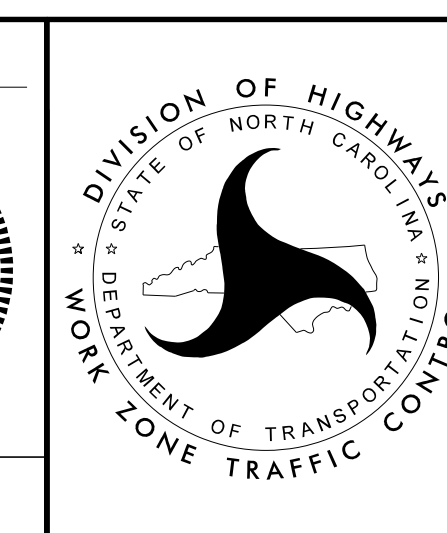
USING RSD 1101.02 (SHEETS 1 AND 4 OF 14) PLACE THE FINAL LAYER OF SURFACE COURSE, FINAL MARKINGS AND MARKERS ON ALL ROADWAYS WITHIN THE PROJECT LIMITS AND PLACE ALL TRAFFIC IN THE FINAL TRAFFIC PATTERN. REMOVE ALL TRAFFIC CONTROL DEVICES FROM THE PROJECT LIMITS.

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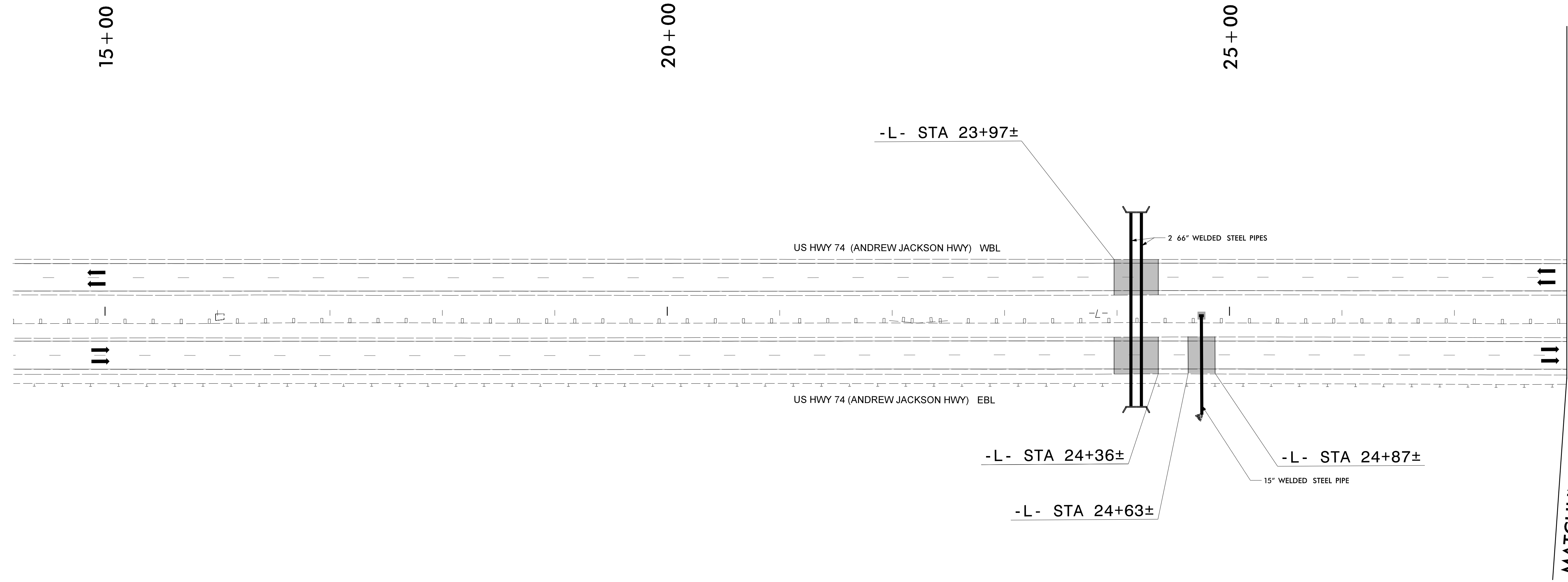


TEMPORARY TRAFFIC CONTROL PHASING



PROJ. REFERENCE NO.	SHEET NO.
R-5751	TMP-4

NAD 83/NA 2011



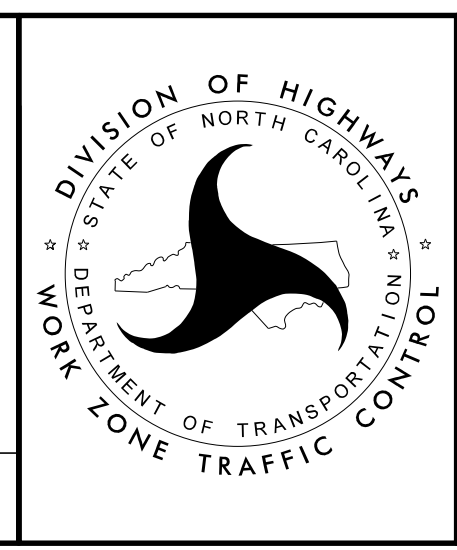
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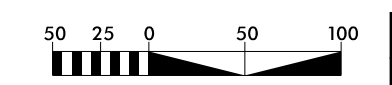
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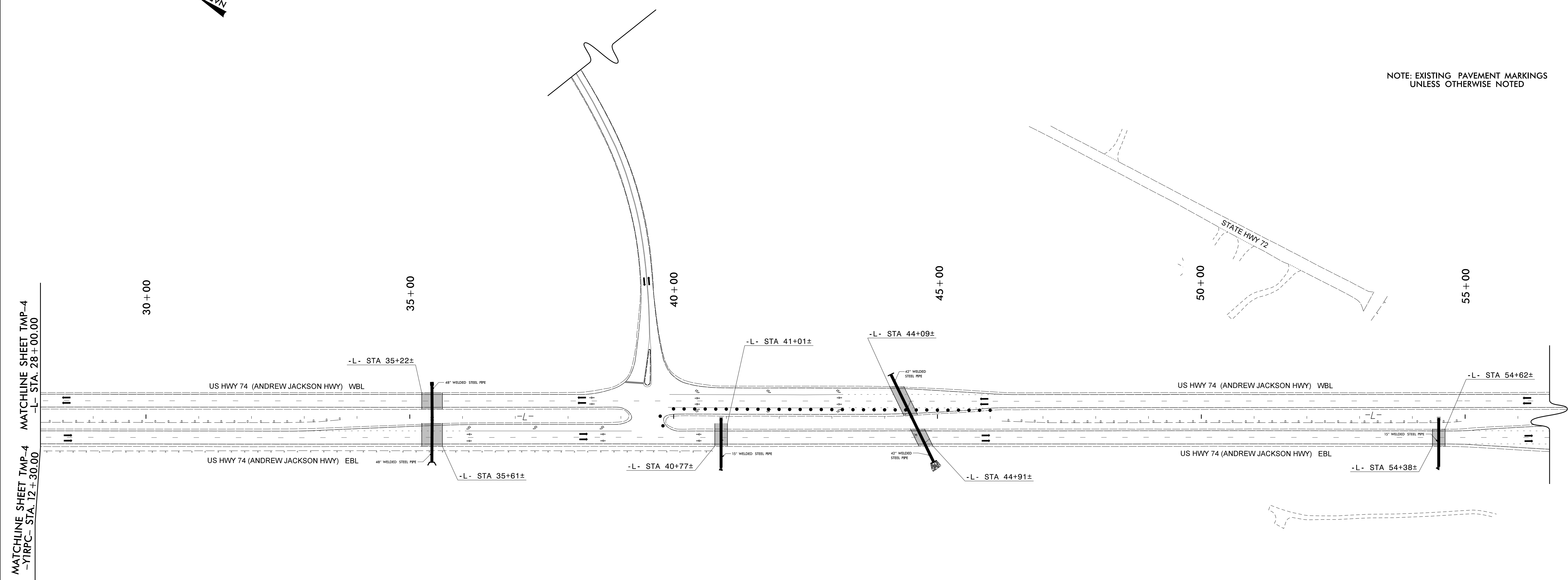
**PHASE I STEP 2
DETAIL**



PROJ. REFERENCE NO.	SHEET NO.
R-5751	TMP-5

NAD 83/NA 2011

NOTE: EXISTING PAVEMENT MARKINGS UNLESS OTHERWISE NOTED



MATCHLINE SHEET TMP-4
-YIRPC- STA. 12+30.00

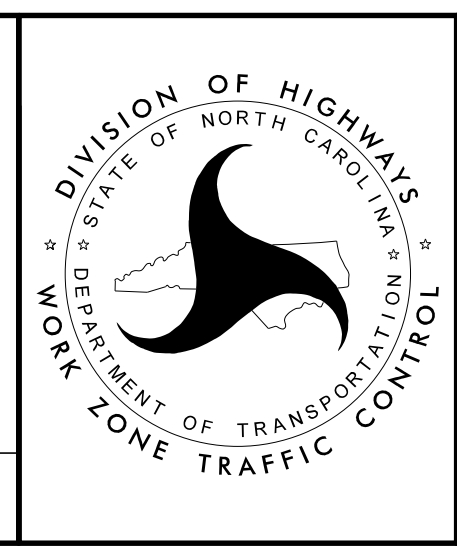
MATCHLINE SHEET TMP-4
-L- STA. 28+00.00

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**PHASE I STEP 2
DETAIL**



PROJ. REFERENCE NO.	SHEET NO.
R-5751	TMP-6

NAD 83/NA 2011

10+00

-L- STA 11+58±
TEMPORARY PAVEMENT

-L- STA 13+50±
TEMPORARY PAVEMENT

US HWY 74 (ANDREW JACKSON HWY) WBL

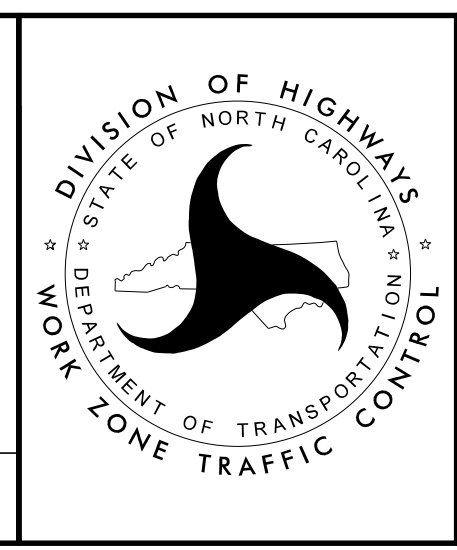
US HWY 74 (ANDREW JACKSON HWY) EBL

MATCHLINE SHEET TMP-7 -L- STA. 14+00.00

RS&H
8521 SIX FORKS ROAD, SUITE 400
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NC FIRM LICENSE No: F-0493

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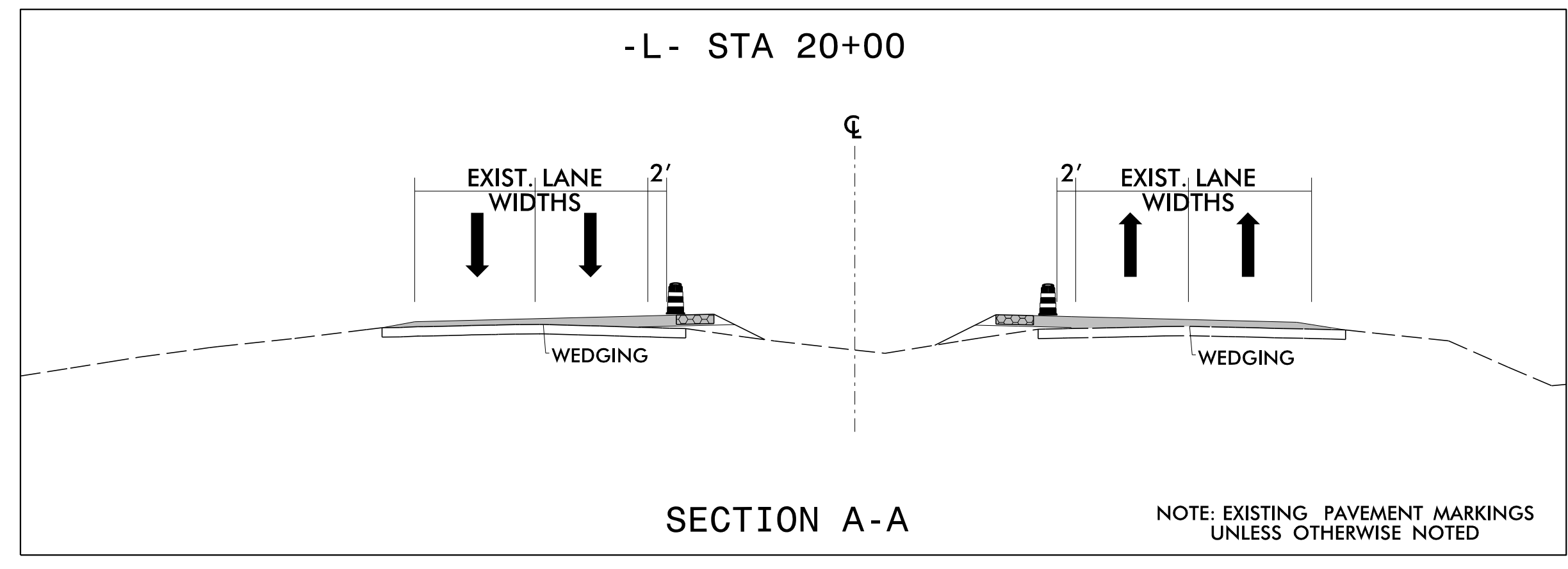
PHASE I STEP 3
DETAIL

10/6/2022
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User: BondJ



PROJ. REFERENCE NO.	SHEET NO.
R-5751	TMP-7

NAD 83/NA 2011

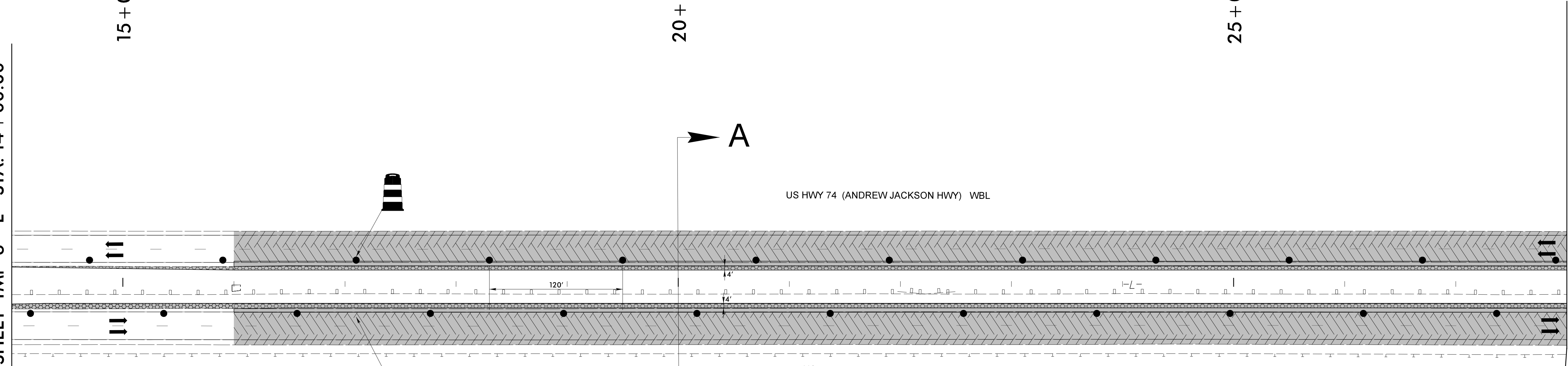


MATCHLINE SHEET TMP-6 -L- STA. 14+00.00

15+00

20+00

25+00



US HWY 74 (ANDREW JACKSON HWY) WBL

US HWY 74 (ANDREW JACKSON HWY) EBL

MATCHLINE SHEET TMP-8
-L- STA. 28+00.00

MATCHLINE SHEET TMP-8
-YIRPC- STA. 12+30.00

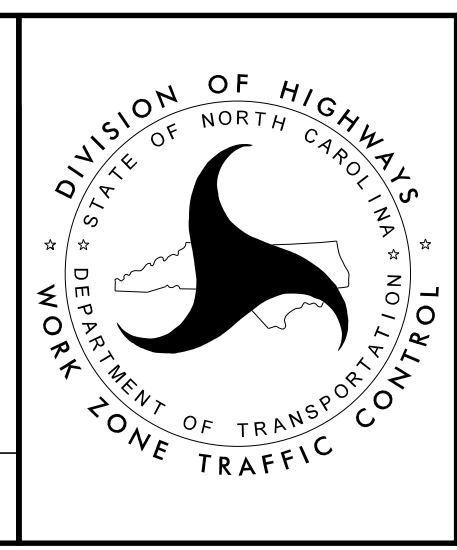
10/6/2022
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User:Bondu



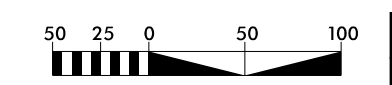
APPROVED: _____

DATE: _____

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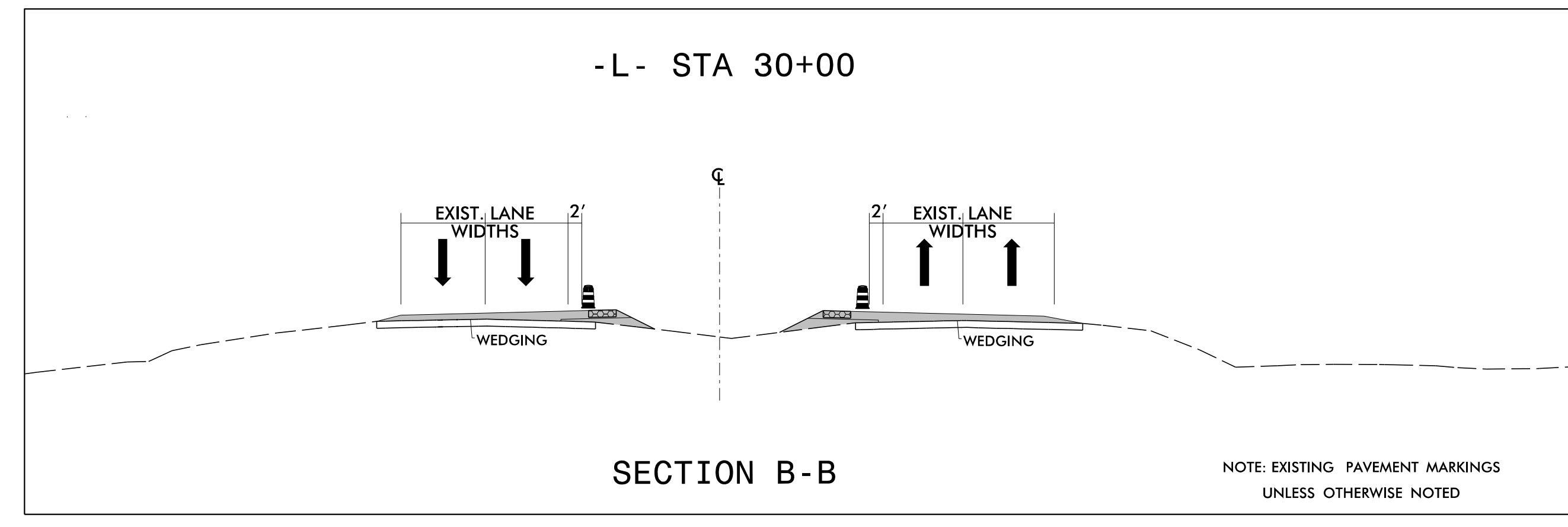
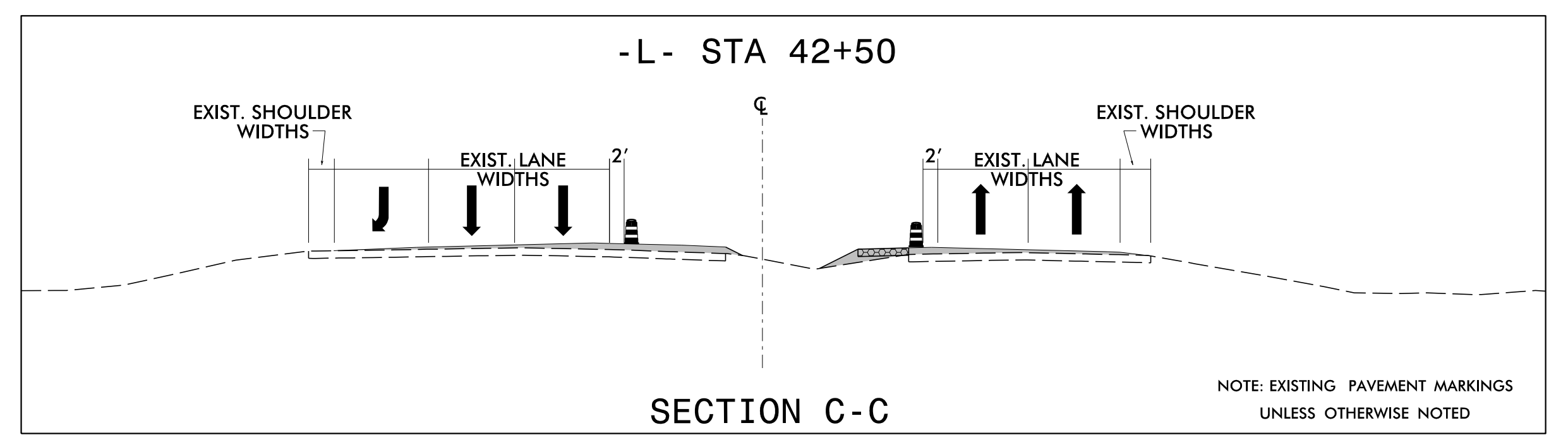
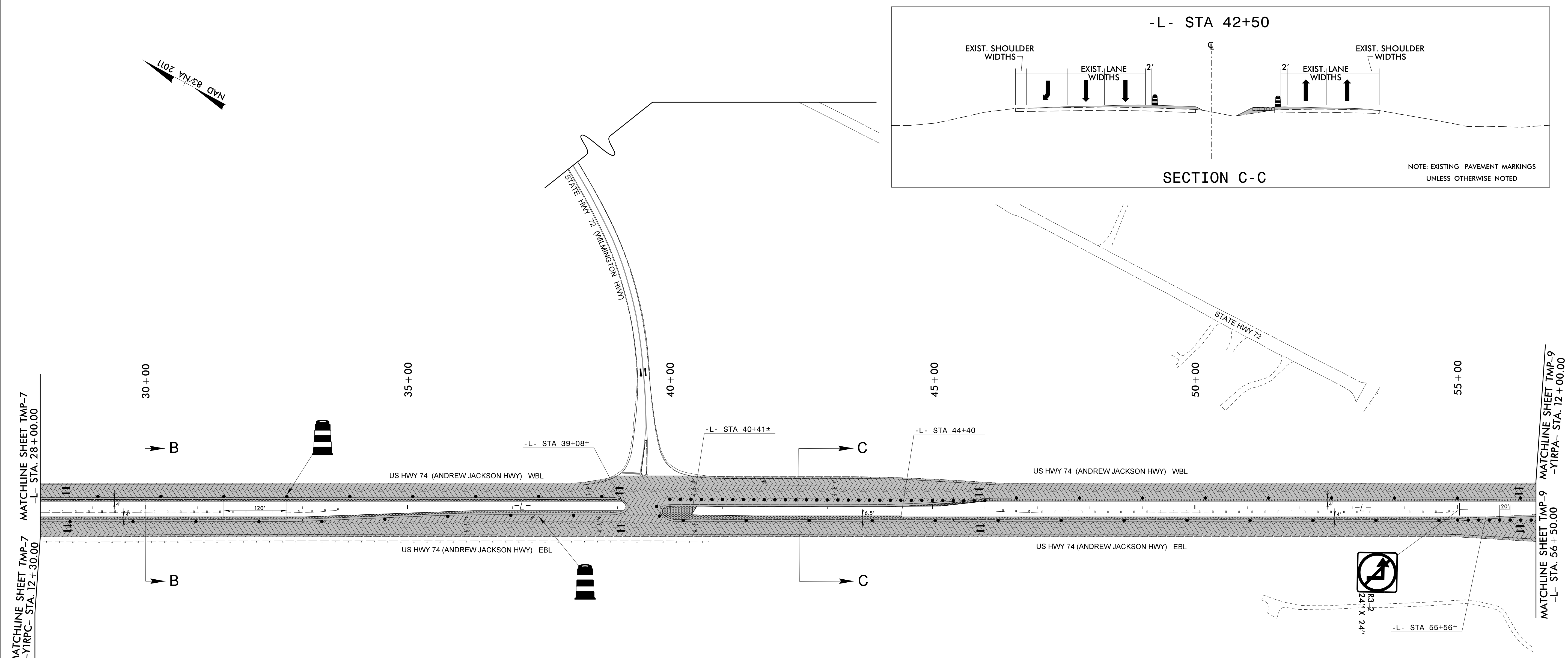


