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 p:\gfnnet-pw.bentley.com:gfnnet-pw-01\Documents\Projects\67945\5-Work\King\Task 010 - I-5955 Greensboro Rehab\Structures\CAD\2.0 100%\Plans\404_003_I-5955_SMU_GD02_002_400327
 I-5955 Greensboro Rehab\Structures\CAD\2.0 100%\Plans\404_003_I-5955_SMU_GD02_002_400327
 I-5955 Greensboro Rehab\Structures\CAD\2.0 100%\Plans\404_003_I-5955_SMU_GD02_002_400327
 I-5955 Greensboro Rehab\Structures\CAD\2.0 100%\Plans\404_003_I-5955_SMU_GD02_002_400327



LOCATION SKETCH

INFORMATION INDICATED ON THE LOCATION SKETCH SHALL BE CONSIDERED GENERAL INFORMATION ONLY. CONTRACTOR SHALL CONFIRM, THROUGH OTHER SOURCES, SPECIFIC INFORMATION REGARDING BRIDGES, ROADWAY, UTILITIES, THE SURROUNDING AREA, AND ANY OTHER ASPECTS THAT MAY BE NECESSARY TO PERFORM AND COMPLETE THE PROJECT.

BRIDGE COORDINATES	
LATITUDE	LONGITUDE
36°-01'-57.05"	79°-48'-49.94"

GENERAL NOTES

SEE TRANSPORTATION MANAGEMENT PLANS FOR LANE WIDTHS, SEQUENCING AND OTHER TRAFFIC CONTROL MEASURES FOR STAGING OF OVERLAY SURFACE PREPARATION AND POLYMER CONCRETE (PC) PLACEMENT.

THE CONTRACTOR'S ATTENTION IS CALLED TO THE FACT THAT DUE TO THE NATURE OF PRESERVATION PROJECTS, THE EXTENT OF WORK CANNOT ALWAYS BE ACCURATELY DETERMINED PRIOR TO COMMENCEMENT OF WORK. REPAIR LOCATIONS AND ESTIMATES OF QUANTITIES ARE GIVEN WITH THE BEST INFORMATION AVAILABLE. IF ADDITIONAL REPAIRS NOT SHOWN ON THE DRAWINGS ARE DEEMED NECESSARY BY THE ENGINEER, THE ENGINEER SHALL NOTE ON THE DRAWINGS THE APPROXIMATE LOCATION AND DESCRIPTION OF THE REPAIR.

EXISTING DIMENSIONS AND BRIDGE CONDITION ARE FROM THE BEST INFORMATION AVAILABLE. THE CONTRACTOR SHALL FIELD VERIFY THE INFORMATION SHOWN ON THE PLANS AND NOTIFY THE ENGINEER IF ACTUAL DIMENSIONS AND CONDITIONS DIFFER.

THE CONTRACTOR SHALL HAVE NO CLAIM WHATSOEVER AGAINST THE DEPARTMENT FOR ANY DELAYS OR ADDITIONAL COST INCURRED BASED ON DIFFERENCES BETWEEN WHAT IS SHOWN ON THE PLANS AND THE ACTUAL CONDITIONS AT THE PROJECT SITE.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO FOLLOW ALL STATE AND FEDERAL SAFETY REQUIREMENTS.

WORK ON THE BRIDGE SHALL BE PERFORMED SO AS NOT TO ALLOW DEBRIS TO FALL BELOW, EXCEPT WHERE THE CONTRACTOR'S PLAN USED PLATFORMS, NETS, SCREEN OR OTHER PROTECTIVE DEVICES TO CATCH THE MATERIAL. THE CONTRACTOR SHALL SUBMIT PLANS FOR CONSTRUCTION IN ACCORDANCE WITH ARTICLE 402-2 OF THE STANDARD SPECIFICATIONS AND THE PROJECT SPECIAL PROVISIONS. ANY DAMAGE TO EXISTING REINFORCING STEEL DURING CONTRACTOR'S OPERATIONS SHALL BE REPAIRED AS DIRECTED BY THE ENGINEER AND PERFORMED AT NO ADDITIONAL COST TO THE DEPARTMENT.

FOR CONTROL OF TRAFFIC AND LIMITS OF PHASING OF CONSTRUCTION, SEE TRANSPORTATION MANAGEMENT PLANS.

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL SUBMIT FOR REVIEW AND APPROVAL A COMPLETE SEQUENCE OF TASKS FOR EACH OPERATION AFFECTING THE BRIDGE SURFACE AND/OR TRAFFIC.

FOR OTHER DESIGN DATA AND GENERAL NOTES, SEE SHEET SN.

FOR FALSEWORK AND FORMWORK, SEE SPECIAL PROVISIONS.

FOR SUBMITTAL OF WORKING DRAWINGS, SEE SPECIAL PROVISIONS.

FOR CRANE SAFETY, SEE SPECIAL PROVISIONS.

FOR GROUT FOR STRUCTURES, SEE SPECIAL PROVISIONS.

ALL PAVEMENT MARKING WILL BE IN ACCORDANCE WITH THE TRANSPORTATION MANAGEMENT PLANS.

EXISTING JOINTS AND DECK DRAINS SHALL BE SEALED PRIOR TO BEGINNING SURFACE PREPARATIONS OF THE BRIDGE DECK. THE CONTRACTOR SHALL TAKE CARE THAT ANY CONSTRUCTION DEBRIS THAT COLLECTS IN THE DRAINS IS CONTAINED. DRAINS IN SHOULDERS OF ADJACENT TRAVEL LANES SHALL BE KEPT FREE AND CLEAR OF DEBRIS.

LONGITUDINAL CONSTRUCTION JOINTS OF OVERLAYS SHALL BE LOCATED ALONG THE CENTERLINE OR EDGE OF TRAVEL LANES.

FOR SHOTBLASTING BRIDGE DECK AND CLASS II SURFACE PREPARATION, SEE OVERLAY SURFACE PREPARATION FOR POLYMER CONCRETE SPECIAL PROVISION.

FOR FINE MILLING, SEE SPECIAL PROVISIONS.

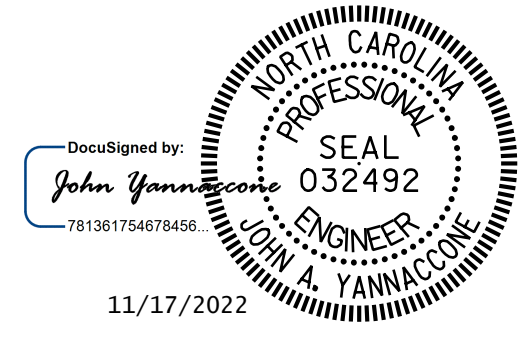
FOR CONCRETE DECK REPAIR FOR PC OVERLAY, PC MATERIALS, AND PLACING AND FINISHING PC OVERLAY, SEE POLYMER CONCRETE BRIDGE DECK OVERLAY SPECIAL PROVISION.

FOR EXPANSION JOINT SEAL REPAIR, SEE SPECIAL PROVISIONS.

FOR EPOXY COATING AND DEBRIS REMOVAL, SEE SPECIAL PROVISIONS.

PROJECT NO. I-5955
GUILFORD COUNTY
 BRIDGE NO. 400327

SHEET 2 OF 2



STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

**GENERAL DRAWING
 FOR BRIDGE ON I-40 EBL
 OVER SOUTH BUFFALO CREEK**

DRAWN BY : J. MYA DATE : 10/2022
 CHECKED BY : J. YANNACCONE DATE : 10/2022

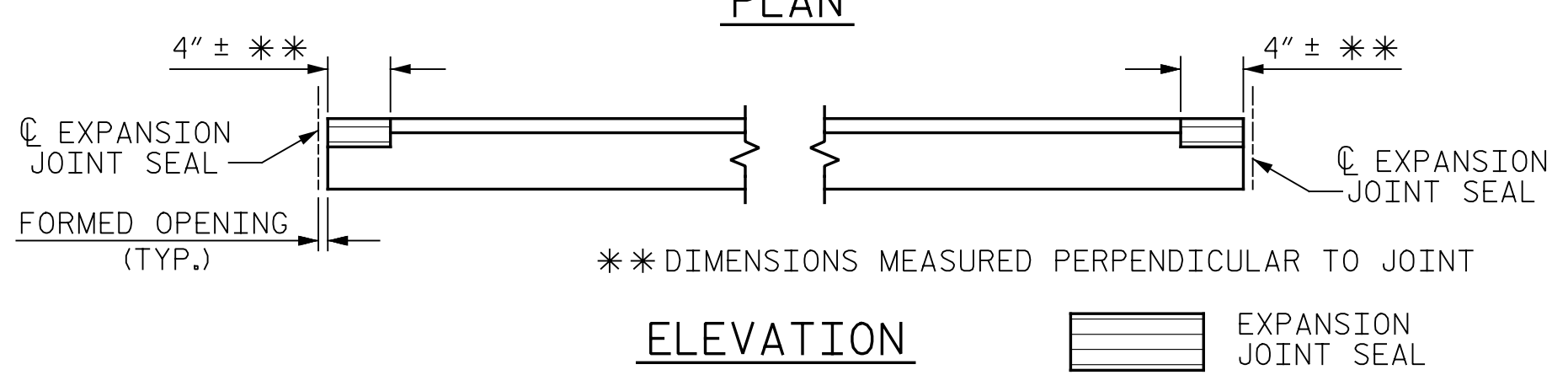
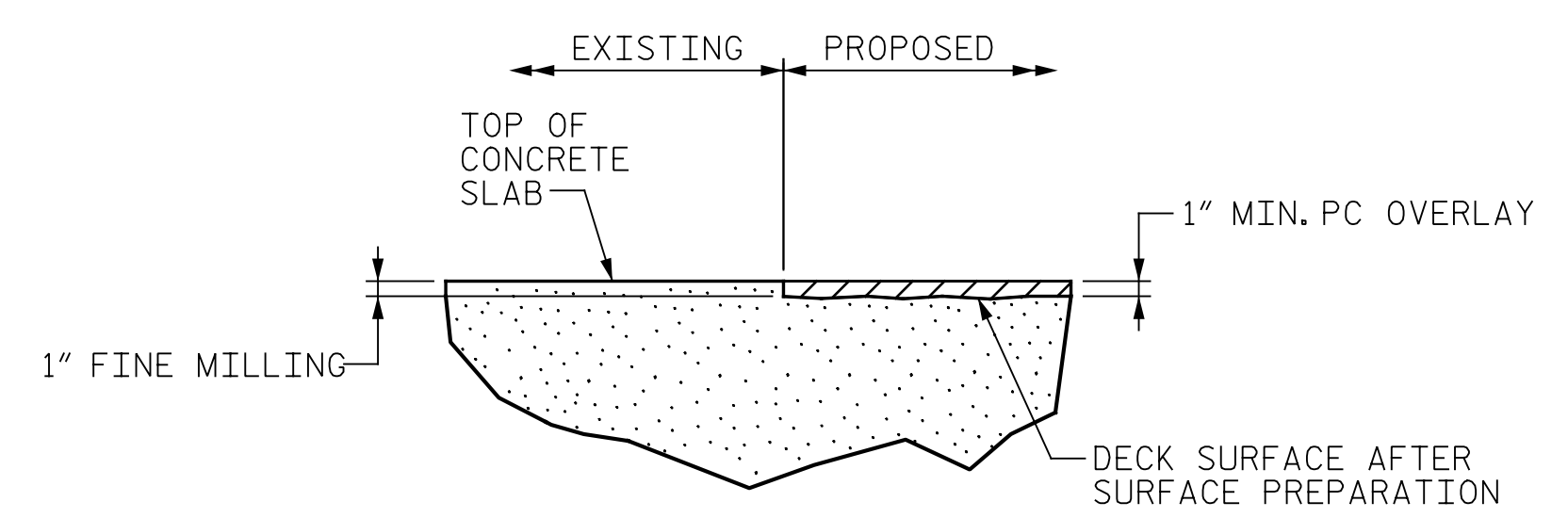
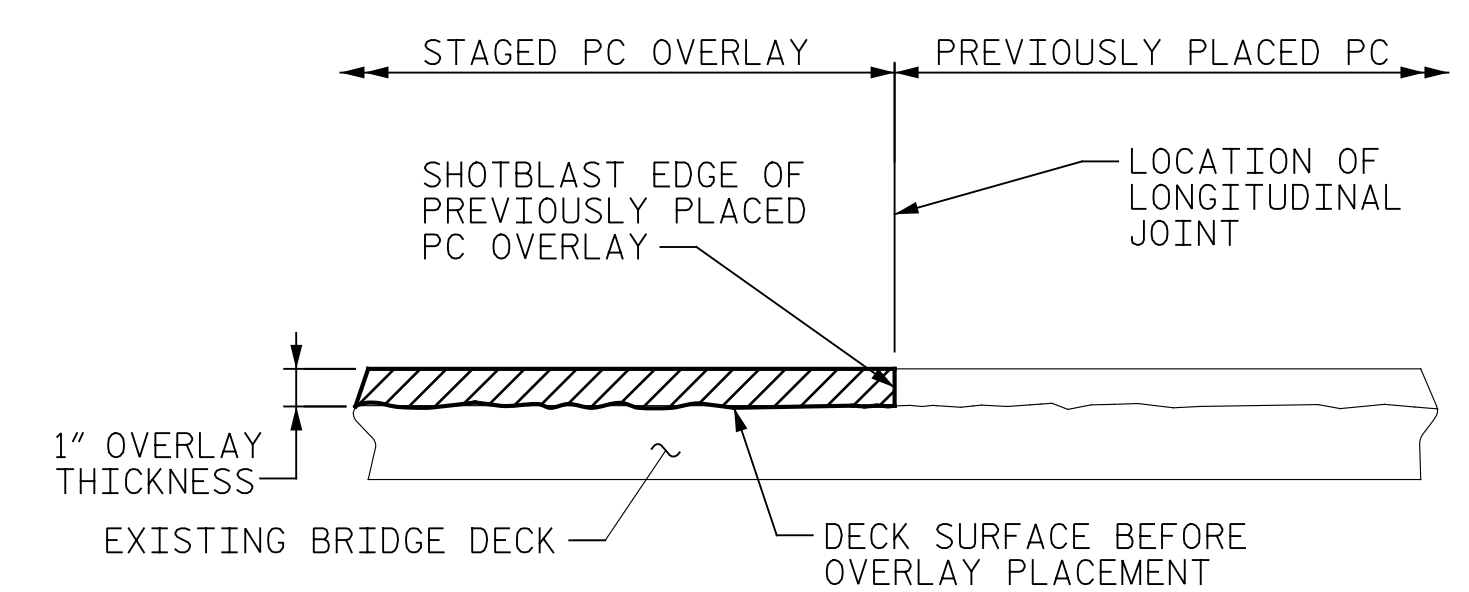
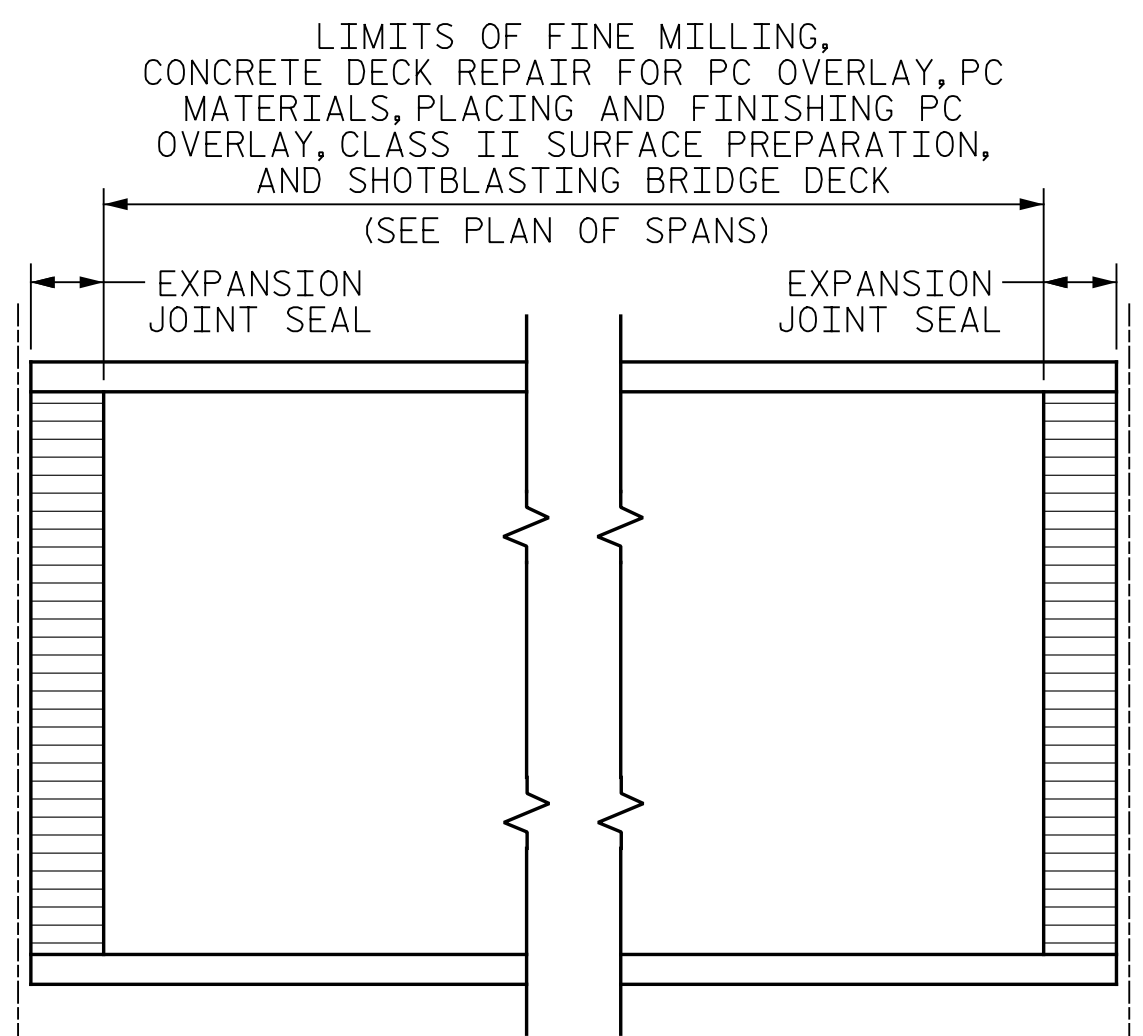
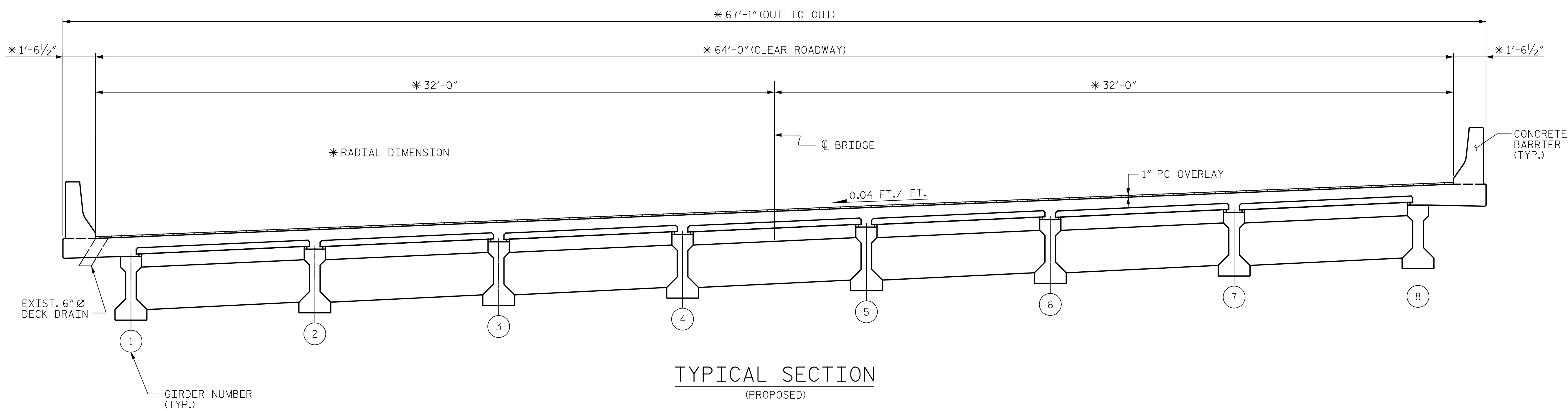


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NOTES:
 SEE TRANSPORTATION MANAGEMENT PLANS FOR LANE WIDTHS, SEQUENCING AND OTHER TRAFFIC CONTROL MEASURES FOR STAGING OF OVERLAY SURFACE PREPARATION AND PC PLACEMENT.



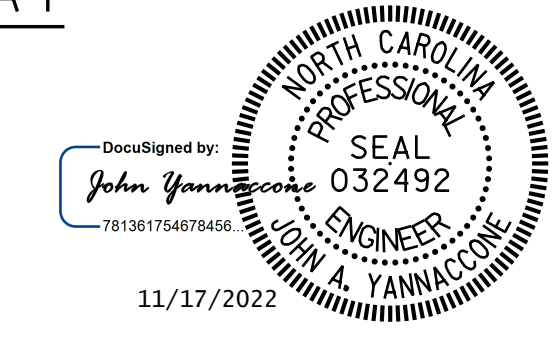
STAGED PC OVERLAY CONSTRUCTION JOINT

DETAIL OF POLYMER CONCRETE OVERLAY

PROJECT NO. I-5955
GUILFORD COUNTY
 BRIDGE NO. 400327

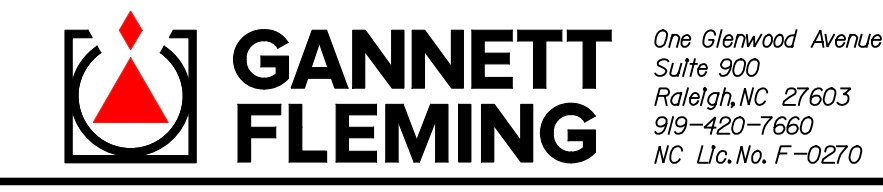
STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

TYPICAL SECTION AND SURFACE PREPARATION DETAILS



PAY LIMITS FOR OVERLAY BID ITEMS

DRAWN BY: J. HARRIS DATE: 10/2022
 CHECKED BY: J. YANNACCONE DATE: 10/2022

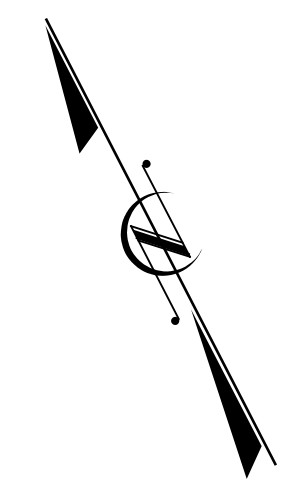
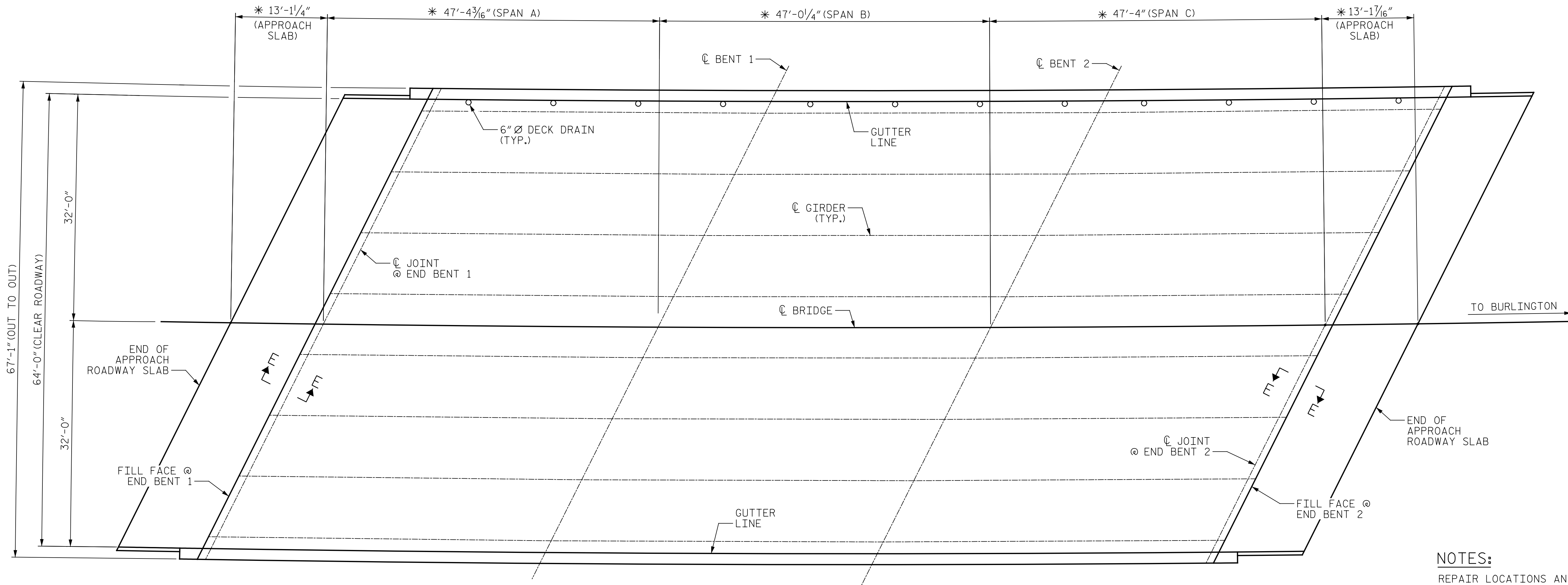


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* DIMENSIONS MEASURED ALONG ARC



APPROACH SLAB @ END BENT 1 SPAN A SPAN B SPAN C APPROACH SLAB @ END BENT 2
PLAN

NOTES:

REPAIR LOCATIONS AND ESTIMATED QUANTITIES ARE GIVEN WITH THE BEST INFORMATION AVAILABLE. IF ADDITIONAL REPAIRS NOT SHOWN ON THE DRAWINGS ARE DEEMED NECESSARY BY THE ENGINEER, THE ENGINEER WILL NOTE ON THE DRAWINGS THE APPROXIMATE LOCATIONS AND DESCRIPTION OF THE REPAIRS AND ADJUST THE ACTUAL QUANTITIES ENTERED INTO THE REPAIR QUANTITY TABLE.

PAYMENT FOR CLASS II SURFACE IS BASED UPON SQUARE YARDS OF ADDITIONAL DEMOLITION REQUIRED FOLLOWING FINE MILLING OF BRIDGE DECK. SEE "OVERLAY SURFACE PREPARATION FOR POLYMER CONCRETE" SPECIAL PROVISION.

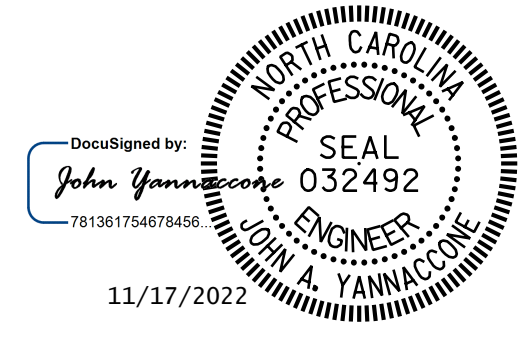
CONCRETE COVER FOR TOP BARS IN THE DECK SLAB IS 2 1/2" PER FIELD MEASUREMENTS.

FOR SECTION E-E, SEE "EXPANSION JOINT SEAL DETAILS" SHEET.

- FINE MILLING AND SHOTBLASTING OF BRIDGE DECK
- CLASS II SURFACE PREPARATION
- UNDERSIDE OF DECK REPAIR

REPAIR QUANTITY TABLE									
	TOP OF DECK REPAIR			UNDERSIDE OF DECK REPAIR					
		ESTIMATE	ACTUAL	SHOTCRETE REPAIRS		ESTIMATE		ACTUAL	
				AREA SF	VOLUME CF	AREA SF	VOLUME CF		
FINE MILLING	APPR. SLAB @ EB1	93.2 SY		UNDERSIDE OF DECK					
	SPAN A	336.7 SY							
	SPAN B	334.4 SY							
	SPAN C	336.6 SY							
CLASS II SURFACE PREPARATION	APPR. SLAB @ EB2	93.3 SY		OVERHANG DIAPHRAGMS					
	APPR. SLAB @ EB1	0.0 SY							
	SPAN A	0.0 SY							
	SPAN B	0.0 SY							
PC MATERIALS	SPAN C	0.0 SY		UNDERSIDE OF OVERHANG					
	APPR. SLAB @ EB2	0.0 SY							
	APPR. SLAB @ EB1	3.0 CY							
	SPAN A	11.6 CY							
PLACING AND FINISHING PC OVERLAY	SPAN B	11.6 CY		INTERIOR DIAPHRAGMS					
	SPAN C	11.6 CY							
	APPR. SLAB @ EB2	3.0 CY							
	APPR. SLAB @ EB1	93.2 SY							
GROOVING BRIDGE FLOORS	SPAN A	336.7 SY		UNDERSIDE EPOXY RESIN INJECTION					
	SPAN B	334.4 SY							
	SPAN C	336.6 SY							
	APPR. SLAB @ EB2	93.3 SY							
	APPR. SLAB @ EB1	719 SF		ESTIMATE		ACTUAL			
	SPAN A	2846 SF		SPAN A	0.0 LF				
	SPAN B	2868 SF		SPAN B	0.0 LF				
	SPAN C	2844 SF		SPAN C	0.0 LF				
	APPR. SLAB @ EB2	840 SF							

PROJECT NO. I-5955
GUILFORD COUNTY
 BRIDGE NO. 400327



STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

PLAN OF SPANS

DRAWN BY: J. MYA DATE: 10/2022
 CHECKED BY: J. YANNACCONE DATE: 10/2022



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LOCATION SKETCH

INFORMATION INDICATED ON THE LOCATION SKETCH SHALL BE CONSIDERED GENERAL INFORMATION ONLY. CONTRACTOR SHALL CONFIRM, THROUGH OTHER SOURCES, SPECIFIC INFORMATION REGARDING BRIDGES, ROADWAY, UTILITIES, THE SURROUNDING AREA, AND ANY OTHER ASPECTS THAT MAY BE NECESSARY TO PERFORM AND COMPLETE THE PROJECT.

