

**This electronic collection of documents is provided
for the convenience of the user
and is Not a Certified Document –**

**The documents contained herein were originally issued
and sealed by the individuals whose names and license
numbers appear on each page, on the dates appearing
with their signature on that page.**

**This file or an individual page
shall not be considered a certified document.**

TIP PROJECT: I-5955

CONTRACT: C204736

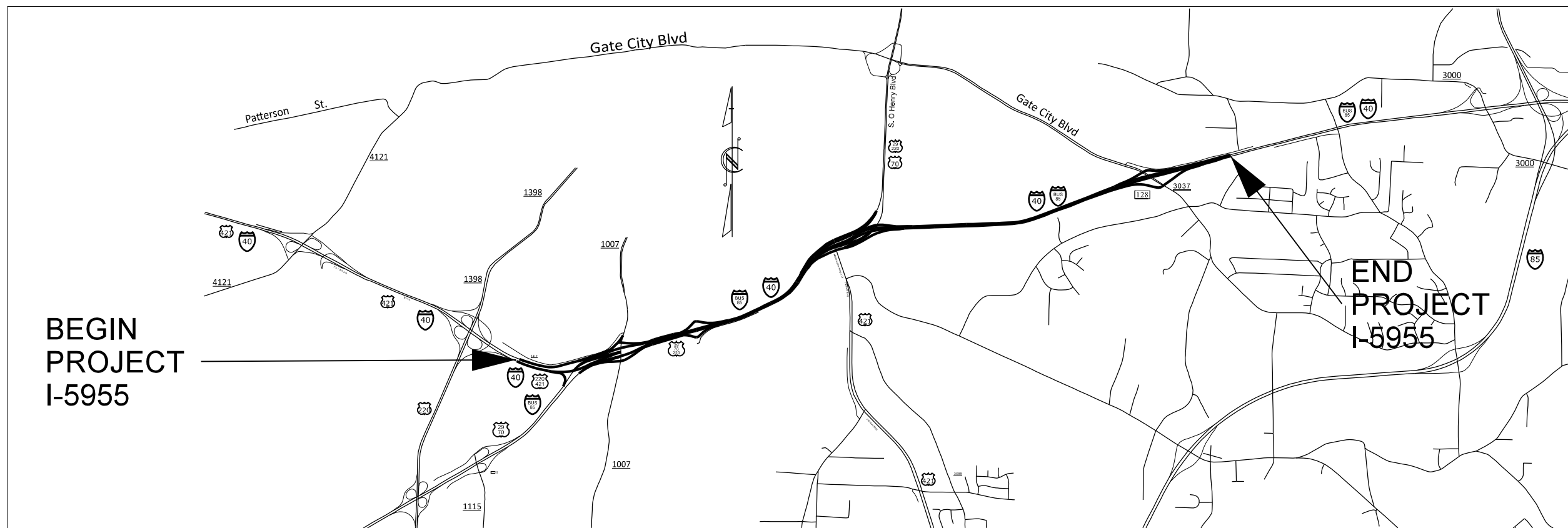
STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

GUILFORD COUNTY

LOCATION: *I-40 FROM S BUFFALO CREEK BRIDGE EAST OF US 220 (FREEMAN MILL ROAD) TO EAST OF SR 3037/SR 4240 (GATE CITY BOULEVARD) IN GREENSBORO*

TYPE OF WORK: *PAVEMENT & BRIDGE REHABILITATION*

| | | | |
|-----------------|-----------------------------|-------------|--------------|
| STATE | STATE PROJECT REFERENCE NO. | SHEET NO. | TOTAL SHEETS |
| N.C. | I-5955 | 1 | |
| STATE PROJ. NO. | F. A. PROJ. NO. | DESCRIPTION | |
| 45908.1.1 | 0040115 | (PE) | |
| 45908.3.1 | 0040115 | (CONST) | |
| | | | |
| | | | |
| | | | |



GRAPHIC SCALES

NOT TO SCALE

DESIGN DATA

PROJECT LENGTH

Length of Roadway TIP Project I-5955 = 4.735 Miles
Length of Structure TIP Project I-5955 = 0.580 Miles
Total Length TIP Project I-5955 = 5.315 Miles

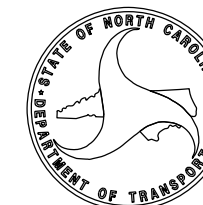
Prepared in the Office of:
DIVISION OF HIGHWAYS