**PHASE I STEP 3
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PROJ. REFERENCE NO.	SHEET NO.
R-5751	TMP-8

NAD 83/NA 2011



MATCHLINE SHEET TMP-7
-YIRPC- STA. 12+30.00

MATCHLINE SHEET TMP-9
-YIRPA- STA. 12+00.00



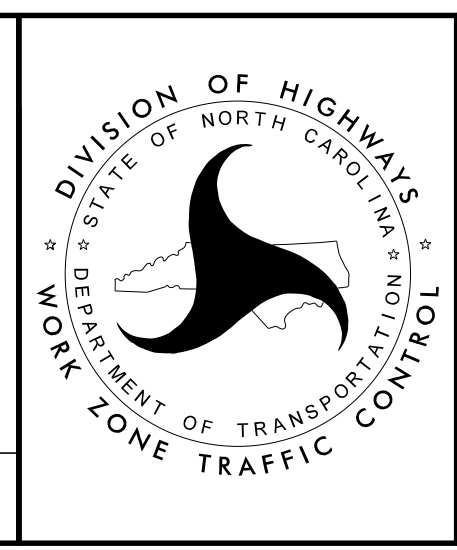
R3-2
24" X 24"

10/27/2022
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User:Bondu

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**PHASE I STEP 3
DETAIL**

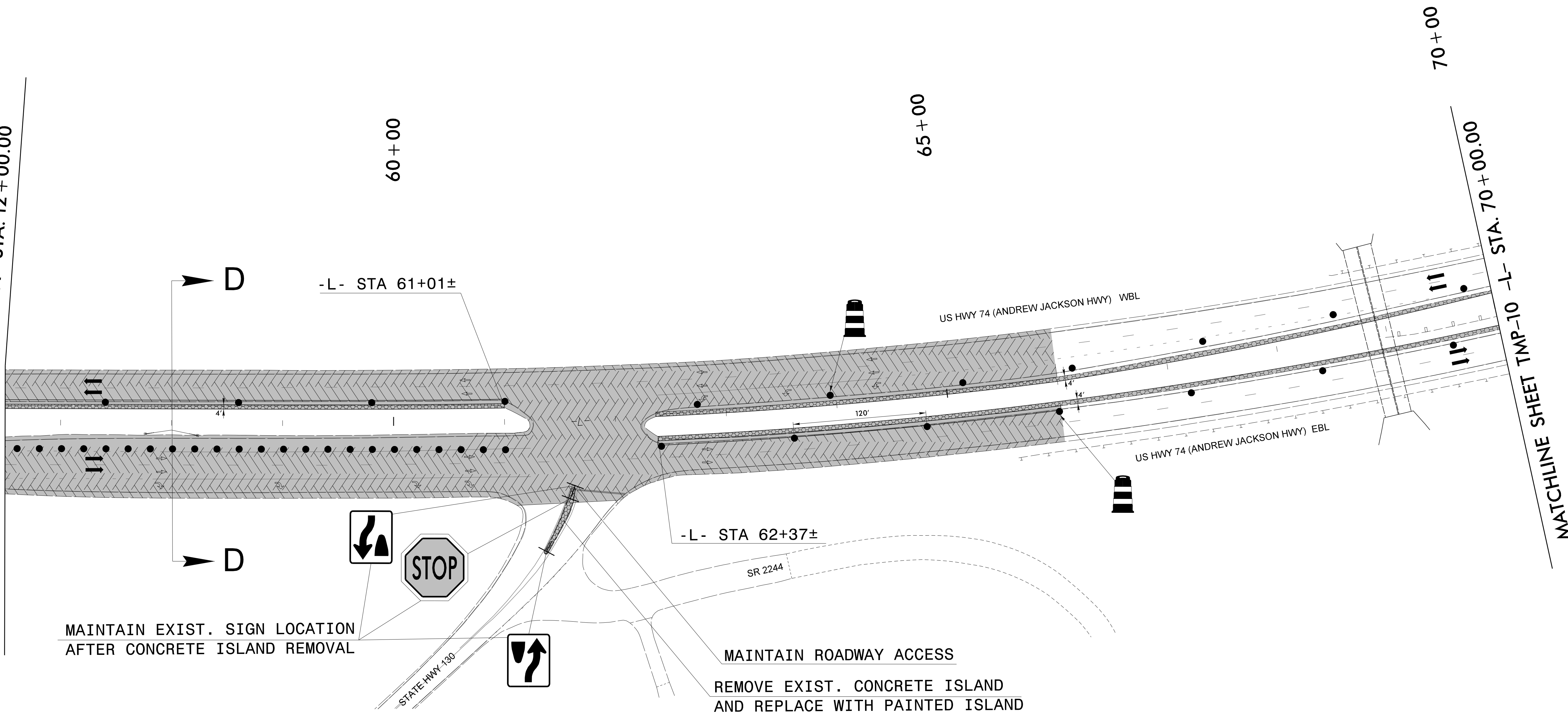


PROJ. REFERENCE NO. R-5751	SHEET NO. TMP-9
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MAD 83/NA 2011

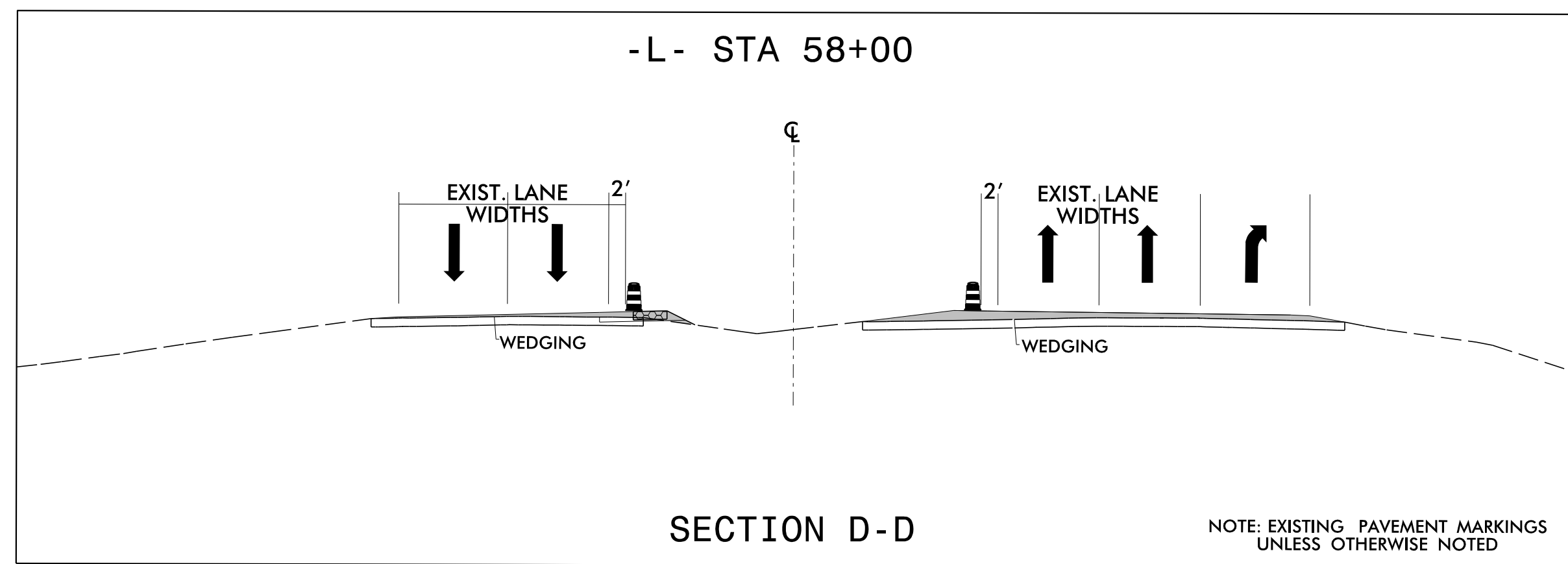
MATCHLINE SHEET TMP-8
-L- STA. 56 + 50.00

MATCHLINE SHEET TMP-8
-YIRPA- STA. 12 + 00.00



MAINTAIN EXIST. SIGN LOCATION
AFTER CONCRETE ISLAND REMOVAL

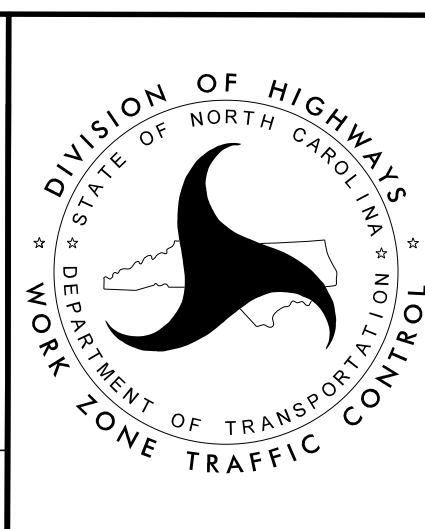
MAINTAIN ROADWAY ACCESS
REMOVE EXIST. CONCRETE ISLAND
AND REPLACE WITH PAINTED ISLAND



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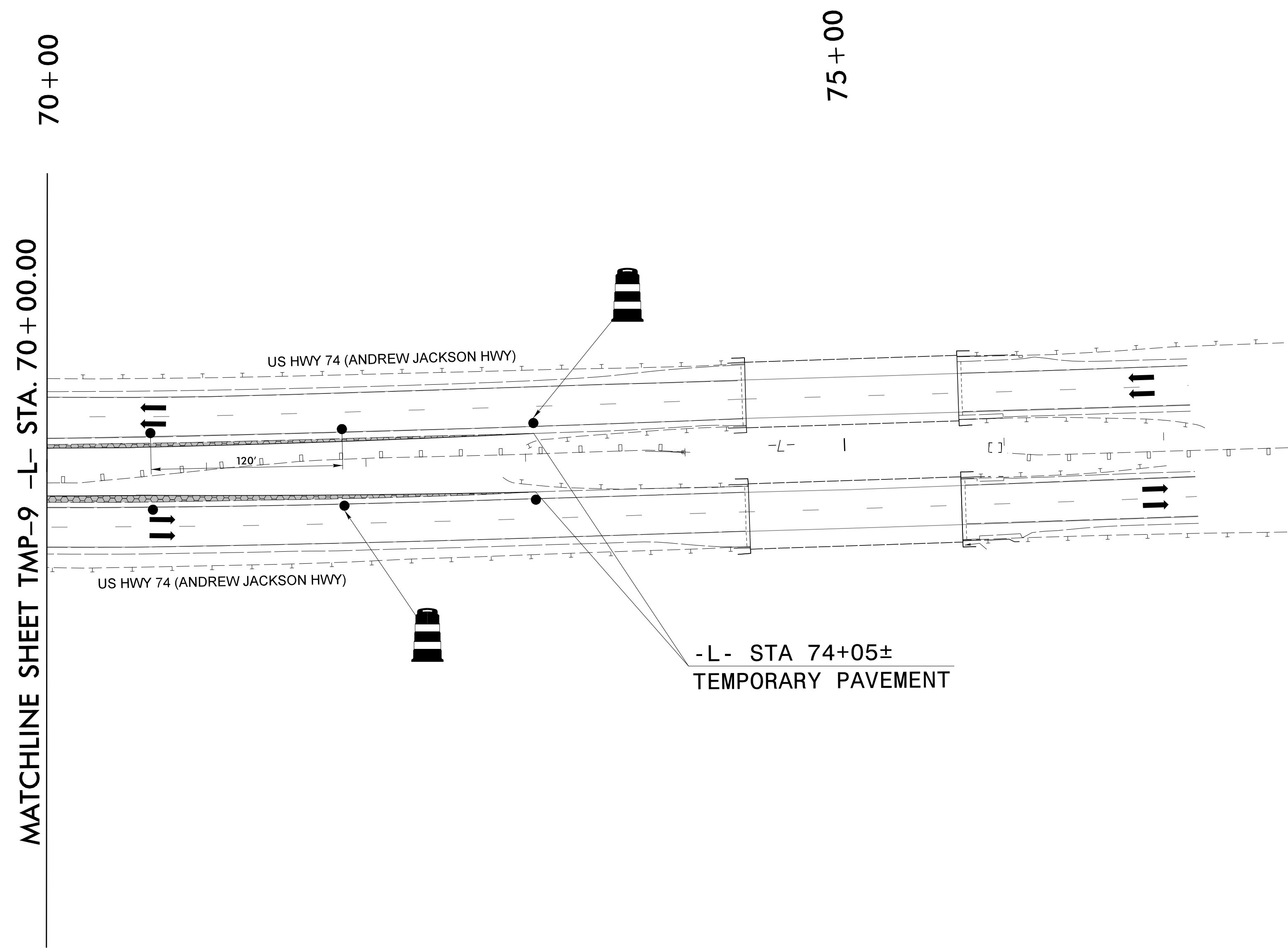
PHASE I STEP 3
DETAIL

10/27/2022
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User:JBond



PROJ. REFERENCE NO.	SHEET NO.
R-5751	TMP-10

NAD 83/NA 2011



MATCHLINE SHEET TMP-9 -L- STA. 70+00.00

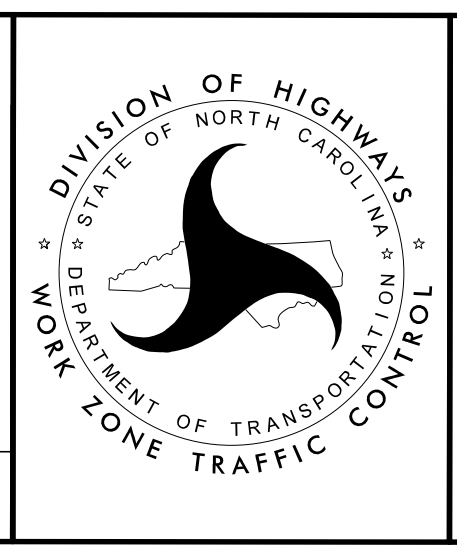
-L- STA 74+05±
TEMPORARY PAVEMENT

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RS&H
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DATE: _____

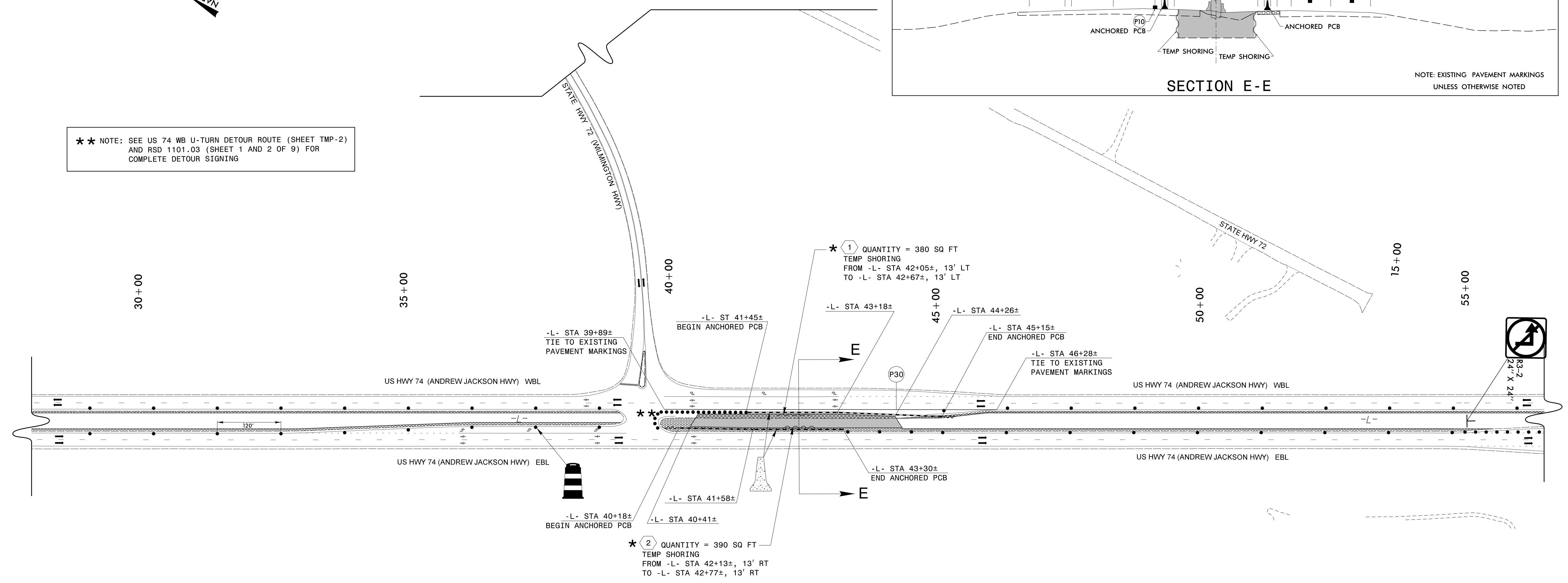
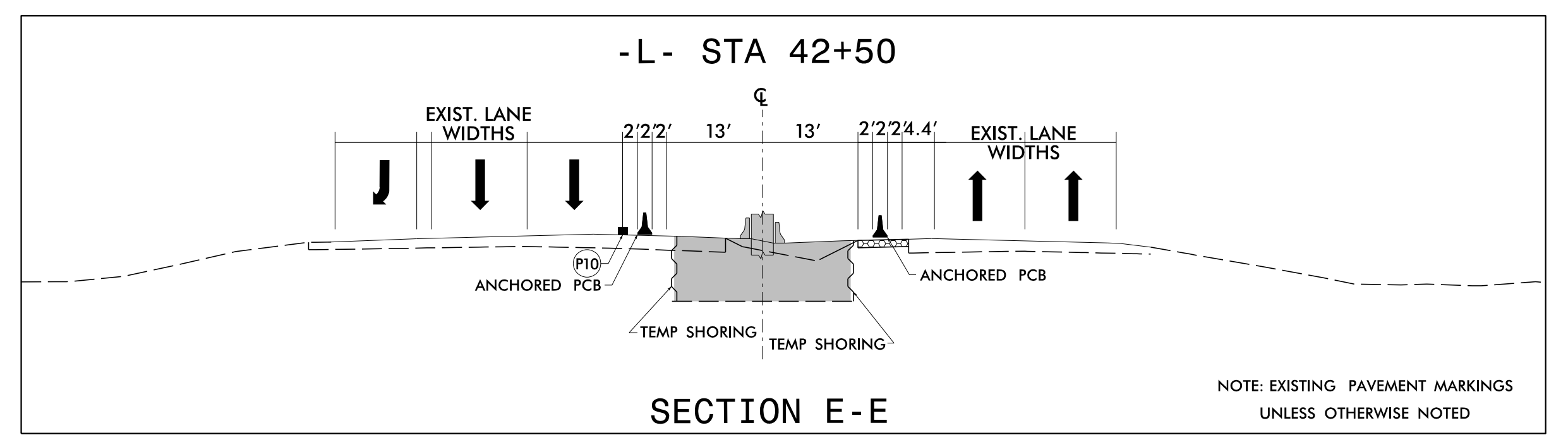
**DOCUMENT NOT CONSIDERED FINAL
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**PHASE I STEP 3
DETAIL**