BRIDGE COORDINATES	
LATITUDE	LONGITUDE
36°-02'-26.63"	79°-46'-43.36"

GENERAL NOTES

SEE TRANSPORTATION MANAGEMENT PLANS FOR LANE WIDTHS, SEQUENCING AND OTHER TRAFFIC CONTROL MEASURES FOR STAGING OF ASPHALT WEARING SURFACE (AWS).

FOR NEW ASPHALT PLACEMENT, SEE STANDARD SPECIFICATIONS.

THE CONTRACTOR'S ATTENTION IS CALLED TO THE FACT THAT DUE TO THE NATURE OF PRESERVATION PROJECTS, THE EXTENT OF WORK CANNOT ALWAYS BE ACCURATELY DETERMINED PRIOR TO COMMENCEMENT OF WORK. REPAIR LOCATIONS AND ESTIMATES OF QUANTITIES ARE GIVEN WITH THE BEST INFORMATION AVAILABLE. IF ADDITIONAL REPAIRS NOT SHOWN ON THE DRAWINGS ARE DEEMED NECESSARY BY THE ENGINEER, THE ENGINEER SHALL NOTE ON THE DRAWINGS THE APPROXIMATE LOCATION AND DESCRIPTION OF THE REPAIRS.

EXISTING DIMENSIONS AND BRIDGE CONDITION ARE FROM THE BEST INFORMATION AVAILABLE. THE CONTRACTOR SHALL FIELD VERIFY THE INFORMATION SHOWN ON THE PLANS AND NOTIFY THE ENGINEER IF ACTUAL DIMENSIONS AND CONDITIONS DIFFER.

THE CONTRACTOR SHALL HAVE NO CLAIM WHATSOEVER AGAINST THE DEPARTMENT FOR ANY DELAYS OR ADDITIONAL COST INCURRED BASED ON DIFFERENCES BETWEEN WHAT IS SHOWN ON THE PLANS AND THE ACTUAL CONDITIONS AT THE PROJECT SITE.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO FOLLOW ALL STATE AND FEDERAL SAFETY REQUIREMENTS.

WORK ON THE BRIDGE SHALL BE PERFORMED SO AS NOT TO ALLOW DEBRIS TO FALL BELOW, EXCEPT WHERE THE CONTRACTOR'S PLAN USES PLATFORMS, NETS, SCREENS OR OTHER PROTECTIVE DEVICES TO CATCH THE MATERIAL. THE CONTRACTOR SHALL SUBMIT PLANS FOR CONSTRUCTION IN ACCORDANCE WITH ARTICLE 402-2 OF THE STANDARD SPECIFICATIONS AND THE PROJECT SPECIAL PROVISIONS.

THE CONTRACTOR SHALL PERFORM ALL WORK WITH CARE SO THAT THE EXISTING STRUCTURE WHICH IS TO REMAIN IN PLACE WILL NOT BE DAMAGED. IF THE CONTRACTOR DAMAGES ANY PART OF THE EXISTING STRUCTURE WHICH IS TO REMAIN IN PLACE, THE DAMAGED AREA SHALL BE REPAIRED OR REPLACED IN A MANNER SATISFACTORY TO THE ENGINEER AT NO ADDITIONAL COST TO THE DEPARTMENT.

ANY DAMAGE TO EXISTING REINFORCING STEEL, DURING CONTRACTOR'S OPERATIONS, SHALL BE REPAIRED AS DIRECTED BY THE ENGINEER AND PERFORMED AT NO ADDITIONAL COST TO THE DEPARTMENT.

FOR CONTROL OF TRAFFIC AND LIMITS ON PHASING OF CONSTRUCTION, SEE TRANSPORTATION MANAGEMENT PLANS.

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL SUBMIT FOR REVIEW AND APPROVAL A COMPLETE SEQUENCE OF TASKS FOR EACH OPERATION AFFECTING THE BRIDGE SURFACE AND/OR TRAFFIC.

FOR OTHER DESIGN DATA AND GENERAL NOTES, SEE SHEET SN.

FOR SUBMITTAL OF WORKING DRAWINGS, SEE SPECIAL PROVISIONS.

FOR FALSEWORK AND FORMWORK, SEE SPECIAL PROVISIONS.

FOR CRANE SAFETY, SEE SPECIAL PROVISIONS.

FOR GROUT FOR STRUCTURES, SEE SPECIAL PROVISIONS.

FOR MAINTENANCE AND PROTECTION OF TRAFFIC BENEATH EXISTING STRUCTURE, SEE SPECIAL PROVISIONS.

ALL PAVEMENT MARKING WILL BE IN ACCORDANCE WITH THE TRANSPORTATION MANAGEMENT PLANS.

EXISTING JOINTS AND DECK DRAINS SHALL BE SEALED PRIOR TO BEGINNING SURFACE PREPARATIONS OF THE BRIDGE DECK. THE CONTRACTOR SHALL TAKE CARE THAT ANY CONSTRUCTION DEBRIS THAT COLLECTS IN THE DRAINS IS CONTAINED. DRAINS IN SHOULDERS OF ADJACENT TRAVEL LANE(S) SHALL BE KEPT FREE AND CLEAR OF DEBRIS.

LONGITUDINAL CONSTRUCTION JOINTS OF OVERLAYS SHALL BE LOCATED ALONG THE CENTERLINE OR EDGE OF TRAVEL LANES.

THE CONTRACTOR SHALL TAKE CARE DURING JOINT REHAB OPERATIONS NOT TO DROP ANY MATERIAL BELOW THE BRIDGE WITHOUT PROTECTIVE DEVICES BELOW TO CATCH THE MATERIAL. ANY MATERIAL THAT FALLS BELOW THE BRIDGE SHALL BE CONTAINED, REMOVED AND DISPOSED OF BY THE CONTRACTOR AT NO EXTRA COST TO THE DEPARTMENT. IF THE ENGINEER DETERMINES THAT PROTECTIVE DEVICES ARE NOT ADEQUATE OR NOT BEING EMPLOYED, THE WORK SHALL BE SUSPENDED UNTIL ADEQUATE PROTECTION IS PROVIDED.

FOR ASPHALT PLUG JOINT FOR PRESERVATION, SEE SPECIAL PROVISIONS.

FOR CONCRETE REPAIRS, SEE SPECIAL PROVISIONS.

FOR SHOTCRETE REPAIRS, SEE SPECIAL PROVISIONS.

FOR EPOXY RESIN INJECTION, SEE SPECIAL PROVISIONS.

FOR EPOXY COATING AND DEBRIS REMOVAL, SEE SPECIAL PROVISIONS.

FOR CONCRETE DECK REPAIR FOR ASPHALT OVERLAY, SEE SPECIAL PROVISIONS.

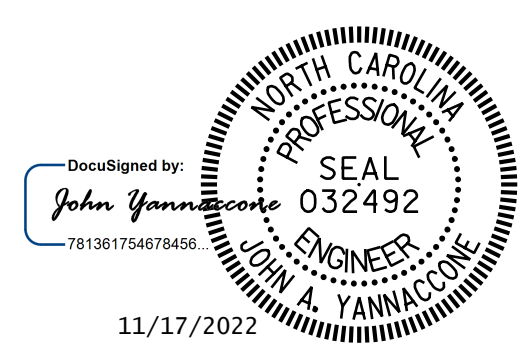
FOR CLASS IB SURFACE PREPARATION AND ASPHALT OVERLAY ON THE BRIDGE DECK, SEE BRIDGE DECK ASPHALT OVERLAY SPECIAL PROVISION.

AFTER MILLING THE ASPHALT WEARING SURFACE OF THE BRIDGE DECK, THE ENGINEER WILL LOCATE AND MARK DECK REPAIR AREAS. AREAS MARKED FOR DECK REPAIR SHALL BE PREPARED AND REPAIRED IN ACCORDANCE WITH THE CONCRETE DECK REPAIR FOR ASPHALT OVERLAY SPECIAL PROVISION.

THE ASPHALT PLUG JOINTS FOR PRESERVATION SHALL BE INSTALLED AFTER PLACEMENT OF THE PROPOSED ASPHALT WEARING SURFACE.

PROJECT NO. I-5955
GUILFORD COUNTY
 BRIDGE NO. 400329

SHEET 2 OF 2



STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

GENERAL DRAWING
 FOR BRIDGE ON PATTON AVENUE
 OVER I-40

DRAWN BY : J. MYA DATE : 10/2022
 CHECKED BY : J. YANNACCONE DATE : 10/2022

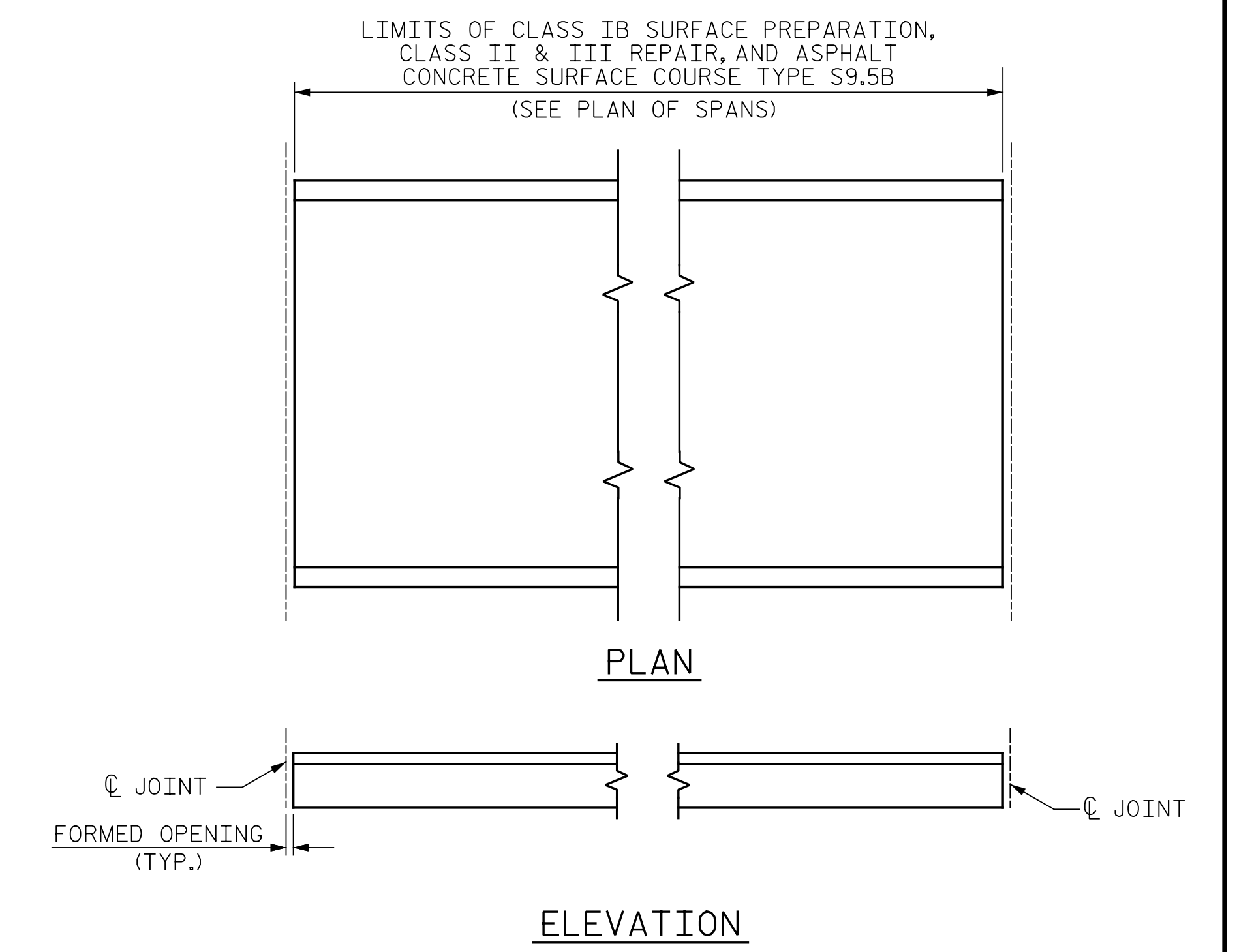
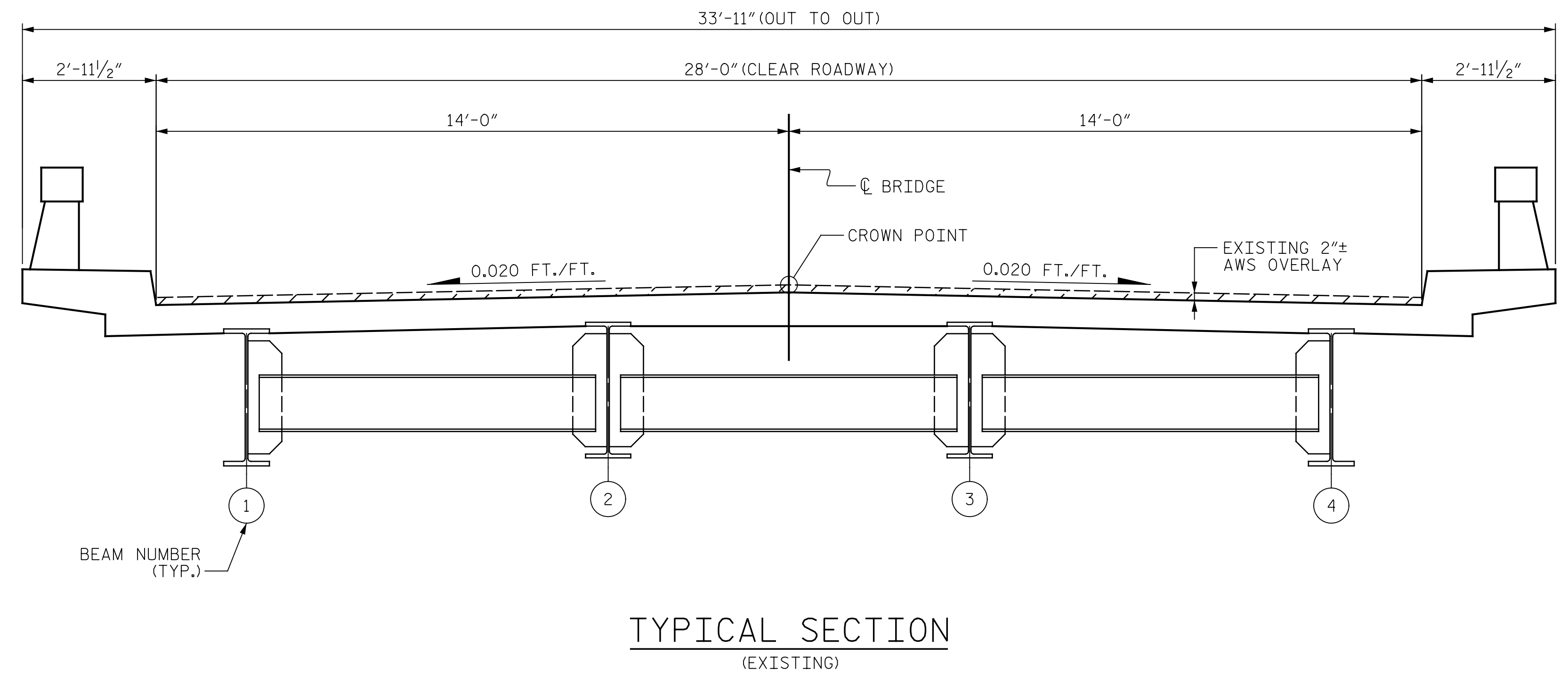


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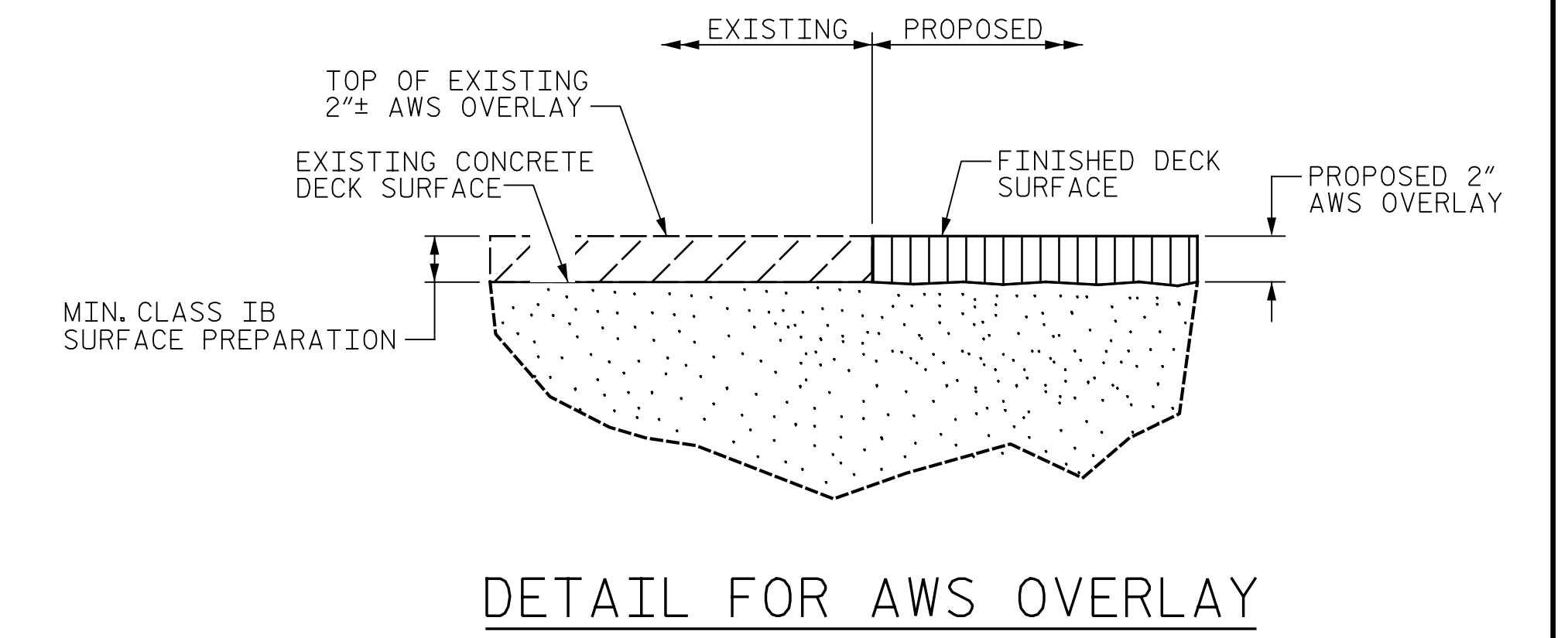
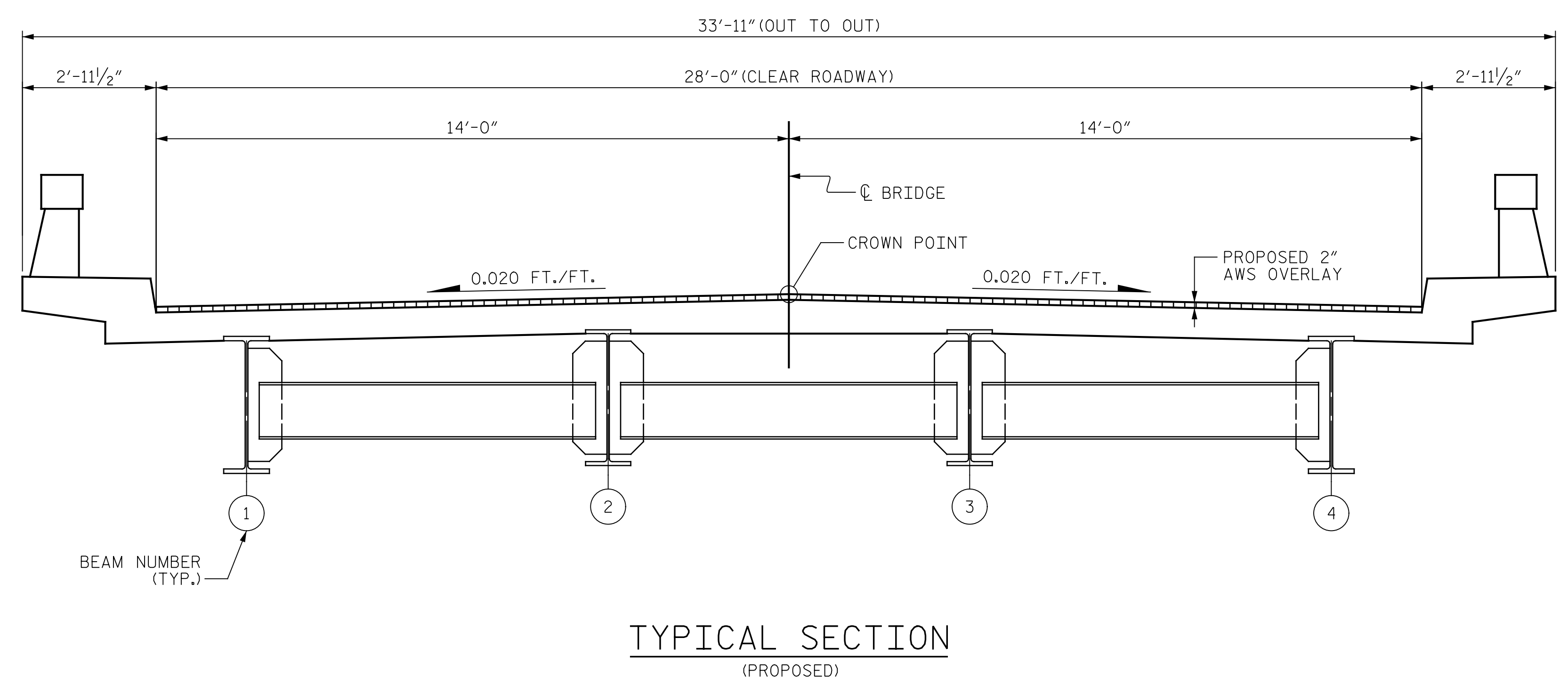
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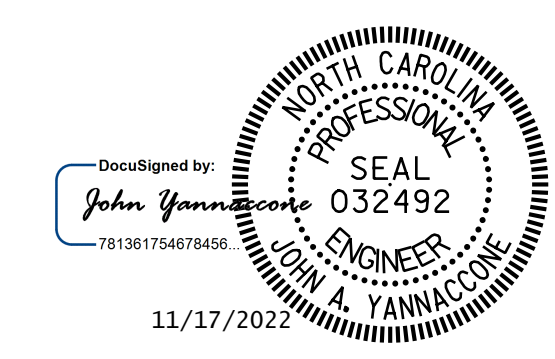
NOTES:
 SEE TRAFFIC MANAGEMENT PLANS FOR LANE WIDTHS, SEQUENCING, AND OTHER TRAFFIC CONTROL MEASURES FOR STAGING OF OVERLAY SURFACE PREPARATION AND ASPHALT WEARING SURFACE (AWS) PLACEMENT.



PAY LIMITS FOR OVERLAY BID ITEMS



PROJECT NO. I-5955
GUILFORD COUNTY
 BRIDGE NO. 400329



STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

**TYPICAL SECTION AND
 SURFACE PREPARATION
 DETAILS**

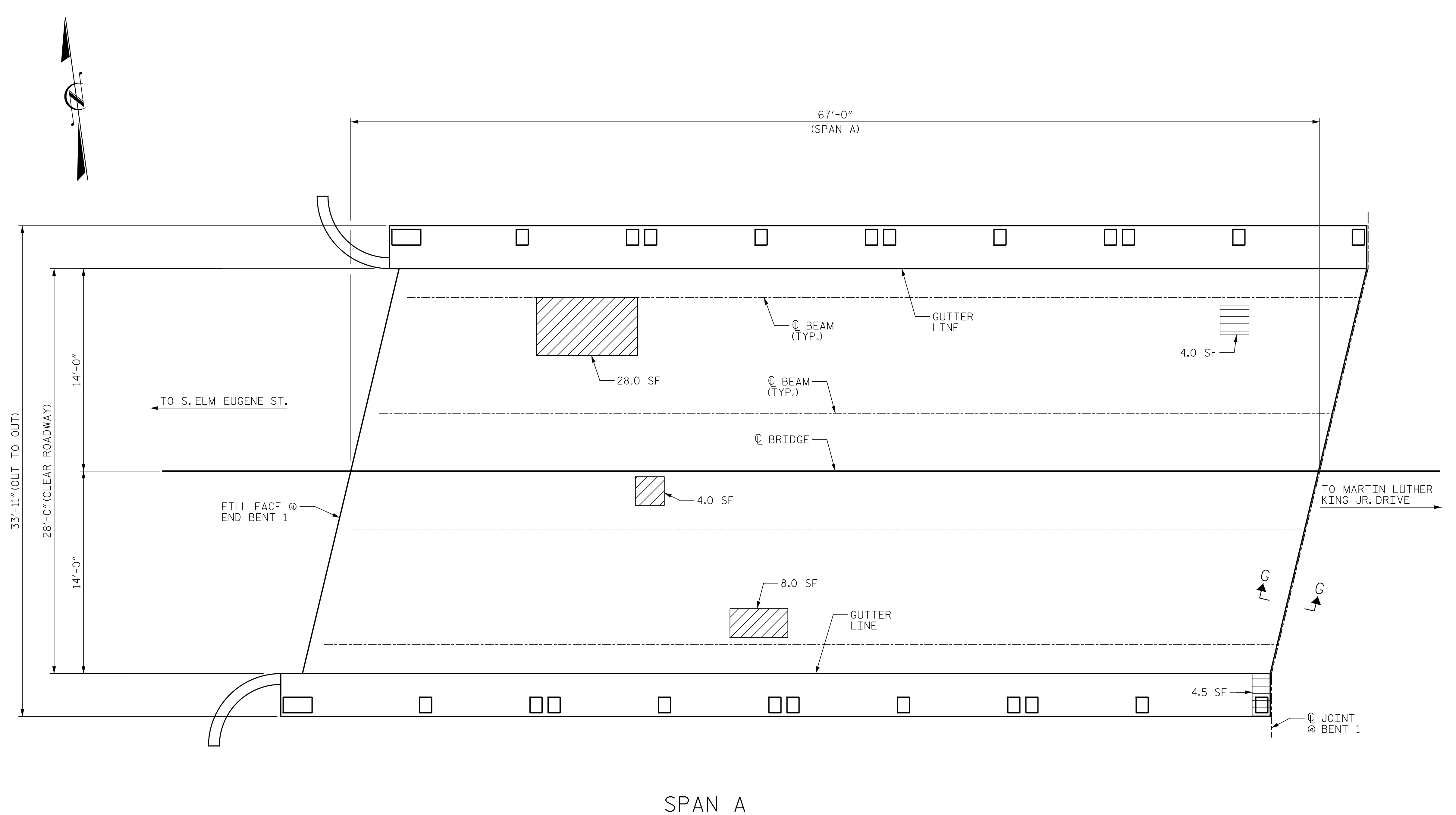
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REPAIR QUANTITY TABLE				
TOP OF DECK REPAIR				
	ESTIMATE		ACTUAL	
CLASS IB SURFACE PREPARATION		208 SY		
CONCRETE DECK REPAIR FOR ASPHALT OVERLAY		40.0 SF		
ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B		23 TONS		
ASPHALT BINDER FOR PLANT MIX		2 TONS		
UNDERSIDE OF DECK REPAIR				
SHOTCRETE REPAIRS	ESTIMATE		ACTUAL	
	AREA SF	VOLUME CF	AREA SF	VOLUME CF
UNDERSIDE OF DECK	4.0	1.0		
OVERHANG DIAPHRAGMS	4.5	1.6		
UNDERSIDE OF OVERHANG	0.0	0.0		
INTERIOR DIAPHRAGMS	0.0	0.0		
	ESTIMATE		ACTUAL	
UNDERSIDE EPOXY RESIN INJECTION		0.0 LF		

- CLASS IB SURFACE PREPARATION
- CONCRETE DECK REPAIR FOR ASPHALT OVERLAY
- UNDERSIDE OF DECK REPAIR

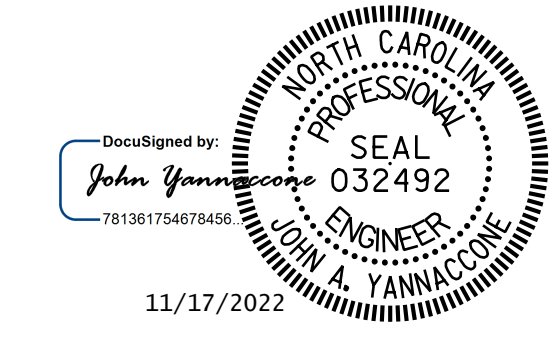
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- CONCRETE COVER FOR TOP BARS IN THE DECK SLAB IS 1 7/8" ± PER THE EXISTING BRIDGE PLANS.
- FOR SECTION G-G, SEE "ASPHALT PLUG JOINT DETAILS" SHEET.
- FOR CONCRETE DECK REPAIR FOR ASPHALT OVERLAY, SEE SPECIAL PROVISIONS.
- CONTRACTOR SHALL SAWCUT TO A NOMINAL DEPTH OF 1/2" BUT REINFORCING STEEL SHALL NOT BE DAMAGED. CONTRACTOR SHALL REMOVE SURFACE CONCRETE TO VERIFY THAT SAWCUT DEPTH WILL NOT DAMAGE EXISTING REINFORCING STEEL.
- FOR OVERHANG AND UNDERSIDE OF DECK REPAIRS, SEE "OVERHANG AND UNDERSIDE REPAIR DETAILS" SHEET.
- FOR CONCRETE DIAPHRAGM REPAIR, SEE SPECIAL PROVISIONS.
- FOR CLASS IB SURFACE PREPARATION, SEE BRIDGE DECK ASPHALT OVERLAY SPECIAL PROVISION.