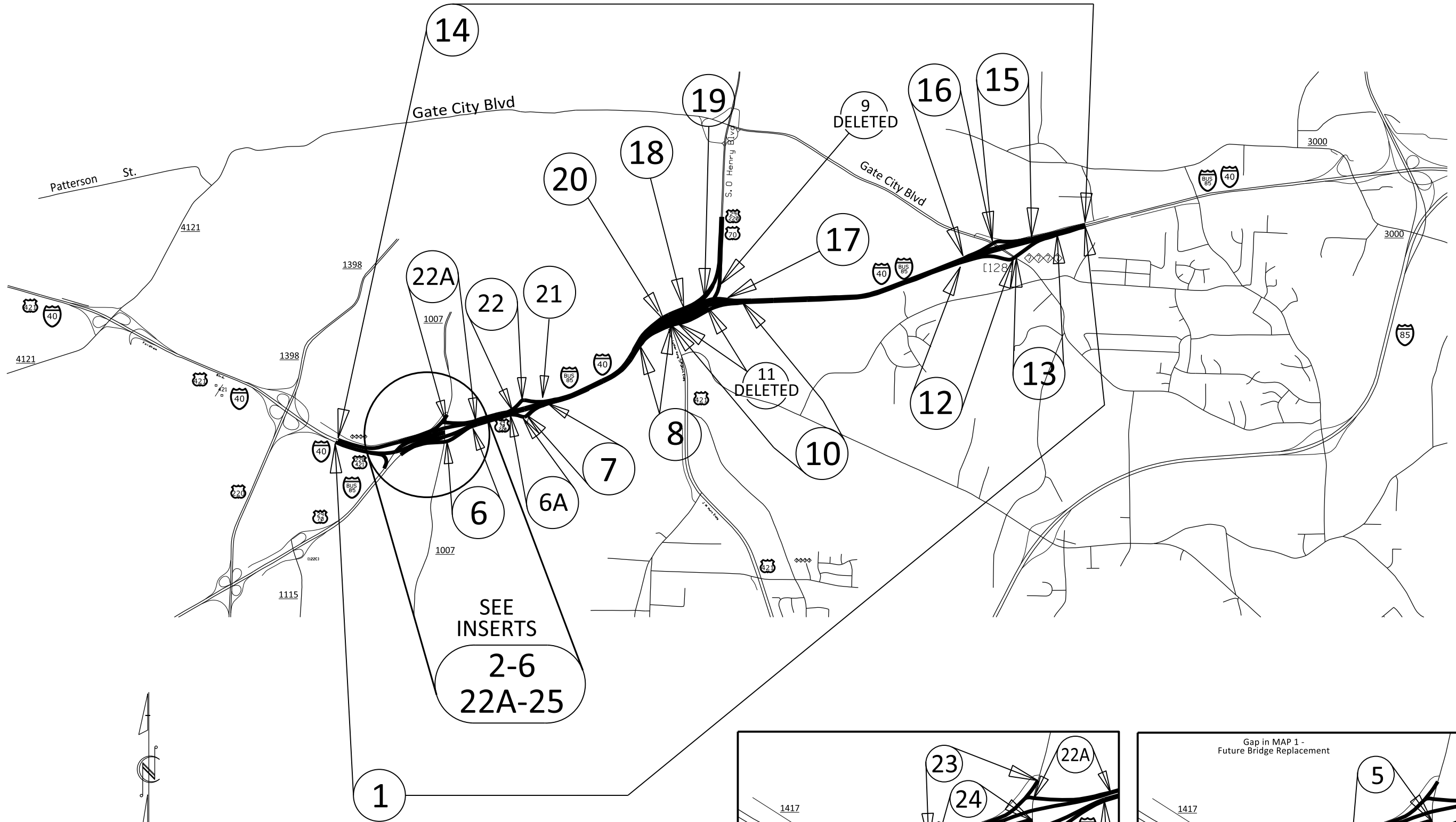
1000 Birch Ridge Dr., Raleigh NC, 27610

2018 STANDARD SPECIFICATIONS

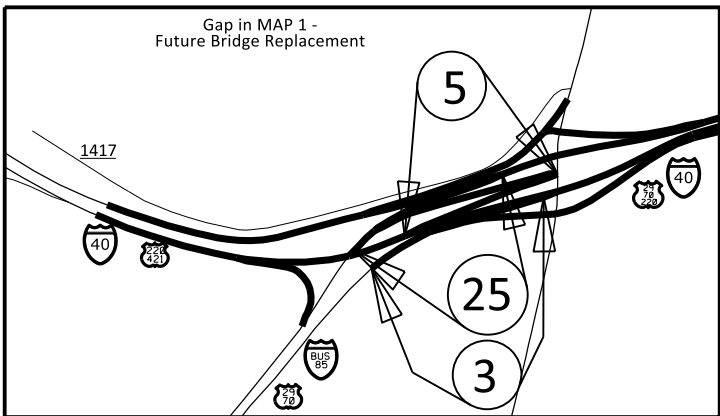
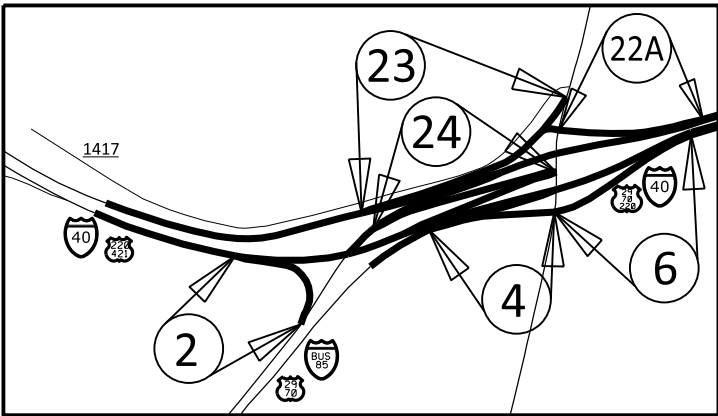
LETTING DATE:
JANUARY 17, 2023