NAD 83/NA 2011

** NOTE: SEE US 74 WB U-TURN DETOUR ROUTE (SHEET TMP-2) AND RSD 1101.03 (SHEET 1 AND 2 OF 9) FOR COMPLETE DETOUR SIGNING

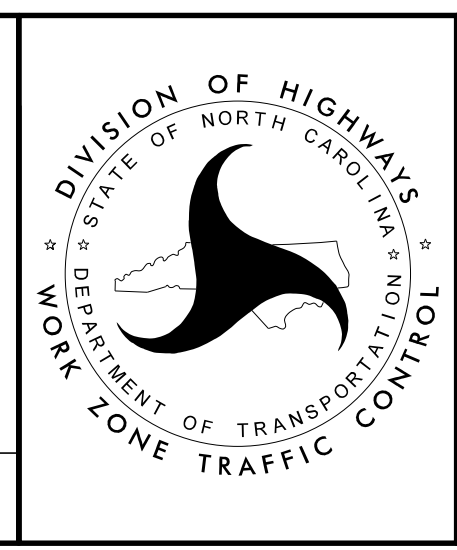


* NOTE: THE LOCATION OF THE TEMPORARY SHORING IS APPROXIMATE AND WILL BE FIELD ADJUSTED

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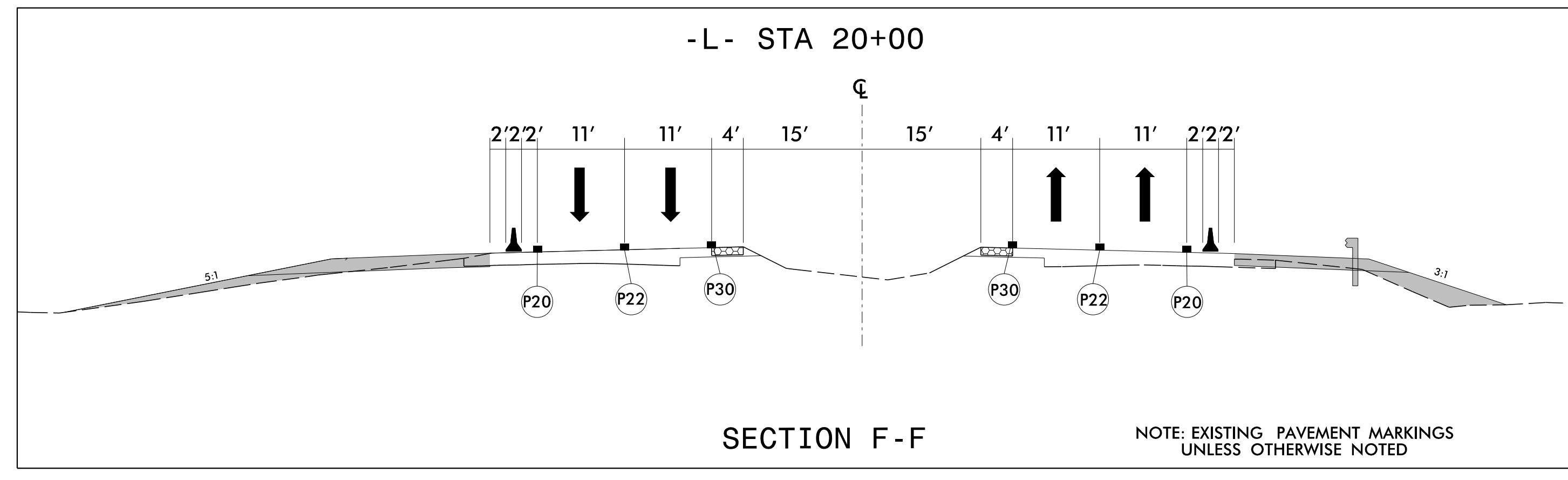
PHASE I STEP 4 DETAIL

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 User: BondJ



PROJ. REFERENCE NO. R-5751	SHEET NO. TMP-12
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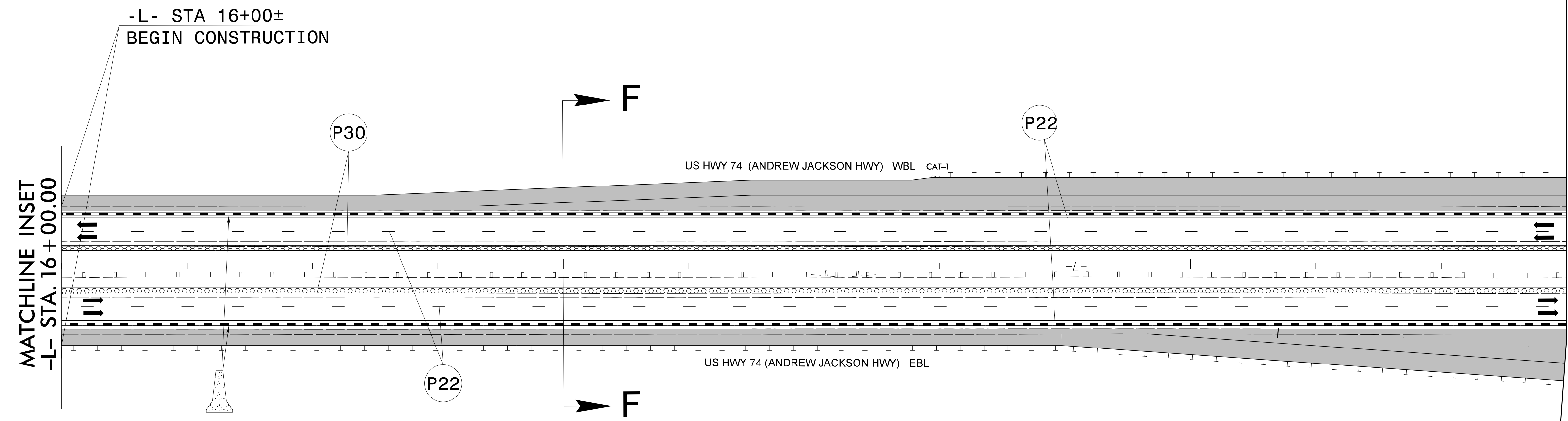
NAD 83/NA 2011



15+00

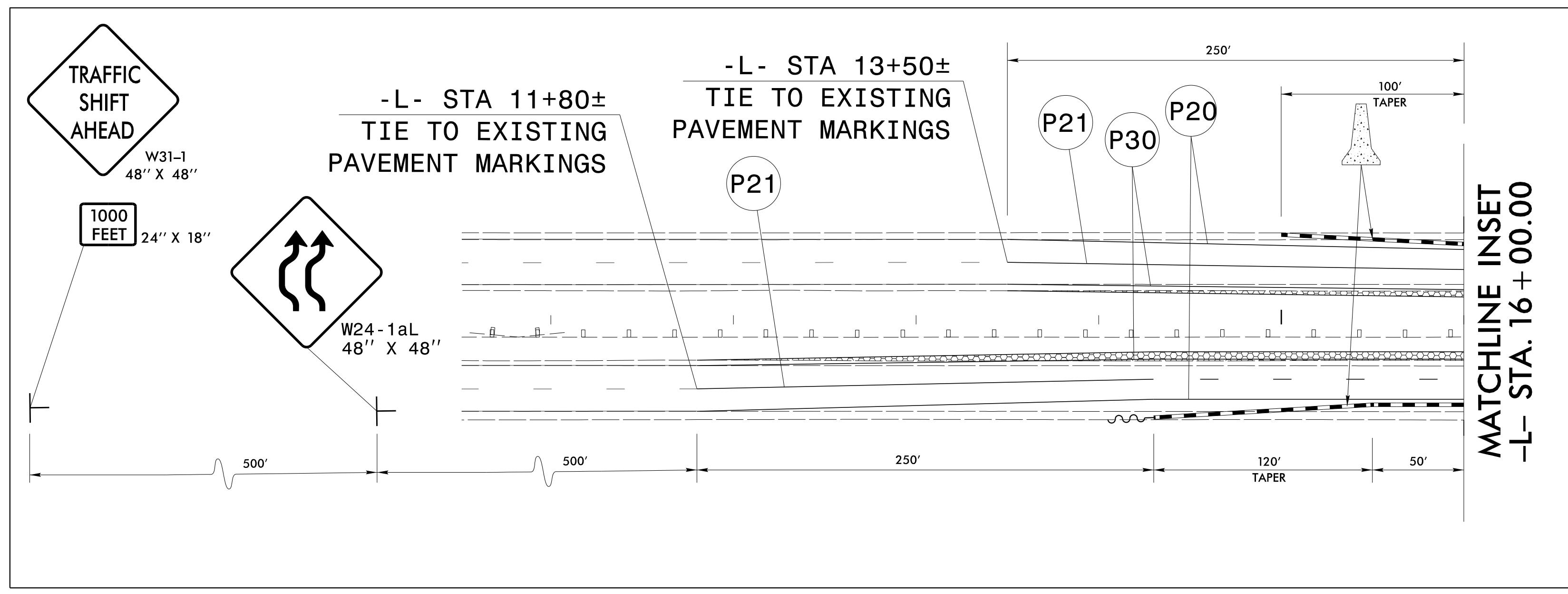
20+00

25+00



MATCHLINE SHEET TMP-13
-L- STA. 28 + 00.00

MATCHLINE SHEET TMP-13
-YR/PC- STA. 12 + 30.00

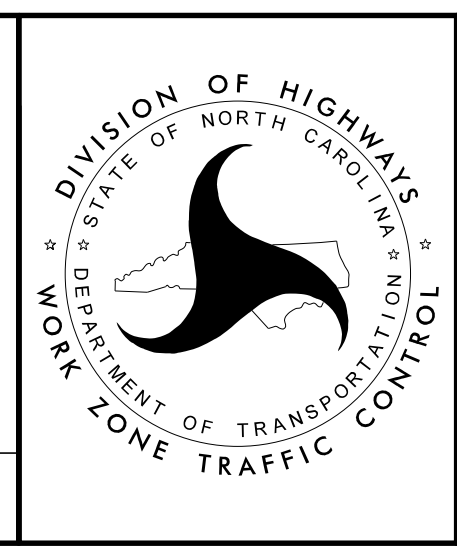


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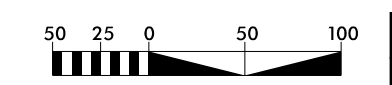
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APPROVED: _____
DATE: _____

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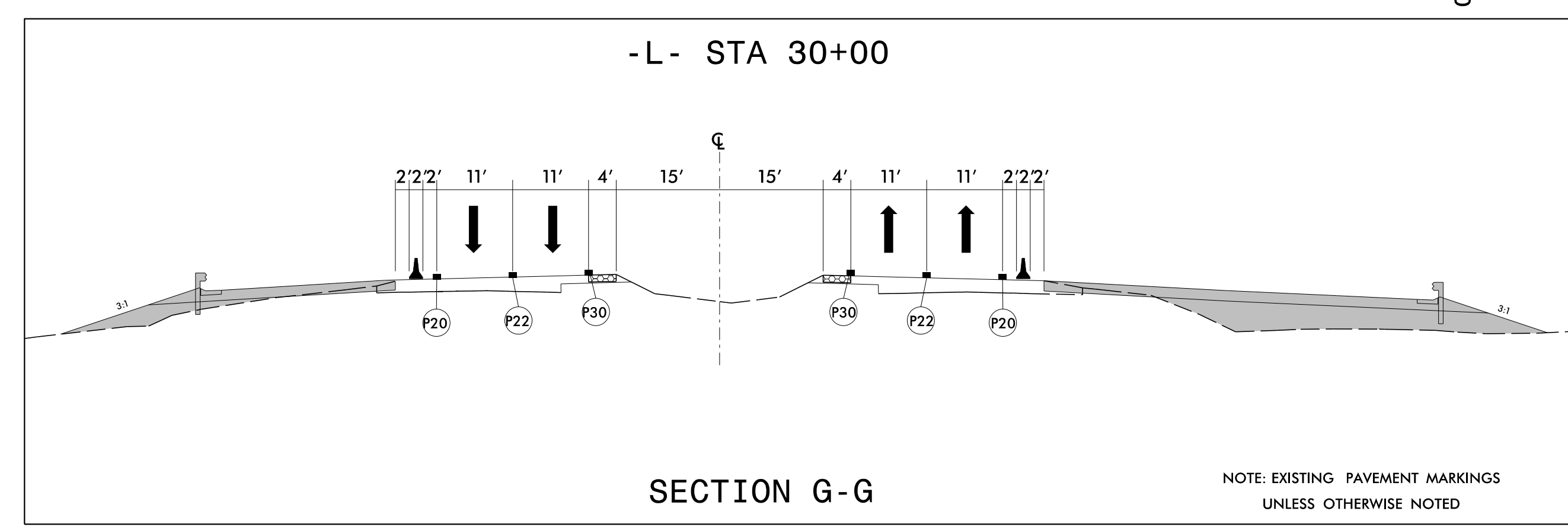
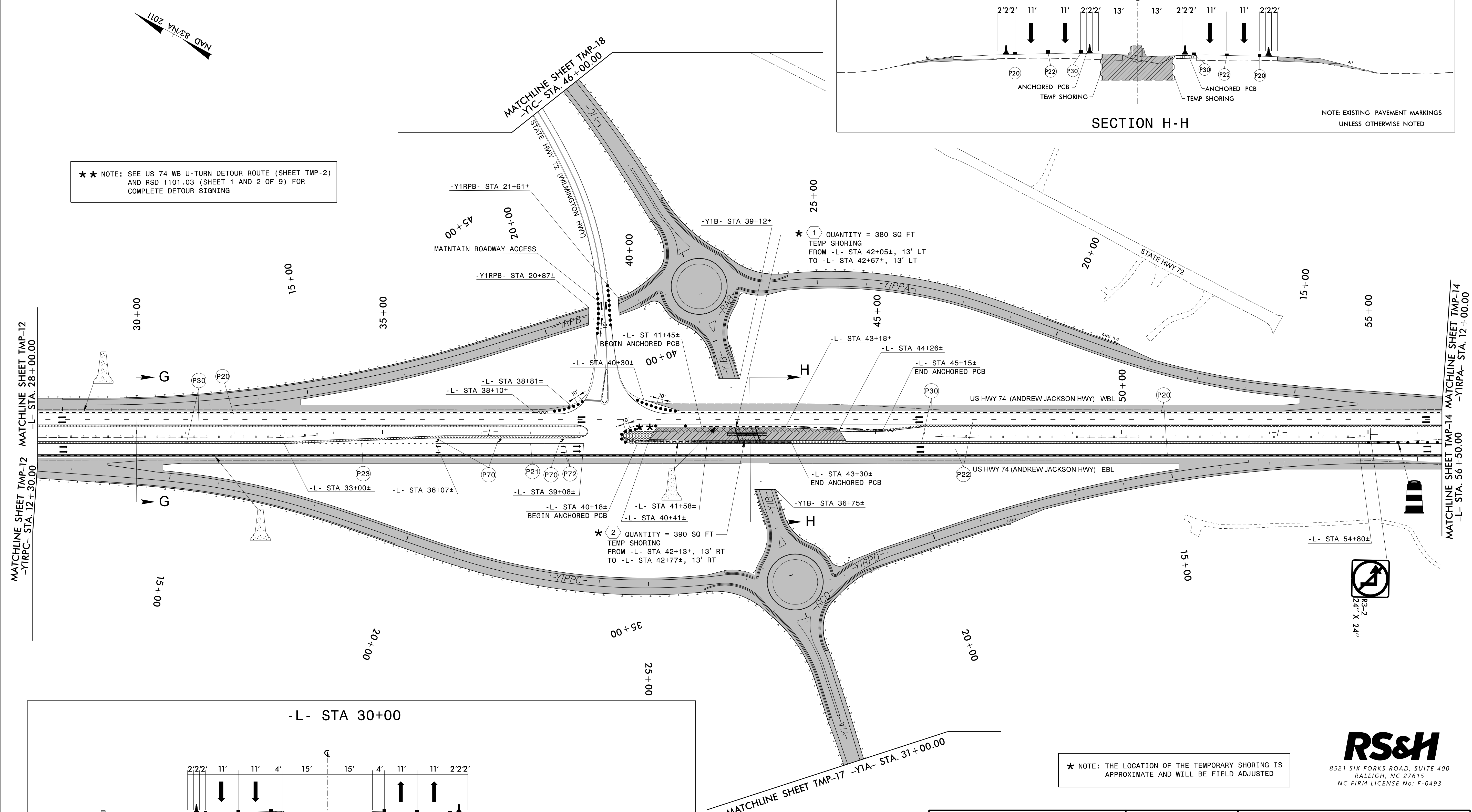
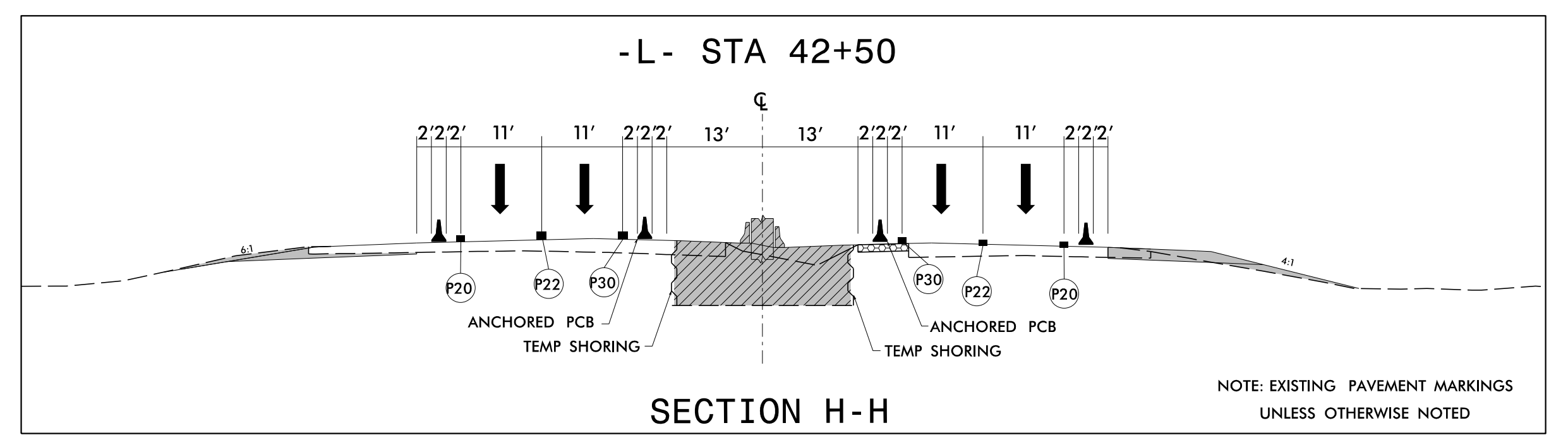


PHASE II STEP 1
DETAIL



PROJ. REFERENCE NO.	SHEET NO.
R-5751	TMP-13

** NOTE: SEE US 74 WB U-TURN DETOUR ROUTE (SHEET TMP-2) AND RSD 1101.03 (SHEET 1 AND 2 OF 9) FOR COMPLETE DETOUR SIGNING



* 1 QUANTITY = 380 SQ FT
TEMP SHORING
FROM -L- STA 42+05±, 13' LT
TO -L- STA 42+67±, 13' LT

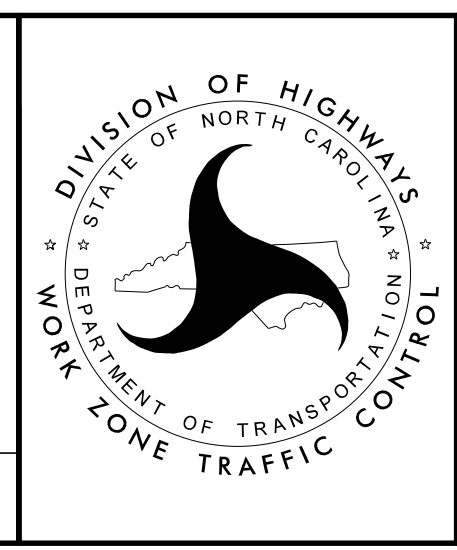
* 2 QUANTITY = 390 SQ FT
TEMP SHORING
FROM -L- STA 42+13±, 13' RT
TO -L- STA 42+77±, 13' RT