PROJECT NO. I-5955
GUILFORD COUNTY
 BRIDGE NO. 400329
 SHEET 1 OF 4

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

**PLAN OF SPANS
 SPAN A**

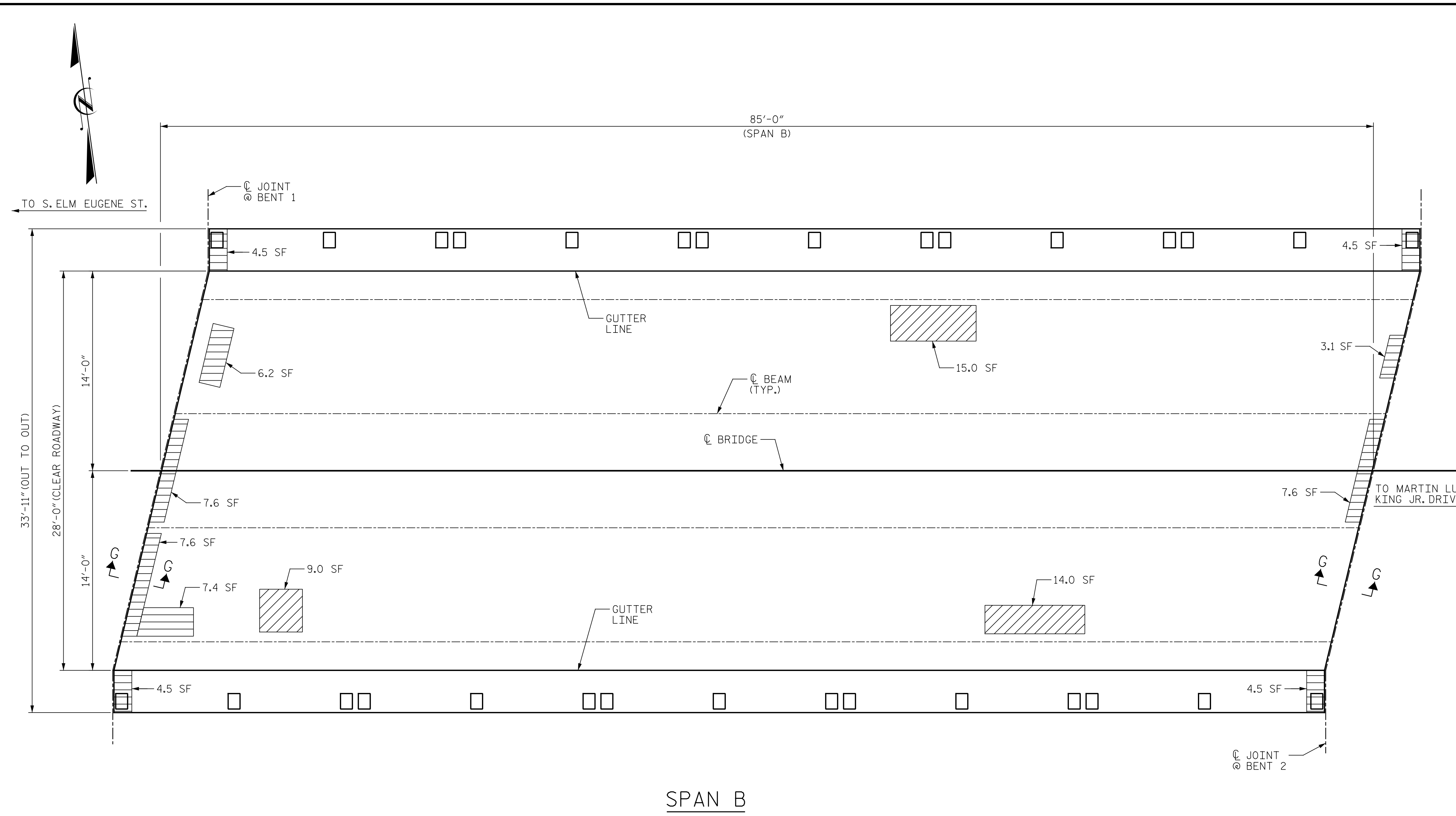


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DRAWN BY : J. MYA DATE : 10/2022
 CHECKED BY : J. YANNACCONE DATE : 10/2022

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REPAIR QUANTITY TABLE				
TOP OF DECK REPAIR				
		ESTIMATE	ACTUAL	
CLASS IB SURFACE PREPARATION		264 SY		
CONCRETE DECK REPAIR FOR ASPHALT OVERLAY		38.0 SF		
ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B		29 TONS		
ASPHALT BINDER FOR PLANT MIX		2 TONS		
UNDERSIDE OF DECK REPAIR				
		ESTIMATE	ACTUAL	
SHOTCRETE REPAIRS	AREA SF	VOLUME CF	AREA SF	VOLUME CF
UNDERSIDE OF DECK	13.6	3.4		
OVERHANG DIAPHRAGMS	18.0	6.4		
UNDERSIDE OF OVERHANG	0.0	0.0		
INTERIOR DIAPHRAGMS	25.9	16.2		
		ESTIMATE	ACTUAL	
UNDERSIDE EPOXY RESIN INJECTION		0.0 LF		

- CLASS IB SURFACE PREPARATION
- CONCRETE DECK REPAIR FOR ASPHALT OVERLAY
- UNDERSIDE OF DECK REPAIR

NOTES:

REPAIR LOCATIONS AND ESTIMATED QUANTITIES ARE GIVEN WITH THE BEST INFORMATION AVAILABLE. IF ADDITIONAL REPAIRS NOT SHOWN ON THE DRAWINGS ARE DEEMED NECESSARY BY THE ENGINEER, THE ENGINEER WILL NOTE ON THE DRAWINGS THE APPROXIMATE LOCATIONS AND DESCRIPTION OF THE REPAIRS AND ADJUST THE ACTUAL QUANTITIES ENTERED INTO THE REPAIR QUANTITY TABLE.

CONCRETE COVER FOR TOP BARS IN THE DECK SLAB IS 1 7/8" ± PER THE EXISTING BRIDGE PLANS.

FOR SECTION G-G, SEE "ASPHALT PLUG JOINT DETAILS" SHEET.

CONTRACTOR SHALL SAWCUT TO A NOMINAL DEPTH OF 1/2" BUT REINFORCING STEEL SHALL NOT BE DAMAGED. CONTRACTOR SHALL REMOVE SURFACE CONCRETE TO VERIFY THAT SAWCUT DEPTH WILL NOT DAMAGE EXISTING REINFORCING STEEL.

FOR CONCRETE DECK REPAIR FOR ASPHALT OVERLAY, SEE SPECIAL PROVISIONS.

FOR OVERHANG AND UNDERSIDE OF DECK REPAIRS, SEE "OVERHANG AND UNDERSIDE REPAIR DETAILS" SHEET.

FOR CONCRETE DIAPHRAGM REPAIR, SEE SPECIAL PROVISIONS.

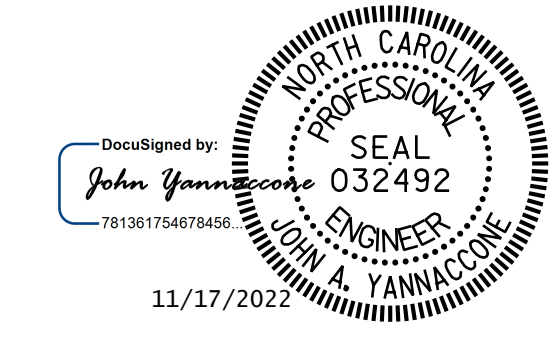
FOR CLASS IB SURFACE PREPARATION, SEE BRIDGE DECK ASPHALT OVERLAY SPECIAL PROVISION.

PROJECT NO. I-5955
GUILFORD COUNTY
 BRIDGE NO. 400329

SHEET 2 OF 4

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

PLAN OF SPANS
 SPAN B

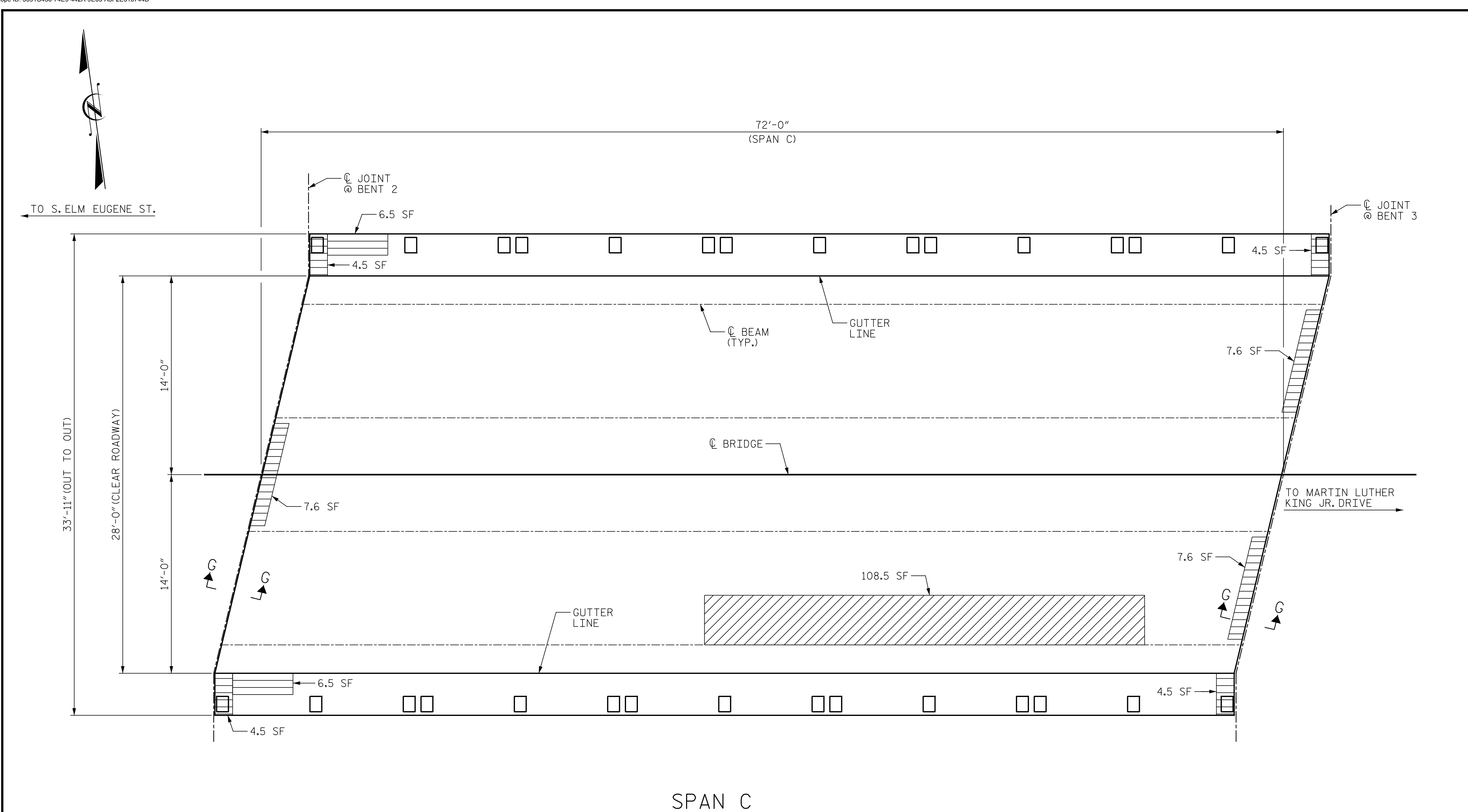


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2			4			127

DRAWN BY: J. MYA DATE: 10/2022
 CHECKED BY: J. YANNACCONE DATE: 10/2022

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CONCRETE COVER FOR TOP BARS IN THE DECK SLAB IS $1\frac{1}{8}$ " ± PER THE EXISTING BRIDGE PLANS.

FOR SECTION G-G, SEE "ASPHALT PLUG JOINT DETAILS" SHEET.

CONTRACTOR SHALL SAWCUT TO A NOMINAL DEPTH OF $\frac{1}{2}$ " BUT REINFORCING STEEL SHALL NOT BE DAMAGED. CONTRACTOR SHALL REMOVE SURFACE CONCRETE TO VERIFY THAT SAWCUT DEPTH WILL NOT DAMAGE EXISTING REINFORCING STEEL.

FOR CONCRETE DECK REPAIR FOR ASPHALT OVERLAY, SEE SPECIAL PROVISIONS.

FOR OVERHANG AND UNDERSIDE OF DECK REPAIRS, SEE "OVERHANG AND UNDERSIDE REPAIR DETAILS" SHEET.

FOR CONCRETE DIAPHRAGM REPAIR, SEE SPECIAL PROVISIONS.

FOR CLASS IB SURFACE PREPARATION, SEE BRIDGE DECK ASPHALT OVERLAY SPECIAL PROVISION.

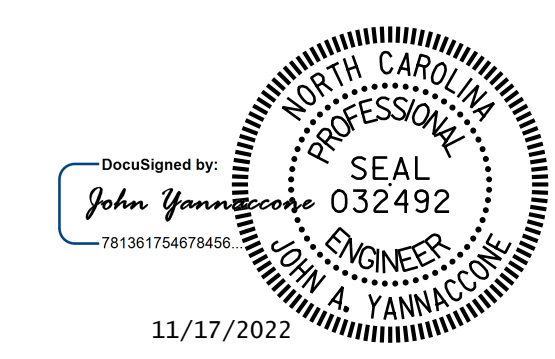
REPAIR QUANTITY TABLE				
TOP OF DECK REPAIR				
	ESTIMATE		ACTUAL	
CLASS IB SURFACE PREPARATION		224 SY		
CONCRETE DECK REPAIR FOR ASPHALT OVERLAY		108.5 SF		
ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B		25 TONS		
ASPHALT BINDER FOR PLANT MIX		2 TONS		
UNDERSIDE OF DECK REPAIR				
	ESTIMATE		ACTUAL	
SHOTCRETE REPAIRS	AREA SF	VOLUME CF	AREA SF	VOLUME CF
UNDERSIDE OF DECK	0.0	0.0		
OVERHANG DIAPHRAGMS	18.0	6.4		
UNDERSIDE OF OVERHANG	13.0	6.5		
INTERIOR DIAPHRAGMS	22.8	14.3		
	ESTIMATE		ACTUAL	
UNDERSIDE EPOXY RESIN INJECTION		0.0 LF		

- CLASS IB SURFACE PREPARATION
- CONCRETE DECK REPAIR FOR ASPHALT OVERLAY
- UNDERSIDE OF DECK REPAIR

PROJECT NO. I-5955
GUILFORD COUNTY
 BRIDGE NO. 400329

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

**PLAN OF SPANS
SPAN C**



11/17/2022

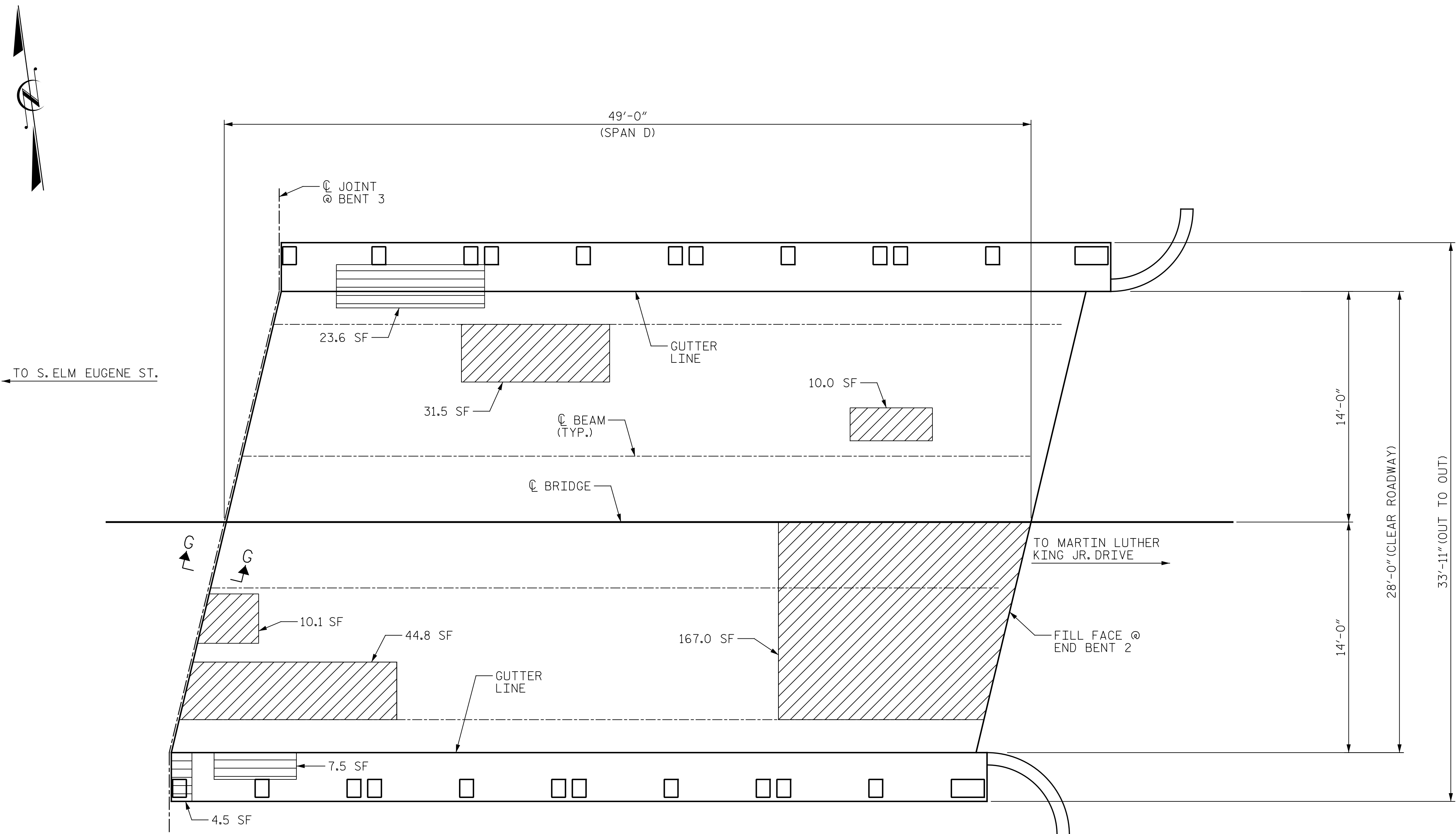
DRAWN BY: J. MYA DATE: 10/2022
 CHECKED BY: J. YANNACCONE DATE: 10/2022



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SPAN D

NOTES:

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- CONCRETE COVER FOR TOP BARS IN THE DECK SLAB IS $1\frac{1}{8}$ " ± PER THE EXISTING BRIDGE PLANS.
- FOR SECTION G-G, SEE "ASPHALT PLUG JOINT DETAILS" SHEET.
- FOR CONCRETE DECK REPAIR FOR ASPHALT OVERLAY, SEE SPECIAL PROVISIONS.
- CONTRACTOR SHALL SAWCUT TO A NOMINAL DEPTH OF $\frac{1}{2}$ " BUT REINFORCING STEEL SHALL NOT BE DAMAGED. CONTRACTOR SHALL REMOVE SURFACE CONCRETE TO VERIFY THAT SAWCUT DEPTH WILL NOT DAMAGE EXISTING REINFORCING STEEL.
- FOR OVERHANG AND UNDERSIDE OF DECK REPAIRS, SEE "OVERHANG AND UNDERSIDE REPAIR DETAILS" SHEET.
- FOR CONCRETE DIAPHRAGM REPAIR, SEE SPECIAL PROVISIONS.
- FOR CLASS IB SURFACE PREPARATION, SEE BRIDGE DECK ASPHALT OVERLAY SPECIAL PROVISION.

REPAIR QUANTITY TABLE

TOP OF DECK REPAIR				
	ESTIMATE		ACTUAL	
CLASS IB SURFACE PREPARATION	152	SY		
CONCRETE DECK REPAIR FOR ASPHALT OVERLAY	263.4	SF		
ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B	17	TONS		
ASPHALT BINDER FOR PLANT MIX	1	TONS		
UNDERSIDE OF DECK REPAIR				
SHOTCRETE REPAIRS	ESTIMATE		ACTUAL	
	AREA SF	VOLUME CF	AREA SF	VOLUME CF
UNDERSIDE OF DECK	0.0	0.0		
OVERHANG DIAPHRAGMS	4.5	1.6		
UNDERSIDE OF OVERHANG	31.1	15.6		
INTERIOR DIAPHRAGMS	0.0	0.0		
	ESTIMATE		ACTUAL	
UNDERSIDE EPOXY RESIN INJECTION	0.0	LF		

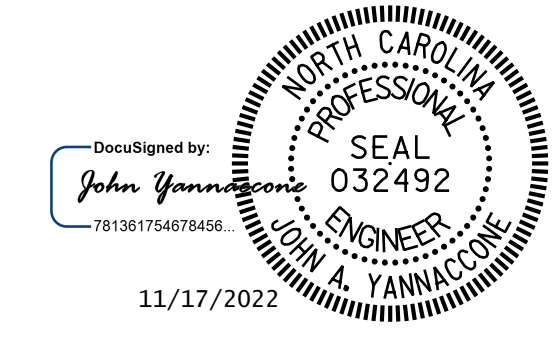
- CLASS IB SURFACE PREPARATION
- CONCRETE DECK REPAIR FOR ASPHALT OVERLAY
- UNDERSIDE OF DECK REPAIR

PROJECT NO. I-5955
GUILFORD COUNTY
 BRIDGE NO. 400329

SHEET 4 OF 4

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

PLAN OF SPANS
 SPAN D



11/17/2022

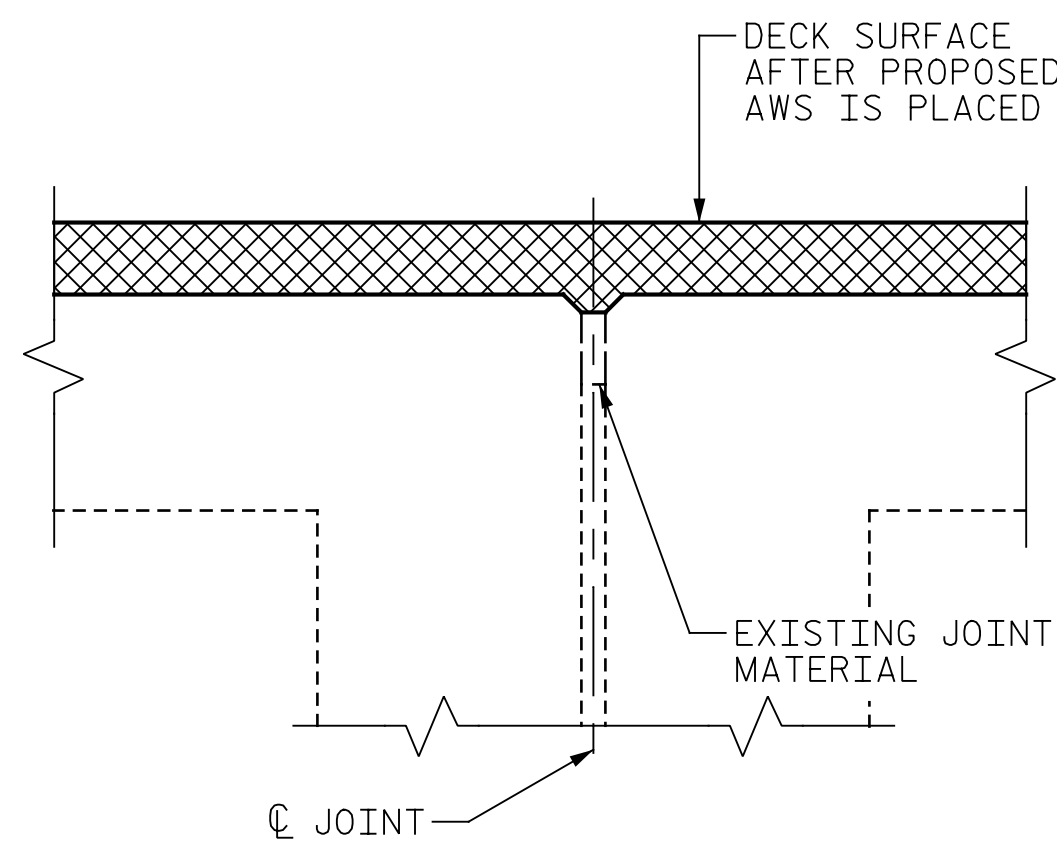


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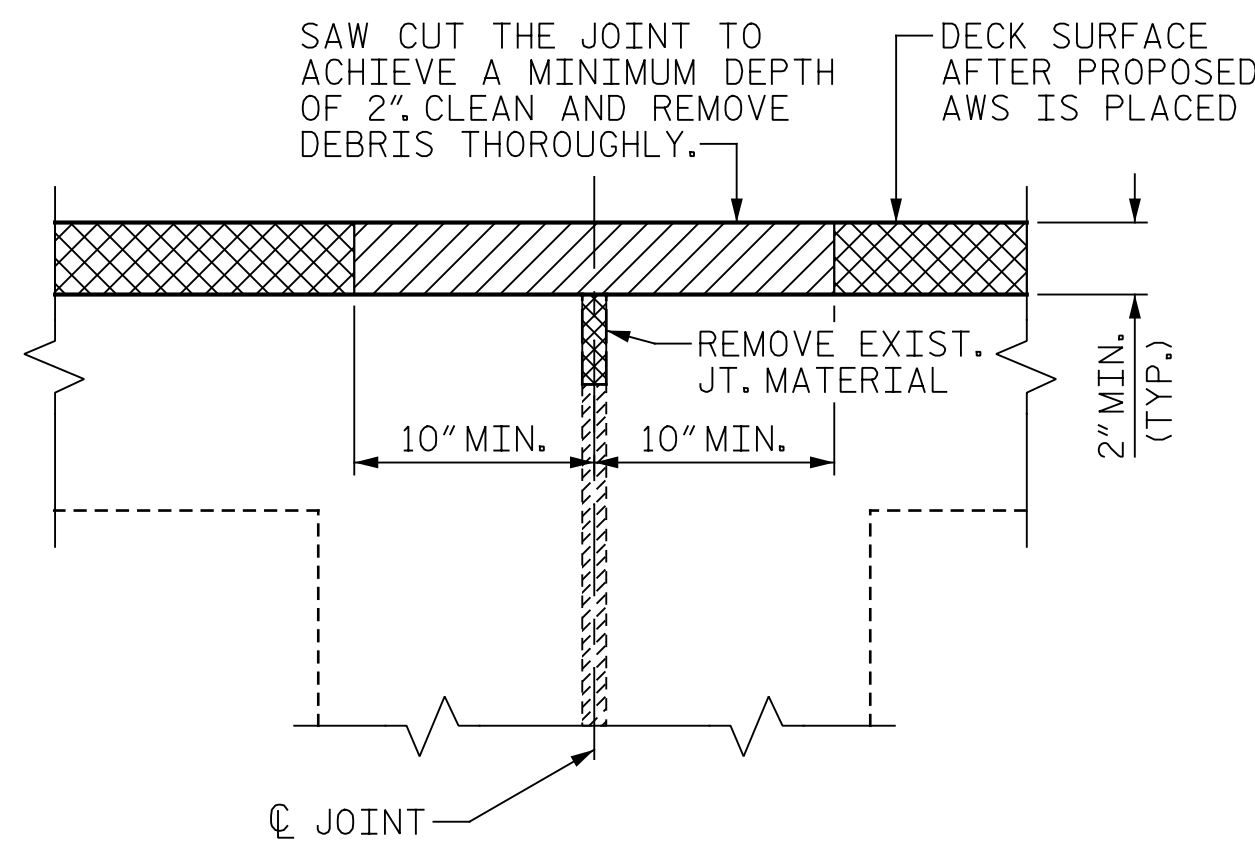
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DRAWN BY : J. MYA DATE : 10/2022
 CHECKED BY : J. YANNACCONE DATE : 10/2022