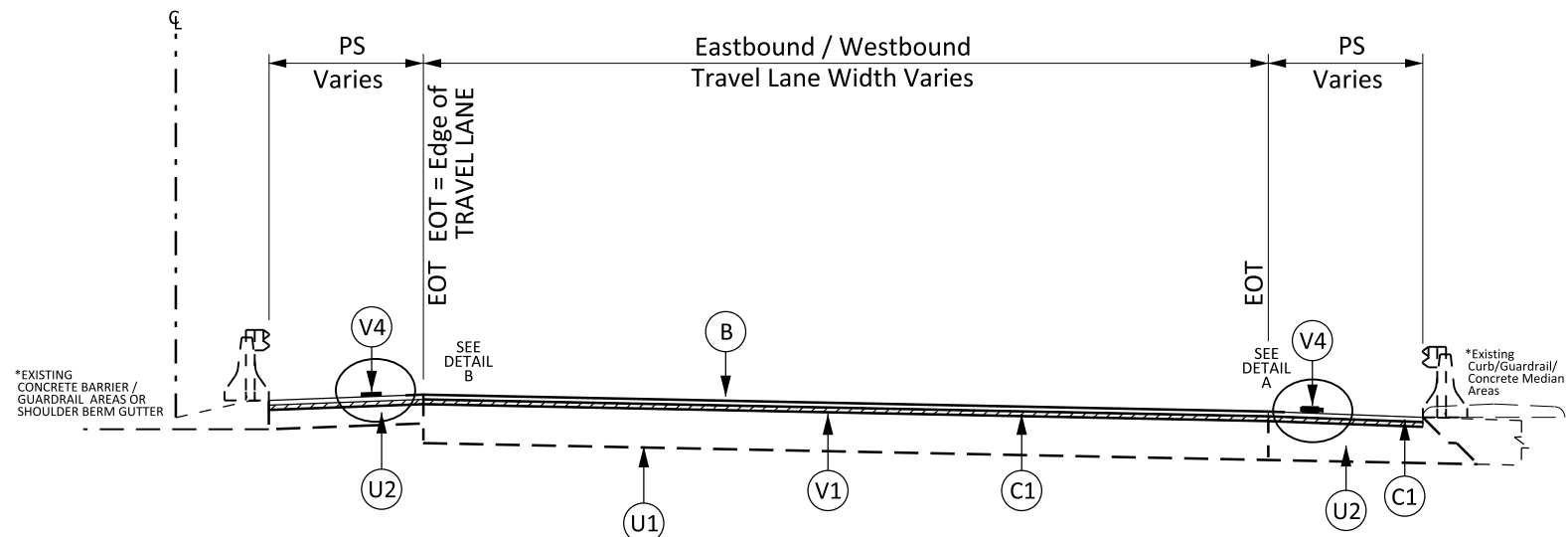
Chad Reimakoski
PROJECT ENGINEER





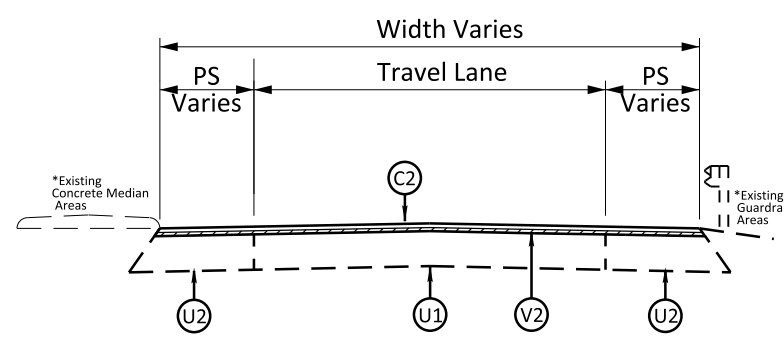
GUILFORD COUNTY



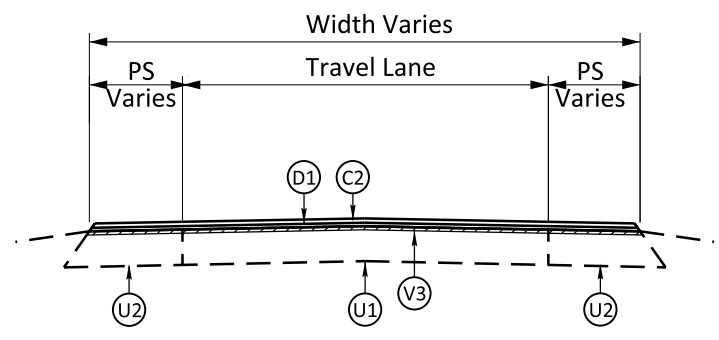


TYPICAL SECTION NO. 1
MAPS 1, 14

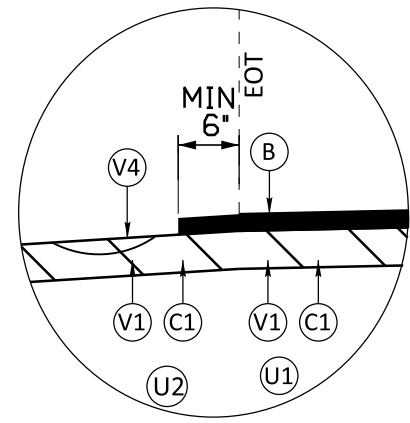
*NOTE: TYPICAL SECTION CONSTRUCTION SEQUENCE:
1. MILL 2" AND FILL WITH 2" SURFACE COURSE, TYPE S9.5D
2. OVERLAY TRAVEL LANES WITH 3/4" OPEN-GRADED ASPHALT FRICTION COURSE, TYPE FC-1 MODIFIED.