* NOTE: THE LOCATION OF THE TEMPORARY SHORING IS APPROXIMATE AND WILL BE FIELD ADJUSTED

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PHASE II STEP 1
DETAIL

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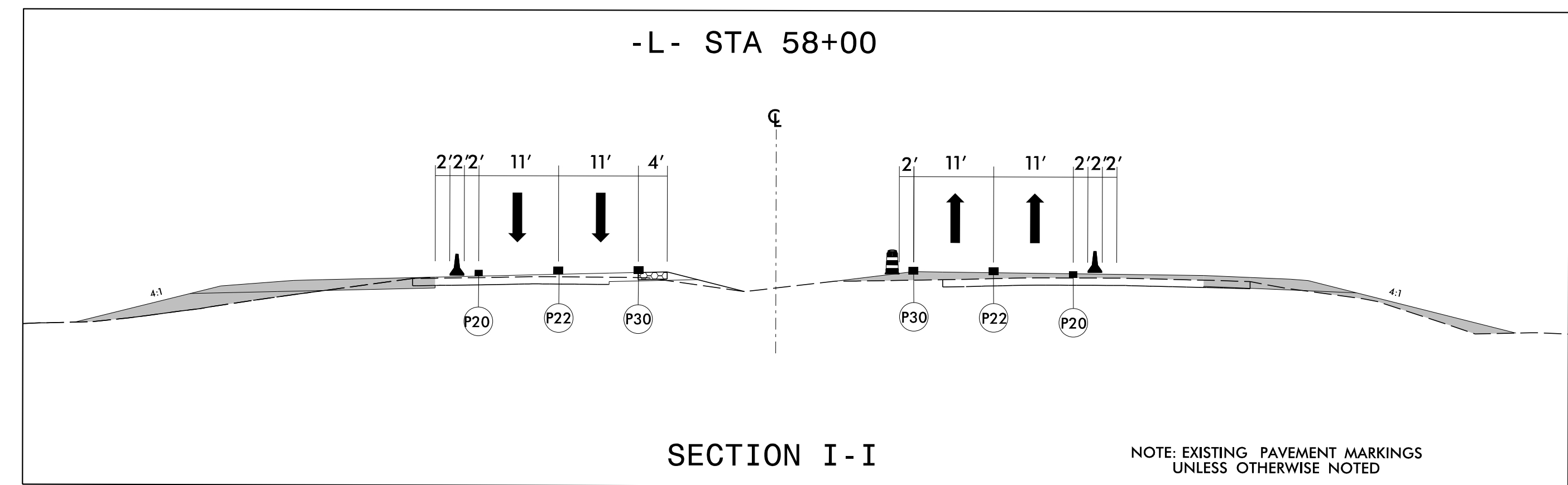
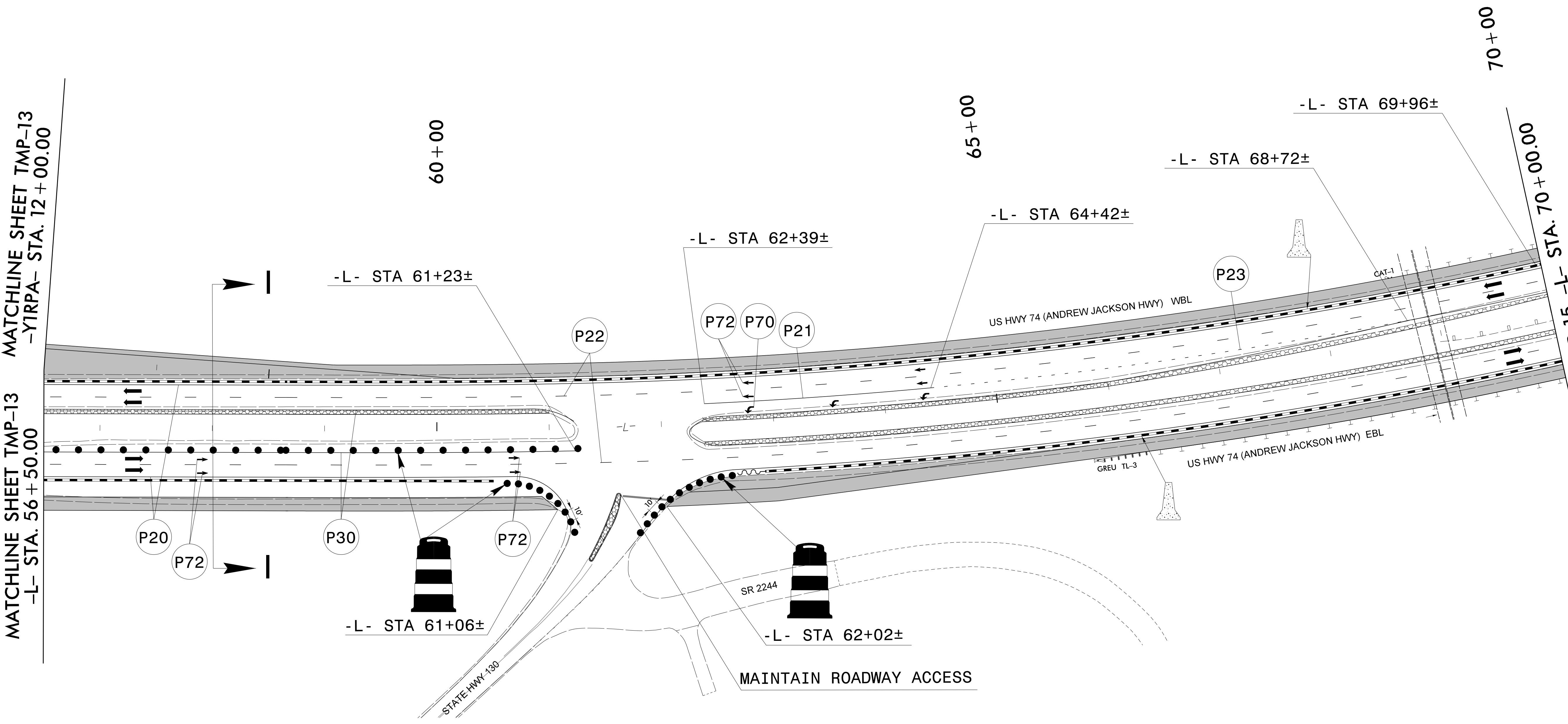
PROJ. REFERENCE NO. R-5751	SHEET NO. TMP-14
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NAD 83/NA 2011

MATCHLINE SHEET TMP-13
-L- STA. 56+50.00

MATCHLINE SHEET TMP-13
-YIRPA- STA. 12+00.00

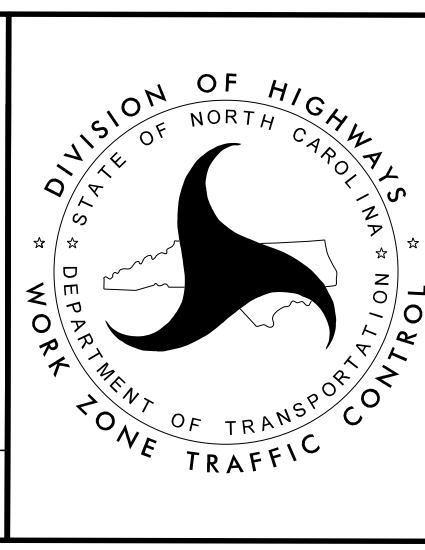
MATCHLINE SHEET TMP-15
-L- STA. 70+00.00



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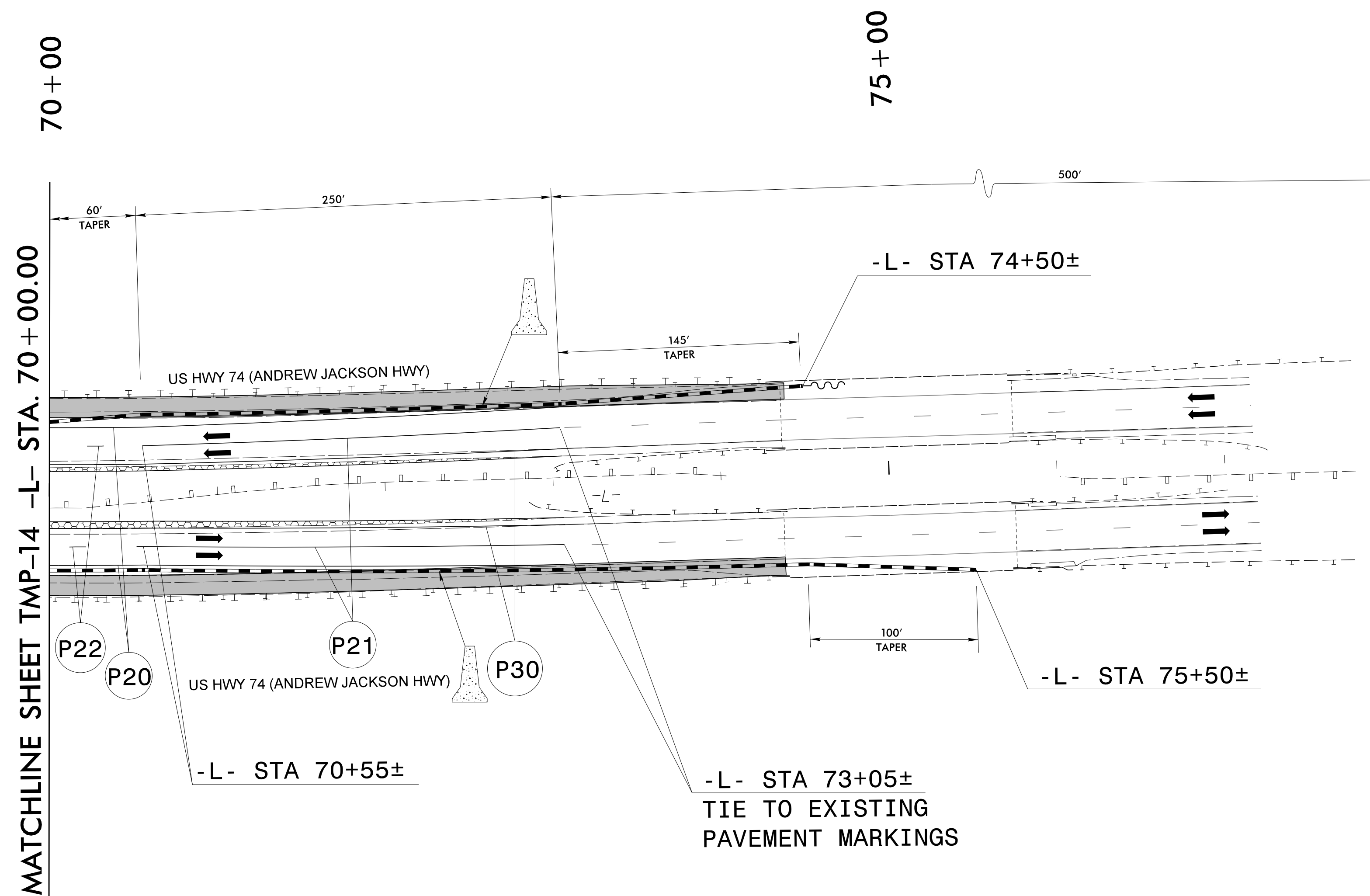
**PHASE II STEP 1
DETAIL**

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User:JBondJ

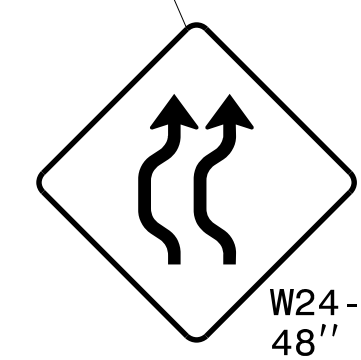


PROJ. REFERENCE NO.	SHEET NO.
R-5751	TMP-15

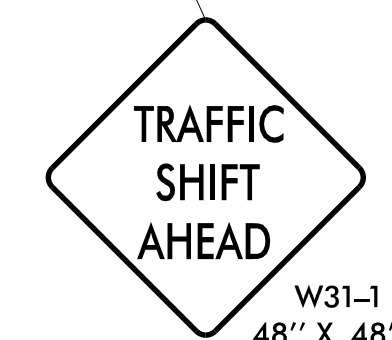
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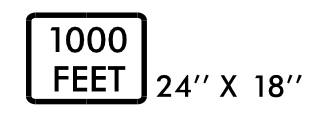
MATCHLINE SHEET TMP-14 -L- STA. 70+00.00



W24-1aL
48'' X 48''



W31-1
48'' X 48''

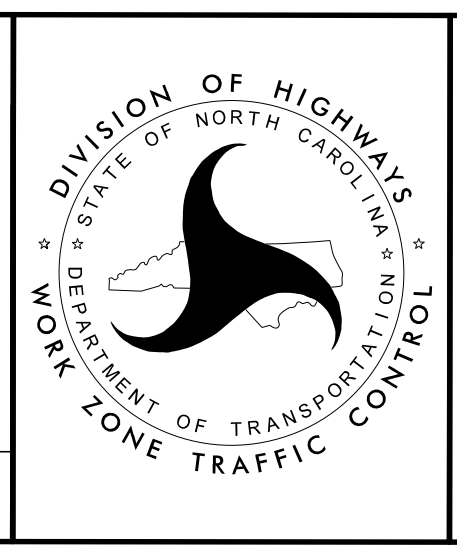


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DATE: _____

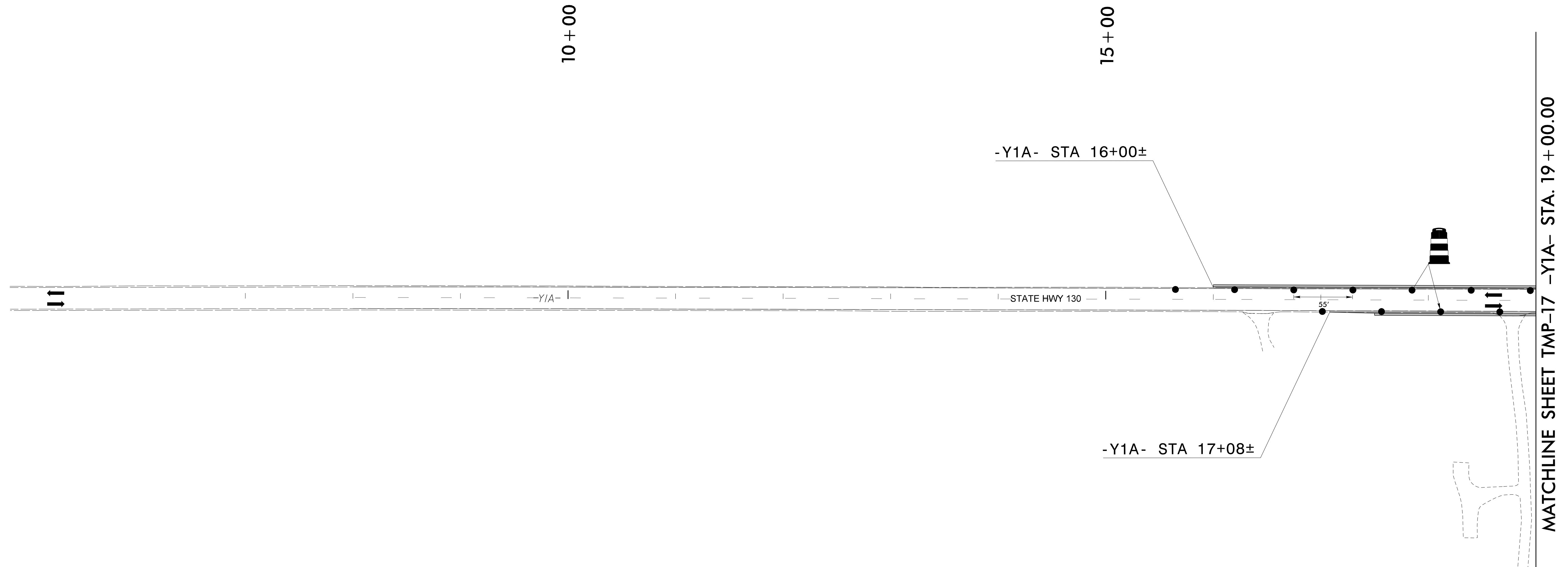
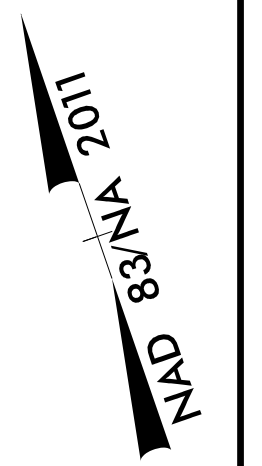
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**PHASE II STEP 1
DETAIL**



PROJ. REFERENCE NO.	SHEET NO.
R-5751	TMP-16



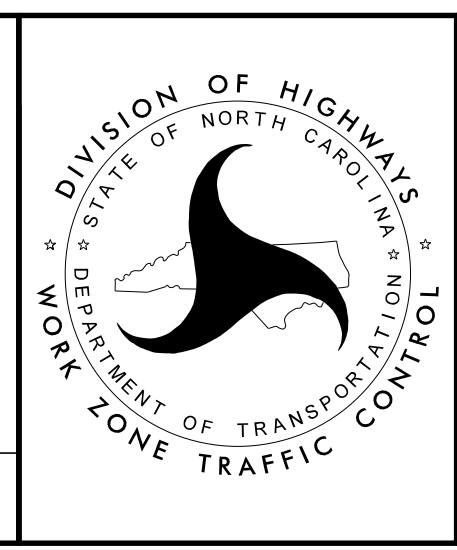
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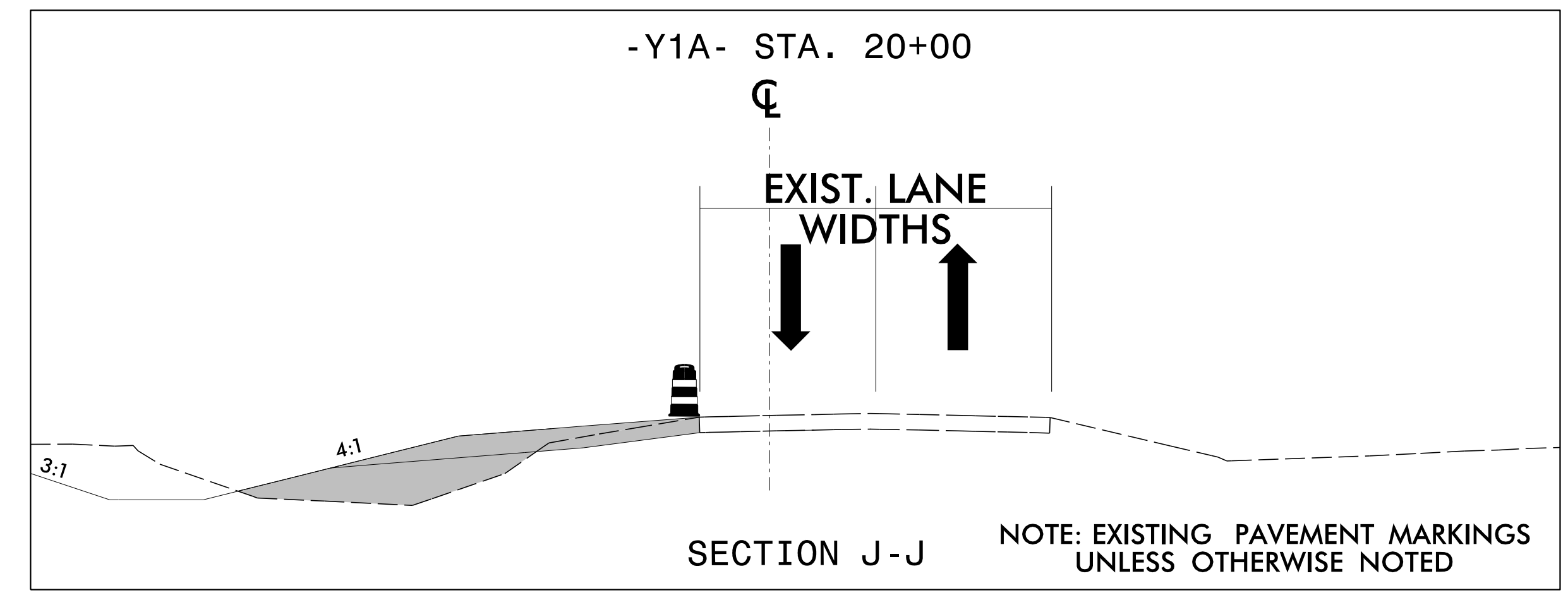
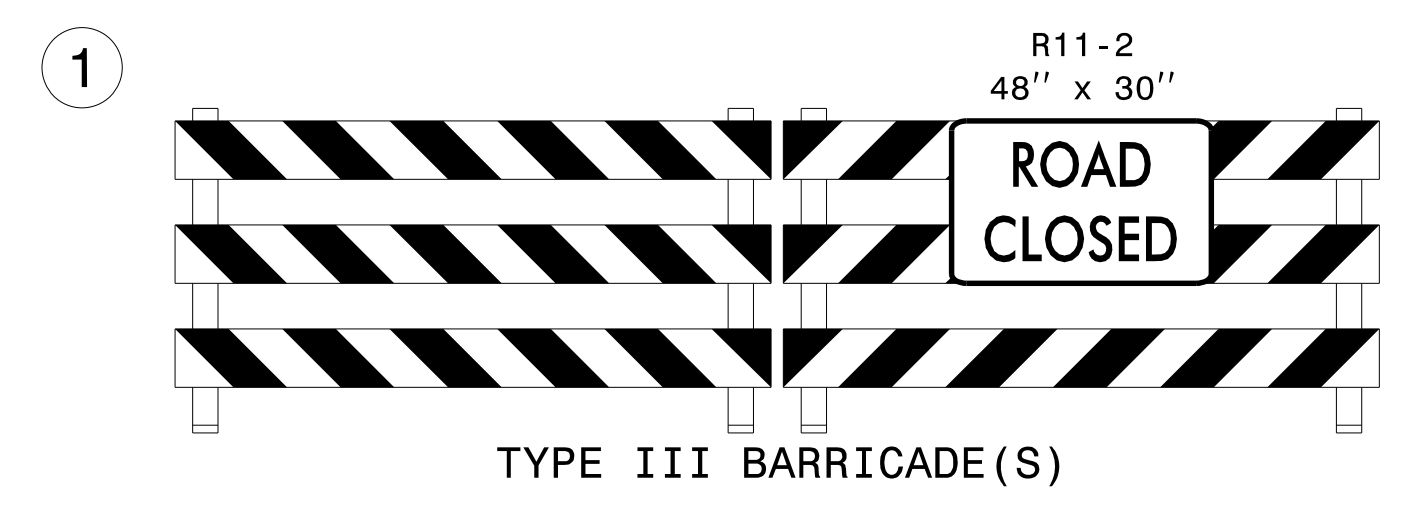
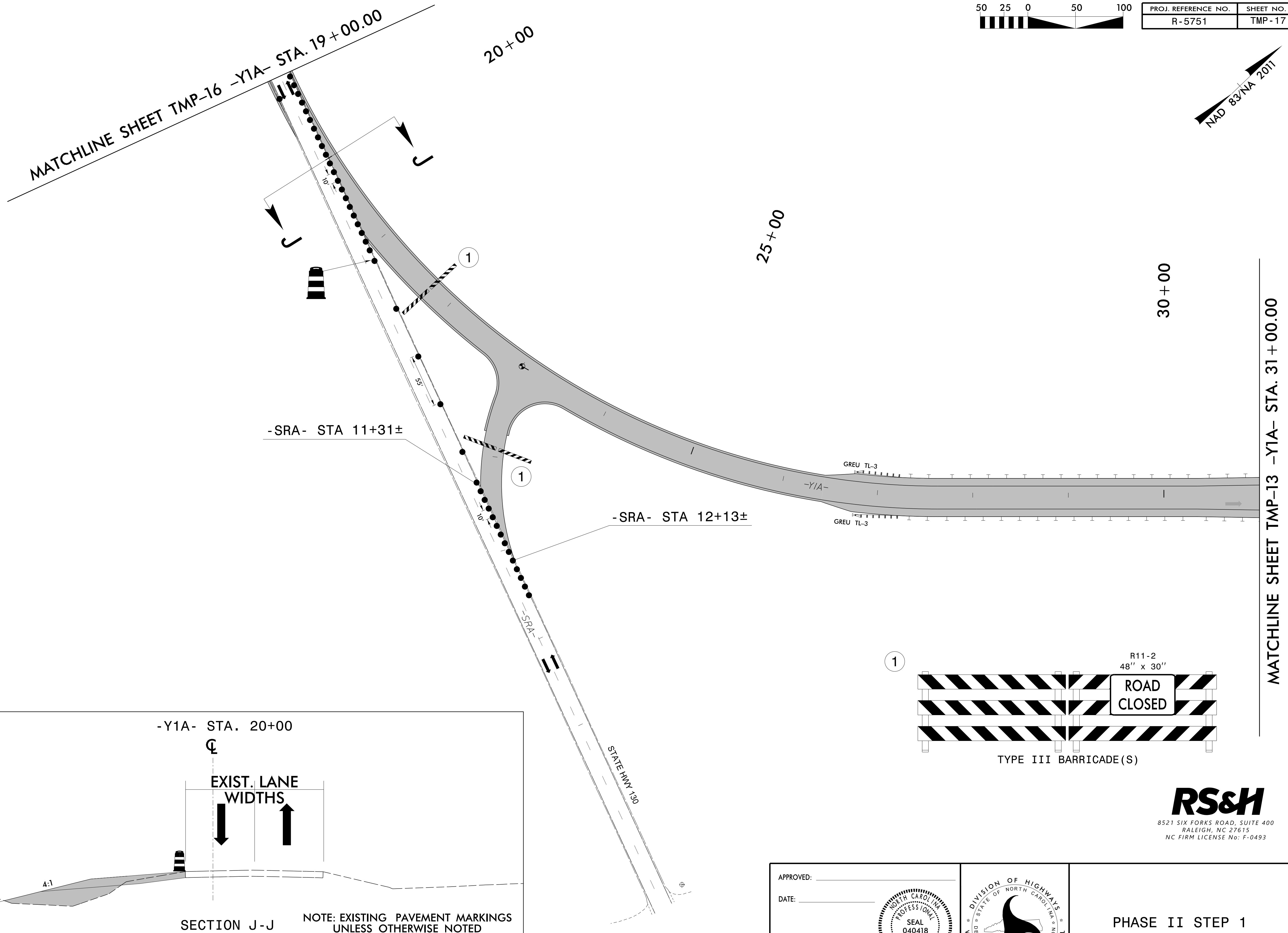
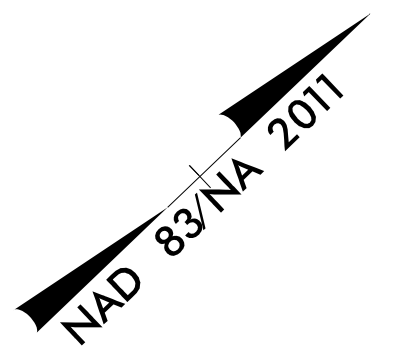
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**PHASE II STEP 1
 DETAIL**