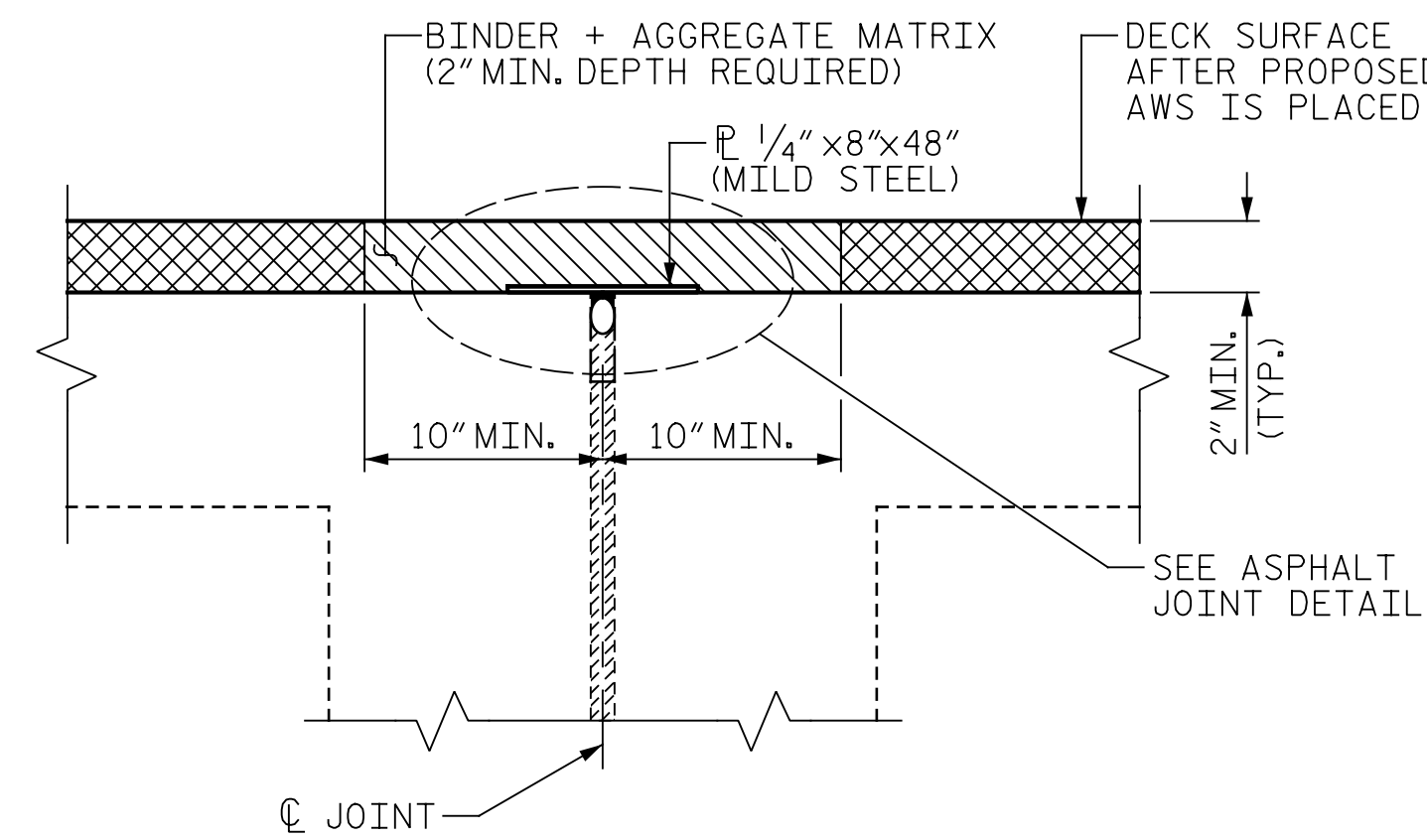
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EXISTING JOINT AFTER
AWS PLACEMENT

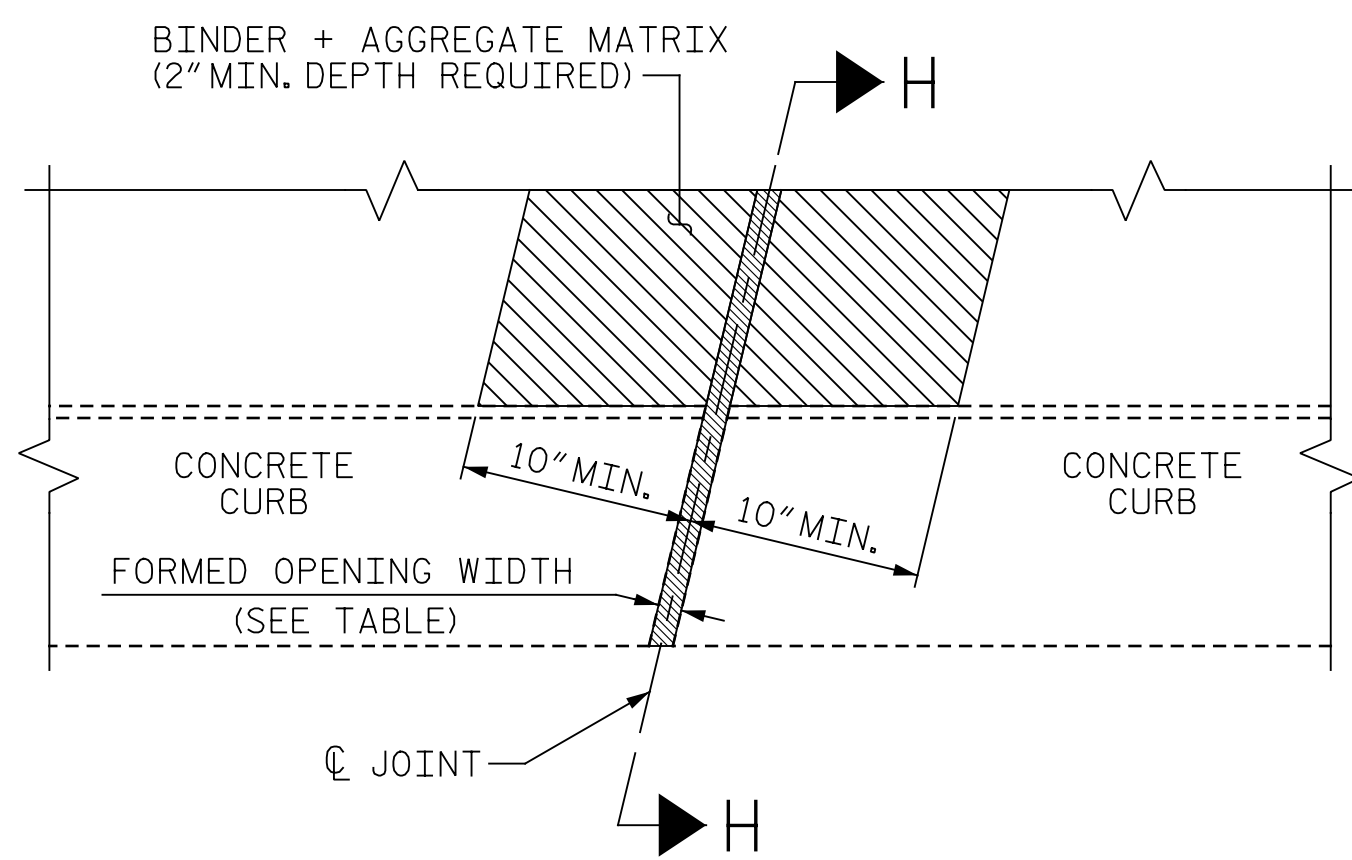


DEMOLITION PREPARATION

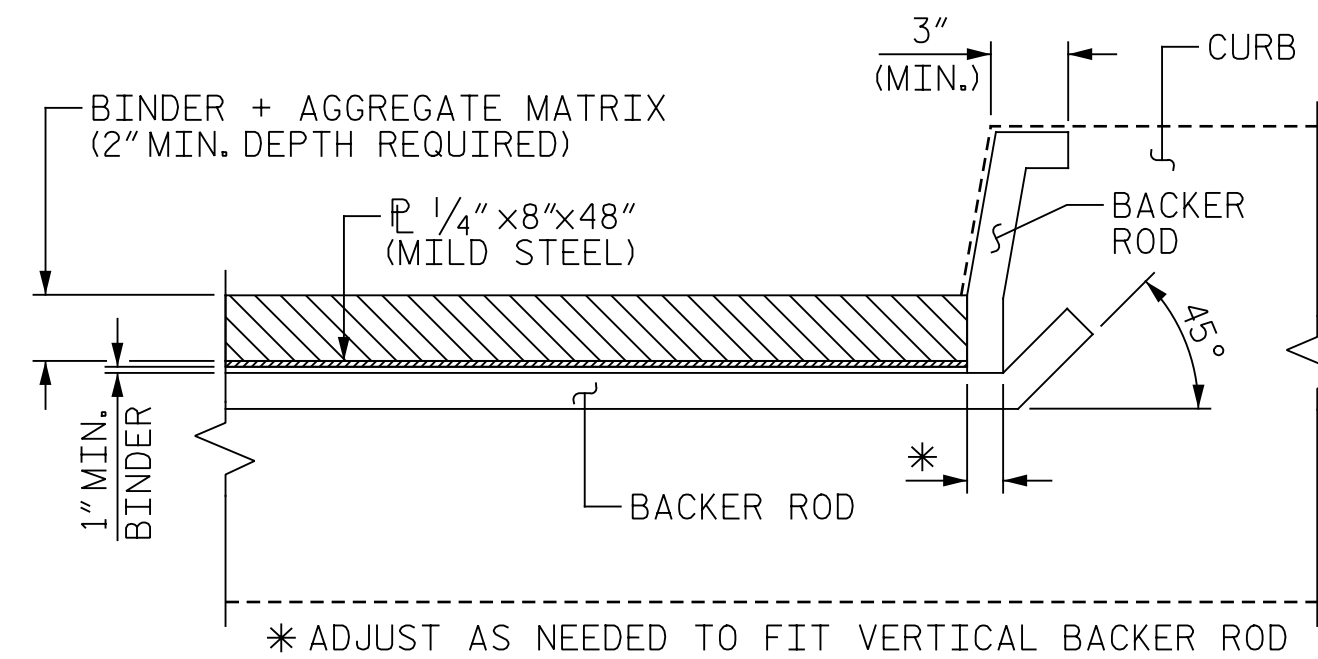


PROPOSED JOINT

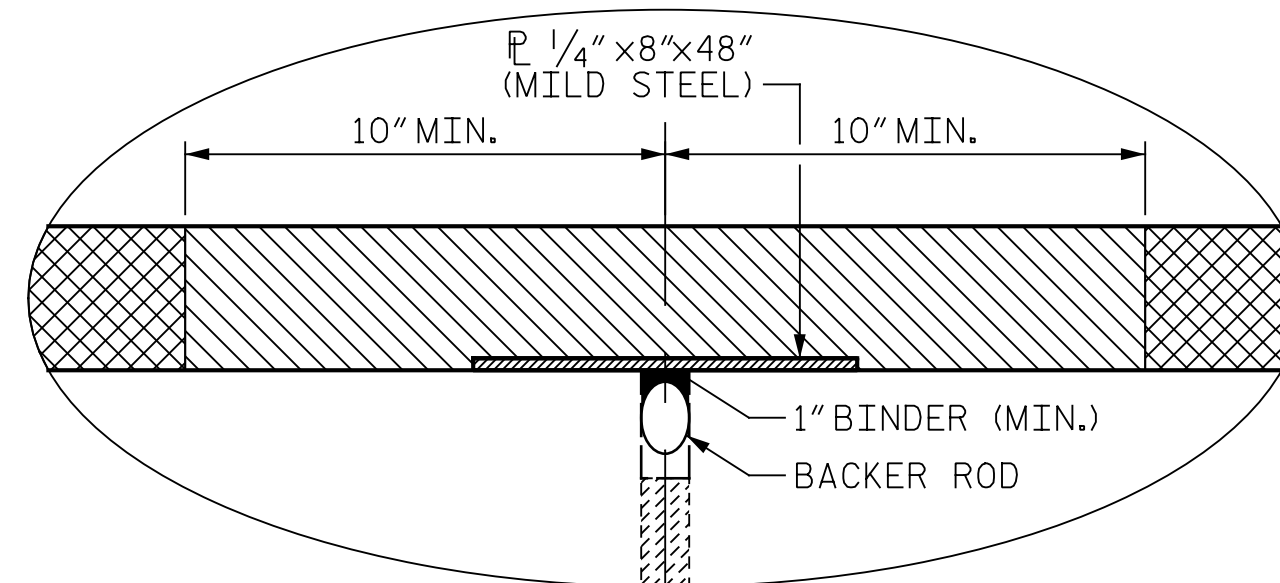
SECTION G-G
(TYP. AT BENTS)



PLAN AT BARRIER
(PROPOSED JOINT SEAL, STEEL P NOT SHOWN FOR CLARITY)



SECTION H-H
(PROPOSED JOINT SEAL)



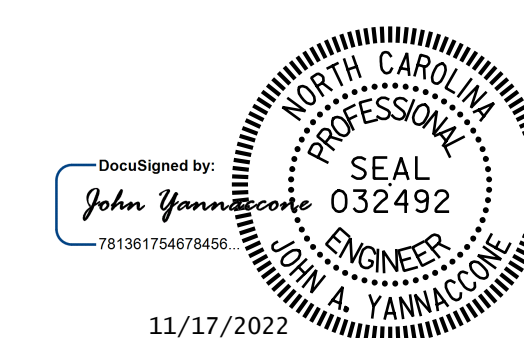
ASPHALT JOINT DETAIL

NOTES:

- THE CONTRACTOR SHALL FIELD VERIFY THE EXISTING JOINT OPENING PRIOR TO ORDERING JOINT SEAL MATERIAL. IF THE ACTUAL OPENING VARIES FROM TO OPENING INDICATED IN THE DETAIL BY MORE THAN 1/4", NOTIFY THE ENGINEER. REVISION OF THE JOINT SEAL SIZE MIGHT BE NECESSARY.
- THE CONTRACTOR SHALL TAKE CARE DURING JOINT REPAIR OPERATIONS NOT TO DROP ANY MATERIAL THAT FALLS BELOW THE BRIDGE WITHOUT PROTECTIVE DEVICES BELOW TO CATCH THE MATERIAL. ANY MATERIAL THAT FALLS BELOW THE BRIDGE SHALL BE CONTAINED, REMOVED AND DISPOSED OF BY THE CONTRACTOR AT NO EXTRA COST TO THE DEPARTMENT. IF THE ENGINEER DETERMINES THAT THE PROTECTIVE DEVICES ARE NOT ADEQUATE OR NOT BEING EMPLOYED, THE WORK SHALL BE SUSPENDED UNTIL ADEQUATE PROTECTION IS PROVIDED.
- THE CONTRACTOR WILL NOT BE PERMITTED TO FORM THE JOINTS IN LIEU OF SAWING THE JOINT.
- THE INSTALLED BACKER ROD AND SILICONE SEALANT SHALL BE WATER TIGHT.
- FOR EXCAVATION BELOW THE BOTTOM OF THE PLANNED JOINT DECK DEMOLITION, CONCRETE FOR DECK REPAIRS SHALL BE PLACED IN THE EXCAVATED AREA TO THE ELEVATION AT BOTTOM OF THE PROPOSED ASPHALT JOINT DETAIL SHOWN.
- DEMOLISH BRIDGE JOINT AREA SUCH THAT THE BOTTOM OF THE EXCAVATION SHALL BE REASONABLY FLAT AND LEVEL AND TO THE NECESSARY DEPTH, SUCH THAT ASPHALT JOINT SHALL BE FOUNDED ON CONCRETE OR REPAIR CONCRETE SUBSTRATE.
- PRIOR TO ASPHALT JOINT REPAIR/REPLACEMENT, PERFORM DECK SURFACE REPAIR IN ACCORDANCE WITH "PLAN OF SPAN" SHEETS.
- BACKER ROD SHALL BE INSTALLED AS PER MANUFACTURER'S RECOMMENDATIONS.
- FOR ASPHALT PLUG JOINTS FOR PRESERVATION, SEE SPECIAL PROVISIONS.
- FOR CONCRETE DECK REPAIR FOR ASPHALT OVERLAY, SEE SPECIAL PROVISIONS.
- SEE ROADWAY PLANS FOR MILLING AND OVERLAY OF THE BRIDGE DECK.

LOCATION	ASPHALT PLUG JOINT FOR PRESERVATION (LIN. FT.)	FORMED OPENING WIDTH PER EXISTING BRIDGE PLANS	TOTAL THEORETICAL MOVEMENT PERPENDICULAR TO THE JOINT
BENT 1	31.0	1"	1/2"
BENT 2	31.0	1"	5/8"
BENT 3	31.0	1 1/4"	15/16"

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 BRIDGE NO. 400329



STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

ASPHALT PLUG
JOINT DETAILS

DRAWN BY : J. HARRIS DATE : 10/2022
 CHECKED BY : J. YANNACCONE DATE : 10/2022



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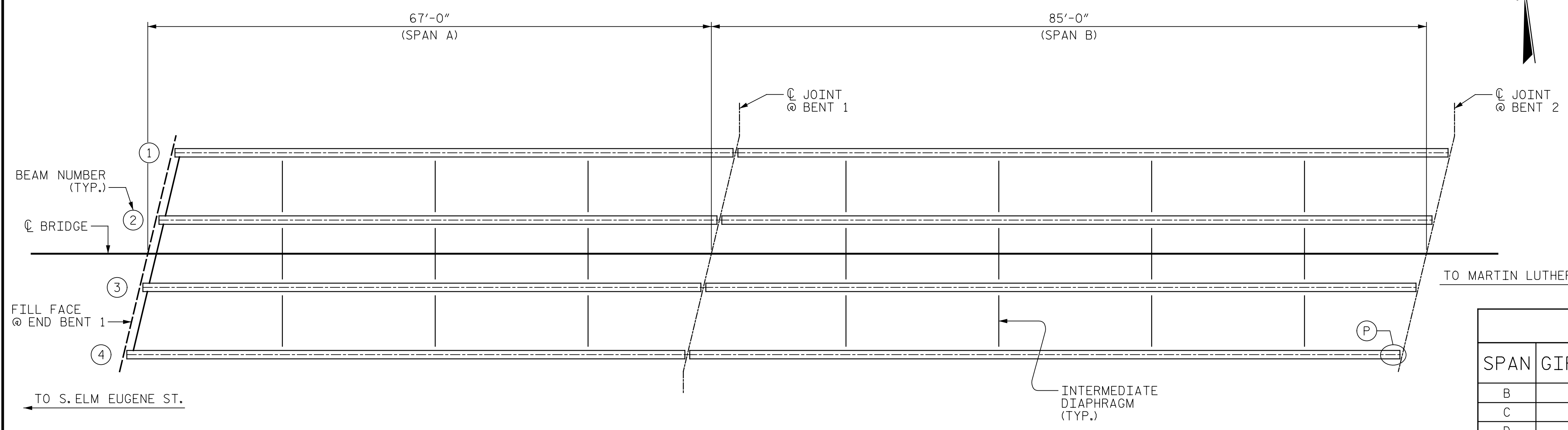
REPAIR QUANTITY TABLE		
GIRDER REPAIR		
BEAM PLATING REPAIR	ESTIMATE	ACTUAL
	120 LBS	

Ⓟ BEAM PLATING REPAIR

NOTES

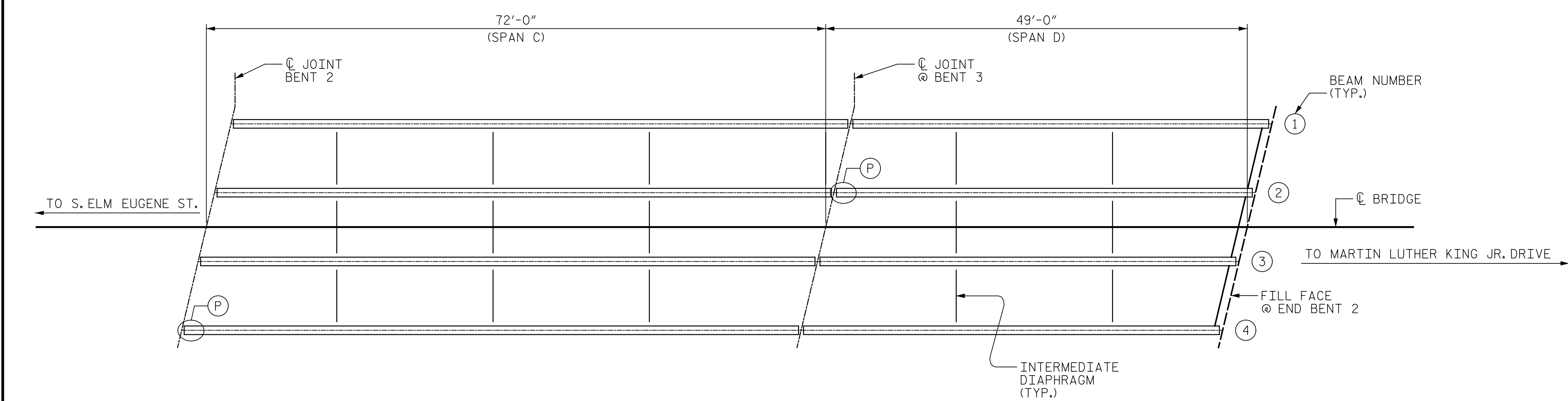
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FOR BEAM PLATING REPAIR, SEE SPECIAL PROVISIONS.
 FOR BEAM REPAIR, SEE "BEAM PLATING REPAIR DETAILS" SHEET.



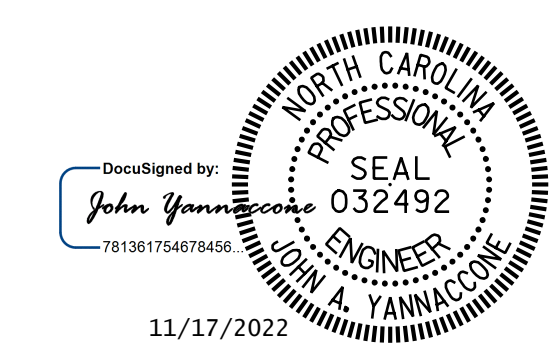
BEAM REPAIR LOCATIONS
 (OTHER LOCATIONS MAY EXIST, SEE NOTES)

ANTICIPATED REPAIR LOCATIONS						
SPAN	GIRDER	LOCATION	DIM "A"	DIM "B"	DIM "C"	DIM "D"
B	4	BENT 2	1"	10"	1"	-
C	4	BENT 2	3"	11"	23"	-
D	2	BENT 3	3"	11"	1"	-
D	2	BENT 3	3"	10"	23"	-



BEAM REPAIR LOCATIONS
 (OTHER LOCATIONS MAY EXIST, SEE NOTES)

PROJECT NO. I-5955
GUILFORD COUNTY
 BRIDGE NO. 400329



STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH
BEAM REPAIR LOCATIONS

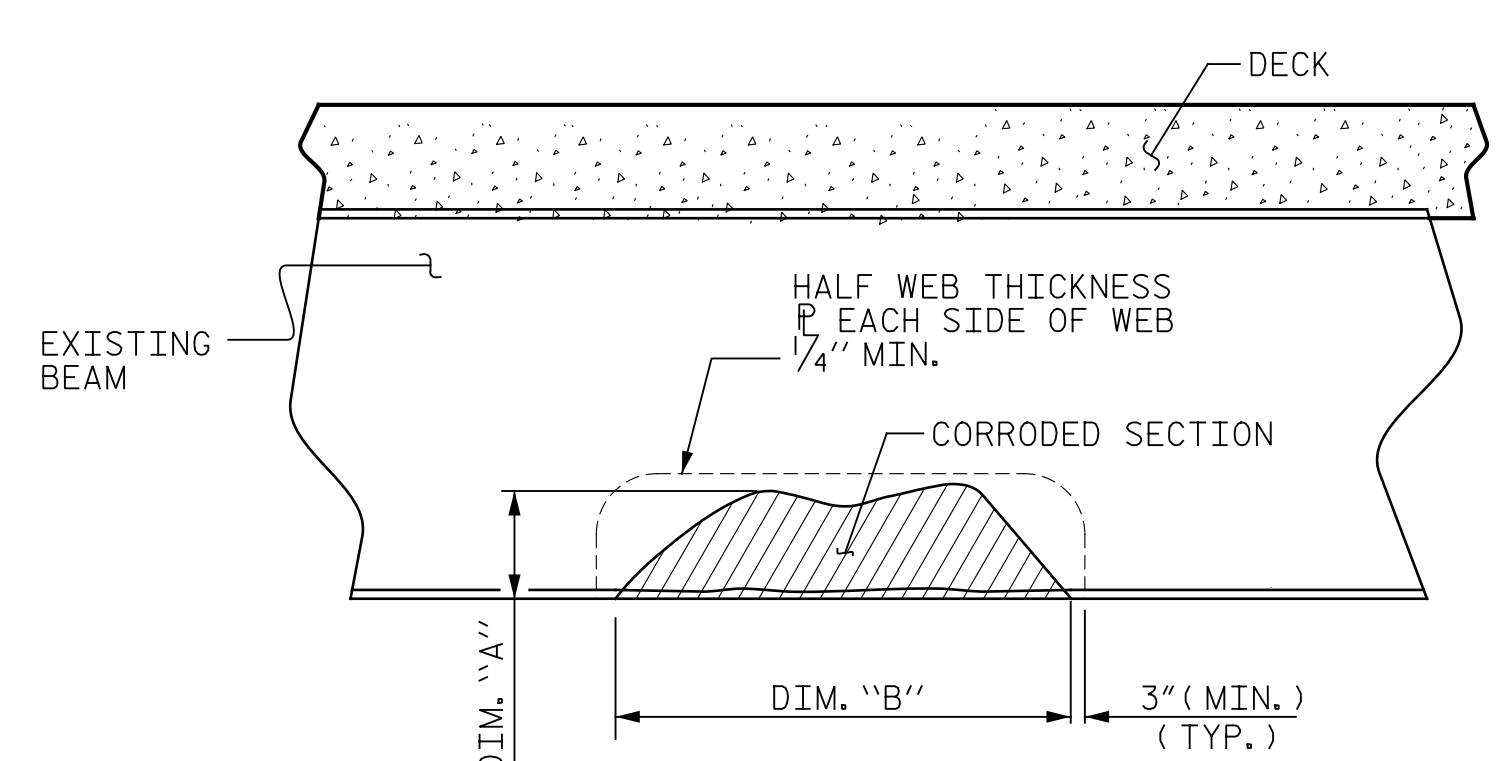
DRAWN BY: J. MYA DATE: 10/2022
 CHECKED BY: J. YANNACCONE DATE: 10/2022



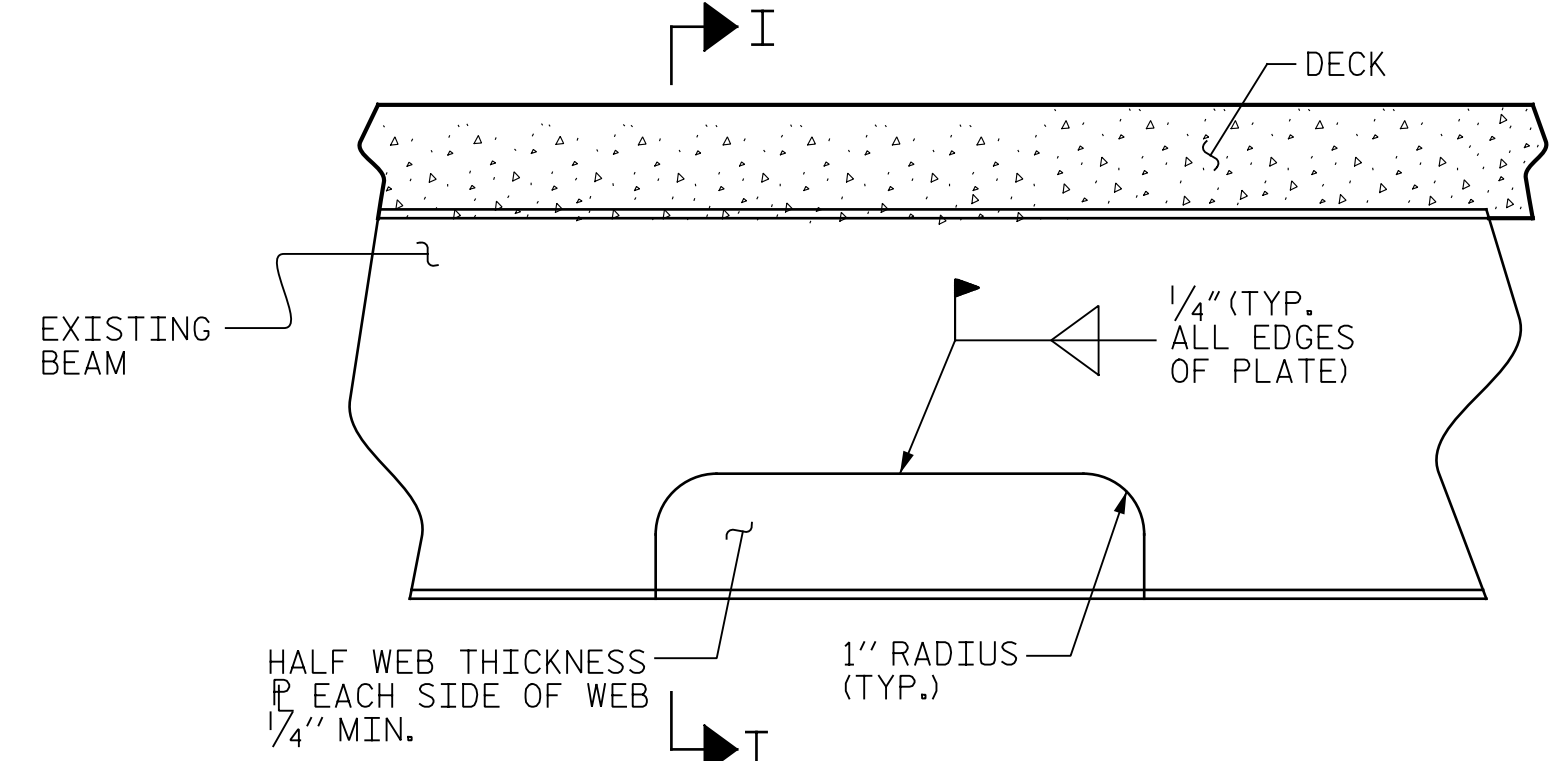
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2			4			TOTAL SHEETS 127

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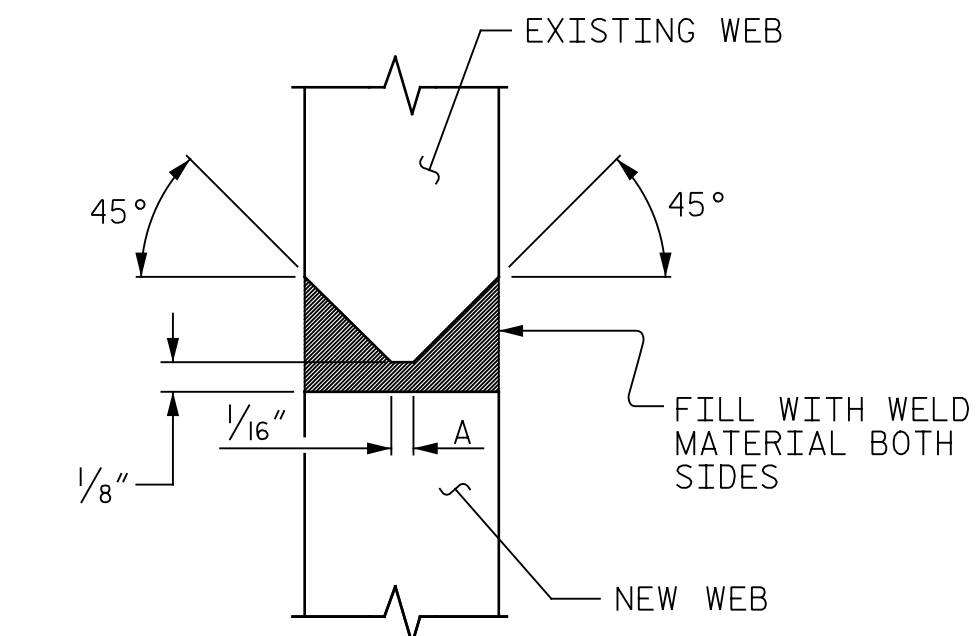


INTERMEDIATE SECTION LOSS
BEAM PLATING REPAIR

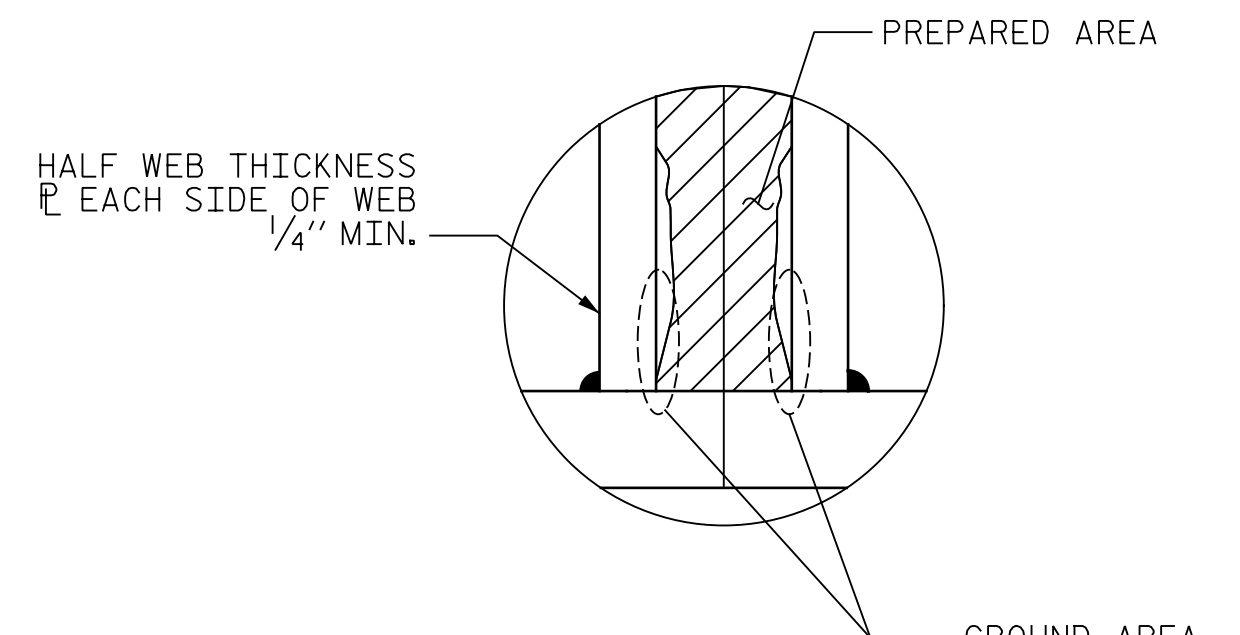


INTERMEDIATE BEAM
PLATING REPAIR

INTERMEDIATE BEAM
PLATING REPAIR



DETAIL "A"



DETAIL "B"

BEAM PLATING REPAIR NOTES:

ALL CONDITIONS AND DIMENSIONS SHALL BE FIELD VERIFIED PRIOR TO FABRICATION OR INSTALLATION OF ANY COMPONENTS.