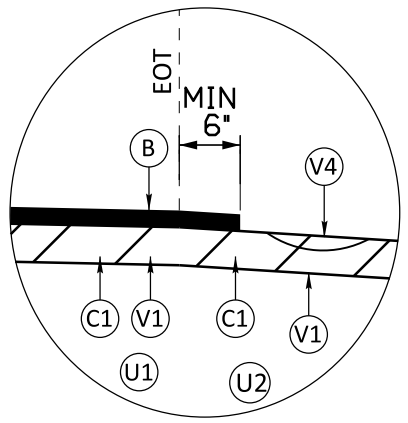
TYPICAL SECTION NO. 4
MAP 5, 6, 19



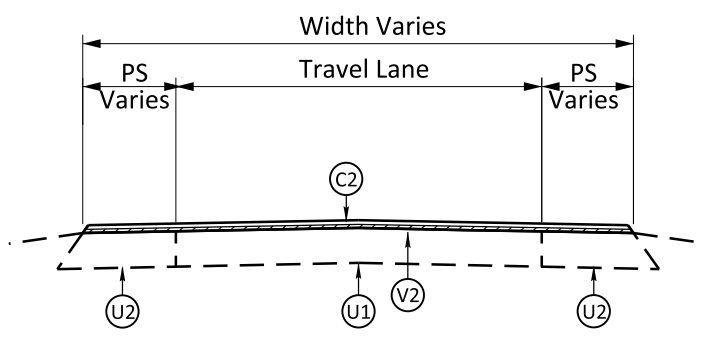
TYPICAL SECTION NO. 5
MAP 13



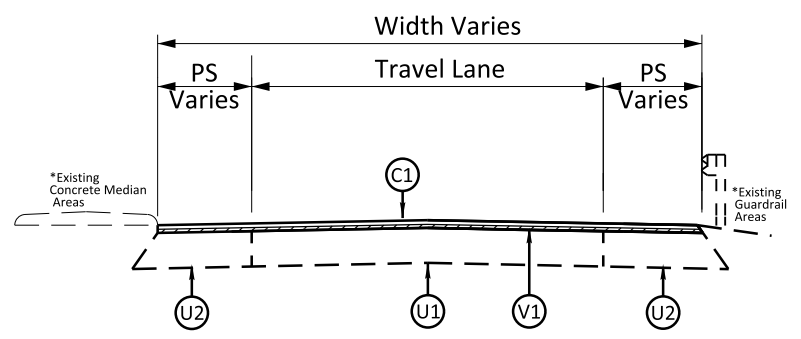
DETAIL B
OVERLAP SHOULDERS



DETAIL A
OVERLAP SHOULDERS

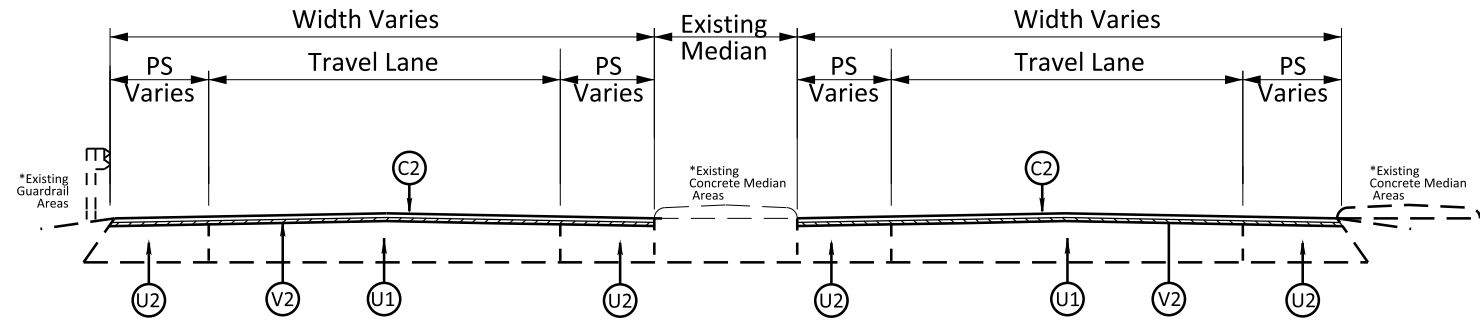


TYPICAL SECTION NO. 2
MAP 2, 4, 5, 6, 12, 15, 16,
17, 19, 20, 21, 22, 23, 24

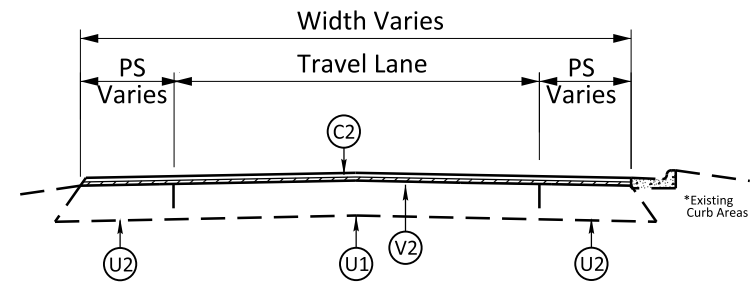


TYPICAL SECTION NO. 3
MAP 3, 25

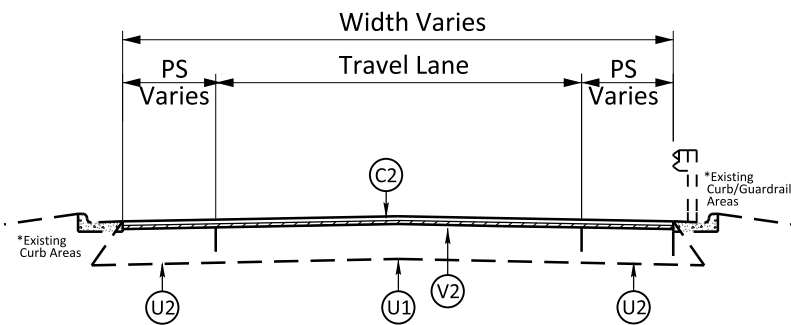
| PAVEMENT SCHEDULE | |
|-------------------|---|
| B | 3/4" OGAF C Type FC-1 Modified |
| C1 | PROP. APPROX. 2" ASPHALT. CONC. SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD. |