PROJ. REFERENCE NO. R-5751	SHEET NO. TMP-17
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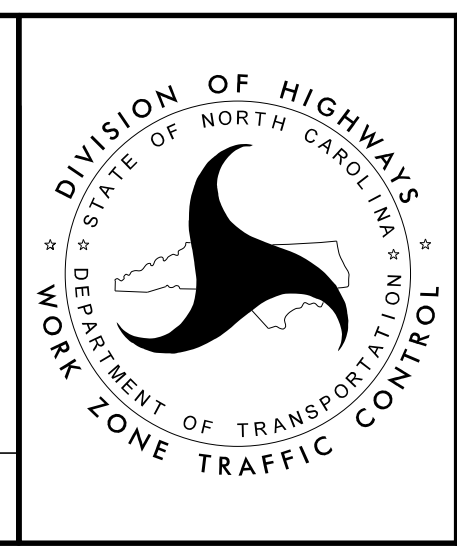
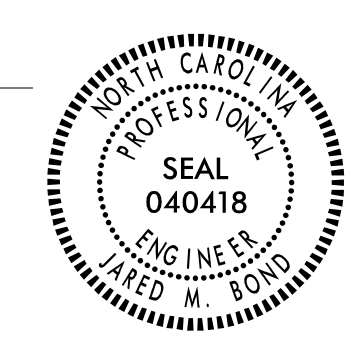


10/27/2022
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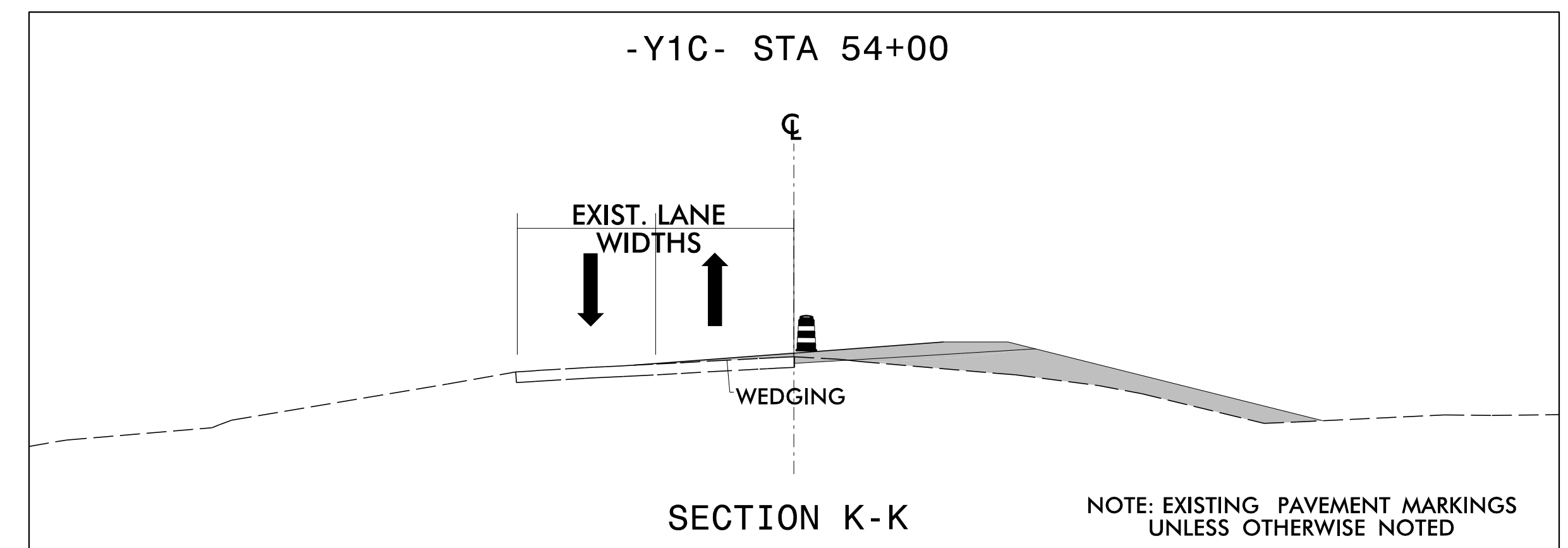
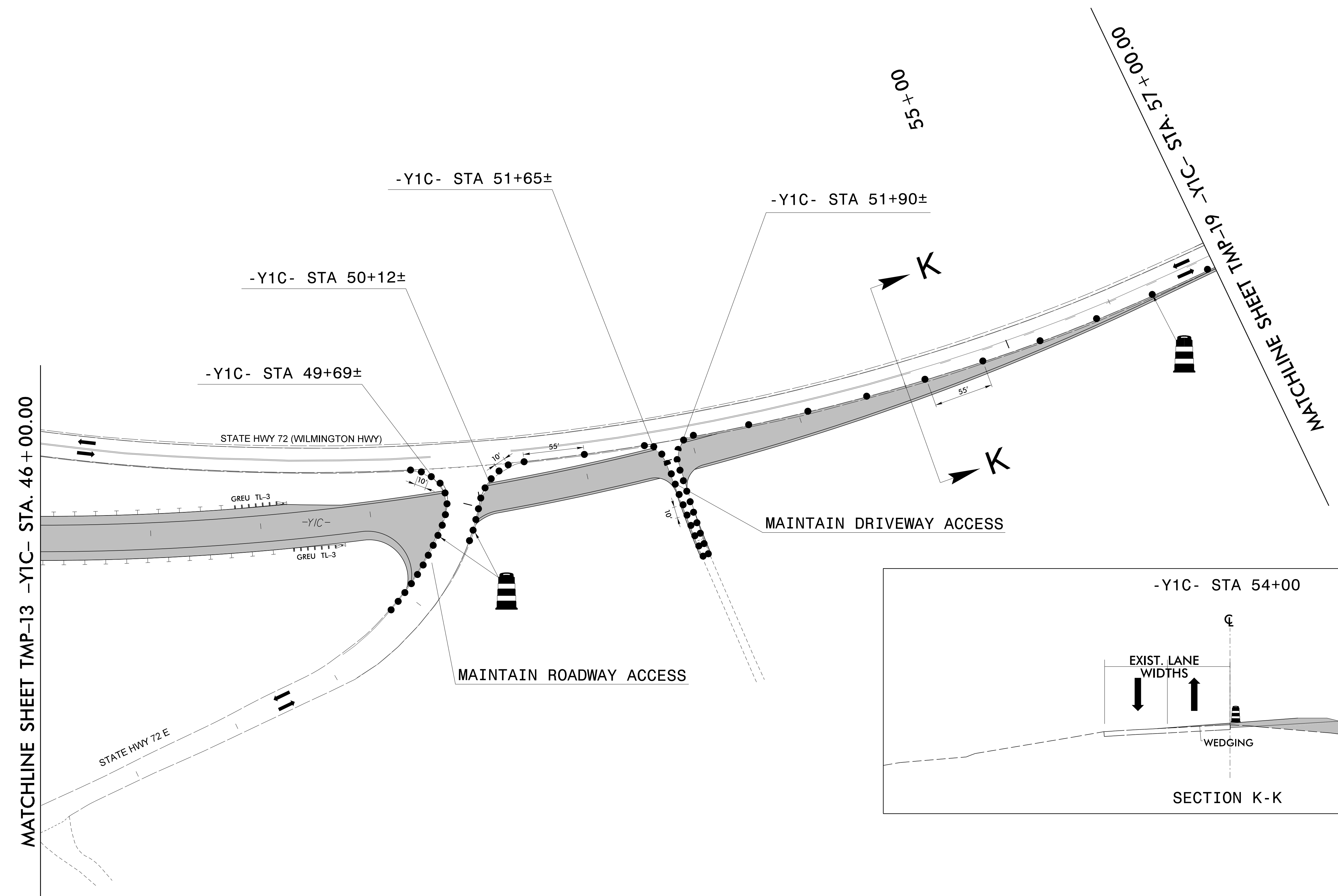
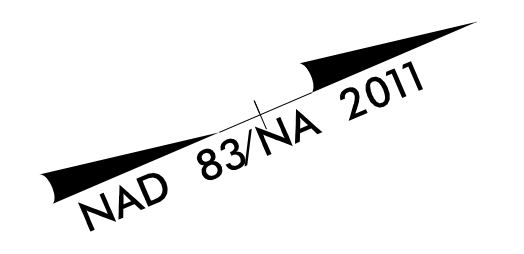


**PHASE II STEP 1
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PROJ. REFERENCE NO.	SHEET NO.
R-5751	TMP-18



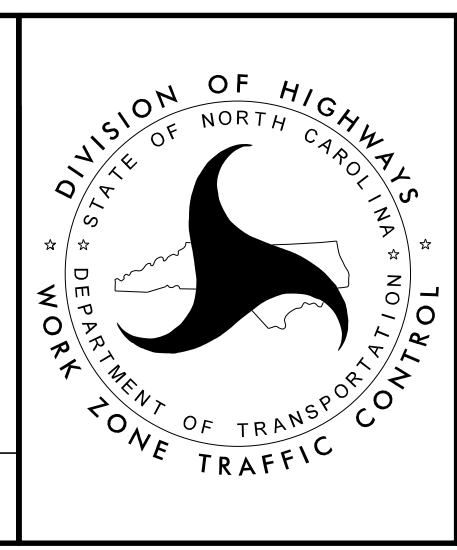
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 User:Bondu

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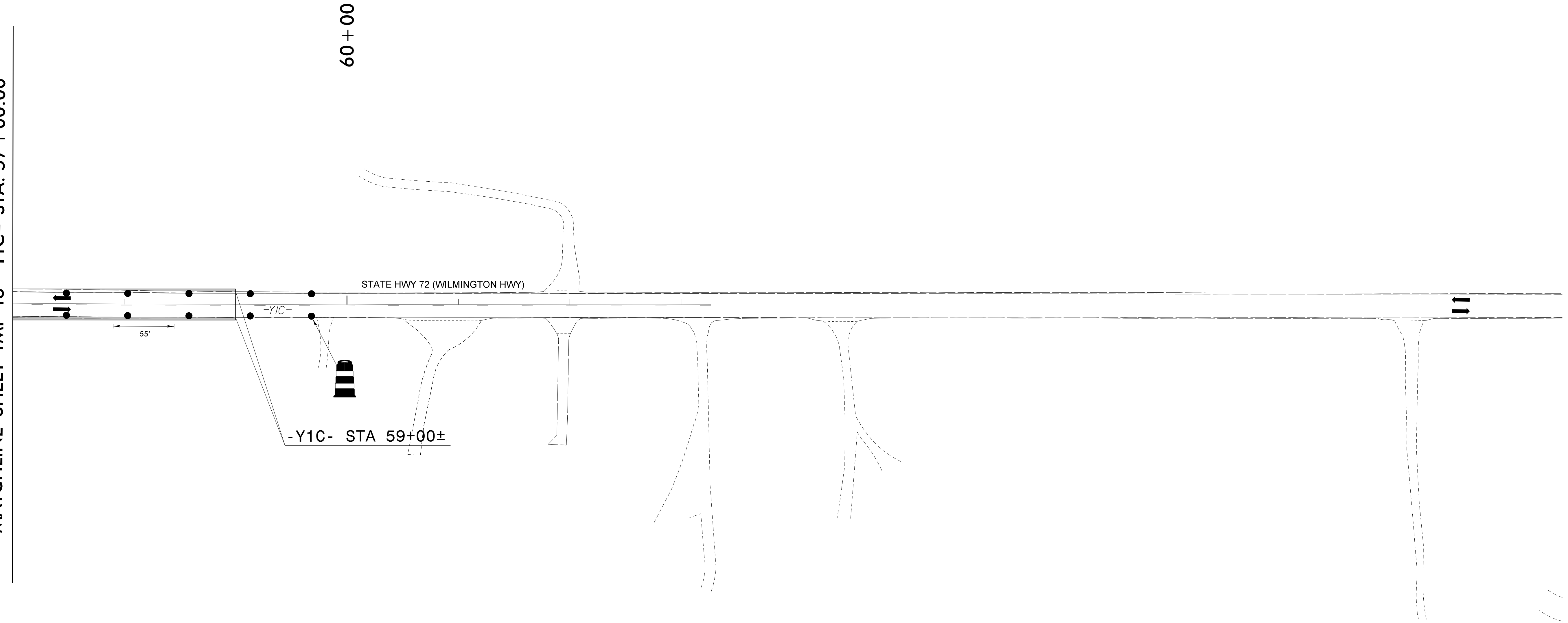
**PHASE II STEP 1
 DETAIL**



PROJ. REFERENCE NO.	SHEET NO.
R-5751	TMP-19

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MATCHLINE SHEET TMP-18 -Y1C- STA. 57 + 00.00



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 User: Bondu

APPROVED: _____			<p>PHASE II STEP 1 DETAIL</p>
DATE: _____			
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PROJ. REFERENCE NO.	SHEET NO.
R-5751	TMP-20

NAD
 8/3/18
 1102

10+00

15+00



MATCHLINE SHEET TMP-21 -Y1A- STA. 19 + 00.00

RS&H

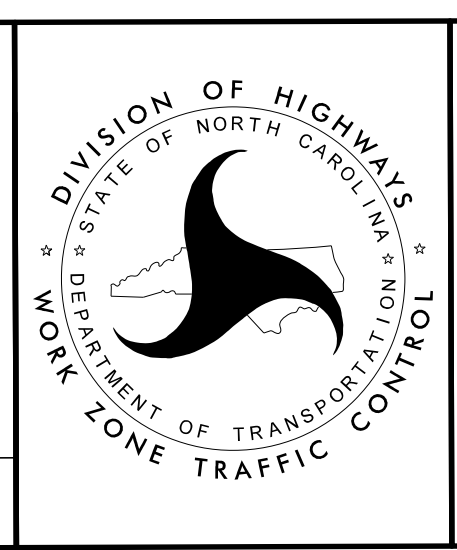
8521 SIX FORKS ROAD, SUITE 400
 RALEIGH, NC 27615
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APPROVED: _____

DATE: _____

NORTH CAROLINA
 PROFESSIONAL
 SEAL
 040418
 ENGINEER
 JARED M. BOND

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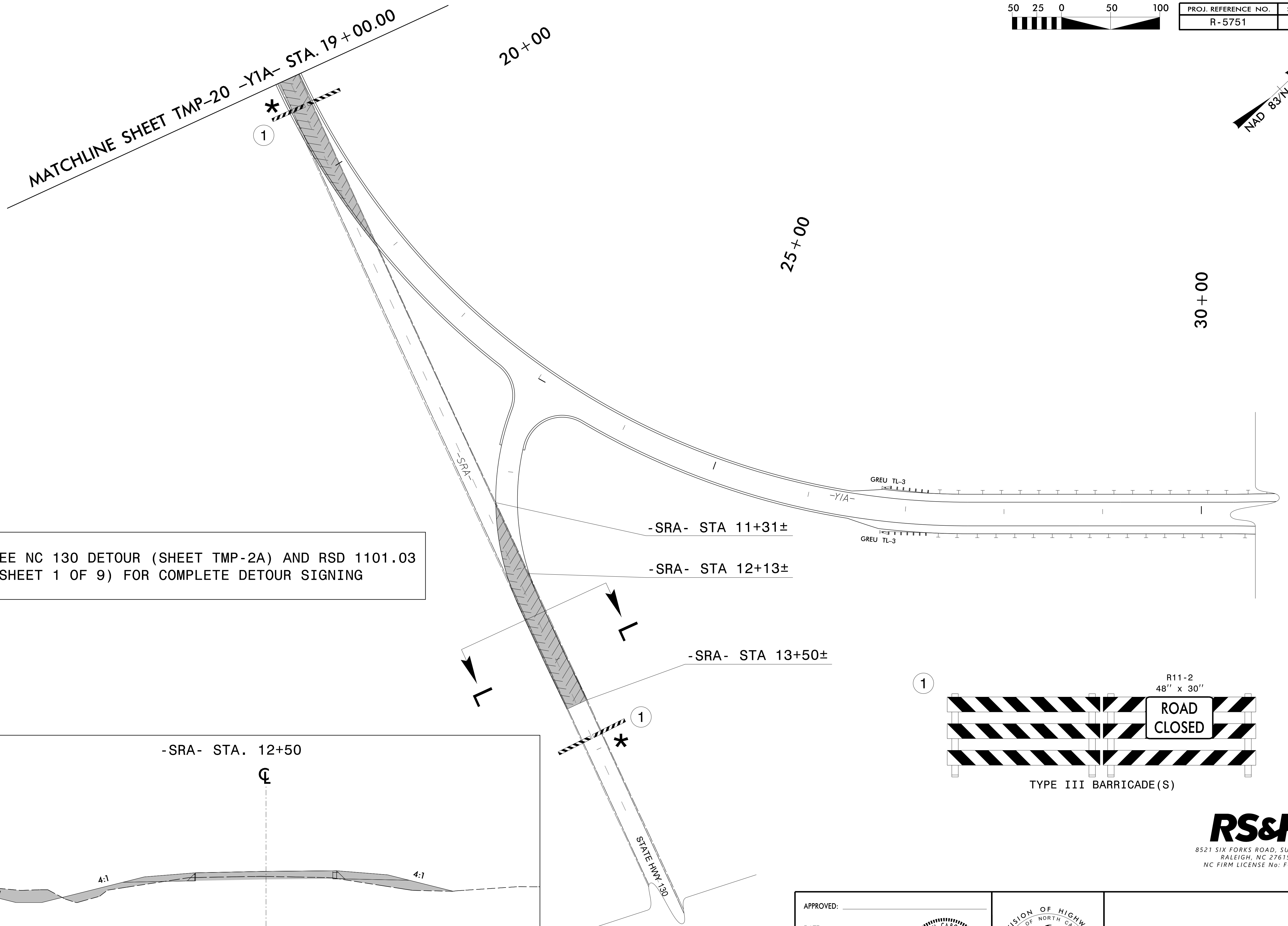