REPAIR PLATES SHALL BE MINIMUM 36 KSI STEEL. USE NEW OR SALVAGED "LIKE NEW" STEEL ONLY.

ALL WELDING SHALL BE IN ACCORDANCE WITH CURRENT APPLICABLE AWS AND NCDOT STANDARD SPECIFICATIONS.

ALL WELDS SHALL BE INSPECTED AND TESTED BY THE NCDOT MATERIALS AND TESTS UNIT IN ACCORDANCE WITH THE CURRENT AWS BRIDGE WELDING CODE AND STANDARD SPECIFICATIONS.

BEAM PLATING REPAIR SEQUENCE:

COORDINATE SCHEDULE WITH MATERIALS AND TESTS UNIT WELD INSPECTOR AT LEAST FOUR DAYS PRIOR TO ANTICIPATED WORK.

REMOVE TRAFFIC LOAD FROM REPAIR AREA BY EITHER CLOSING BRIDGE TO TRAFFIC OR SHIFTING TRAFFIC AWAY FROM REPAIR AREA.

MECHANICALLY CLEAN RUST, SCALE AND EXISTING PAINT TO AT LEAST 3" BEYOND REPAIR AREA.

IF NECESSARY, REMOVE EXISTING STIFFENER TO INSTALL WELDED PLATE REPAIR. REPLACE WITH A NEW STIFFENER PLATE OF SIMILAR SIZE. FOR STIFFENER/CONNECTOR PLATE REPAIR DETAILS, SEE "BEAM REPAIR DETAILS" SHEET.

IF BEAM DETERIORATION EXTENDS INTO THE CONCRETE DIAPHRAGM THEN CHIP AWAY CONCRETE TO DETERMINE THE EXTENT OF THE DAMAGE.

PRIME ENTIRE REPAIR AREA AND REPAIR PLATES WITH AN ORGANIC ZINC PRIMER PRIOR TO WELDING NEW PLATES. REMOVE PRIMER IN WELD AREA.

ONE PLATE SHALL BE PLACED, AS INDICATED, ON EACH SIDE OF THE BEAM WEB.

EACH PLATE SHALL BE APPROXIMATELY ONE-HALF THE ORIGINAL THICKNESS OF THE BEAM WEB.

FULLY WELD ALONG TOP AND SIDES OF PLATE.

ONCE THE REPAIR IS COMPLETE, GRIND ALL WELDS FLUSH. ANY GOUGES OR INDENTATIONS FROM IMPACT ON BEAMS SHALL BE GROUND SMOOTH. CLEAN AREA TO REMOVE DEBRIS AND OILS FROM REPAIR PROCESS PRIOR TO PAINTING.

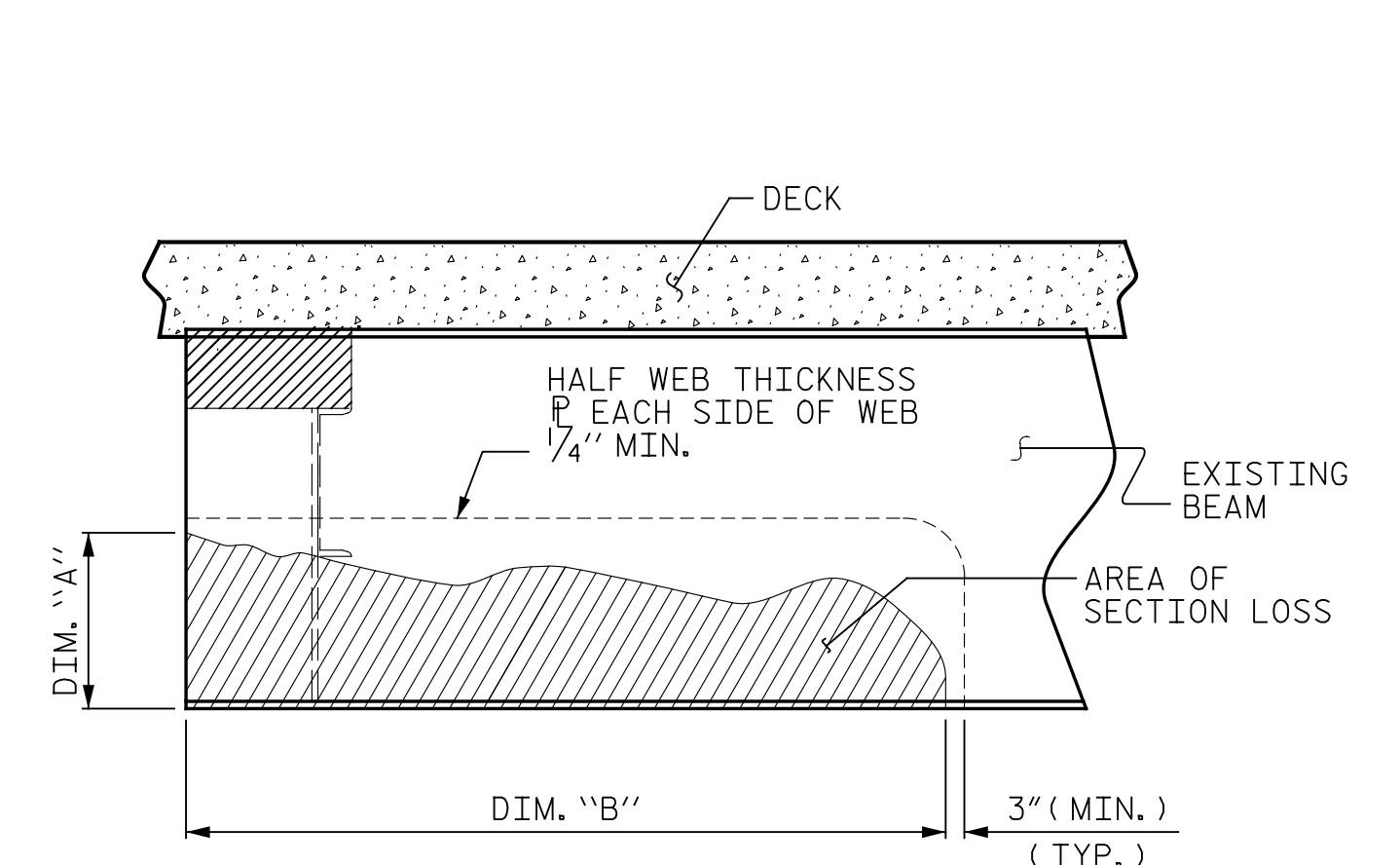
FOR PAINTING STEEL STRUCTURES, SEE SECTION 442 OF THE STANDARD SPECIFICATIONS.

THOROUGHLY CLEAN BEAM PLATING LOCATIONS BY WIRE BRUSHING THE DAMAGED AREAS AND REMOVING ALL LOOSE AND CRACKED COATING, AFTER WHICH GIVE THE CLEANED AREA 2 COATS OF NON-AEROSOL ORGANIC ZINC REPAIR PAINT, IN ACCORDANCE WITH ARTICLE 1076-7 OF THE STANDARD SPECIFICATIONS.

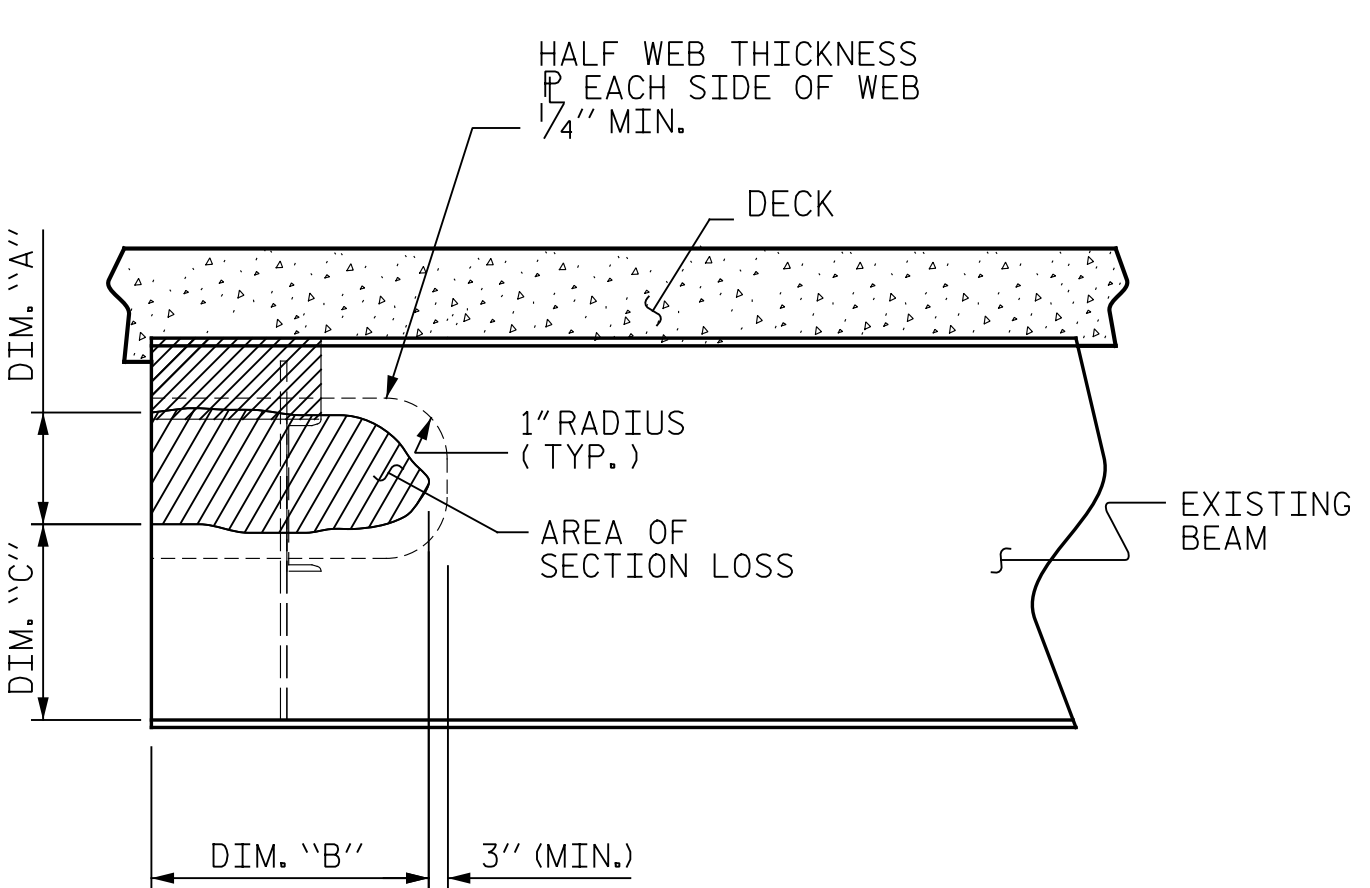
SPOT PAINTING OF BEAM PLATING REPAIR LOCATIONS WILL BE INCLUDED IN THE UNIT PRICE BID FOR BEAM PLATING REPAIR.

AFTER BEAMS AND/OR STIFFENERS/CONNECTOR PLATES ARE REPAIRED AND PAINTED, ANY CONCRETE REMOVED FROM THE BENT DIAPHRAGMS SHALL BE CAST BACK. ANY REINFORCING STEEL CUT DURING THE REMOVAL PROCESS SHALL BE SPLICED WITH A SIMILAR SIZE BAR WITH AT LEAST A ONE FOOT SPLICE TO THE EXISTING STEEL. NO SEPARATE PAYMENT SHALL BE MADE FOR CONCRETE AND REINFORCING STEEL AS THIS IS CONSIDERED INCIDENTAL TO THE PAY ITEM "BEAM PLATING REPAIR". FOR BEAM PLATING REPAIR, SEE SPECIAL PROVISIONS.

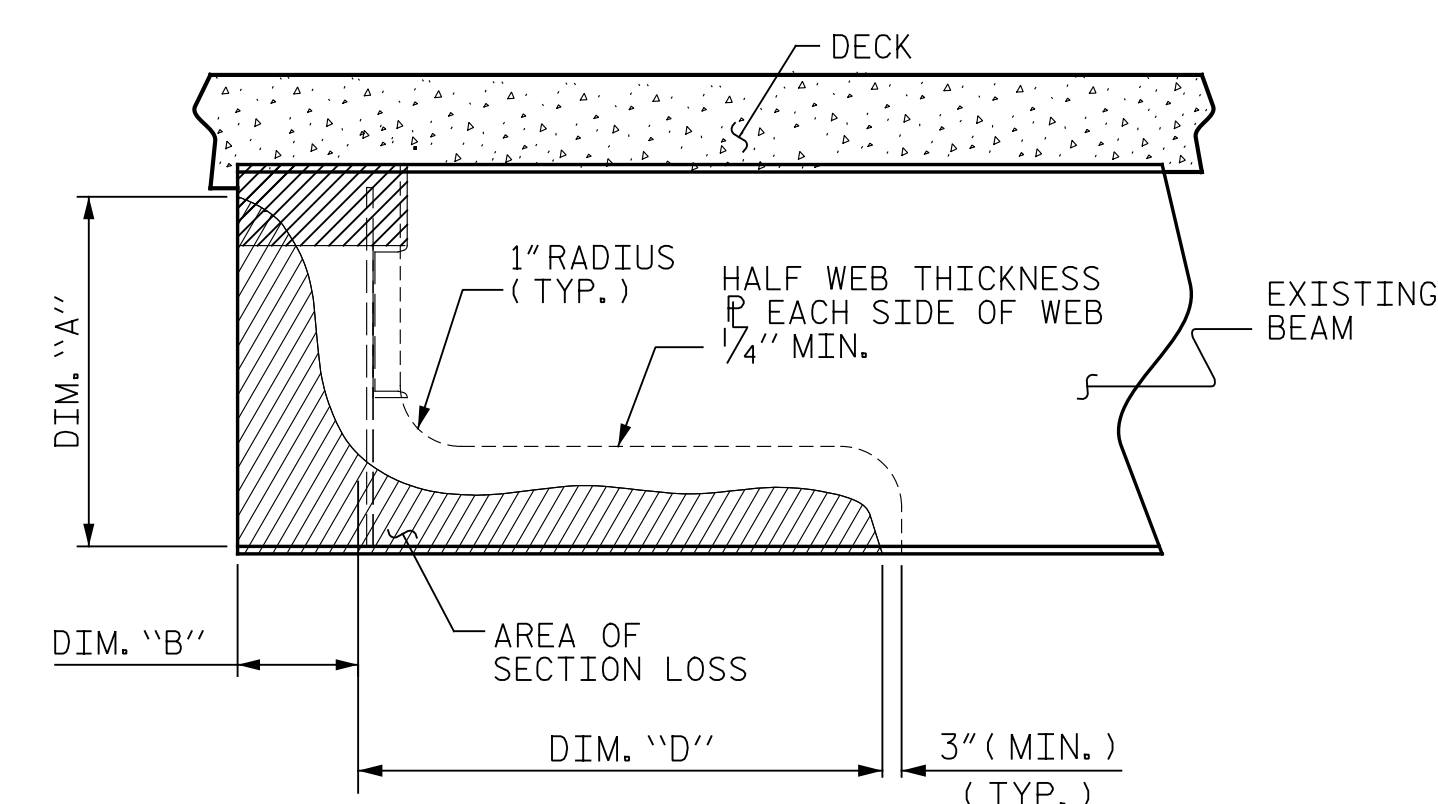
RETURN TRAFFIC TO NORMAL PATTERN.



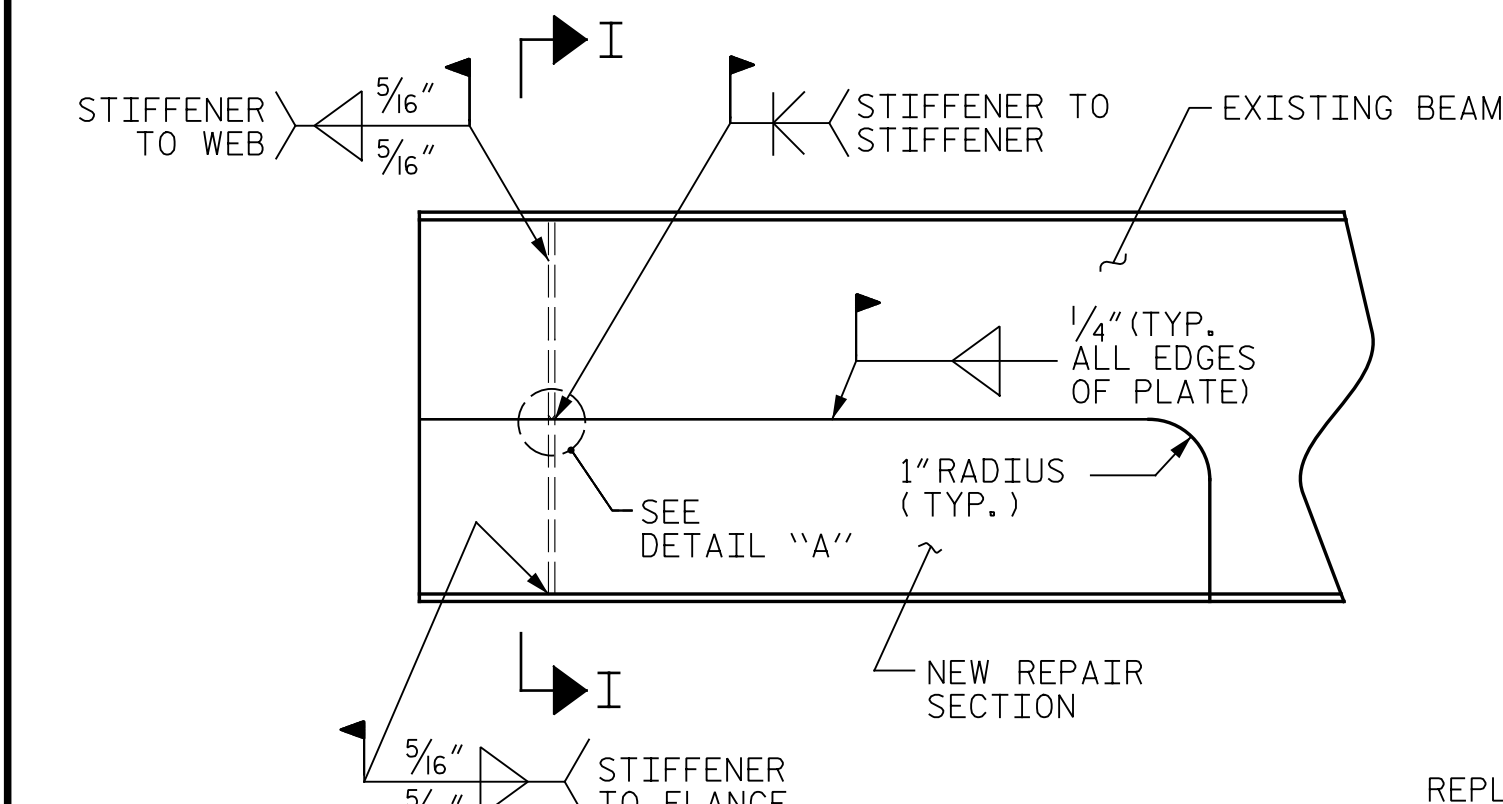
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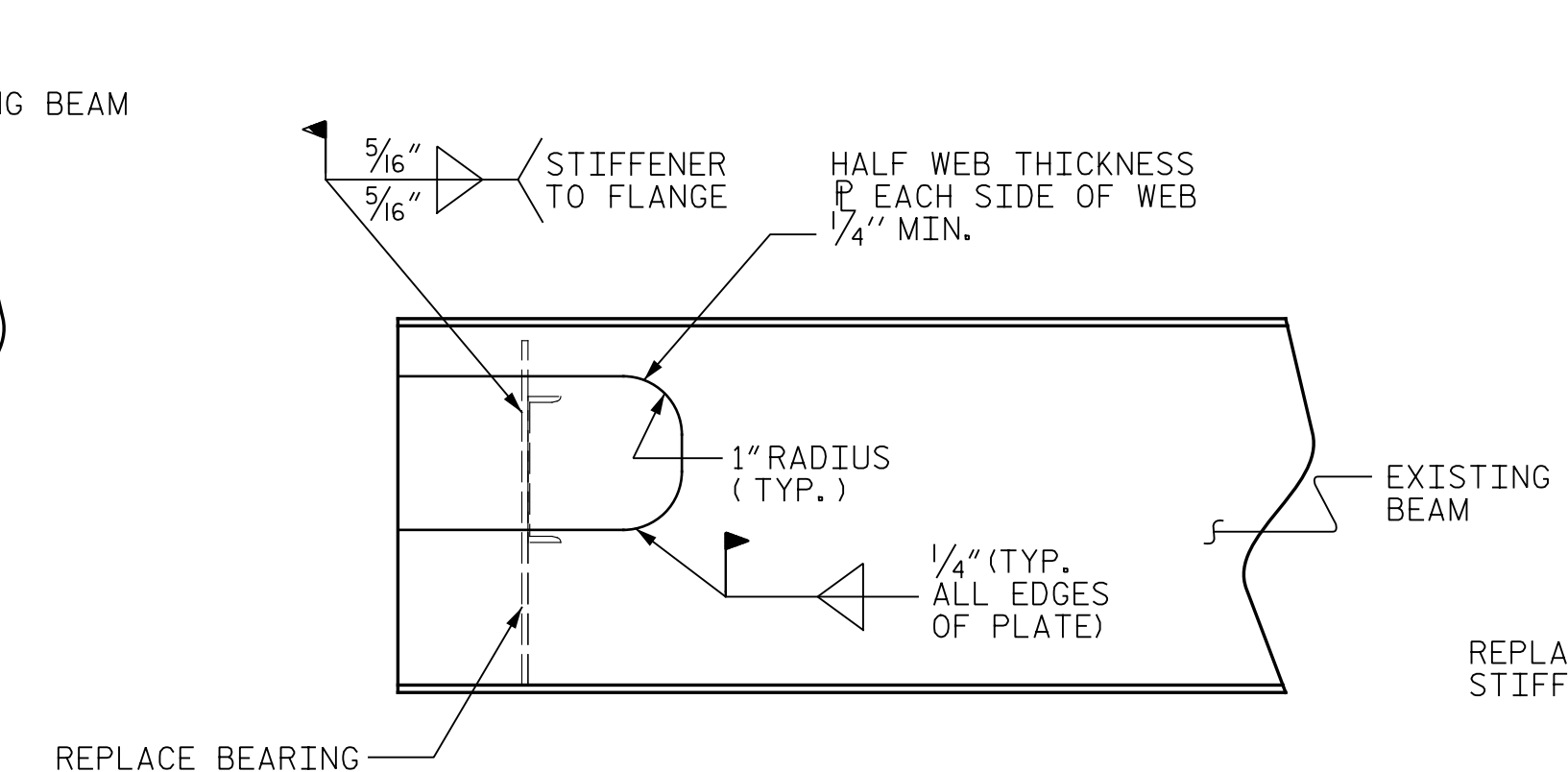
BEAM END SECTION LOSS