| C2 | PROP. APPROX. 1 1/2" ASPHALT. CONC. SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. |
| D1 | PROP. APPROX. 2 1/2" ASPHALT. CONC. INTERMEDIATE COURSE, TYPE I19.0C AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD. |
| U1 | EXISTING TRAVELWAY |
| U2 | EXISTING PAVED SHOULDER |
| V1 | MILL ASPHALT PAVEMENT 2" DEPTH |
| V2 | MILL ASPHALT PAVEMENT 1 1/2" DEPTH |
| V3 | MILL ASPHALT PAVEMENT 4" DEPTH |
| V4 | MILLED RUMBLE STRIPS, USE IN CONJUNCTION WITH STANDARD DRAWING NO. 665.01 |



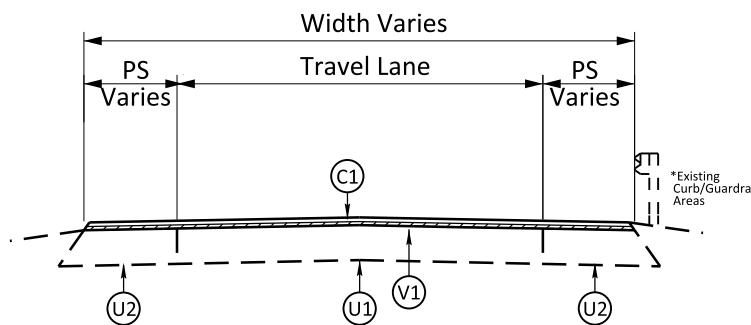
TYPICAL SECTION NO. 6
MAP 6



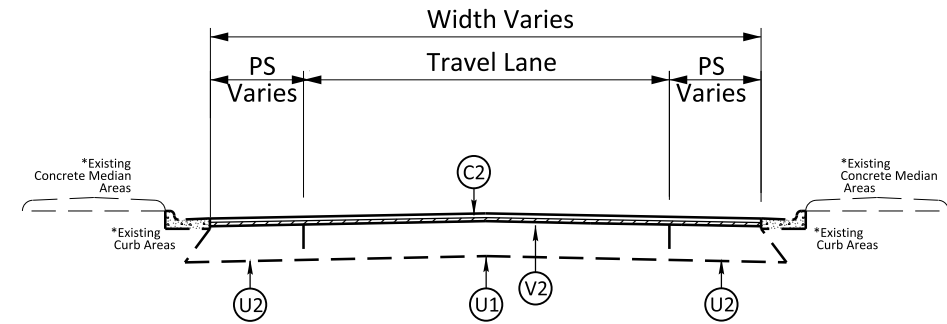
TYPICAL SECTION NO. 13
MAP 4, 20, 21, 22