PHASE II STEP 2
 DETAIL

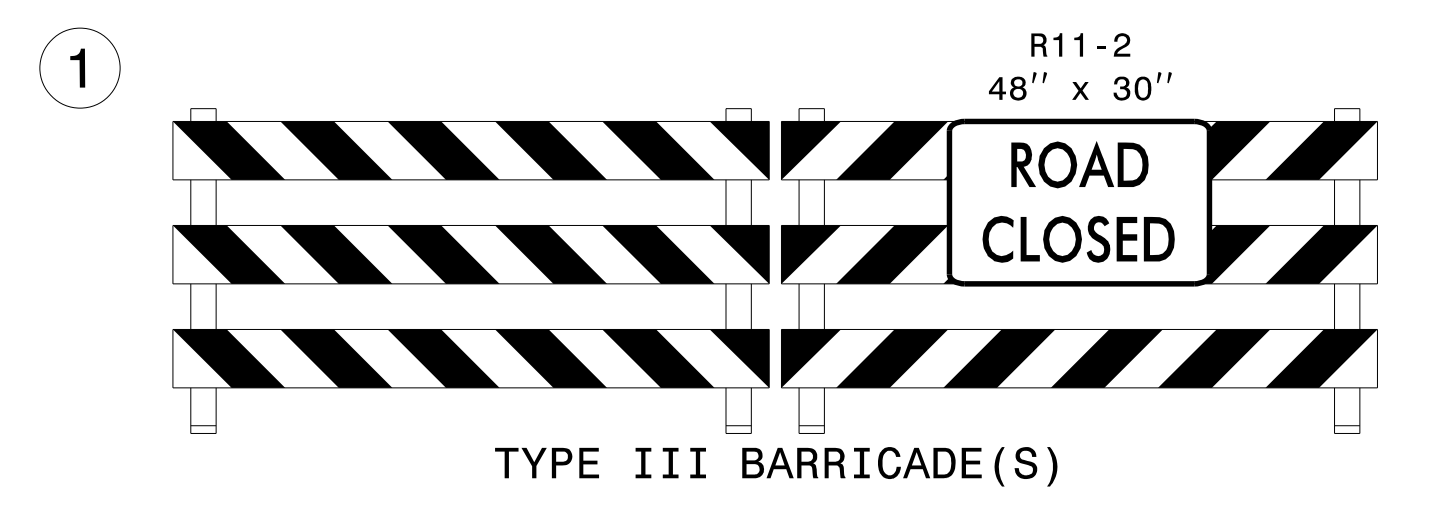
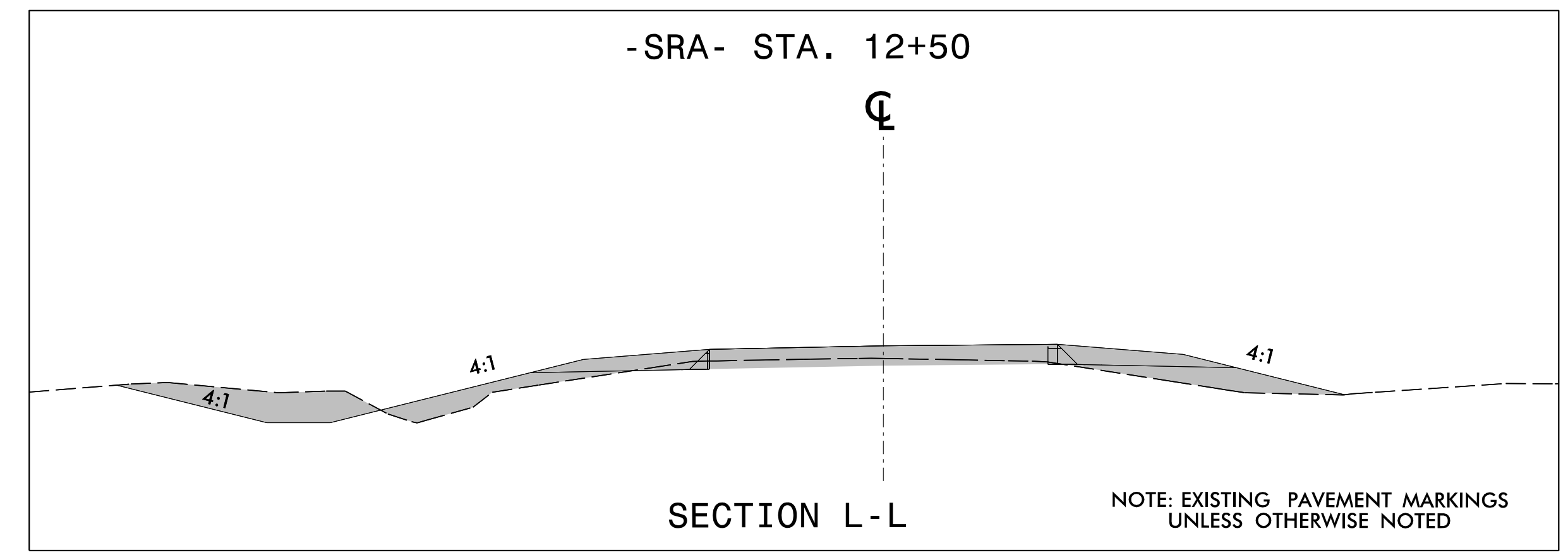
10/6/2022
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PROJ. REFERENCE NO. R-5751	SHEET NO. TMP-21
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* NOTE: SEE NC 130 DETOUR (SHEET TMP-2A) AND RSD 1101.03 (SHEET 1 OF 9) FOR COMPLETE DETOUR SIGNING

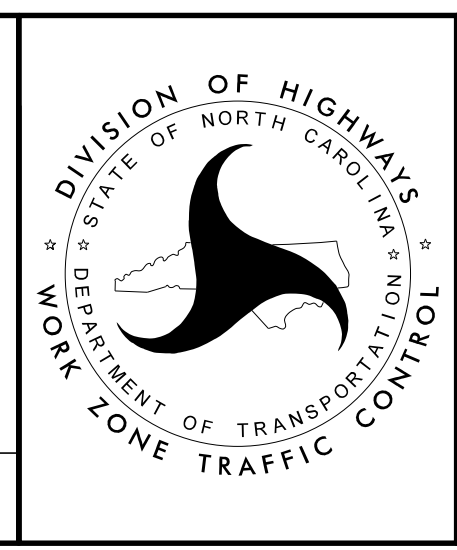


10/6/2022
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 User:Bondu

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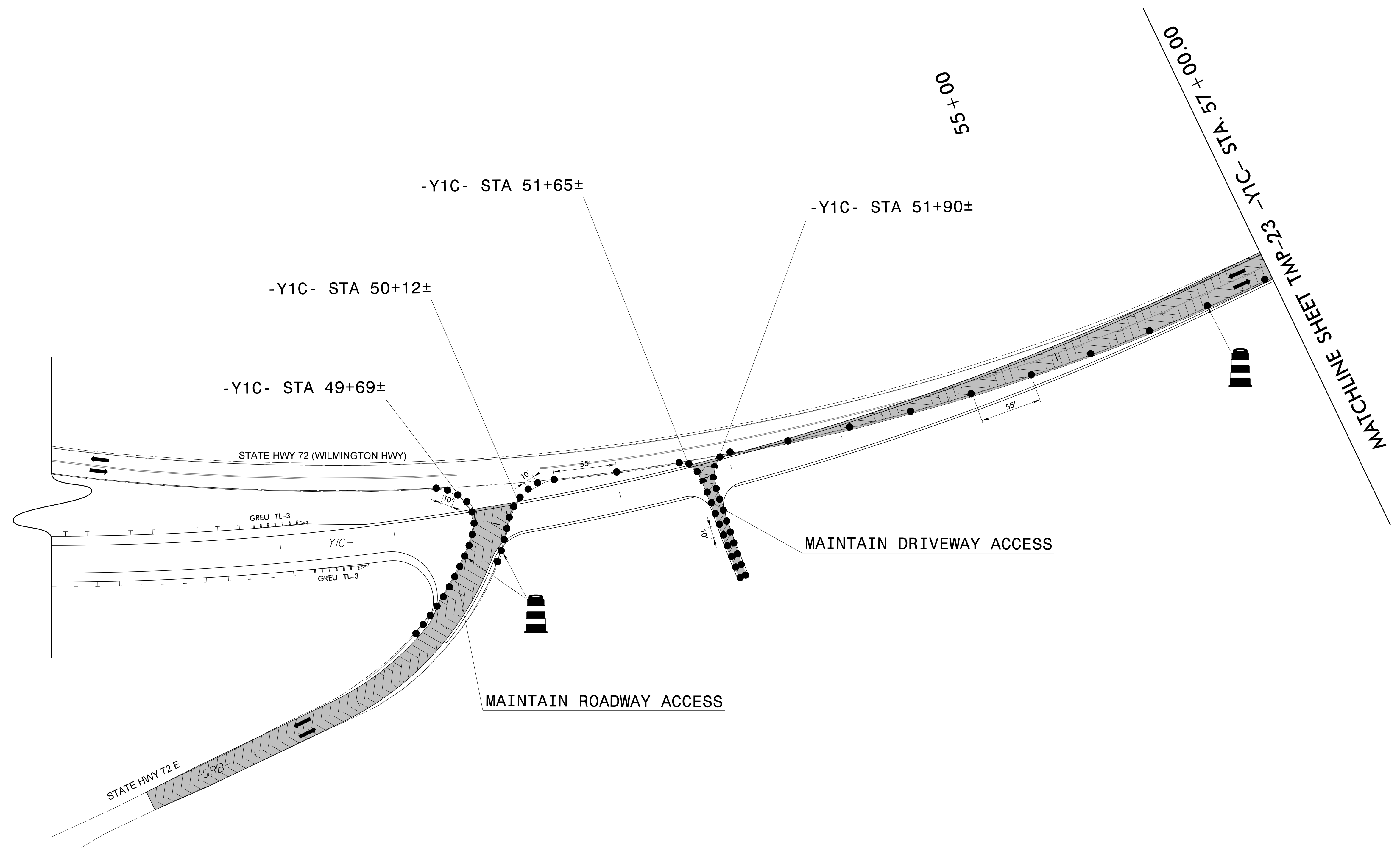
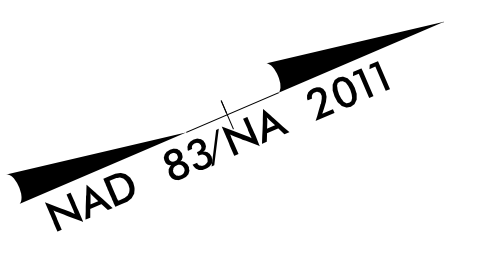
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**PHASE II STEP 2
 DETAIL**



PROJ. REFERENCE NO.	SHEET NO.
R-5751	TMP-22



10/6/2022
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 User:Bondu

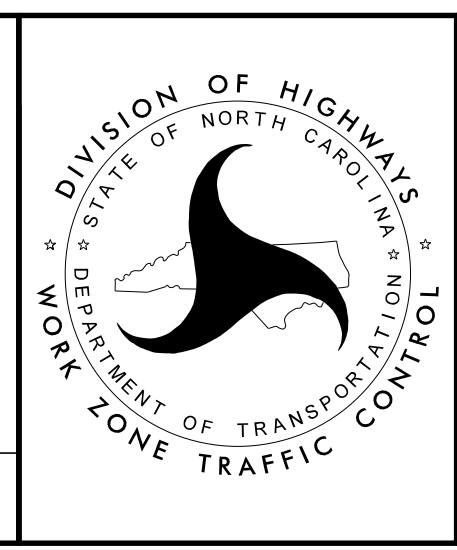


APPROVED: _____

DATE: _____

SEAL
 040418
 ENGINEER
 JARED W. BOND

**DOCUMENT NOT CONSIDERED FINAL
UNLESS ALL SIGNATURES COMPLETED**



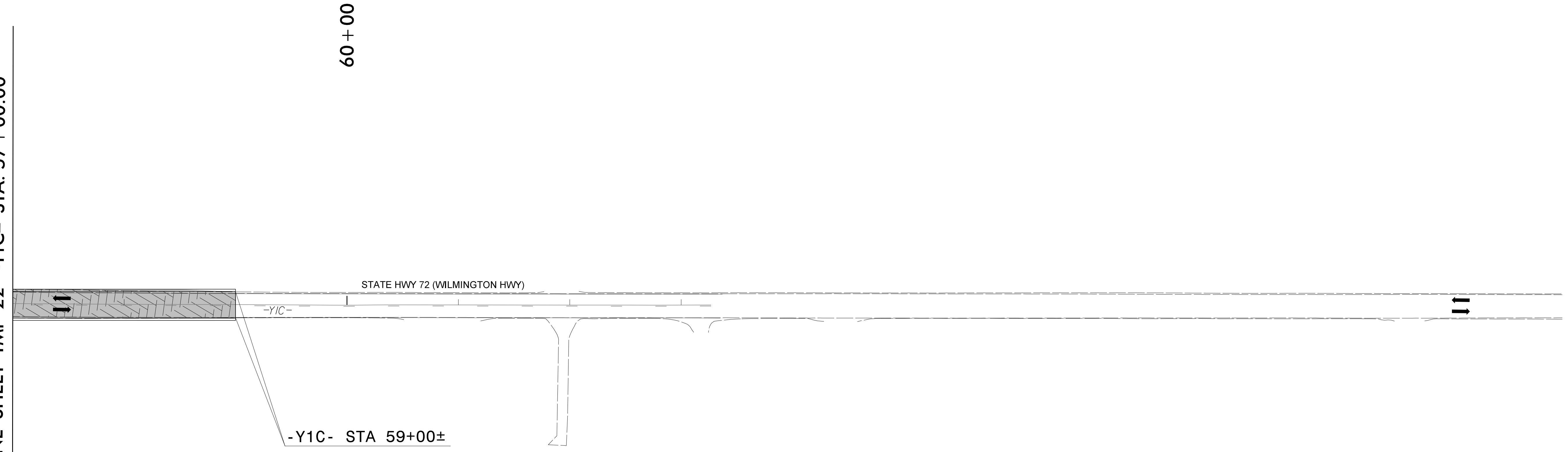
**PHASE II STEP 2
DETAIL**



PROJ. REFERENCE NO.	SHEET NO.
R-5751	TMP-23

NAD 83/NA 2011

MATCHLINE SHEET TMP-22 -Y1C- STA. 57 + 00.00



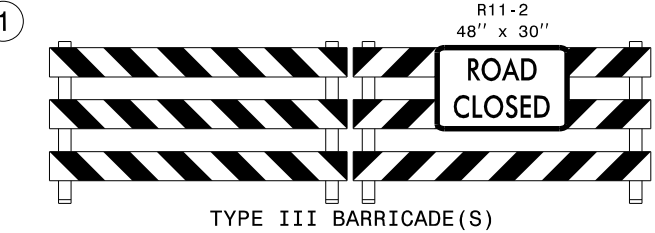
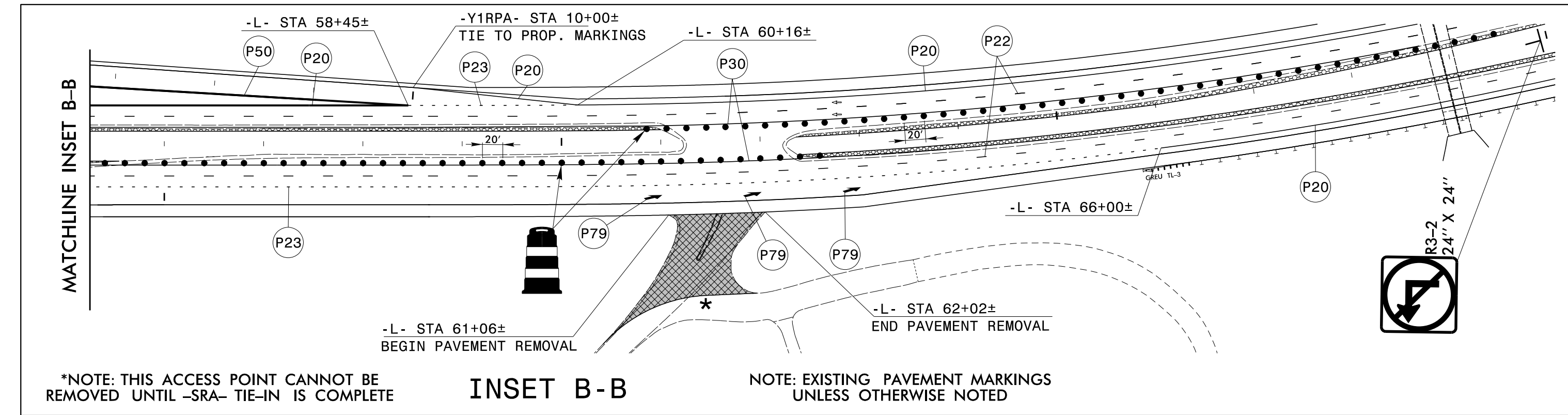
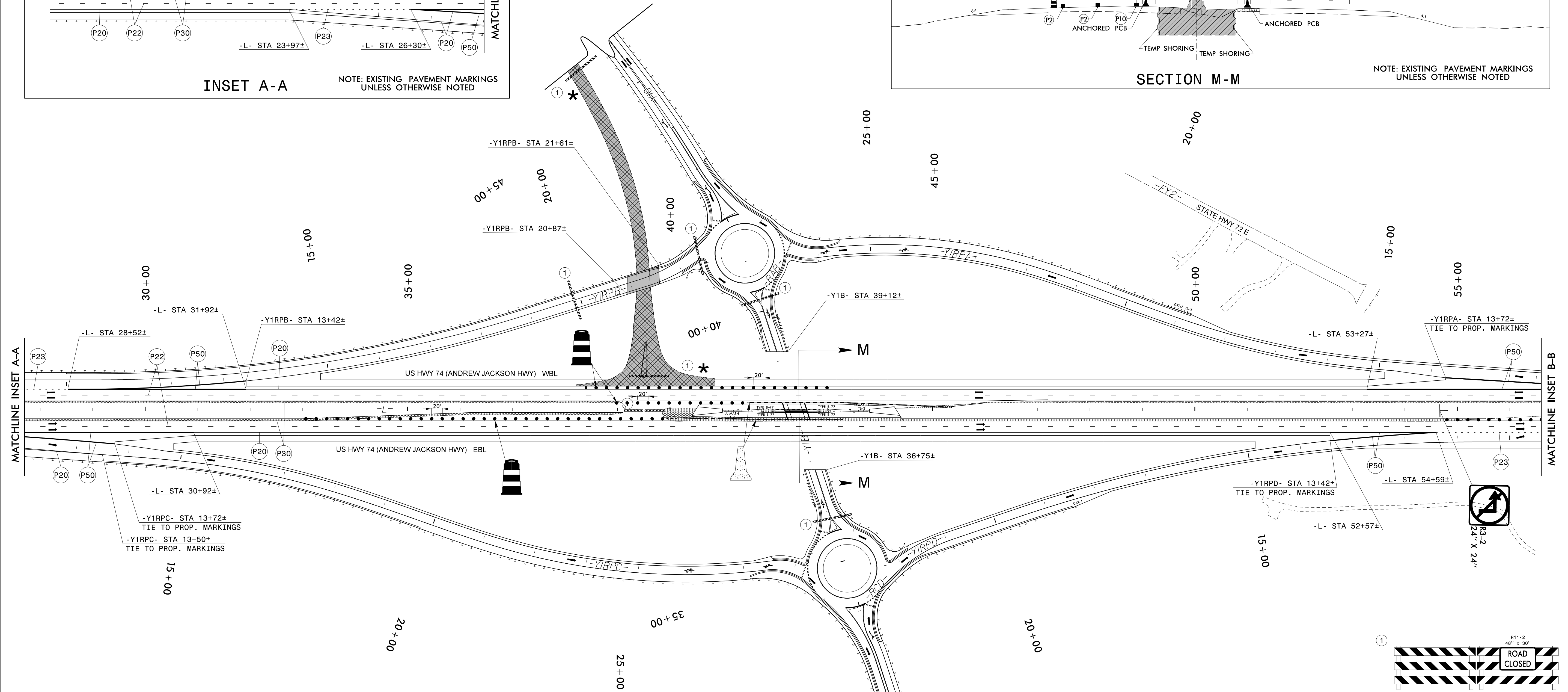
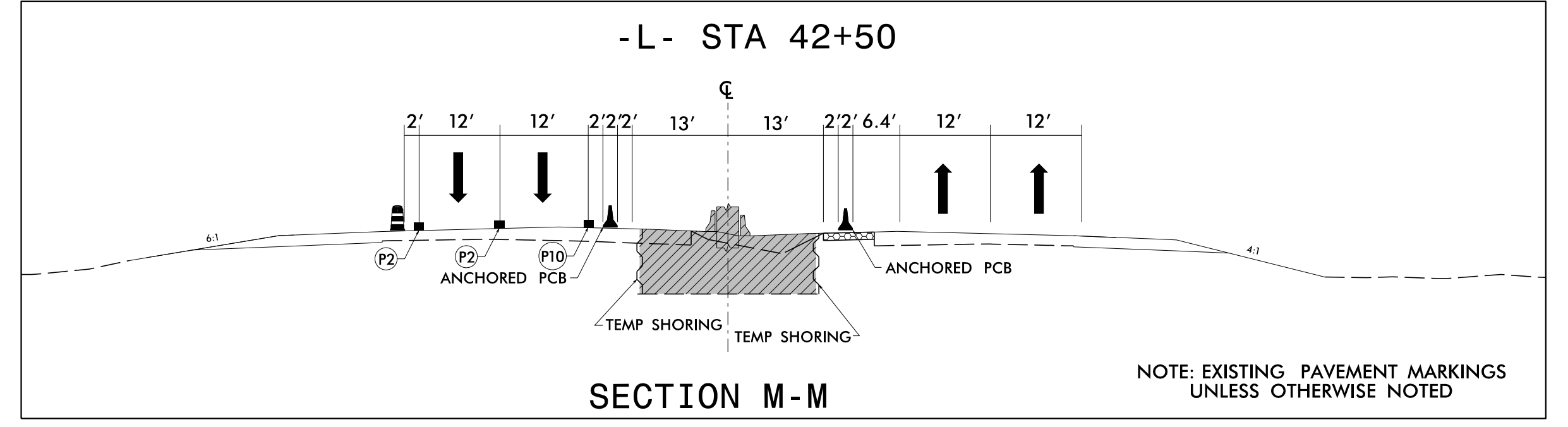
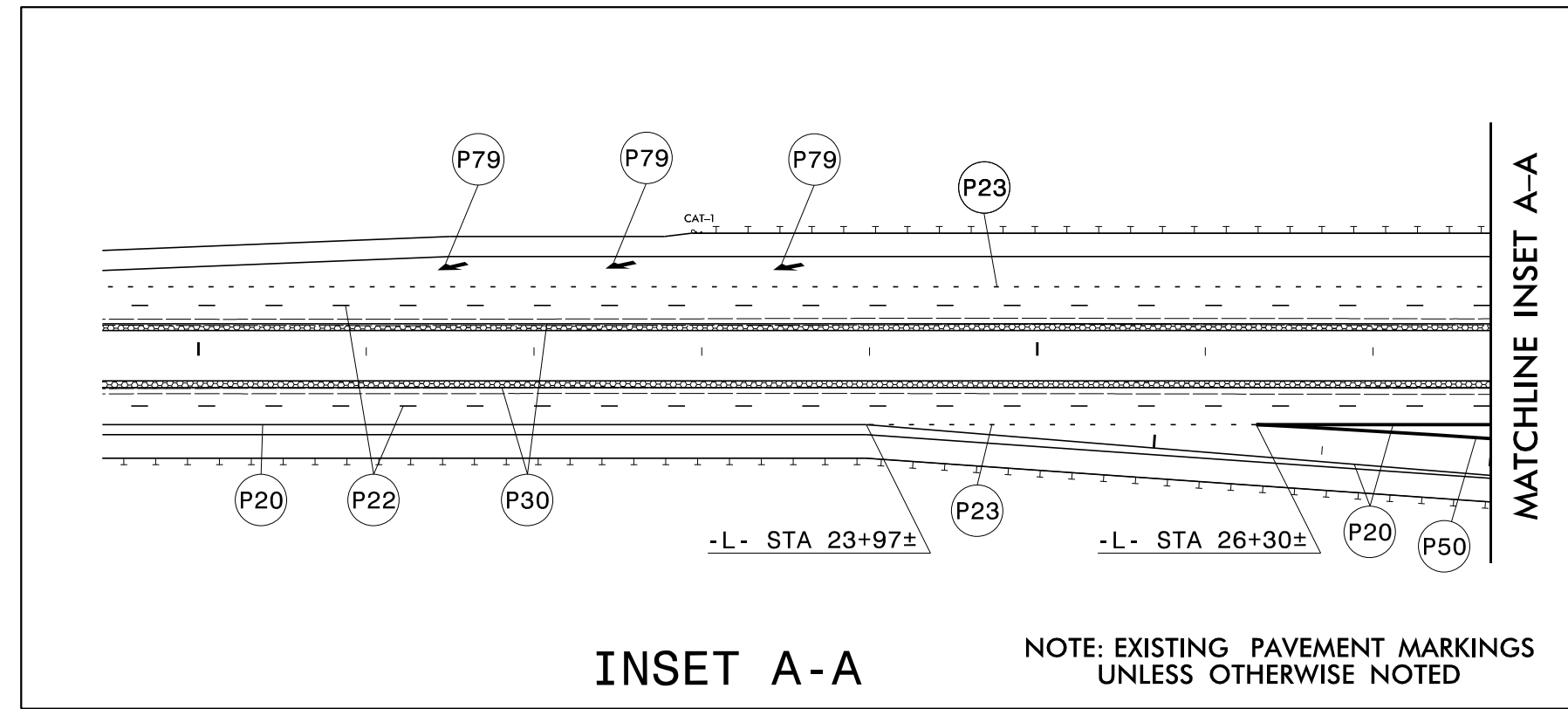
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 User: Bondu