BEAM END SECTION LOSS

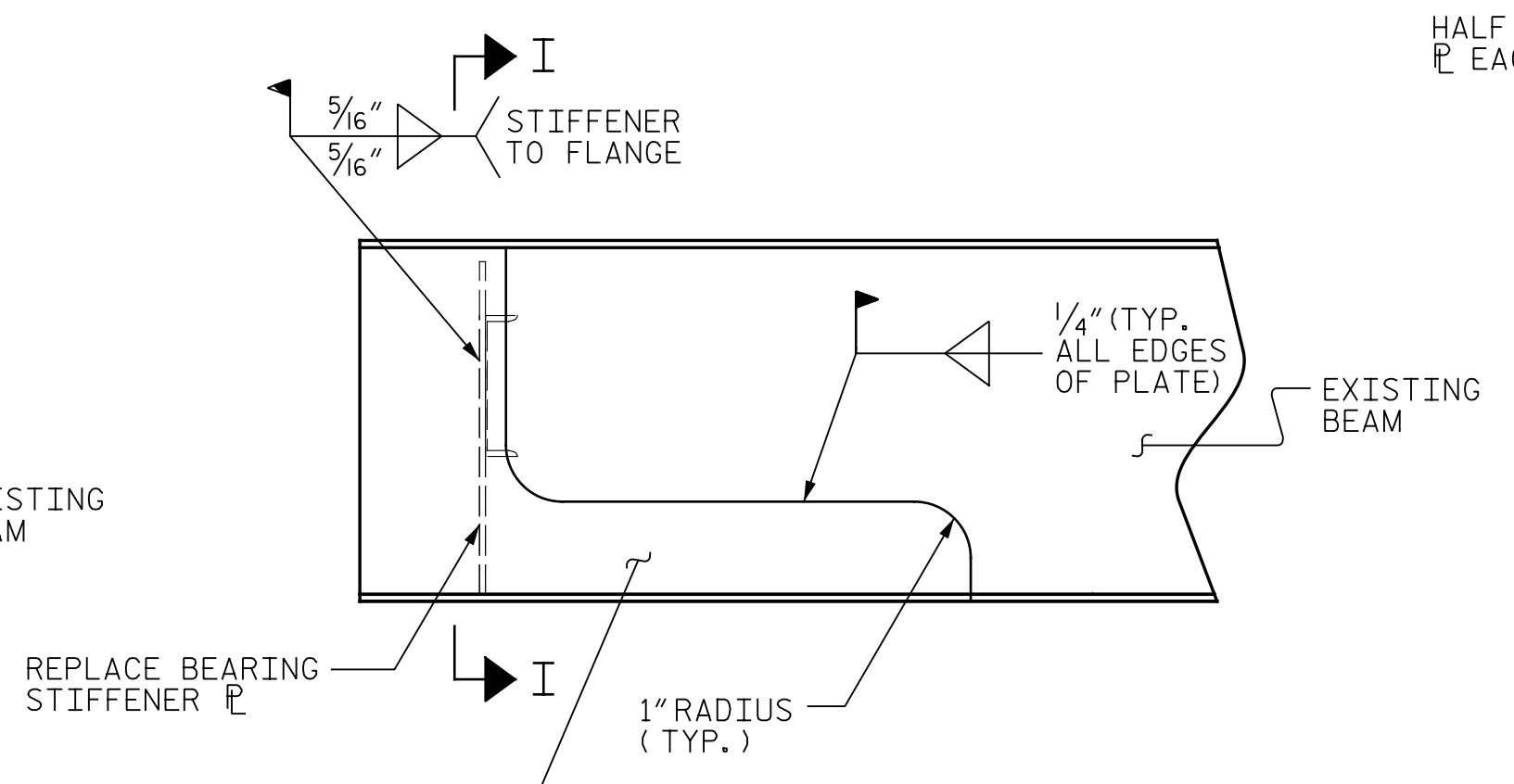


BEAM END
PLATING REPAIR

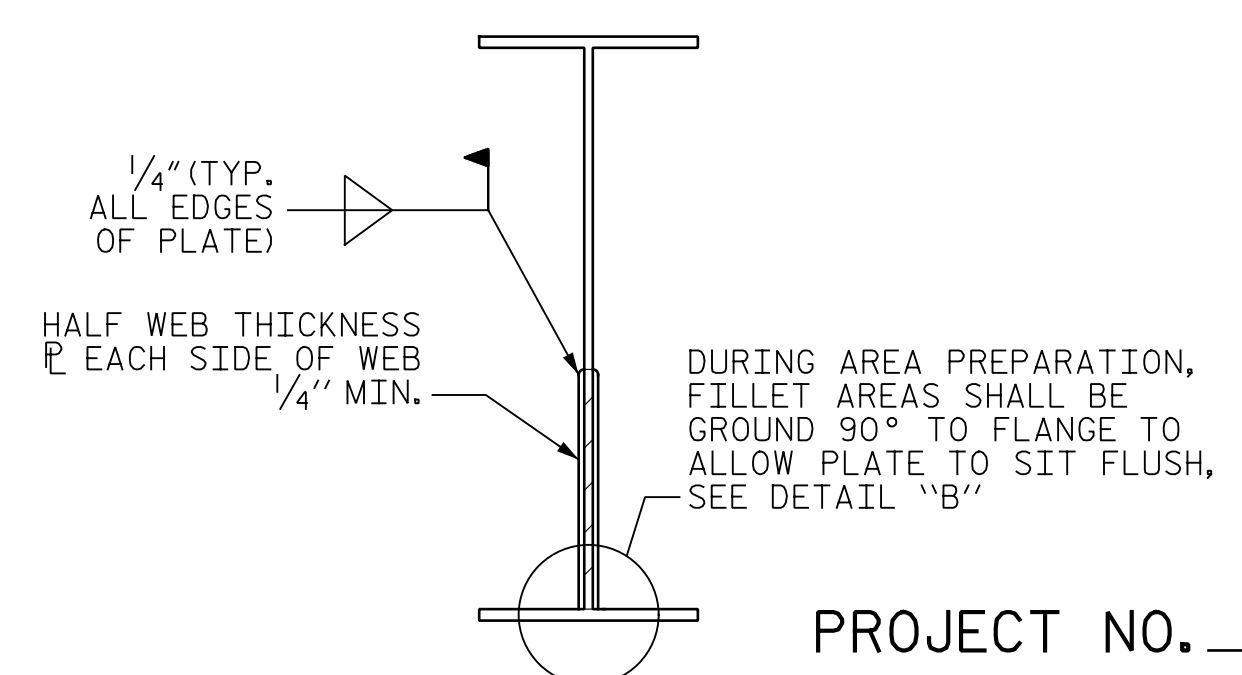


BEAM END
PLATING REPAIR

BEAM END PLATING REPAIR

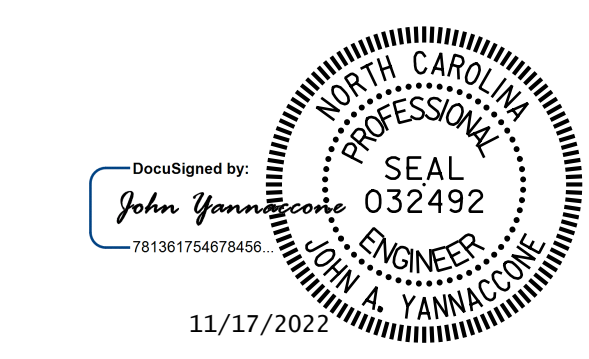


BEAM END
PLATING REPAIR



SECTION I-I

PROJECT NO. I-5955
 COUNTY GUILFORD
 BRIDGE NO. 400329



STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

BEAM PLATING
 REPAIR DETAILS

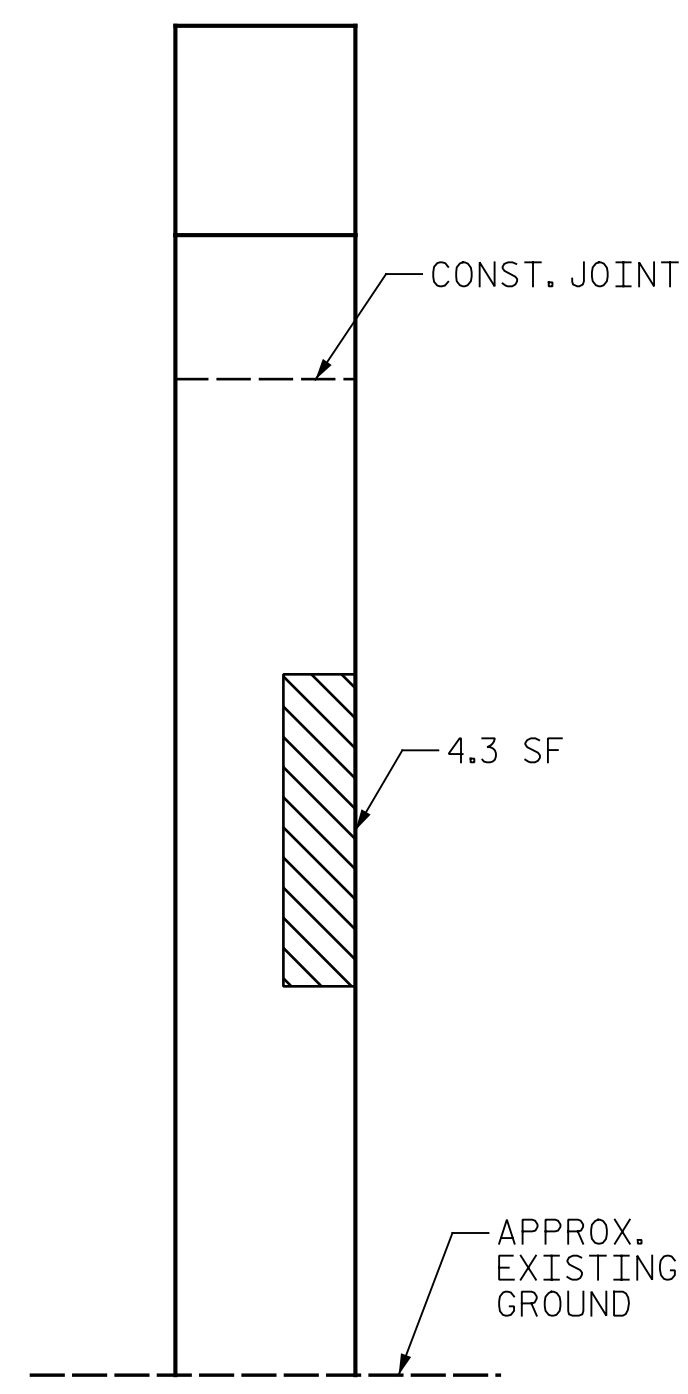
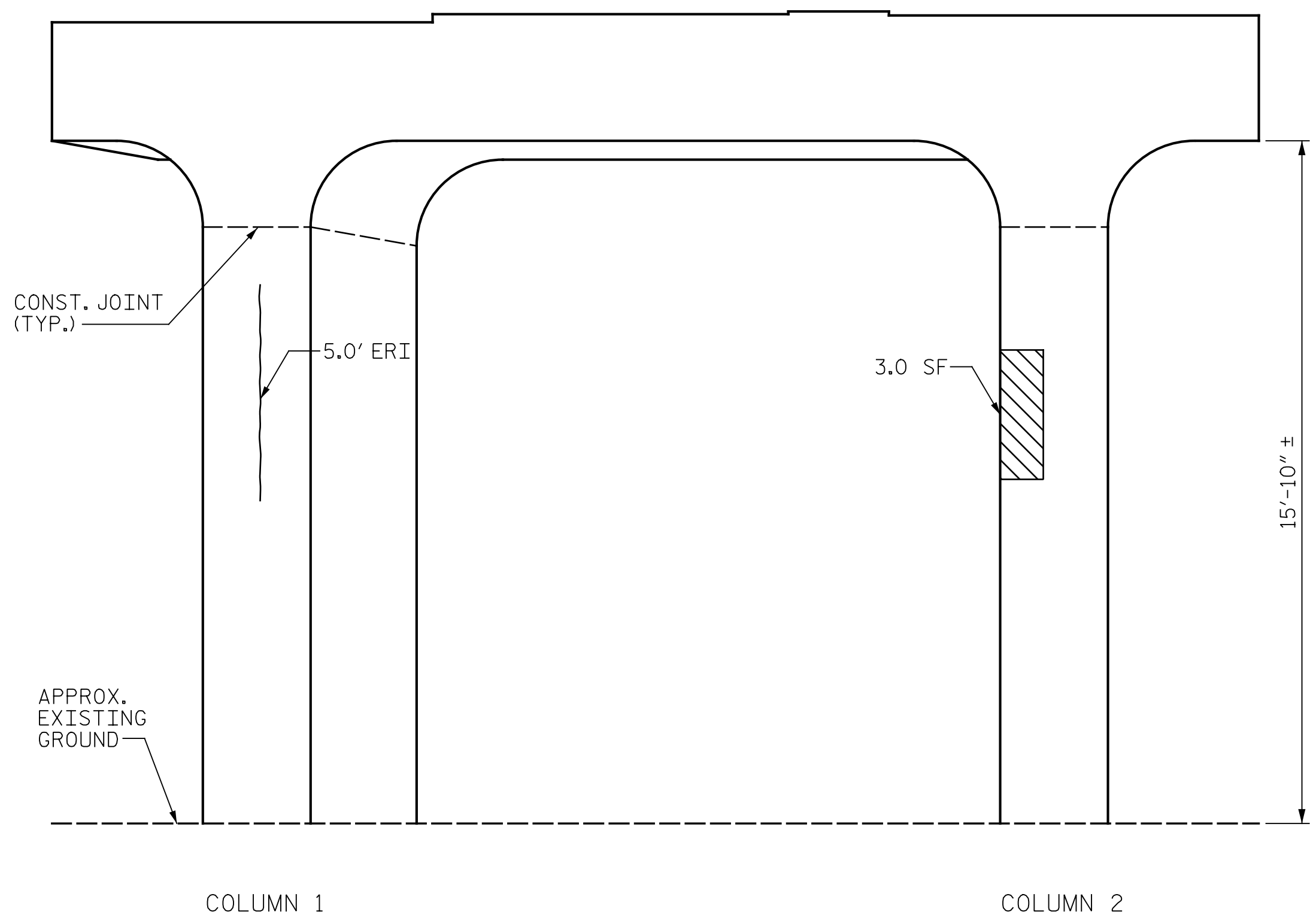
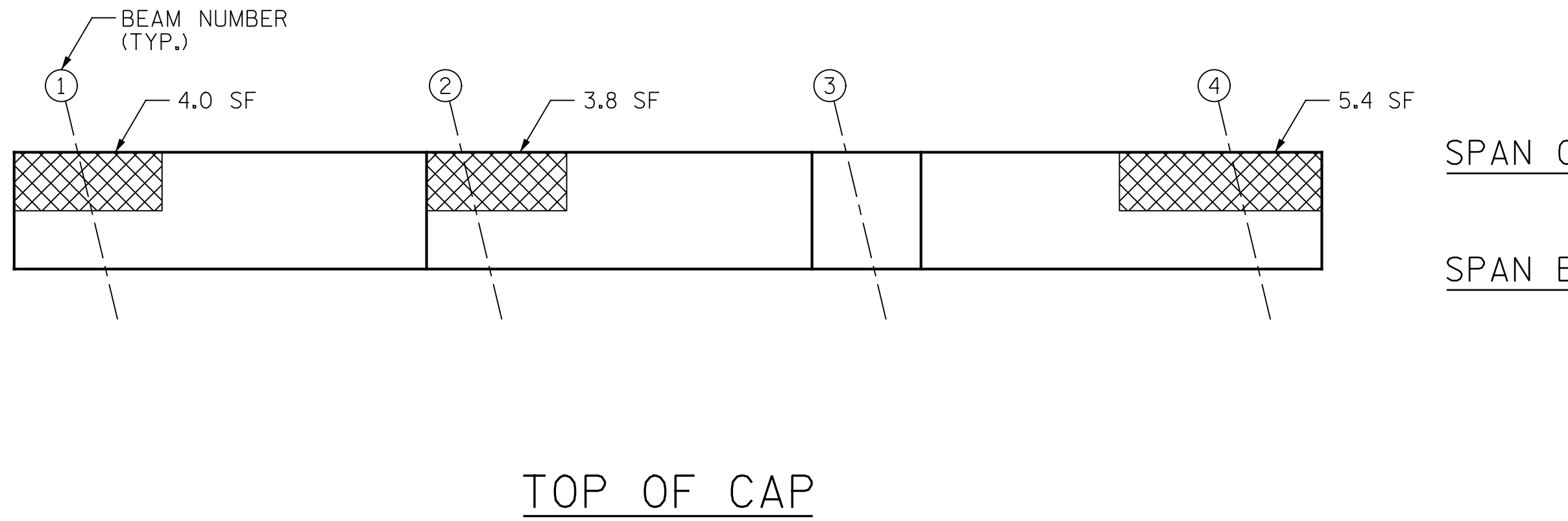
DRAWN BY: J. MYA DATE: 10/2022
 CHECKED BY: J. YANNACCONE DATE: 10/2022



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2			4			127

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AS-BUILT REPAIR QUANTITY TABLE

BENT 2 REPAIRS	QUANTITIES				
	ESTIMATE		ACTUAL		
SHOTCRETE REPAIRS	AREA SF	VOLUME CF	AREA SF	DEPTH FT	VOLUME CF
CAP	13.8	6.9			
COLUMN	14.6	7.3			
CONCRETE REPAIRS	18.5	9.3			
EPOXY RESIN INJECTION		LENGTH LF		LENGTH LF	
CAP		0.0			
COLUMN		5.0			
EPOXY COATING		SQ. FT		SQ. FT	
TOP OF BENT CAP		64			

VALUES IN CHART REPRESENT ESTIMATED REPAIR TOTALS AFTER REMOVAL OF UNSOUND CONCRETE, MINIMUM OF 1" BEHIND REBAR AND MINIMUM OF 2" CLEARANCE TO SAWCUT. FOR REPAIR DETAILS, SEE "TYPICAL CAP AND COLUMN REPAIR DETAILS" SHEET.

NOTES:

REPAIR LOCATIONS AND ESTIMATE OF QUANTITIES ARE BASED ON THE BEST INFORMATION AVAILABLE. IF ADDITIONAL REPAIRS NOT SHOWN ON THE DRAWINGS ARE DEEMED NECESSARY BY THE ENGINEER, THE ENGINEER WILL NOTE ON THE DRAWINGS THE APPROXIMATE LOCATIONS AND DESCRIPTION OF THE REPAIRS AND ENTER THE ACTUAL QUANTITIES INTO THE AS-BUILT REPAIR QUANTITY TABLE.

FOR REPAIRS, SEE "TYPICAL CAP AND COLUMN REPAIR DETAILS" SHEET.
 FOR SHOTCRETE REPAIRS, SEE SPECIAL PROVISIONS.
 FOR CONCRETE REPAIRS, SEE SPECIAL PROVISIONS.
 SHOTCRETE REPAIRS MAY BE REPLACED WITH CONCRETE REPAIRS WITH THE APPROVAL OF THE ENGINEER.

CONTRACTOR SHALL SAW CUT TO A MINIMUM DEPTH OF 1/2" BUT REINFORCING STEEL SHALL NOT BE DAMAGED. CONTRACTOR SHALL REMOVE SURFACE CONCRETE TO VERIFY THAT SAW CUT DEPTH WILL NOT DAMAGE EXISTING REINFORCING STEEL.

CONTRACTOR SHALL SAW CUT THE REPAIR AREAS SO THAT THE CORNERS ARE SQUARE AS INDICATED ON THE DETAILS.

CLEAN AND REMOVE DEBRIS FROM THE TOP OF THE CAP AND APPLY EPOXY PROTECTIVE COATING. EPOXY COATING SHALL BE APPLIED TO THE TOP SURFACE OF THE CAP. THE CONTRACTOR SHALL NOT COAT THE AREA OF THE CAP BENEATH THE MASONRY PLATES. FOR EPOXY COATING, SEE SPECIAL PROVISIONS.

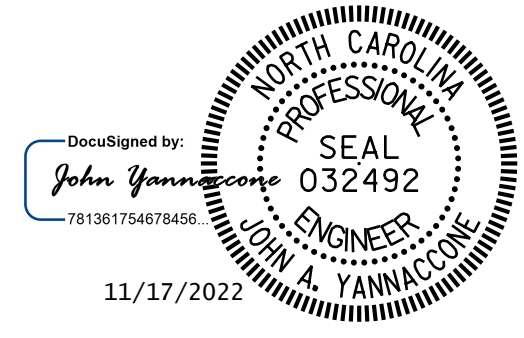
FOR EPOXY RESIN INJECTION, SEE SPECIAL PROVISIONS.

WHEN COLUMN REPAIRS ARE INDICATED TO GROUND LINE, EXTEND REPAIR ONE (1) FOOT MIN. BELOW GROUND LINE.

- SHOTCRETE REPAIR
- CONCRETE REPAIR (FORM & POUR)
- ERI - EPOXY RESIN INJECTION

PROJECT NO. I-5955
GUILFORD COUNTY
 BRIDGE NO. 400329

SHEET 1 OF 2



STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

BENT 2
SPAN B SIDE

DRAWN BY : J. HARRIS DATE : 10/2022
 CHECKED BY : J. YANNACCONE DATE : 10/2022



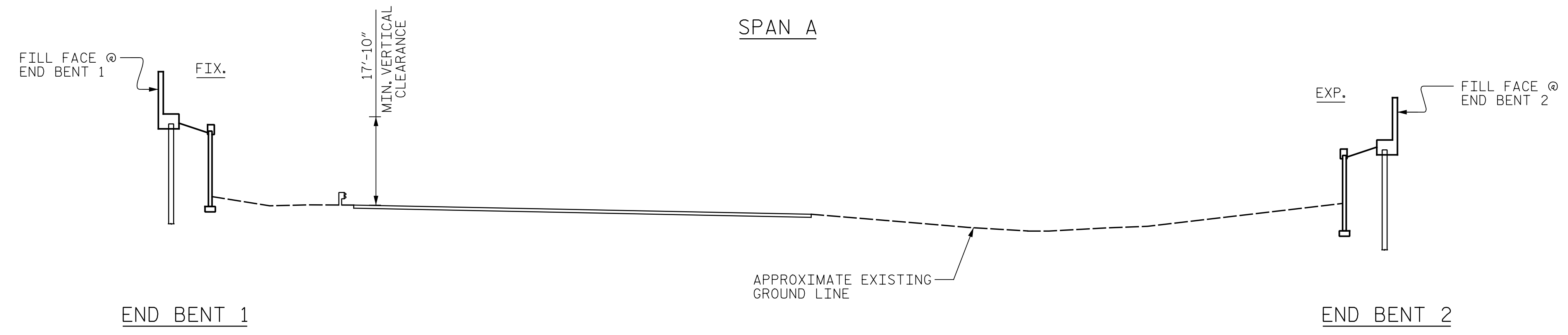
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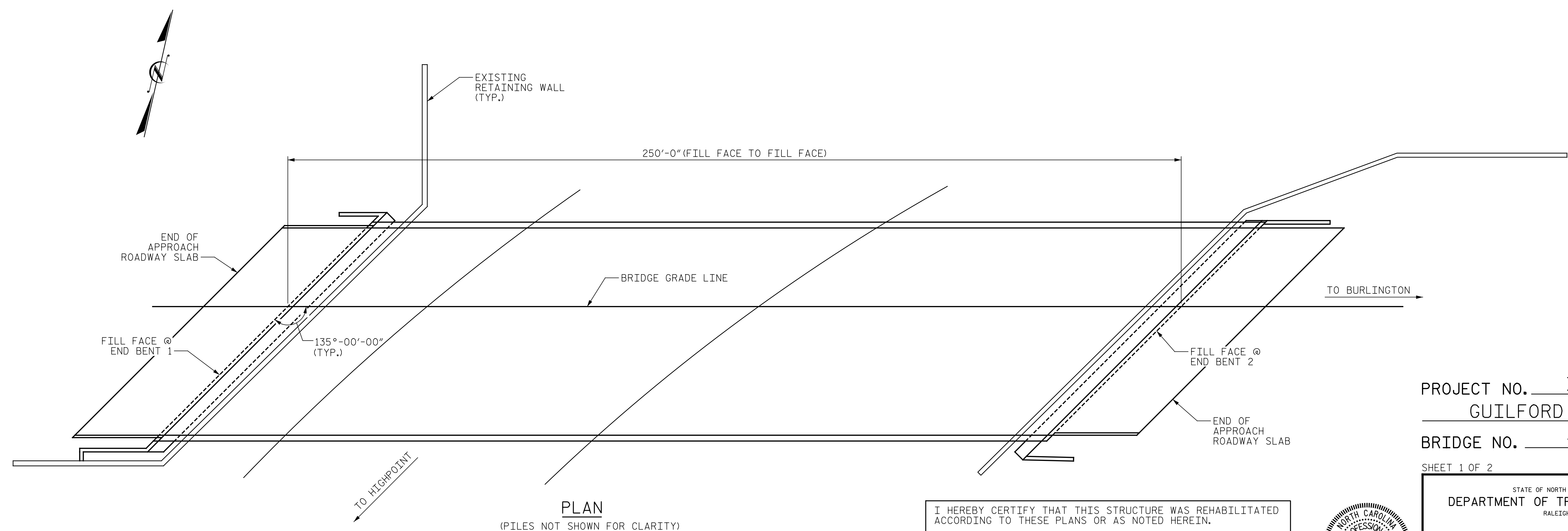
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NOTE:
GENERAL DRAWING INFORMATION IS TAKEN FROM THE ORIGINAL PLANS AND THE ROUTINE INSPECTION REPORT DATED 02/16/2021.
BRIDGE ORIENTATION CONFORMS TO THE EXISTING BRIDGE PLANS AND ROUTINE INSPECTION REPORT.

SCOPE OF WORK
- APPLY DECK SEALANT TO PREPARED TOP OF BRIDGE DECK.

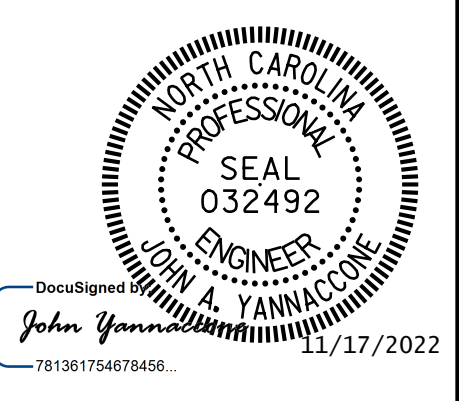


SECTION ALONG BRIDGE GRADE LINE
(SECTION AT BENTS AND END BENTS ARE AT RIGHT ANGLES)



PLAN
(PILES NOT SHOWN FOR CLARITY)

I HEREBY CERTIFY THAT THIS STRUCTURE WAS REHABILITATED ACCORDING TO THESE PLANS OR AS NOTED HEREIN.
RESIDENT ENGINEER _____ DATE _____



PROJECT NO. I-5955
GUILFORD COUNTY
BRIDGE NO. 400336

SHEET 1 OF 2
STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH
**GENERAL DRAWING
FOR BRIDGE ON I-40 EBL
OVER US 29 SBL/US 70 WBL**

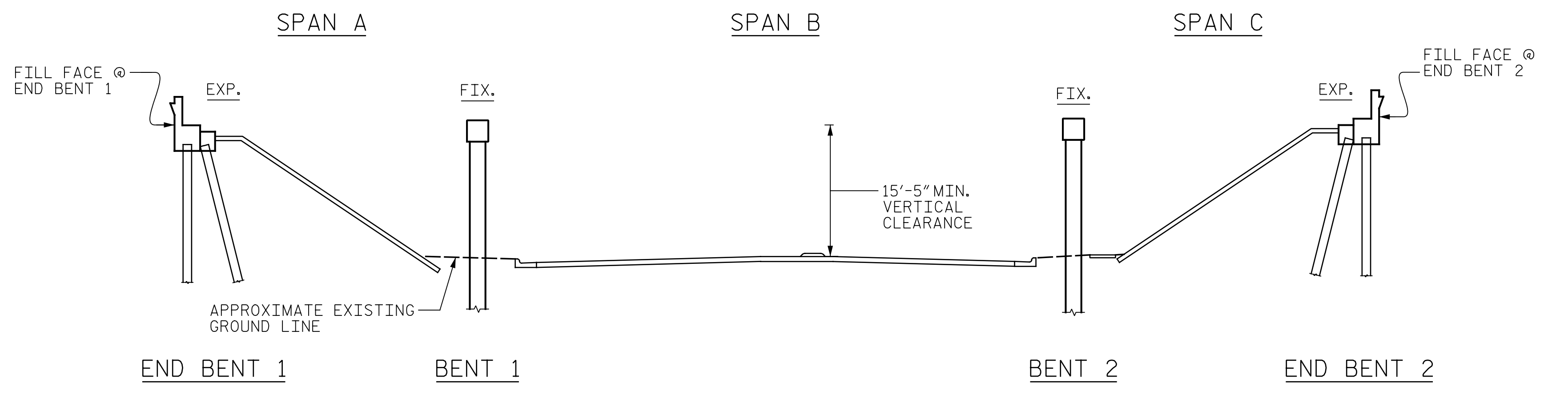
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CHECKED BY : J. YANNACCONE DATE : 10/2022



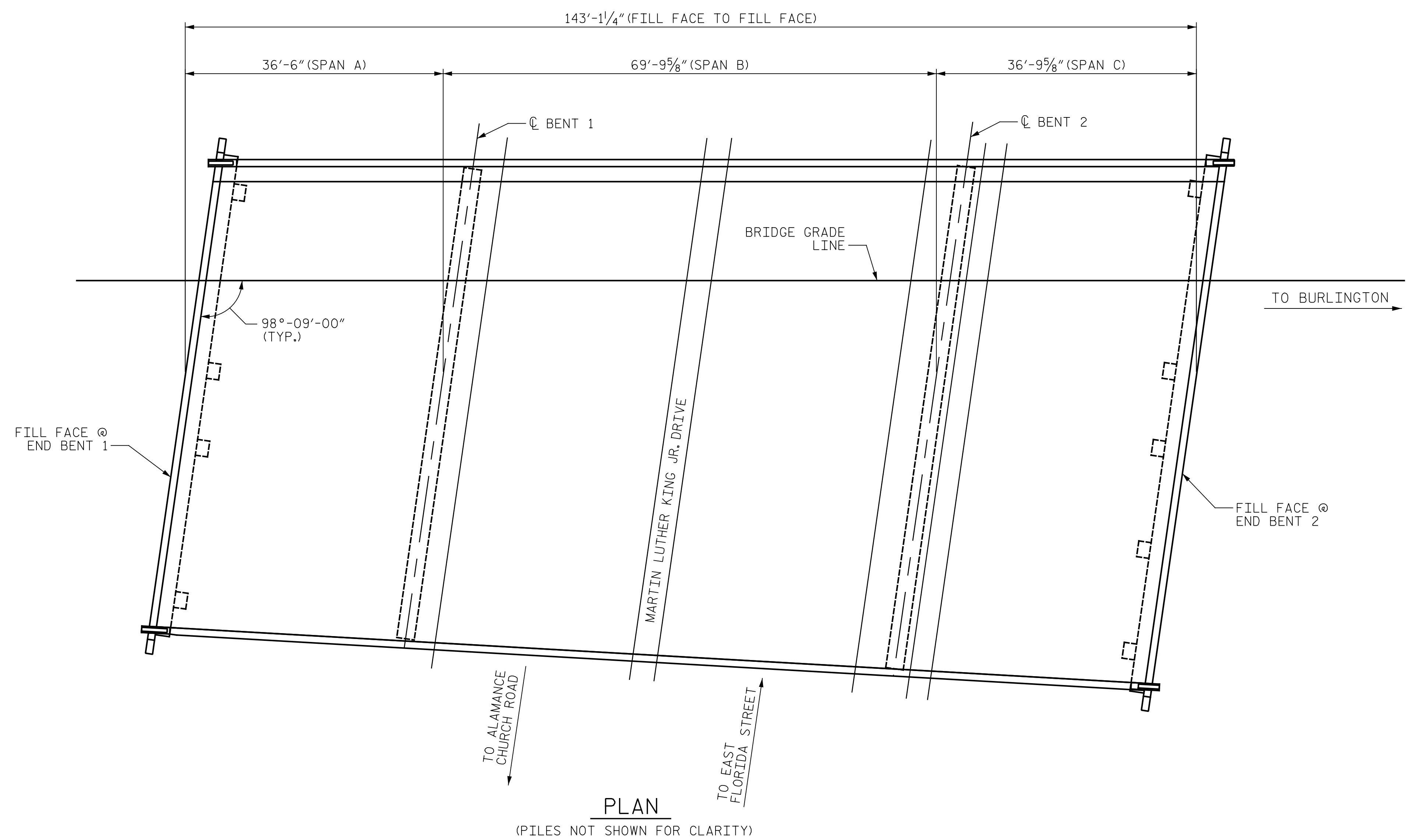
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SECTION ALONG C BRIDGE
(SECTION AT BENTS AND END BENTS ARE AT RIGHT ANGLES)



PLAN
(PILES NOT SHOWN FOR CLARITY)

NOTE:
 GENERAL DRAWING INFORMATION IS TAKEN FROM THE ORIGINAL PLANS AND THE ROUTINE INSPECTION REPORT DATED 12/09/2020.
 BRIDGE ORIENTATION CONFORMS TO THE EXISTING BRIDGE PLANS AND ROUTINE INSPECTION REPORT.