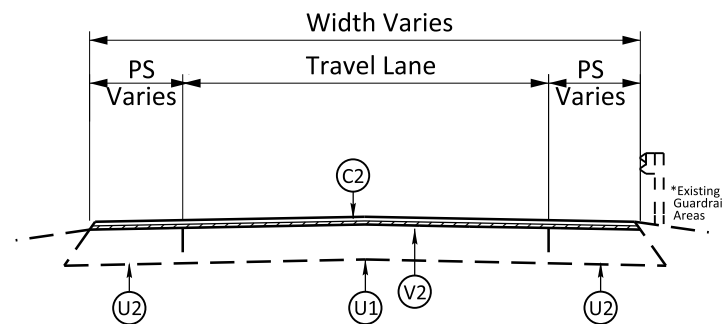
TYPICAL SECTION NO. 7
MAP 6A, 22A



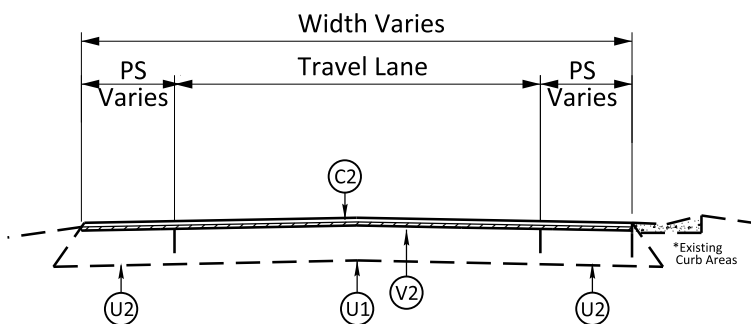
TYPICAL SECTION NO. 10
MAP 10, 18



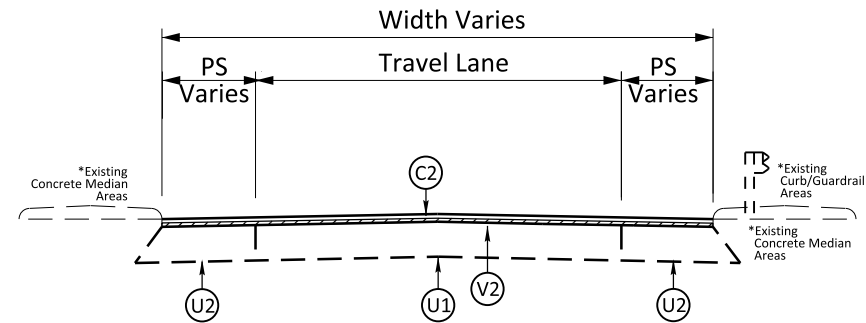
TYPICAL SECTION NO. 14
MAP 23



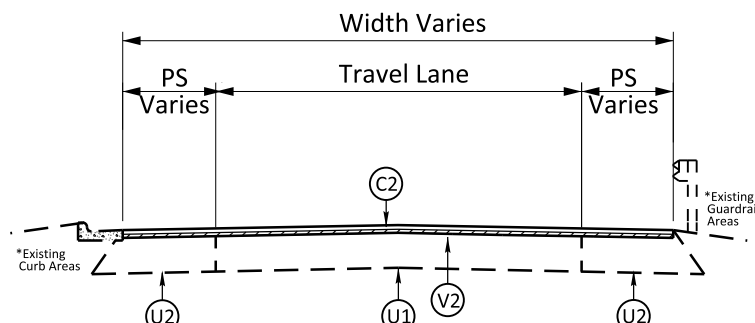
TYPICAL SECTION NO. 8
MAP 4, 6A, 7, 8, 17