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 NC FIRM LICENSE No: F-0493

APPROVED: _____ DATE: _____			<p>PHASE II STEP 2 DETAIL</p>
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PROJ. REFERENCE NO.	SHEET NO.
R-5751	TMP-24

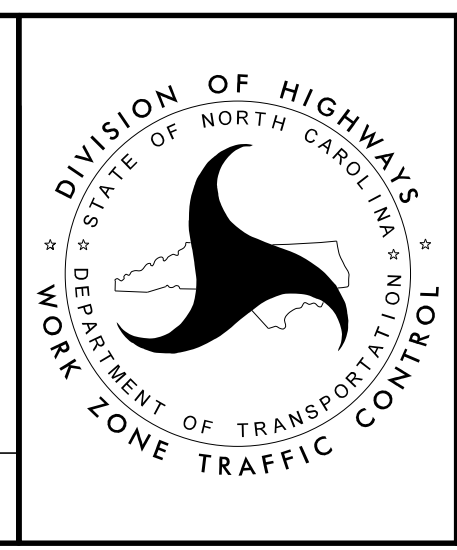


* SEE NORTH CREEK RD., DETOUR (SHEET TMP-2B) AND RSD 1101.03 (SHEET 1 OF 9) FOR COMPLETE DETOUR SIGNING

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NC FIRM LICENSE No: F-0493

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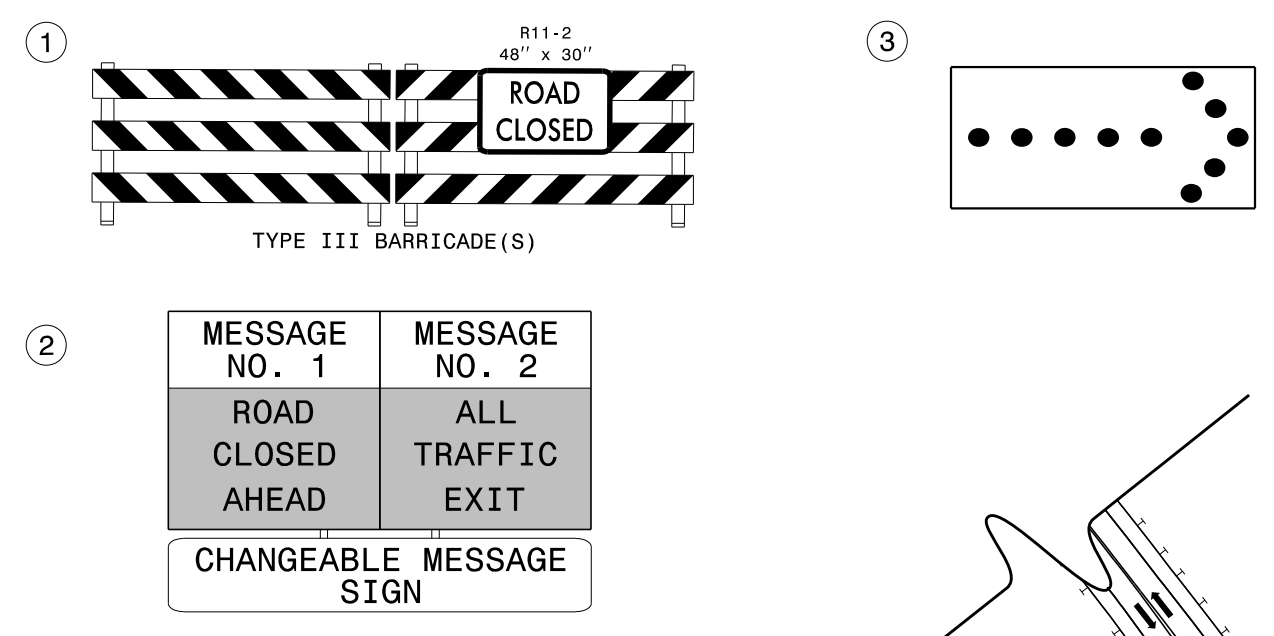


PHASE II STEP 3 DETAIL

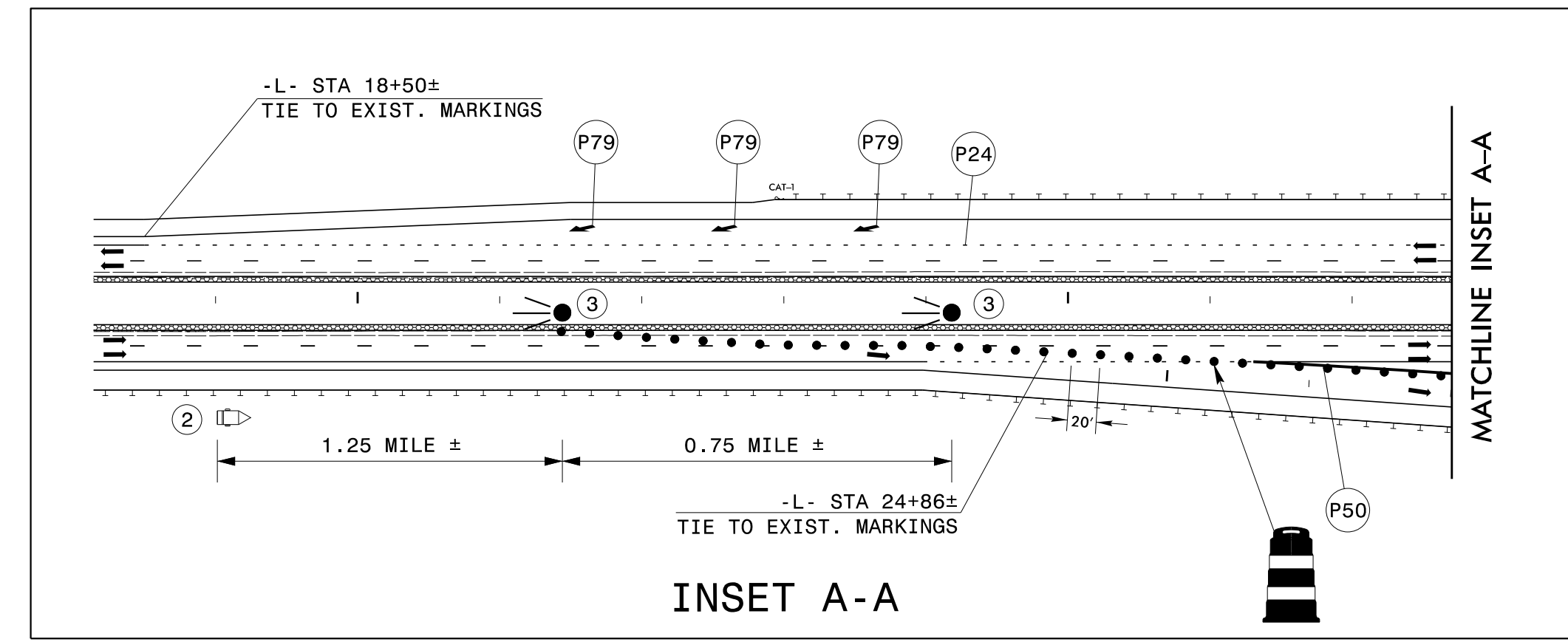
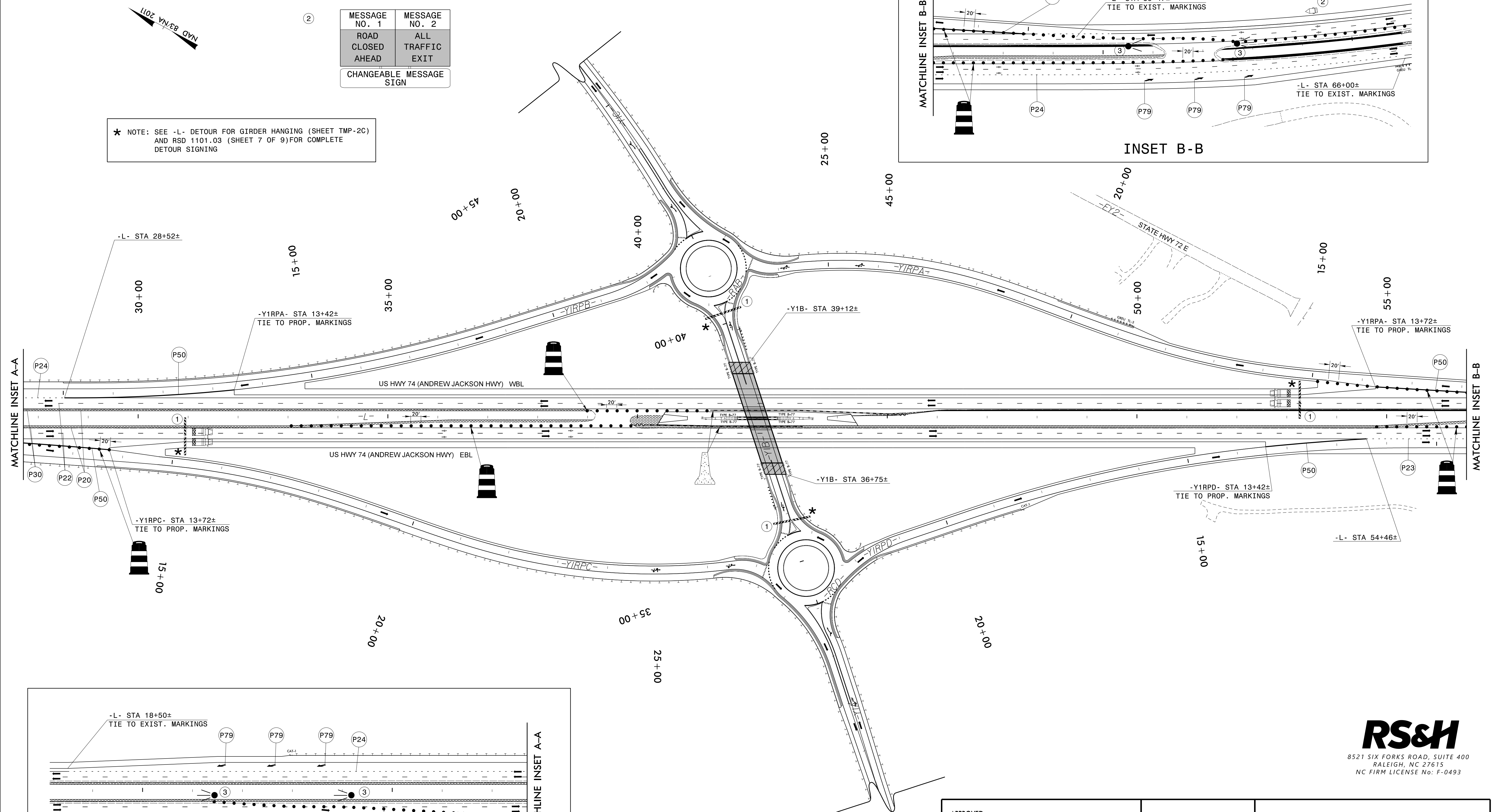
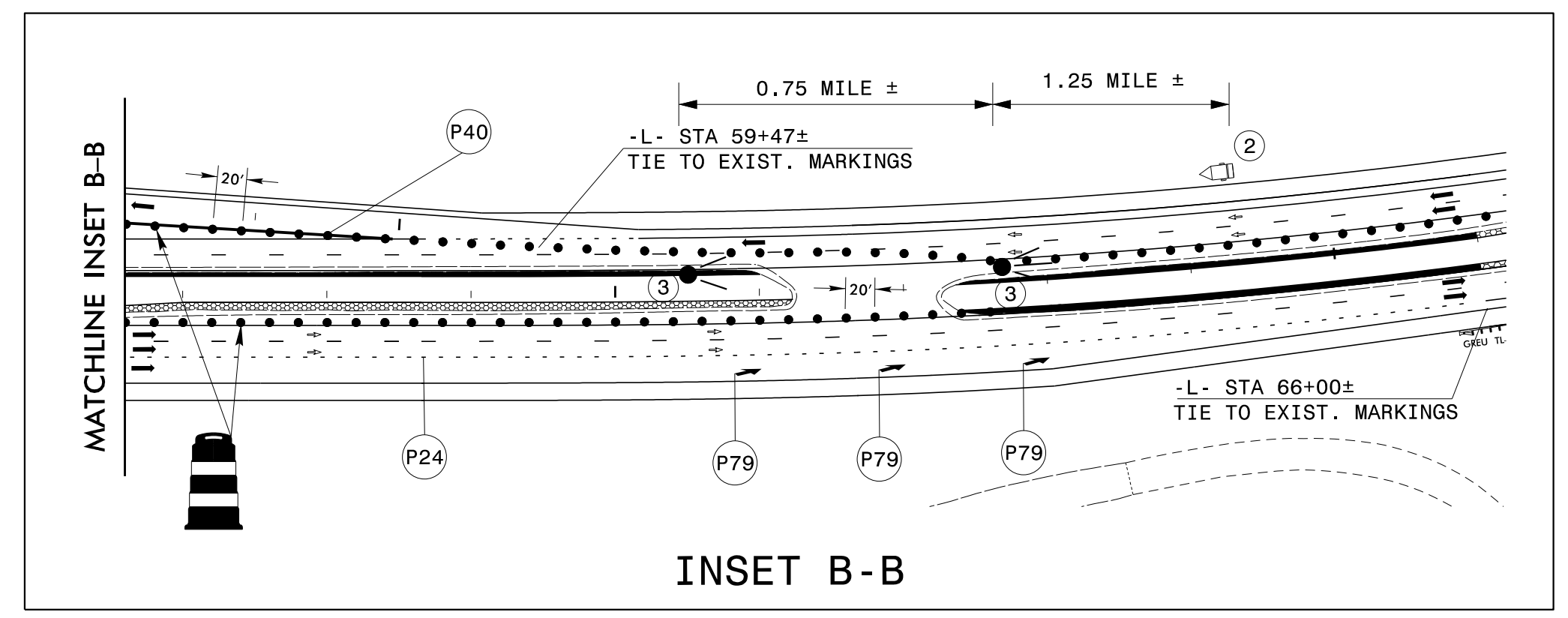
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PROJ. REFERENCE NO.	SHEET NO.
R-5751	TMP-25



★ NOTE: SEE -L- DETOUR FOR GIRDER HANGING (SHEET TMP-2C) AND RSD 1101.03 (SHEET 7 OF 9) FOR COMPLETE DETOUR SIGNING

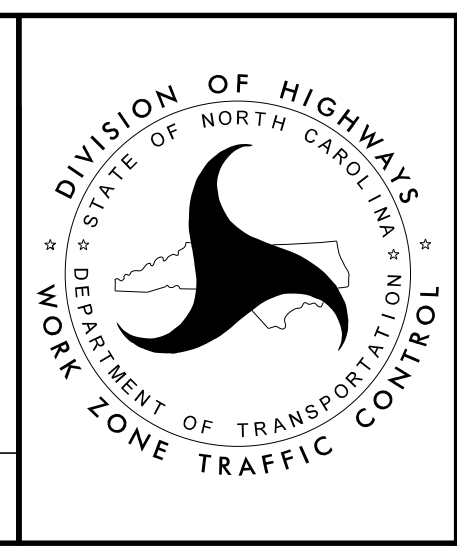


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 NC FIRM LICENSE No: F-0493

APPROVED: _____
 DATE: _____

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 UNLESS ALL SIGNATURES COMPLETED**

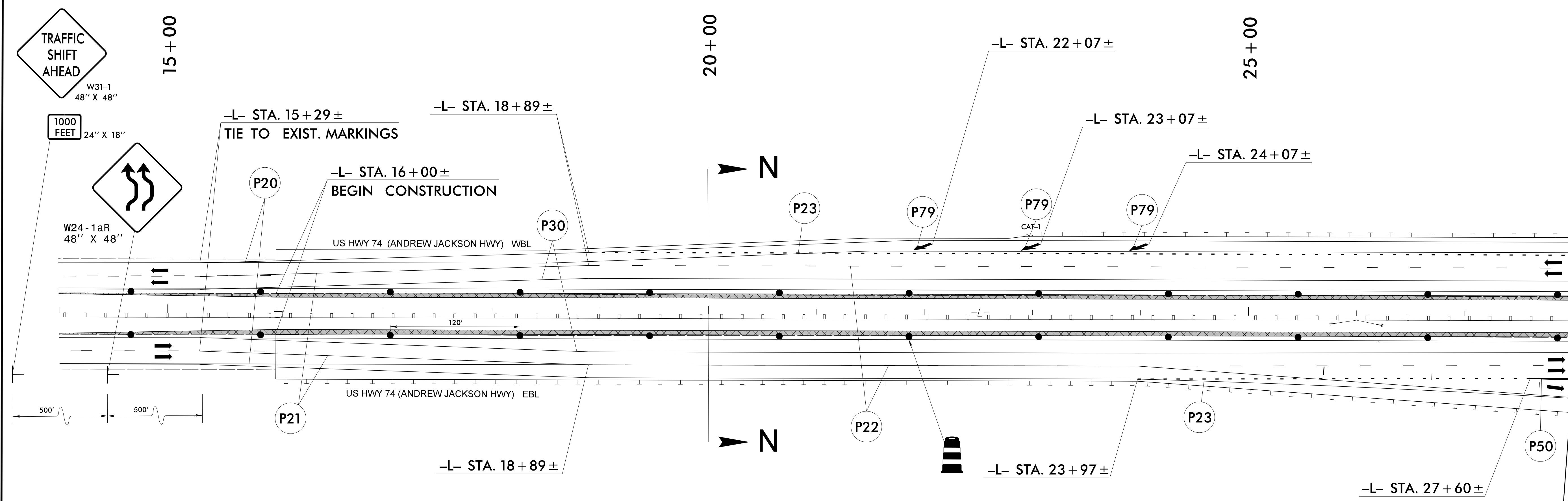
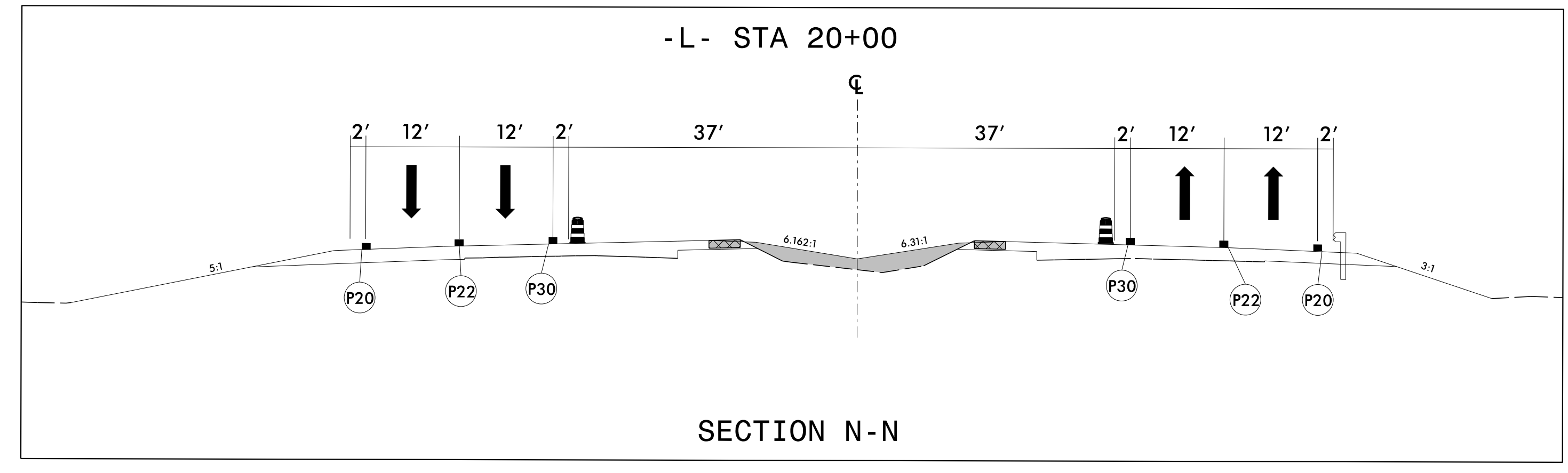