- SCOPE OF WORK**
- REMOVE EXISTING JOINT AND INSTALL PLUG JOINTS.
 - REMOVE DEBRIS FROM TOP OF EXISTING END BENT CAPS AND APPLY EPOXY COATING.
 - EPOXY RESIN INJECTION OF CONCRETE CRACKS
 - REMOVE UNSOUND CONCRETE AND PROPERLY PREPARE EXISTING END BENT AND BENT AREAS FOR SHOTCRETE AND CONCRETE REPAIRS.
 - REMOVE AND REPLACE DAMAGED PORTIONS OF TUBULAR STEEL BEAM BRIDGE RAIL.



I HEREBY CERTIFY THAT THIS STRUCTURE WAS REHABILITATED ACCORDING TO THESE PLANS OR AS NOTED HEREIN.

RESIDENT ENGINEER _____ DATE _____

PROJECT NO. I-5955
GUILFORD COUNTY
 BRIDGE NO. 400339

SHEET 1 OF 2

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

GENERAL DRAWING
 FOR BRIDGE ON I-40 EBL
 OVER SR 3762 (MARTIN LUTHER
 KING JR. DRIVE)

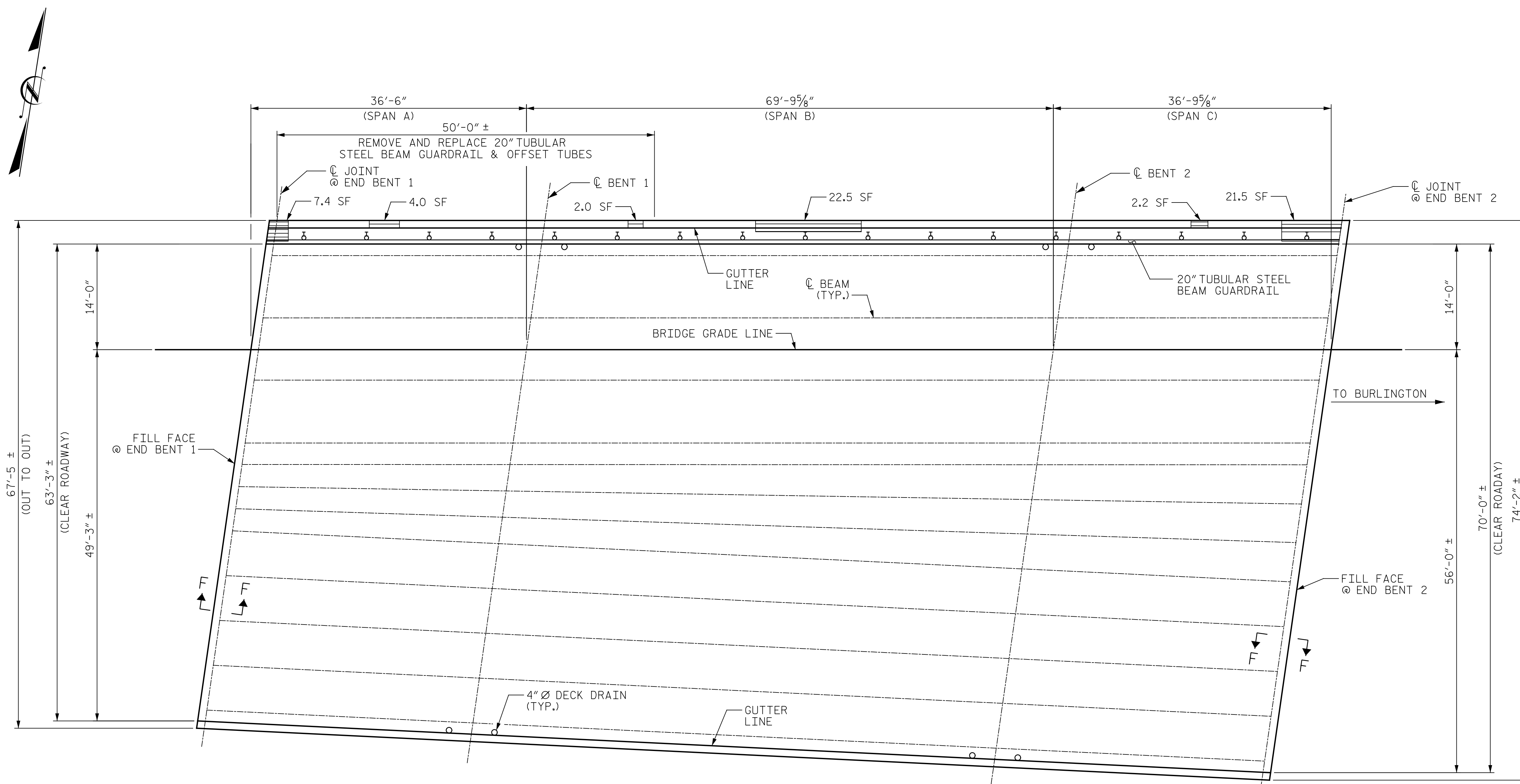


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DRAWN BY : J. MYA DATE : 10/2022
 CHECKED BY : J. YANNACCONE DATE : 10/2022

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SPAN A
SPAN B
SPAN C
PLAN

NOTES:

REPAIR LOCATIONS AND ESTIMATED QUANTITIES ARE GIVEN WITH THE BEST INFORMATION AVAILABLE. IF ADDITIONAL REPAIRS NOT SHOWN ON THE DRAWINGS ARE DEEMED NECESSARY BY THE ENGINEER, THE ENGINEER WILL NOTE ON THE DRAWINGS THE APPROXIMATE LOCATIONS AND DESCRIPTION OF THE REPAIRS AND ADJUST THE ACTUAL QUANTITIES ENTERED INTO THE REPAIR QUANTITY TABLE.

CONCRETE COVER FOR TOP BARS IN THE DECK SLAB IS 1 1/8" PER THE EXISTING BRIDGE PLANS.

FOR SECTION F-F, SEE "ASPHALT PLUG JOINT DETAILS" SHEET.

CONTRACTOR SHALL SAWCUT TO A NOMINAL DEPTH OF 1/2" BUT REINFORCING STEEL SHALL NOT BE DAMAGED. CONTRACTOR SHALL REMOVE SURFACE CONCRETE TO VERIFY THAT SAWCUT DEPTH WILL NOT DAMAGE EXISTING REINFORCING STEEL.

FOR CONCRETE DIAPHRAGM REPAIR, SEE SPECIAL PROVISIONS.

FOR CONCRETE DECK REPAIR FOR ASPHALT OVERLAY SEE SPECIAL PROVISIONS.

THE CONTRACTOR SHALL ENSURE THAT THE EXISTING DECK DRAINS ARE OPEN AND FULLY FUNCTIONAL AFTER PLACEMENT OF THE ASPHALT WEARING SURFACE.

THE BOUNDARIES OF AREAS IDENTIFIED FOR CLASS II SURFACE PREPARATION ARE APPROXIMATE AND MAY NOT REFLECT ACTUAL CONDITIONS THAT WILL BE ENCOUNTERED AT THE PROJECT SITE.

FOR OVERHANG AND UNDERSIDE OF DECK REPAIRS, SEE "OVERHANG AND UNDERSIDE REPAIR DETAILS" SHEET.

THE CONTRACTOR SHALL REMOVE AND REPLACE THE 20" TUBULAR STEEL BEAM GUARDRAIL AS NOTED ON THE PLANS AND AS DIRECTED BY THE ENGINEER. THE SECTIONS OF GUARDRAIL WHICH ARE REMOVED SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE REMOVED FROM THE PROJECT.

FOR DIMENSIONS OF TUBULAR BEAM GUARDRAIL AND OFFSET TUBES, SEE "TUBULAR BEAM GUARDRAIL DETAILS" SHEET.

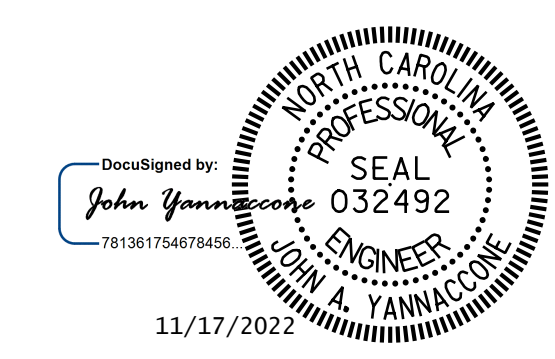
FOR REMOVAL AND REPLACEMENT OF TUBULAR STEEL BEAM GUARDRAIL, SEE SPECIAL PROVISIONS.

REPAIR QUANTITY TABLE				
TOP OF DECK REPAIR				
	ESTIMATE		ACTUAL	
CONCRETE DECK REPAIR FOR ASPHALT OVERLAY	0.0	SY		
UNDERSIDE OF DECK REPAIR				
SHOTCRETE REPAIRS	AREA	VOLUME	AREA	VOLUME
	SF	CF	SF	CF
UNDERSIDE OF DECK	0.0	0.0		
OVERHANG DIAPHRAGMS	0.0	0.0		
UNDERSIDE OF OVERHANG	59.6	29.8		
INTERIOR DIAPHRAGMS	0.0	0.0		
	ESTIMATE		ACTUAL	
UNDERSIDE EPOXY RESIN INJECTION	0.0	LF		

CONCRETE DECK REPAIR FOR ASPHALT OVERLAY
 UNDERSIDE OF DECK REPAIR

BRIDGE RAIL QUANTITIES	
REMOVE 20" TUBULAR STEEL BEAM GUARDRAIL	50.0 LF
20" TUBULAR STEEL BEAM GUARDRAIL	50.0 LF

PROJECT NO. I-5955
GUILFORD COUNTY
 BRIDGE NO. 400339



STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

PLAN OF SPANS

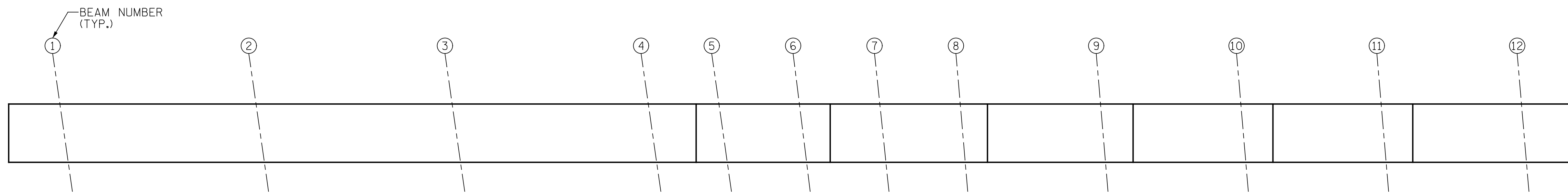
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 CHECKED BY : J. YANNACCONE DATE : 10/2022



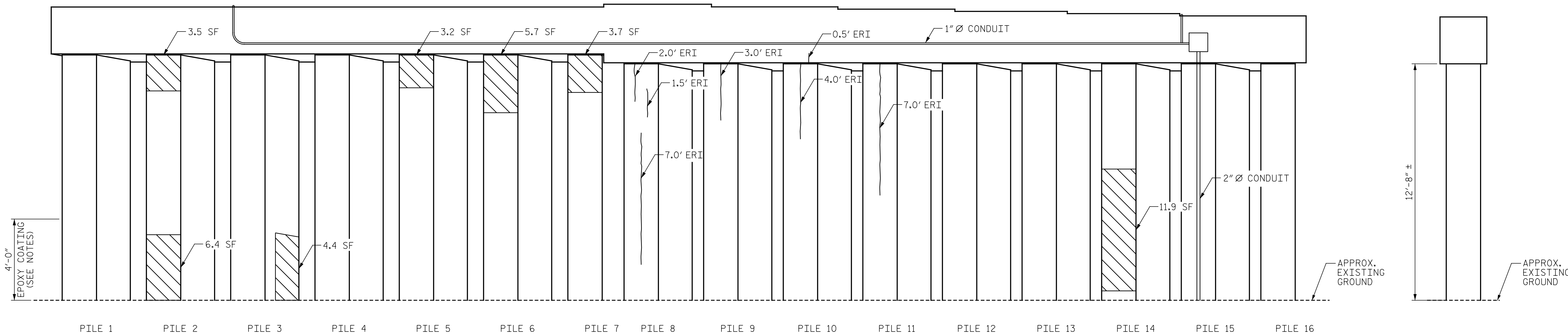
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TOP OF CAP



END VIEW

PILE 16

ELEVATION

AS-BUILT REPAIR QUANTITY TABLE					
BENT 1 REPAIRS	QUANTITIES				
	ESTIMATE		ACTUAL		
SHOTCRETE REPAIRS	AREA SF	VOLUME CF	AREA SF	DEPTH FT	VOLUME CF
CAP	0.0	0.0			
COLUMN	77.2	38.6			
CONCRETE REPAIRS	0.0	0.0			
EPOXY RESIN INJECTION	LENGTH LF		LENGTH LF		
CAP	0.0				
COLUMN	27.5				
EPOXY COATING	SQ. FT		SQ. FT		
CONCRETE PILES	470				

VALUES IN CHART REPRESENT ESTIMATED REPAIR TOTALS AFTER REMOVAL OF UNSOUND CONCRETE, MINIMUM OF 1" BEHIND REBAR AND MINIMUM OF 2" CLEARANCE TO SAWCUT. FOR REPAIR DETAILS, SEE "TYPICAL CAP AND COLUMN REPAIR DETAILS" SHEET.

NOTES:

REPAIR LOCATIONS AND ESTIMATE OF QUANTITIES ARE BASED ON THE BEST INFORMATION AVAILABLE. IF ADDITIONAL REPAIRS NOT SHOWN ON THE DRAWINGS ARE DEEMED NECESSARY BY THE ENGINEER, THE ENGINEER WILL NOTE ON THE DRAWINGS THE APPROXIMATE LOCATIONS AND DESCRIPTION OF THE REPAIRS AND ENTER THE ACTUAL QUANTITIES INTO THE AS-BUILT REPAIR QUANTITY TABLE.

FOR REPAIRS, SEE "TYPICAL CAP AND COLUMN REPAIR DETAILS" SHEET.

FOR SHOTCRETE REPAIRS, SEE SPECIAL PROVISIONS.

FOR CONCRETE REPAIRS, SEE SPECIAL PROVISIONS.

SHOTCRETE REPAIRS MAY BE REPLACED WITH CONCRETE REPAIRS WITH THE APPROVAL OF THE ENGINEER.

CONTRACTOR SHALL SAW CUT TO A MINIMUM DEPTH OF 1/2" BUT REINFORCING STEEL SHALL NOT BE DAMAGED. CONTRACTOR SHALL REMOVE SURFACE CONCRETE TO VERIFY THAT SAW CUT DEPTH WILL NOT DAMAGE EXISTING REINFORCING STEEL.

CONTRACTOR SHALL SAW CUT THE REPAIR AREAS SO THAT THE CORNERS ARE SQUARE AS INDICATED ON THE DETAILS.

APPLY EPOXY PROTECTIVE COATING TO ALL SIDES OF THE CONCRETE PILES. EPOXY COATING SHALL BE APPLIED 4 FEET UP THE PILES FROM THE GROUND LINE. FOR EPOXY COATING, SEE SPECIAL PROVISIONS.

FOR EPOXY RESIN INJECTION, SEE SPECIAL PROVISIONS.

WHEN COLUMN REPAIRS ARE INDICATED TO GROUND LINE, EXTEND REPAIR ONE (1) FOOT MIN. BELOW GROUND LINE.

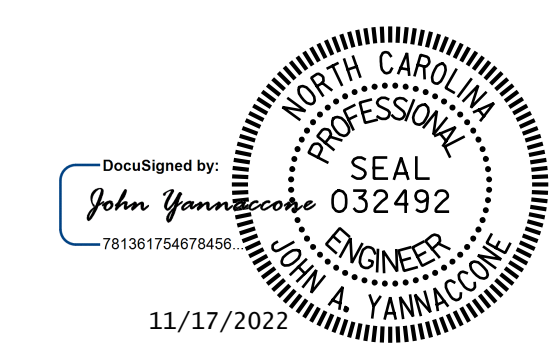
- SHOTCRETE REPAIR
- CONCRETE REPAIR (FORM & POUR)
- ERI - EPOXY RESIN INJECTION

PROJECT NO. I-5955
GUILFORD COUNTY
 BRIDGE NO. 400339

SHEET 1 OF 2

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

**BENT 1
 SPAN A SIDE**



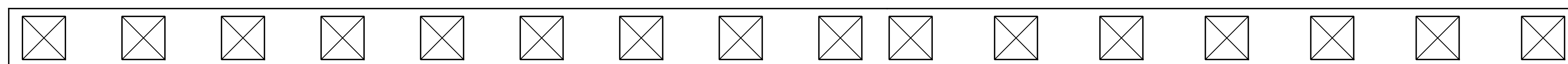
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 CHECKED BY: J. YANNAACONE DATE: 10/2022



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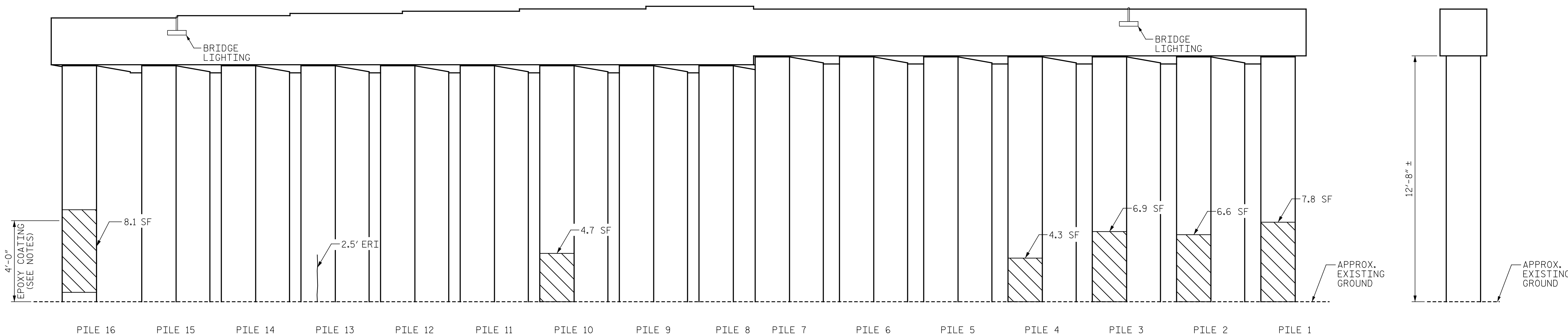
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SPAN A

SPAN B

BOTTOM OF CAP



ELEVATION

END VIEW

PILE 1

NOTES:

REPAIR LOCATIONS AND ESTIMATE OF QUANTITIES ARE BASED ON THE BEST INFORMATION AVAILABLE. IF ADDITIONAL REPAIRS NOT SHOWN ON THE DRAWINGS ARE DEEMED NECESSARY BY THE ENGINEER, THE ENGINEER WILL NOTE ON THE DRAWINGS THE APPROXIMATE LOCATIONS AND DESCRIPTION OF THE REPAIRS AND ENTER THE ACTUAL QUANTITIES INTO THE AS-BUILT REPAIR QUANTITY TABLE.

FOR REPAIRS, SEE "TYPICAL CAP AND COLUMN REPAIR DETAILS" SHEET.

FOR SHOTCRETE REPAIRS, SEE SPECIAL PROVISIONS.

FOR CONCRETE REPAIRS, SEE SPECIAL PROVISIONS.

SHOTCRETE REPAIRS MAY BE REPLACED WITH CONCRETE REPAIRS WITH THE APPROVAL OF THE ENGINEER.

CONTRACTOR SHALL SAW CUT TO A MINIMUM DEPTH OF 1/2" BUT REINFORCING STEEL SHALL NOT BE DAMAGED. CONTRACTOR SHALL REMOVE SURFACE CONCRETE TO VERIFY THAT SAW CUT DEPTH WILL NOT DAMAGE EXISTING REINFORCING STEEL.

CONTRACTOR SHALL SAW CUT THE REPAIR AREAS SO THAT THE CORNERS ARE SQUARE AS INDICATED ON THE DETAILS.

APPLY EPOXY PROTECTIVE COATING TO ALL SIDES OF THE CONCRETE PILES. EPOXY COATING SHALL BE APPLIED 4 FEET UP THE PILES FROM THE GROUND LINE. FOR EPOXY COATING, SEE SPECIAL PROVISIONS.

FOR EPOXY RESIN INJECTION, SEE SPECIAL PROVISIONS.

WHEN COLUMN REPAIRS ARE INDICATED TO GROUND LINE, EXTEND REPAIR ONE (1) FOOT MIN. BELOW GROUND LINE.

PROJECT NO. I-5955

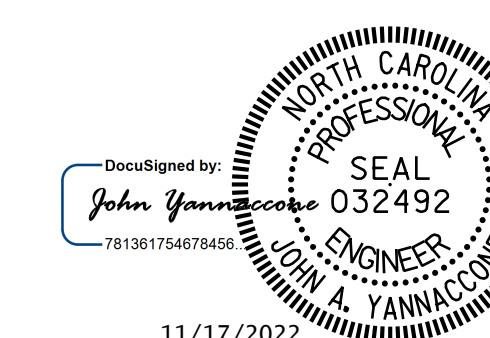
GUILFORD COUNTY

BRIDGE NO. 400339

SHEET 2 OF 2

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

BENT 1
SPAN B SIDE

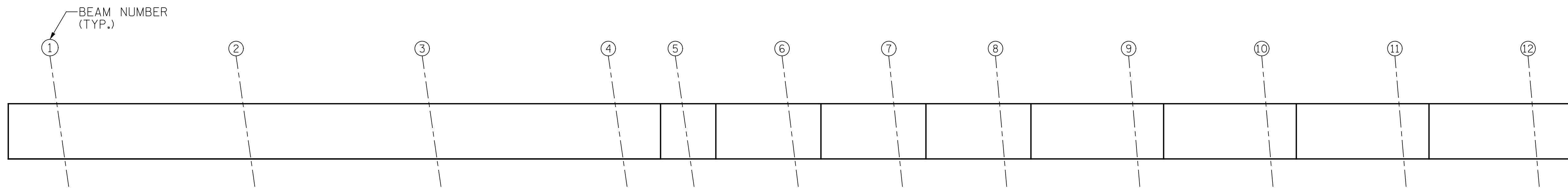


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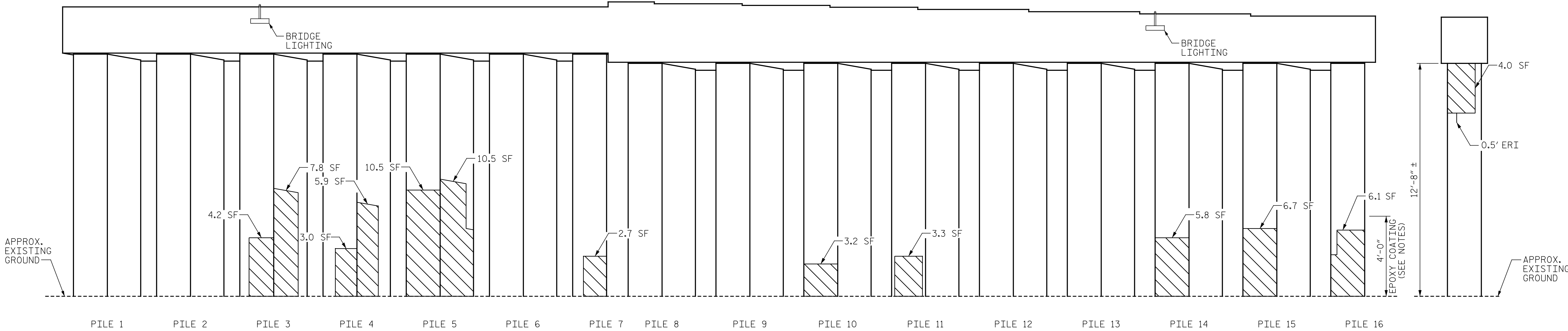
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CHECKED BY : J. YANNACCONE DATE : 10/2022

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TOP OF CAP



ELEVATION

NOTES:

REPAIR LOCATIONS AND ESTIMATE OF QUANTITIES ARE BASED ON THE BEST INFORMATION AVAILABLE. IF ADDITIONAL REPAIRS NOT SHOWN ON THE DRAWINGS ARE DEEMED NECESSARY BY THE ENGINEER, THE ENGINEER WILL NOTE ON THE DRAWINGS THE APPROXIMATE LOCATIONS AND DESCRIPTION OF THE REPAIRS AND ENTER THE ACTUAL QUANTITIES INTO THE AS-BUILT REPAIR QUANTITY TABLE.

FOR REPAIRS, SEE "TYPICAL CAP AND COLUMN REPAIR DETAILS" SHEET.

FOR SHOTCRETE REPAIRS, SEE SPECIAL PROVISIONS.

FOR CONCRETE REPAIRS, SEE SPECIAL PROVISIONS.

SHOTCRETE REPAIRS MAY BE REPLACED WITH CONCRETE REPAIRS WITH THE APPROVAL OF THE ENGINEER.

CONTRACTOR SHALL SAW CUT TO A MINIMUM DEPTH OF 1/2" BUT REINFORCING STEEL SHALL NOT BE DAMAGED. CONTRACTOR SHALL REMOVE SURFACE CONCRETE TO VERIFY THAT SAW CUT DEPTH WILL NOT DAMAGE EXISTING REINFORCING STEEL.

CONTRACTOR SHALL SAW CUT THE REPAIR AREAS SO THAT THE CORNERS ARE SQUARE AS INDICATED ON THE DETAILS.

APPLY EPOXY PROTECTIVE COATING TO ALL SIDES OF THE CONCRETE PILES. EPOXY COATING SHALL BE APPLIED 4 FEET UP THE PILES FROM THE GROUND LINE. FOR EPOXY COATING, SEE SPECIAL PROVISIONS.

FOR EPOXY RESIN INJECTION, SEE SPECIAL PROVISIONS.

WHEN COLUMN REPAIRS ARE INDICATED TO GROUND LINE, EXTEND REPAIR ONE (1) FOOT MIN. BELOW GROUND LINE.

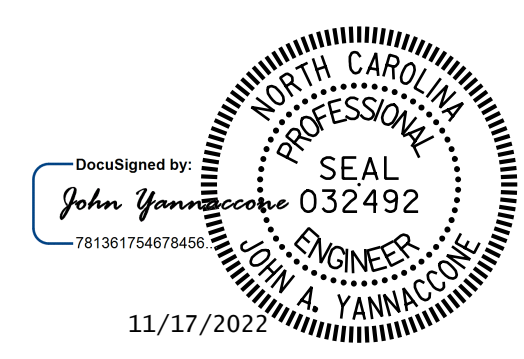
- SHOTCRETE REPAIR
- CONCRETE REPAIR (FORM & POUR)
- ERI - EPOXY RESIN INJECTION

AS-BUILT REPAIR QUANTITY TABLE					
BENT 2 REPAIRS	QUANTITIES				
	ESTIMATE		ACTUAL		
SHOTCRETE REPAIRS	AREA SF	VOLUME CF	AREA SF	DEPTH FT	VOLUME CF
CAP	0.0	0.0			
COLUMN	125.8	62.9			
CONCRETE REPAIRS	0.0	0.0			
EPOXY RESIN INJECTION	LENGTH LF		LENGTH LF		
CAP	0.0				
COLUMN	0.5				
EPOXY COATING	SQ. FT		SQ. FT		
CONCRETE PILES	470				

VALUES IN CHART REPRESENT ESTIMATED REPAIR TOTALS AFTER REMOVAL OF UNSOUND CONCRETE, MINIMUM OF 1" BEHIND REBAR AND MINIMUM OF 2" CLEARANCE TO SAWCUT. FOR REPAIR DETAILS, SEE "TYPICAL CAP AND COLUMN REPAIR DETAILS" SHEET.