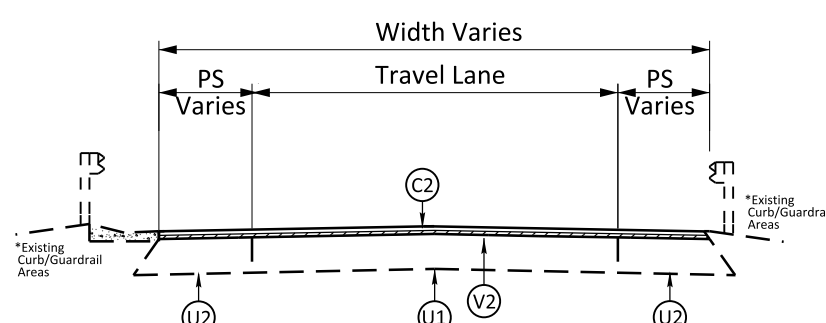
TYPICAL SECTION NO. 11
MAP 15



TYPICAL SECTION NO. 15
MAP 24

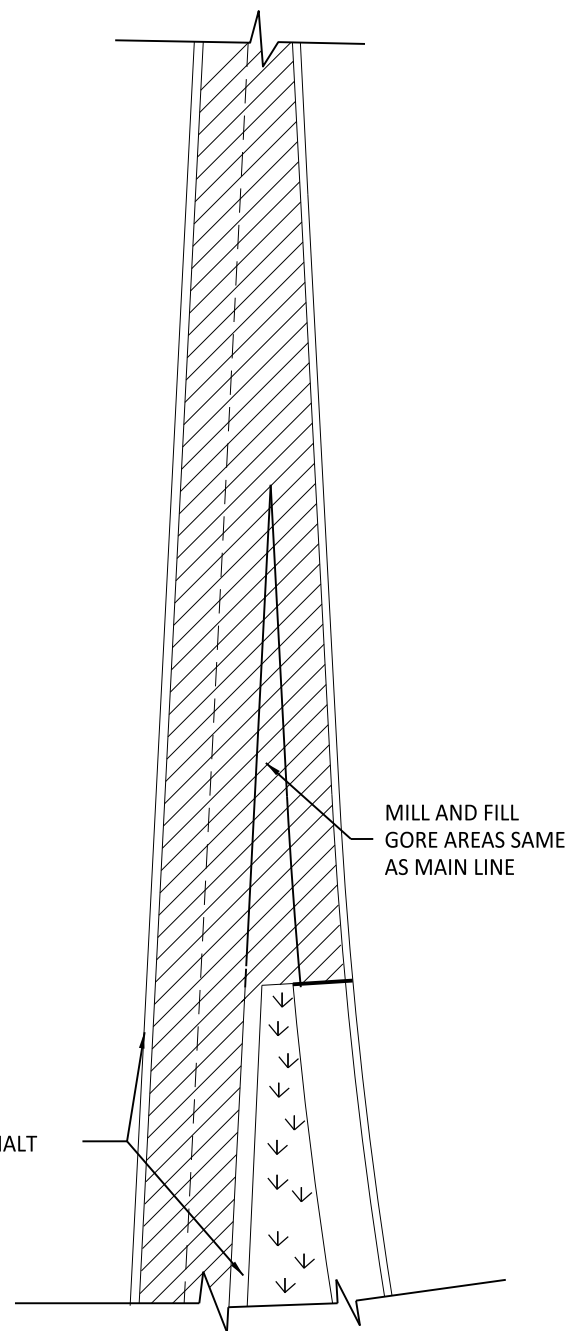


TYPICAL SECTION NO. 9
MAP 8

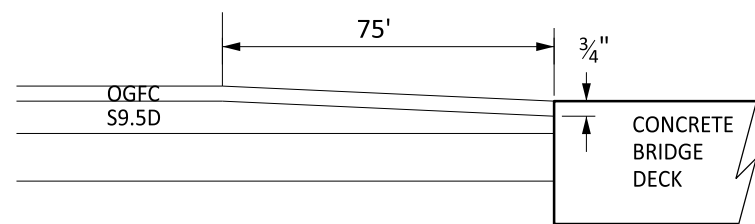


TYPICAL SECTION NO. 12
MAP 17

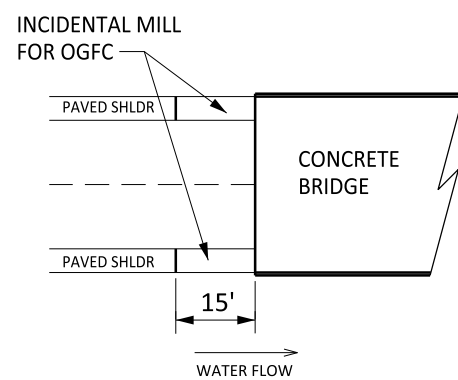
| PAVEMENT SCHEDULE | |
|-------------------|--|
| B | ¾" OGAF C Type FC-1 Modified |
| C1 | PROP. APPROX. 2" ASPHALT. CONC. SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD. |
| C2 | PROP. APPROX. 1½" ASPHALT. CONC. SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. |
| D1 | PROP. APPROX. 2½" ASPHALT. CONC. INTERMEDIATE COURSE, TYPE I19.0C AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD. |
| U1 | EXISTING TRAVELWAY |
| U2 | EXISTING PAVED SHOULDER |
| V1 | MILL ASPHALT PAVEMENT 2" DEPTH |
| V2 | MILL ASPHALT PAVEMENT 1½" DEPTH |
| V3 | MILL ASPHALT PAVEMENT 4" DEPTH |
| V4 | MILLED RUMBLE STRIPS, USE IN CONJUNCTION WITH STANDARD DRAWING NO. 665.01 |