**PHASE II STEP 4
 DETAIL**



PROJ. REFERENCE NO.	SHEET NO.
R-5751	TMP-26

NAD 83/NA 2011



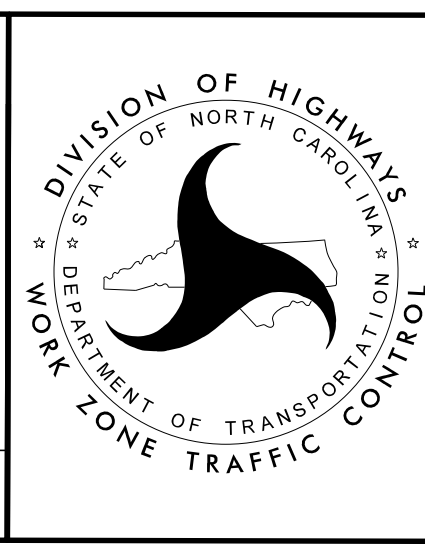
MATCHLINE SHEET TMP-27
 -YTRPC- STA. 12 + 30.00
 MATCHLINE SHEET TMP-27
 -L- STA. 28 + 00.00

10/6/2022
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 DATE: _____

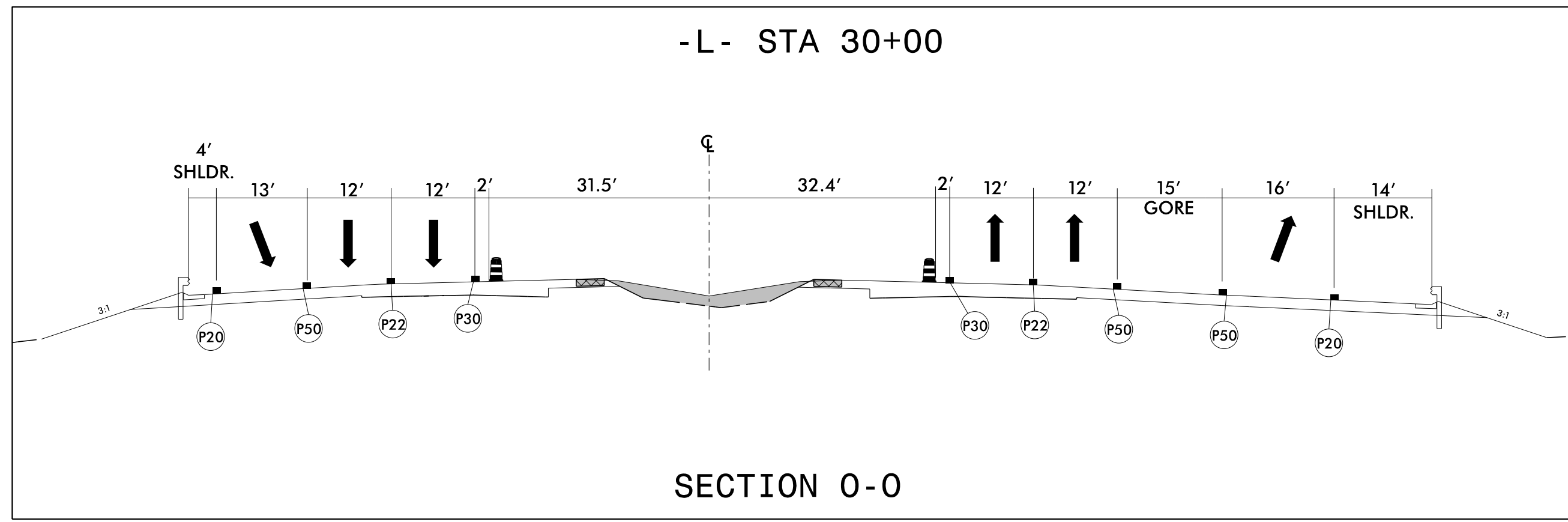
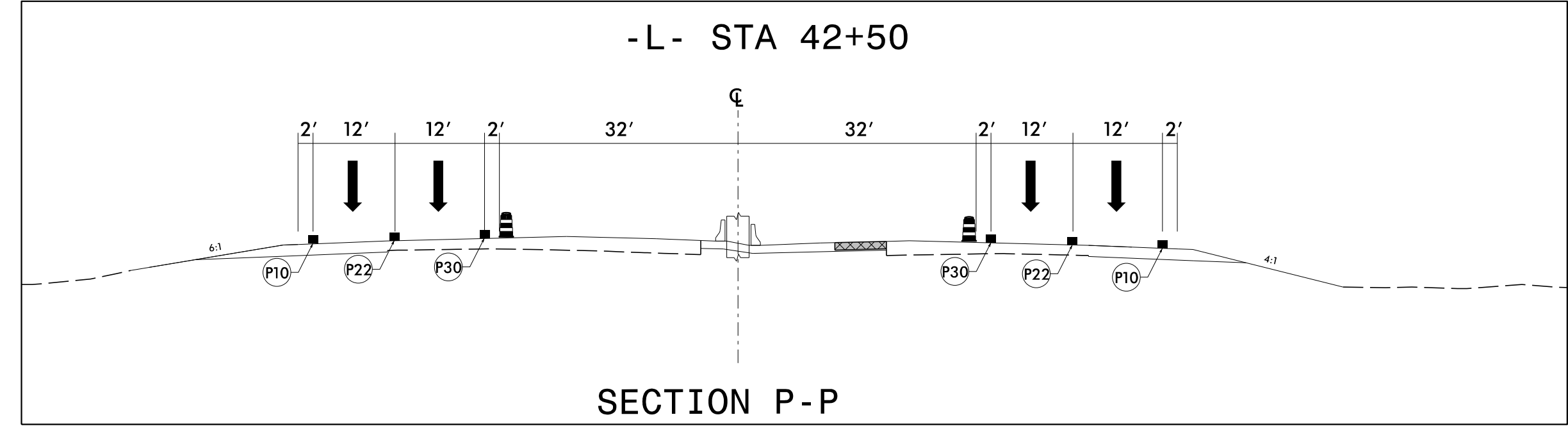
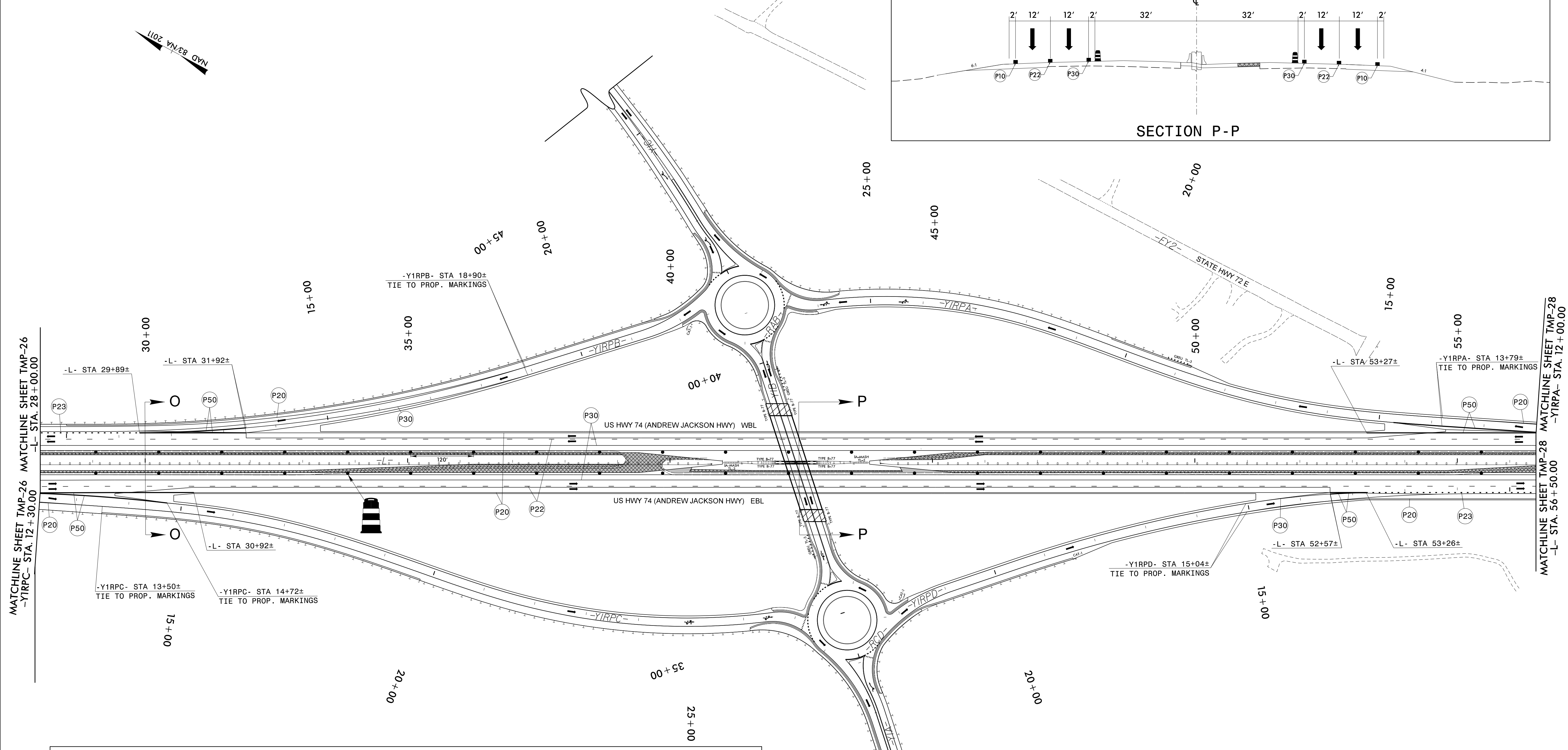
**DOCUMENT NOT CONSIDERED FINAL
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**PHASE III
DETAIL**



PROJ. REFERENCE NO.	SHEET NO.
R-5751	TMP-27

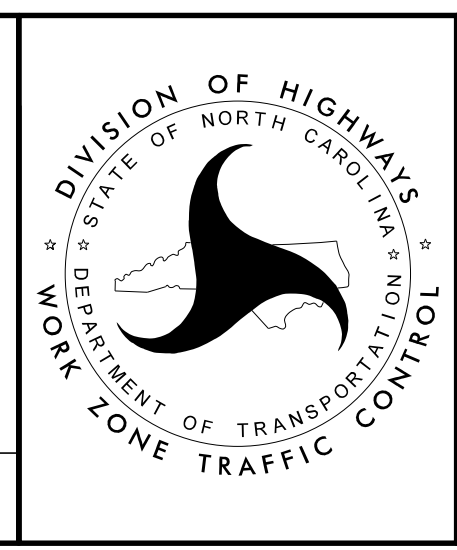


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PHASE III
 DETAIL

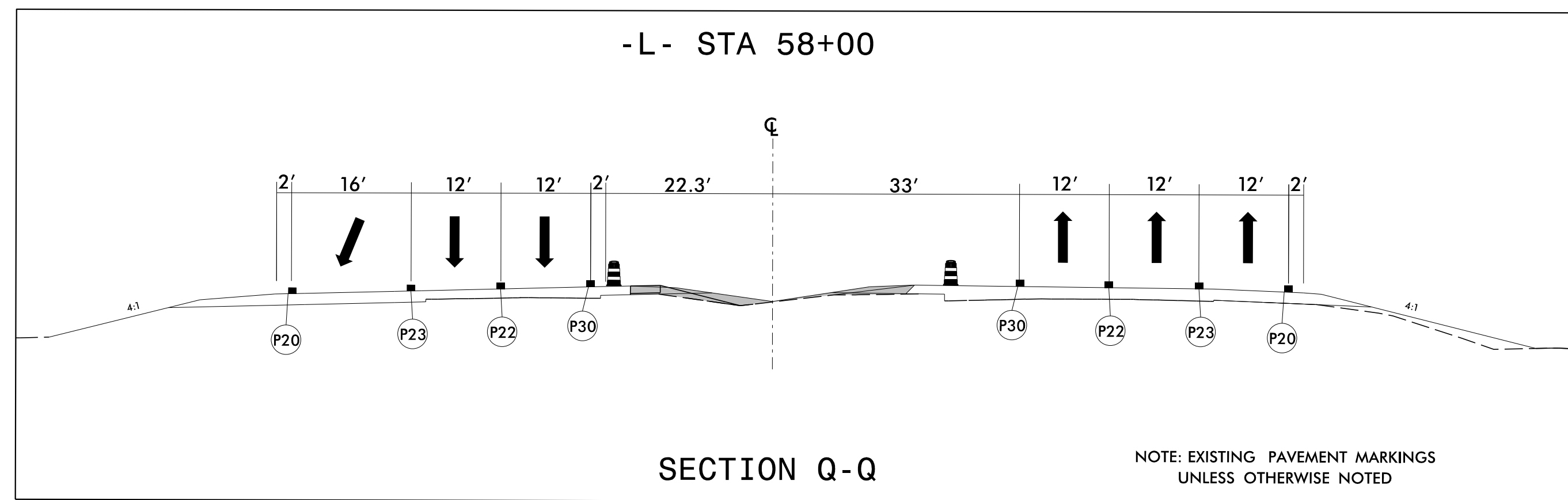
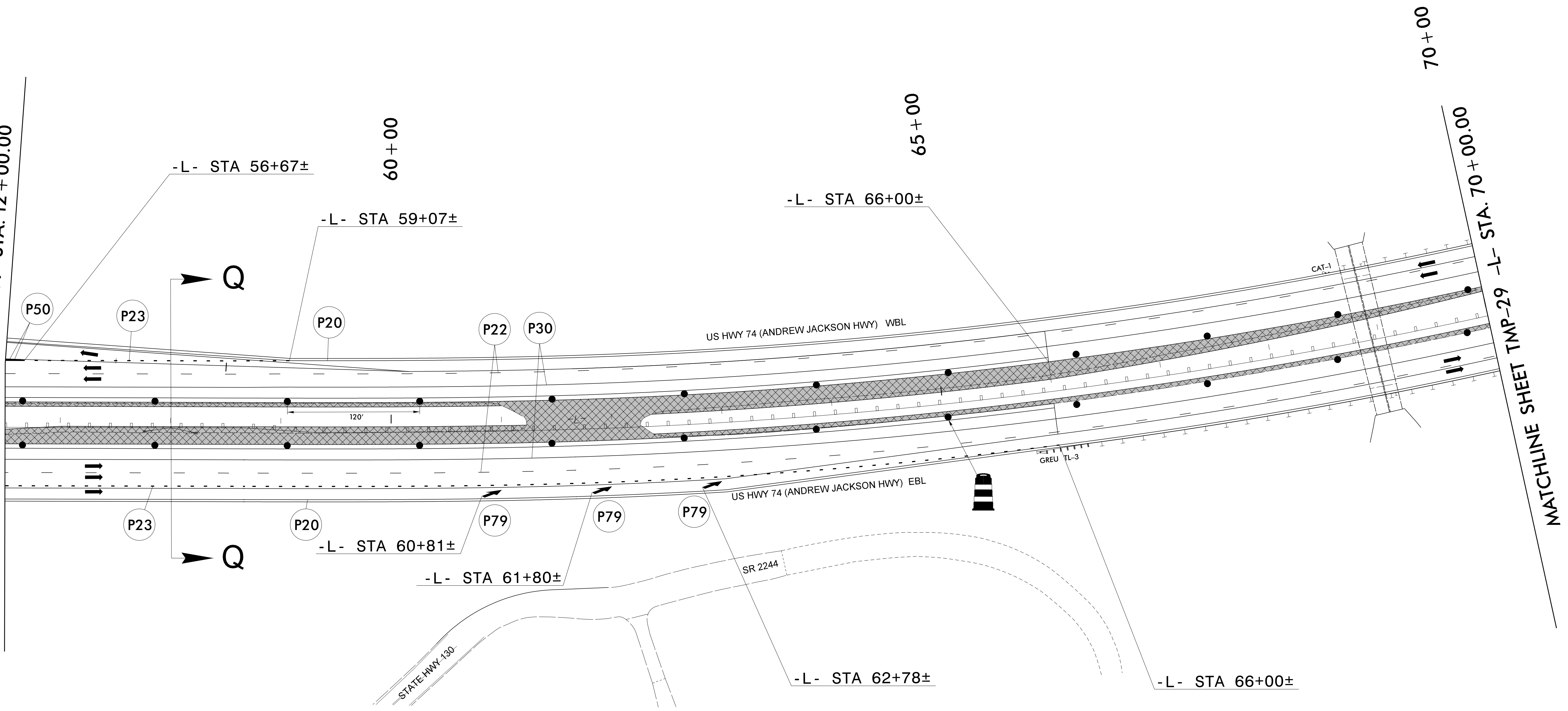


PROJ. REFERENCE NO. R-5751	SHEET NO. TMP-28
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MAD 83/NA 2011

MATCHLINE SHEET TMP-27
-L- STA. 56+50.00

MATCHLINE SHEET TMP-27
-YRPA- STA. 12+00.00



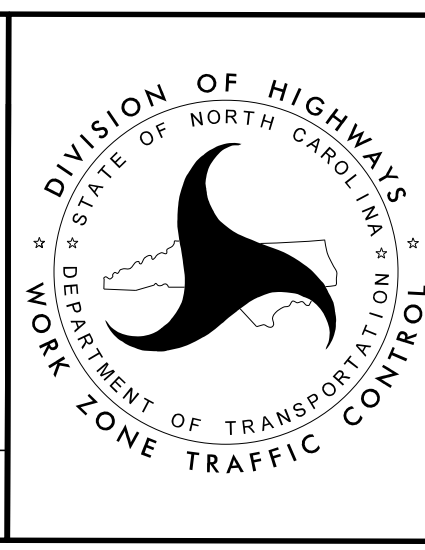
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User: Bondu

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APPROVED: _____

DATE: _____

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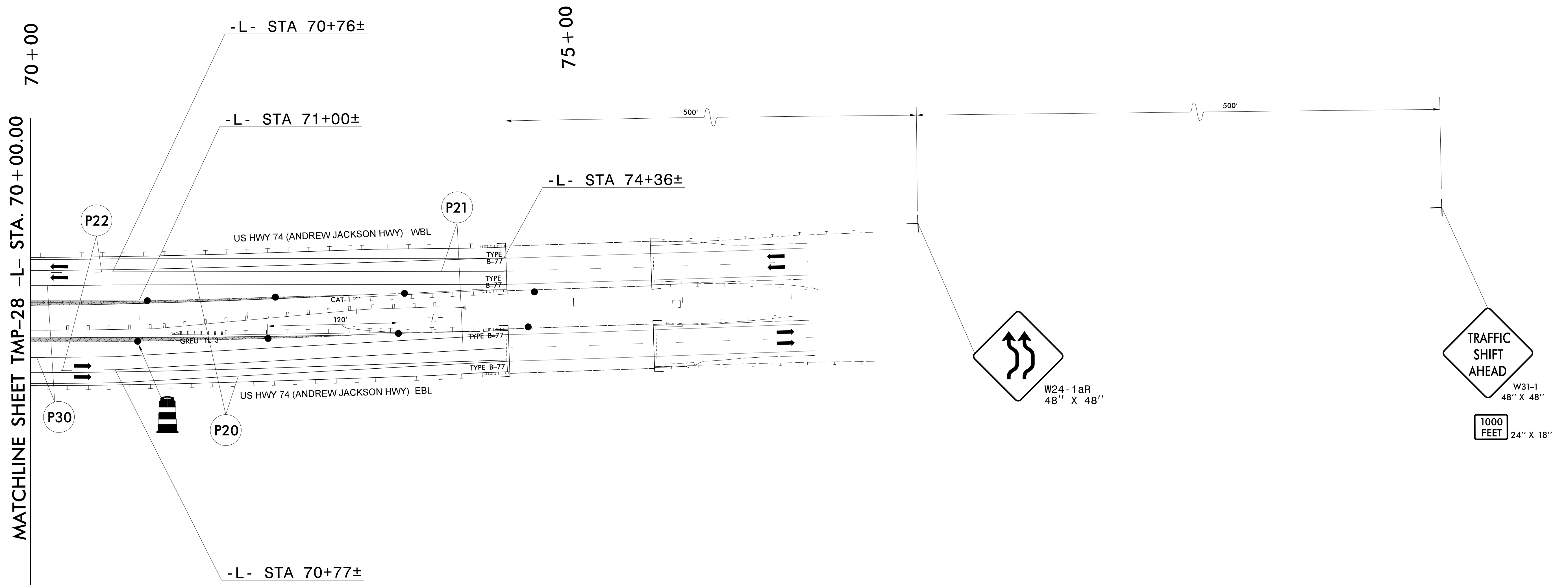


**PHASE III
DETAIL**





PROJ. REFERENCE NO.	SHEET NO.
R-5751	TMP-29

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10/6/2022
 R:\TrafficControl\CP\N-5751\TC-TMP_piii.dtl.05.dgn
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APPROVED: _____ DATE: _____ 		<p style="text-align: center;">PHASE III DETAIL</p>
<p>DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED</p>		