PROJECT NO. I-5955
GUILFORD COUNTY
 BRIDGE NO. 400339

SHEET 1 OF 2



STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

BENT 2
SPAN B SIDE

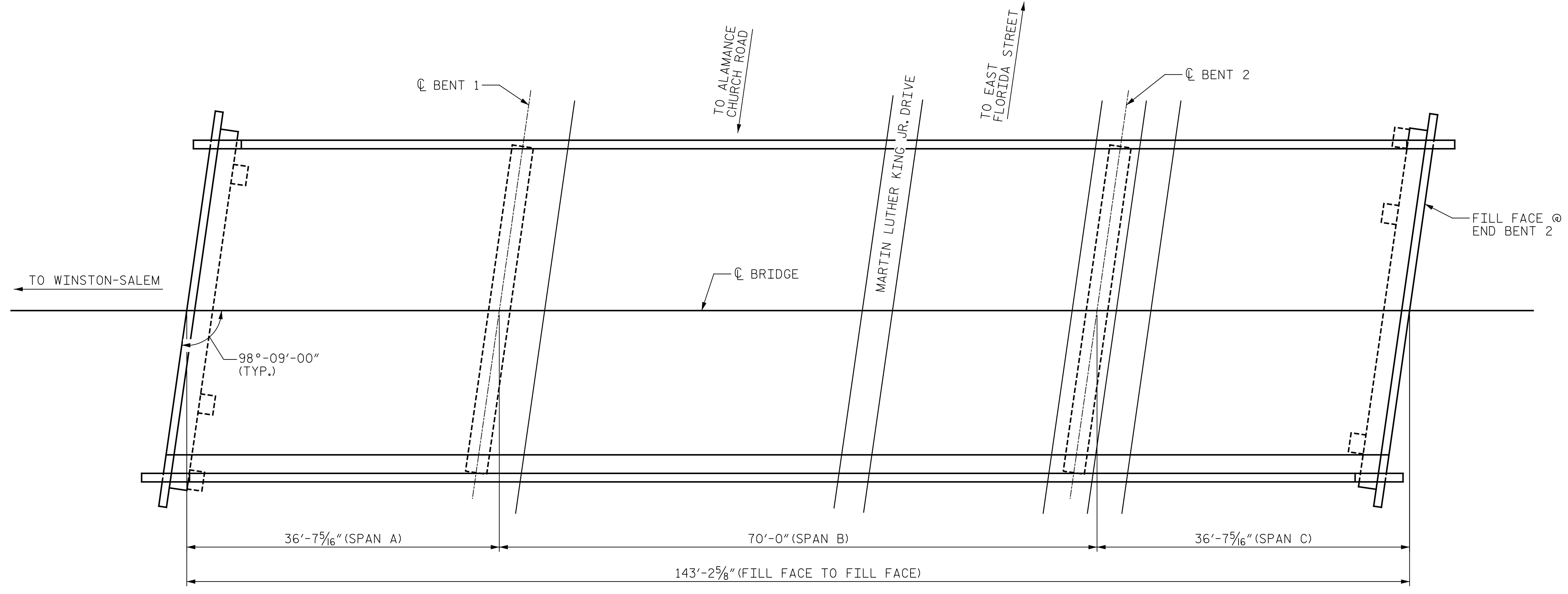
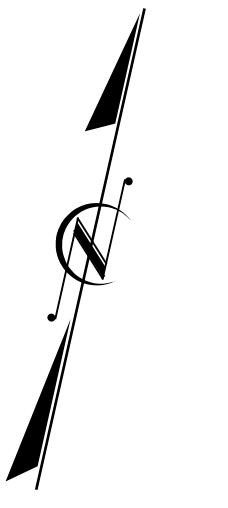
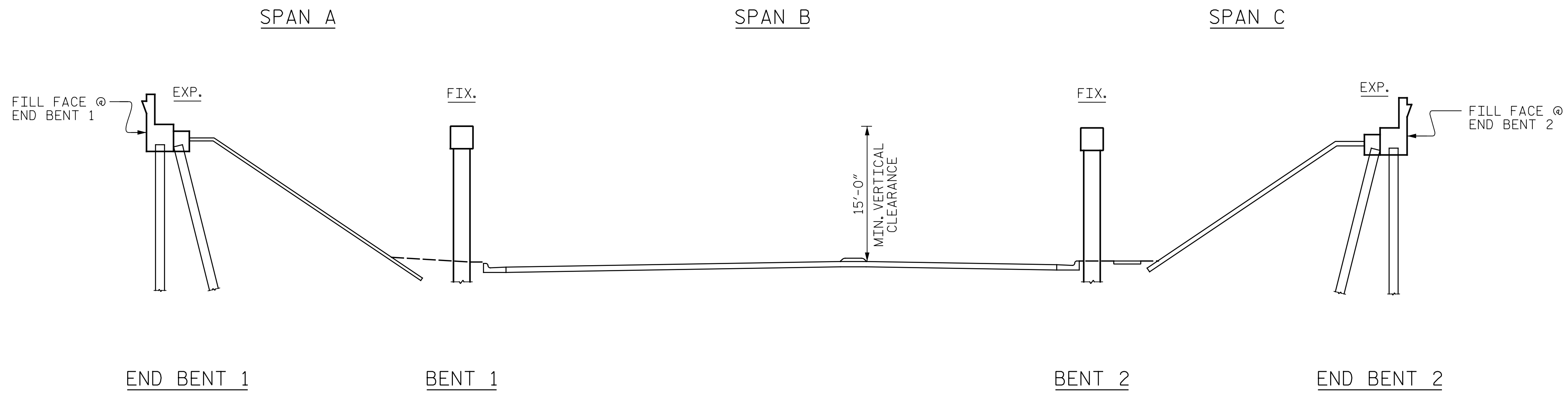
DRAWN BY: J. HARRIS DATE: 10/2022
 CHECKED BY: J. YANNACCONE DATE: 10/2022



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NO.	BY:	DATE:	NO.	BY:	DATE:	S7-10
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2			4			127

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NOTE:
GENERAL DRAWING INFORMATION IS TAKEN FROM THE ORIGINAL PLANS AND THE ROUTINE INSPECTION REPORT DATED 12/09/2020.
BRIDGE ORIENTATION CONFORMS TO THE EXISTING BRIDGE PLANS AND ROUTINE INSPECTION REPORT.

- SCOPE OF WORK**
- REMOVE EXISTING JOINT AND INSTALL PLUG JOINTS.
 - REMOVE DEBRIS FROM TOP OF EXISTING END BENT CAPS AND APPLY EPOXY COATING.
 - EPOXY RESIN INJECTION OF CONCRETE CRACKS.
 - REMOVE UNSOUND CONCRETE AND PROPERLY PREPARE EXISTING END BENT AND BENT AREAS FOR SHOTCRETE AND CONCRETE REPAIRS.
 - REMOVE AND REPLACE DAMAGED PORTIONS OF TUBULAR STEEL BEAM BRIDGE RAIL.

PROJECT NO. I-5955
GUILFORD COUNTY
BRIDGE NO. 400340

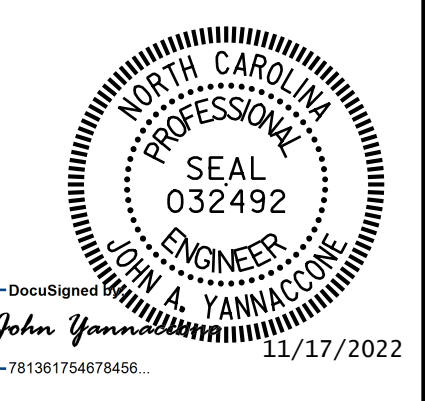
SHEET 1 OF 2

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

GENERAL DRAWING
FOR BRIDGE ON I-40 WBL
OVER SR 3762 (MARTIN LUTHER
KING JR. DRIVE)

I HEREBY CERTIFY THAT THIS STRUCTURE WAS REHABILITATED ACCORDING TO THESE PLANS OR AS NOTED HEREIN.

RESIDENT ENGINEER _____ DATE _____



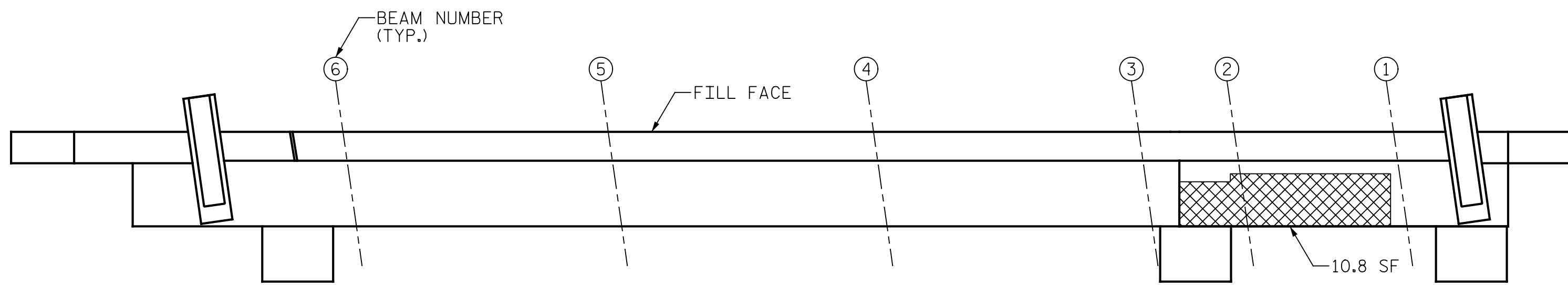
DRAWN BY : J. MYA DATE : 10/2022
CHECKED BY : J. YANNACCONE DATE : 10/2022



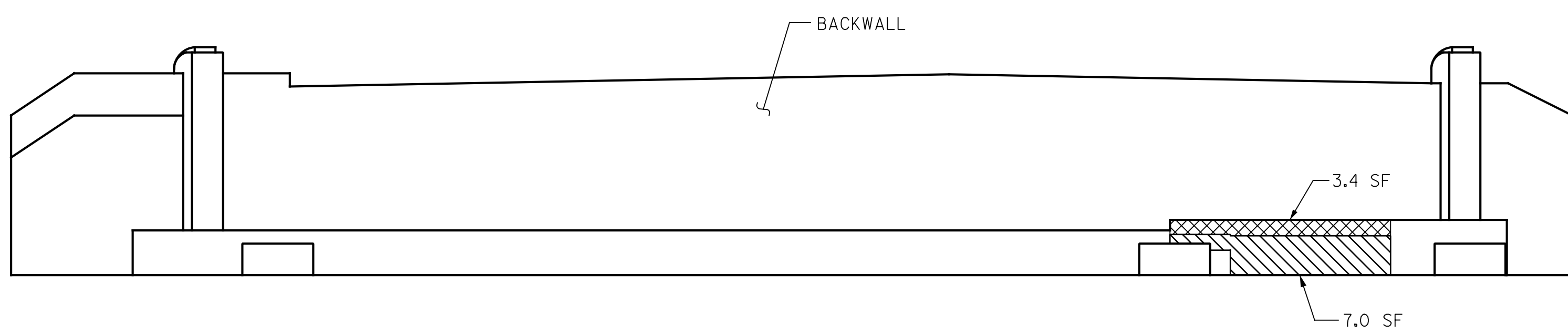
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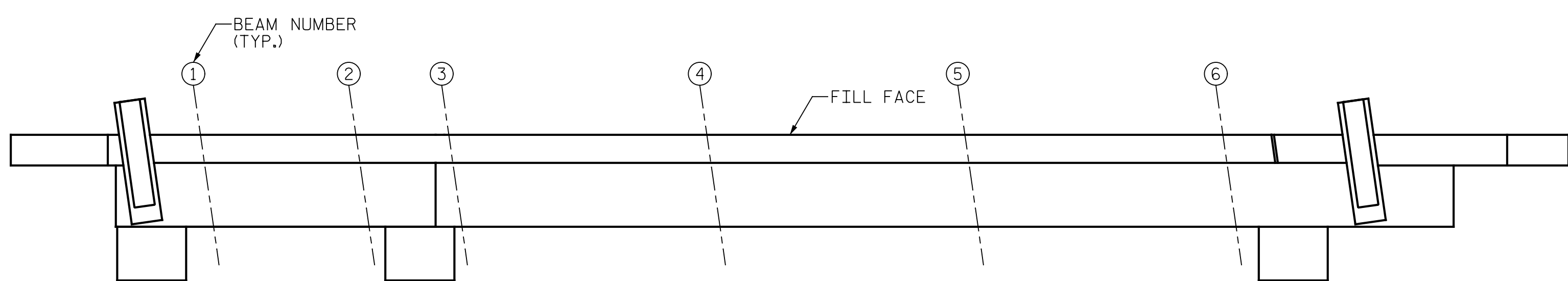
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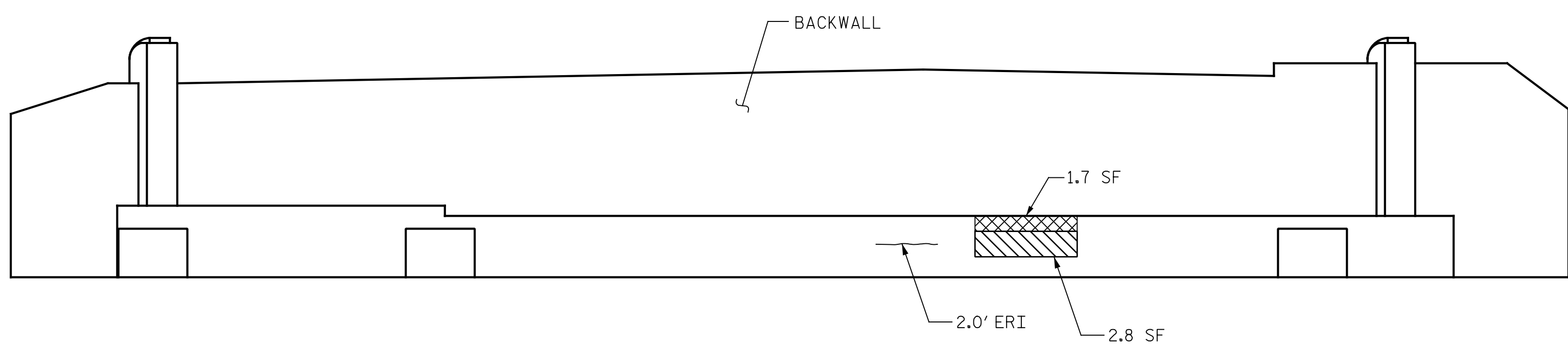
PLAN



ELEVATION
END BENT 1



PLAN



ELEVATION
END BENT 2

- SHOTCRETE REPAIR
- CONCRETE REPAIR (FORM & POUR)
- ERI - EPOXY RESIN INJECTION

AS-BUILT REPAIR QUANTITY TABLE					
END BENT 1 REPAIRS	QUANTITIES				
	ESTIMATE		ACTUAL		
SHOTCRETE REPAIRS	AREA SF	VOLUME CF	AREA SF	DEPTH FT	VOLUME CF
CAP	7.0	3.5			
BACKWALL	0.0	0.0			
CONCRETE REPAIRS	14.2	7.1			
EPOXY RESIN INJECTION		LENGTH LF		LENGTH LF	
CAP		0.0			
BACKWALL		0.0			
EPOXY COATING		SQ. FT		SQ. FT	
TOP OF BENT CAP		82			

END BENT 2 REPAIRS	QUANTITIES				
	ESTIMATE		ACTUAL		
SHOTCRETE REPAIRS	AREA SF	VOLUME CF	AREA SF	DEPTH FT	VOLUME CF
CAP	2.8	1.4			
BACKWALL	0.0	0.0			
CONCRETE REPAIRS	1.7	0.9			
EPOXY RESIN INJECTION		LENGTH LF		LENGTH LF	
CAP		2.0			
BACKWALL		0.0			
EPOXY COATING		SQ. FT		SQ. FT	
TOP OF BENT CAP		82			

VALUES IN CHART REPRESENT ESTIMATED REPAIR TOTALS AFTER REMOVAL OF UNSOUND CONCRETE, MINIMUM OF 1" BEHIND REBAR AND MINIMUM OF 2" CLEARANCE TO SAWCUT. FOR REPAIR DETAILS, SEE "TYPICAL CAP AND COLUMN REPAIR DETAILS" SHEET.

NOTES:
 REPAIR LOCATIONS AND ESTIMATE OF QUANTITIES ARE GIVEN WITH THE BEST INFORMATION AVAILABLE. IF ADDITIONAL REPAIRS NOT SHOWN ON THE DRAWINGS ARE DEEMED NECESSARY BY THE ENGINEER, THE ENGINEER WILL NOTE ON THE DRAWINGS THE APPROXIMATE LOCATION AND DESCRIPTION OF THE REPAIRS AND ADJUST THE ACTUAL QUALITIES ENTERED INTO THE AS-BUILT REPAIR QUANTITY TABLE.

CONTRACTOR SHALL SAW CUT TO A MINIMUM DEPTH OF 1/2" BUT REINFORCING STEEL SHALL NOT BE DAMAGED

CONTRACTOR SHALL REMOVE SURFACE CONCRETE TO VERIFY THAT SAW CUT DEPTH WILL NOT DAMAGE EXISTING REINFORCING STEEL.

CONTRACTOR SHALL SAW CUT THE REPAIR AREAS SO THAT THE CORNERS ARE SQUARE AS INDICATED ON THE DETAILS.

FOR CONCRETE REPAIRS, SEE SPECIAL PROVISIONS.

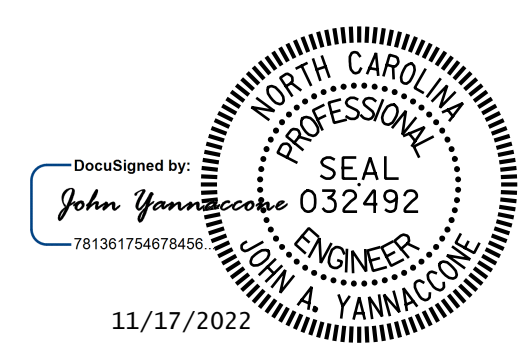
FOR SHOTCRETE REPAIRS, SEE SPECIAL PROVISIONS.

SHOTCRETE REPAIRS MAY BE REPLACED WITH CONCRETE REPAIRS WITH THE APPROVAL OF THE ENGINEER.

FOR EPOXY RESIN INJECTION, SEE SPECIAL PROVISIONS.

CLEAN AND REMOVE DEBRIS FROM THE TOP OF THE CAPS, AND AROUND THE END DIAPHRAGMS, BEARINGS AND BEAMS. APPLY EPOXY PROTECTIVE COATING TO THE TOP SURFACE OF THE CAPS. THE CONTRACTOR SHALL NOT COAT THE AREA OF THE CAPS BENEATH THE MASONRY PLATES. FOR EPOXY COATING, SEE SPECIAL PROVISIONS.

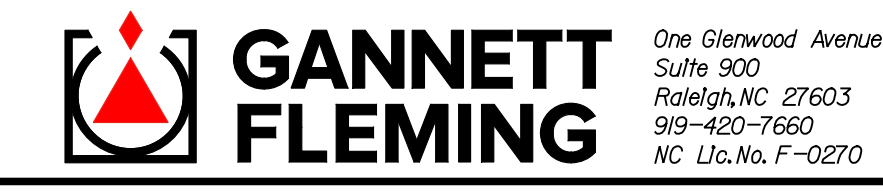
PROJECT NO. I-5955
GUILFORD COUNTY
 BRIDGE NO. 400340



STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

END BENT 1 & 2

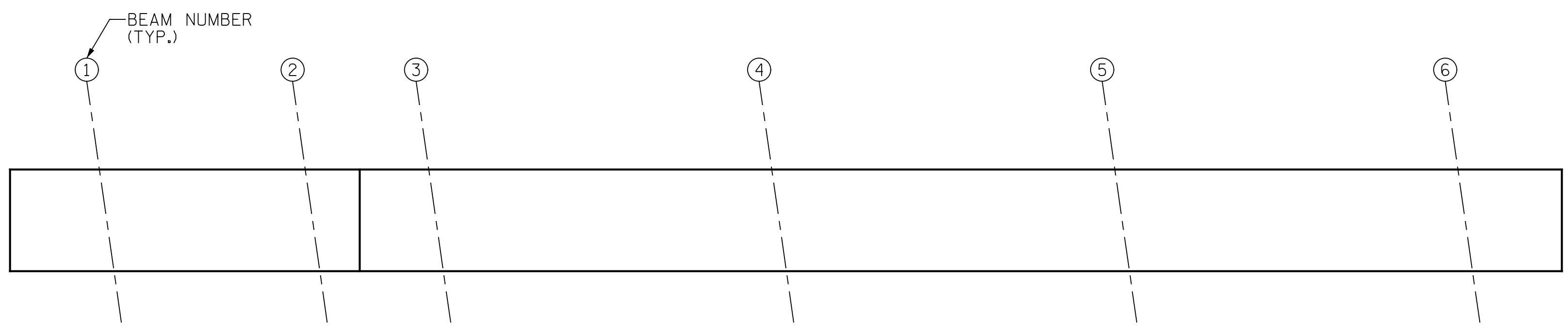
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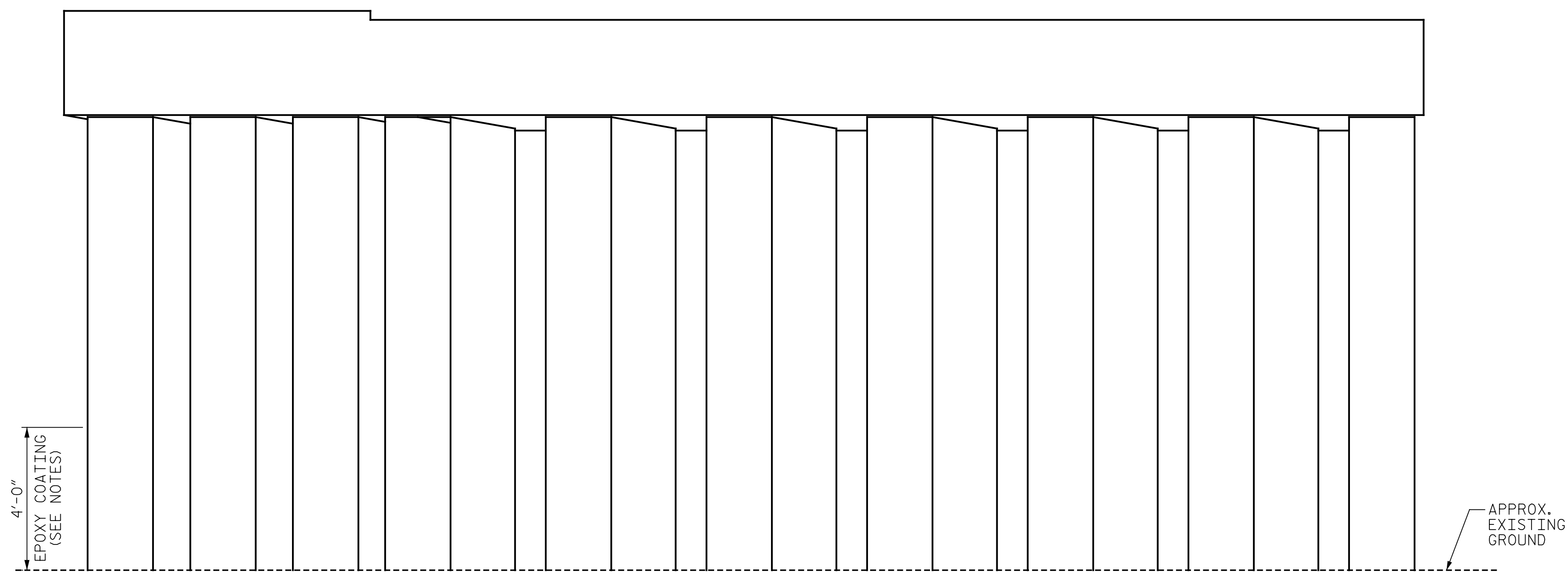
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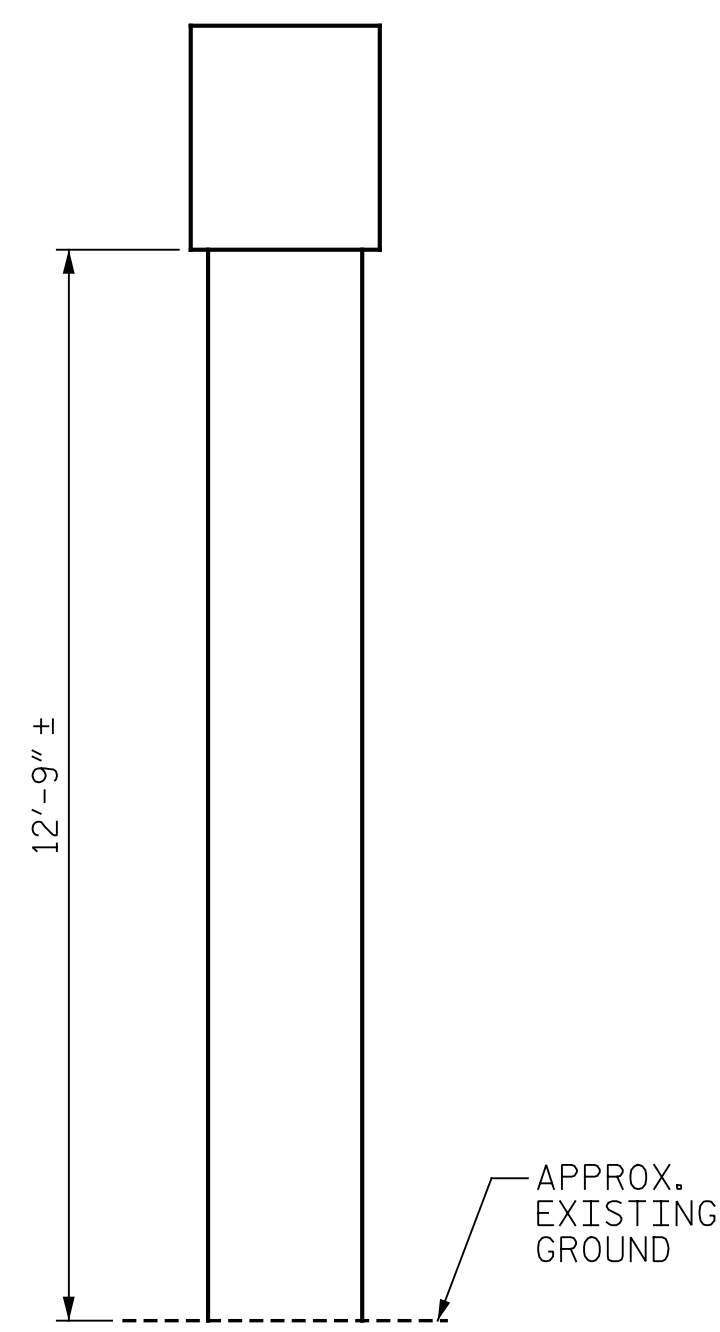
SPAN B
SPAN A

TOP OF CAP



PILE 1 PILE 2 PILE 3 PILE 4 PILE 5 PILE 6 PILE 7 PILE 8 PILE 9 PILE 10

ELEVATION



END VIEW
PILE 10

- SHOTCRETE REPAIR
- CONCRETE REPAIR (FORM & POUR)
- ERI - EPOXY RESIN INJECTION

AS-BUILT REPAIR QUANTITY TABLE					
BENT 1 REPAIRS	QUANTITIES				
	ESTIMATE		ACTUAL		
SHOTCRETE REPAIRS	AREA SF	VOLUME CF	AREA SF	DEPTH FT	VOLUME CF
CAP	0.0	0.0			
COLUMN	25.8	12.9			
CONCRETE REPAIRS	0.0	0.0			
EPOXY RESIN INJECTION		LENGTH LF	LENGTH LF		
CAP		0.0			
COLUMN		0.0			
EPOXY COATING		SQ. FT	SQ. FT		
CONCRETE PILES		294			

VALUES IN CHART REPRESENT ESTIMATED REPAIR TOTALS AFTER REMOVAL OF UNSOUND CONCRETE, MINIMUM OF 1" BEHIND REBAR AND MINIMUM OF 2" CLEARANCE TO SAWCUT. FOR REPAIR DETAILS, SEE "TYPICAL CAP AND COLUMN REPAIR DETAILS" SHEET.

NOTES:
 REPAIR LOCATIONS AND ESTIMATE OF QUANTITIES ARE BASED ON THE BEST INFORMATION AVAILABLE. IF ADDITIONAL REPAIRS NOT SHOWN ON THE DRAWINGS ARE DEEMED NECESSARY BY THE ENGINEER, THE ENGINEER WILL NOTE ON THE DRAWINGS THE APPROXIMATE LOCATIONS AND DESCRIPTION OF THE REPAIRS AND ENTER THE ACTUAL QUANTITIES INTO THE AS-BUILT REPAIR QUANTITY TABLE.

FOR REPAIRS, SEE "TYPICAL CAP AND COLUMN REPAIR DETAILS" SHEET.
 FOR SHOTCRETE REPAIRS, SEE SPECIAL PROVISIONS.
 FOR CONCRETE REPAIRS, SEE SPECIAL PROVISIONS.
 SHOTCRETE REPAIRS MAY BE REPLACED WITH CONCRETE REPAIRS WITH THE APPROVAL OF THE ENGINEER.

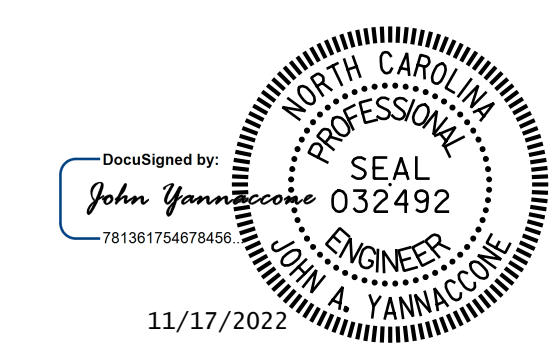
CONTRACTOR SHALL SAW CUT TO A MINIMUM DEPTH OF 1/2" BUT REINFORCING STEEL SHALL NOT BE DAMAGED. CONTRACTOR SHALL REMOVE SURFACE CONCRETE TO VERIFY THAT SAW CUT DEPTH WILL NOT DAMAGE EXISTING REINFORCING STEEL.
 CONTRACTOR SHALL SAW CUT THE REPAIR AREAS SO THAT THE CORNERS ARE SQUARE AS INDICATED ON THE DETAILS.

APPLY EPOXY PROTECTIVE COATING TO ALL SIDES OF THE CONCRETE PILES. EPOXY COATING SHALL BE APPLIED 4 FEET UP THE PILES FROM THE GROUND LINE. FOR EPOXY COATING, SEE SPECIAL PROVISIONS.

FOR EPOXY RESIN INJECTION, SEE SPECIAL PROVISIONS.
 WHEN COLUMN REPAIRS ARE INDICATED TO GROUND LINE, EXTEND REPAIR ONE (1) FOOT MIN. BELOW GROUND LINE.

PROJECT NO. I-5955
GUILFORD COUNTY
 BRIDGE NO. 400340

SHEET 1 OF 2



STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

**BENT 1
 SPAN A SIDE**

DRAWN BY : J. HARRIS DATE : 10/2022
 CHECKED BY : J. YANNACCONE DATE : 10/2022



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2			4			127