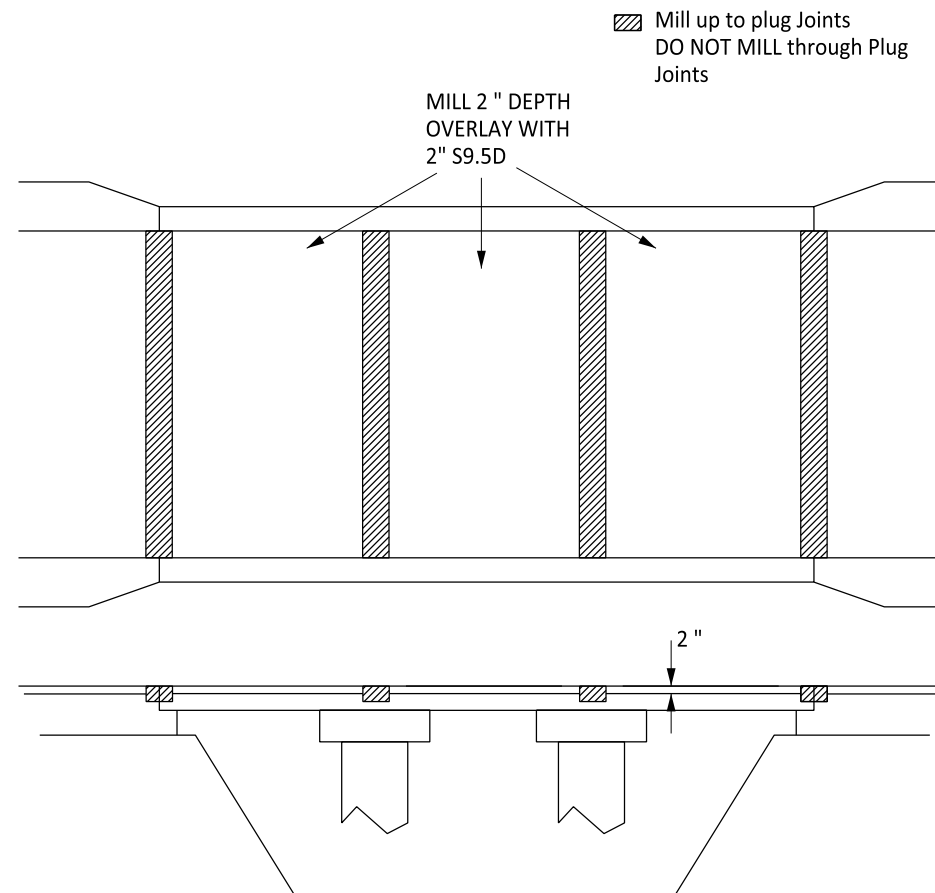
DETAIL - GORE AREAS



**INCIDENTAL MILLING PAVEMENT
DETAIL AT CONCRETE BRIDGES**



MILLING DETAIL 1

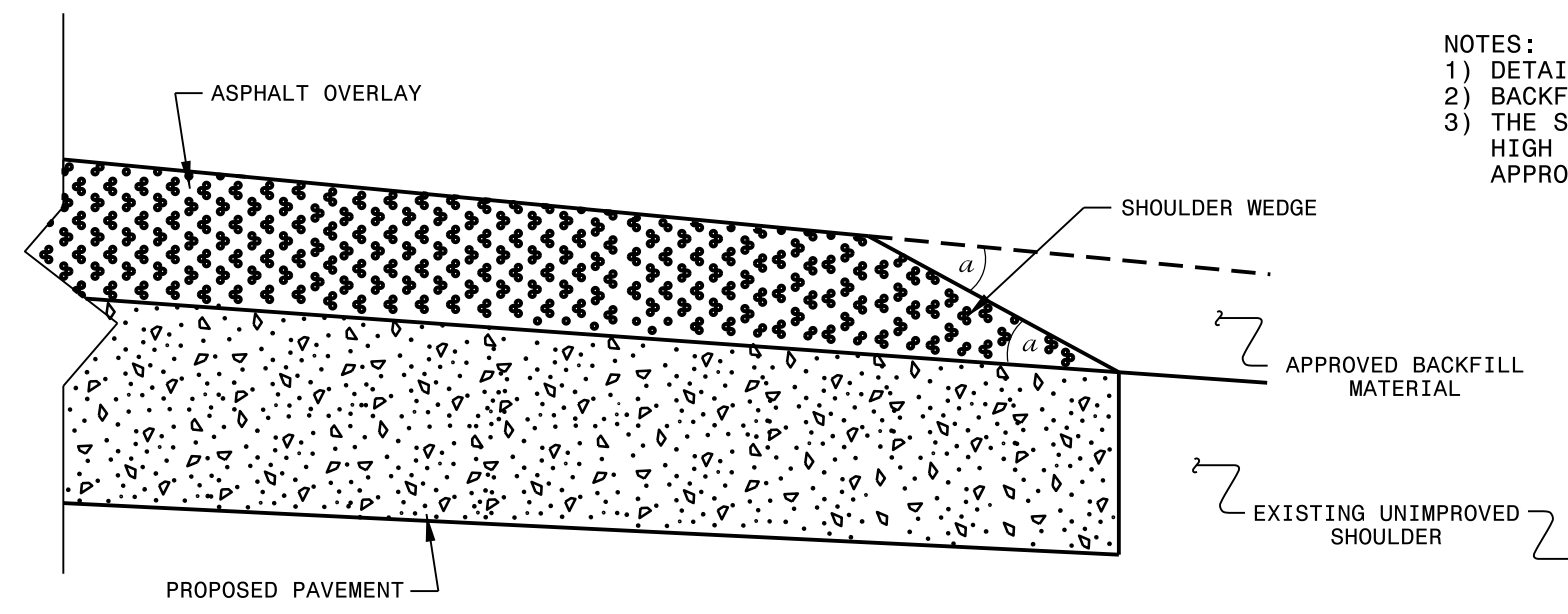


ASPHALT BRIDGE MILLING

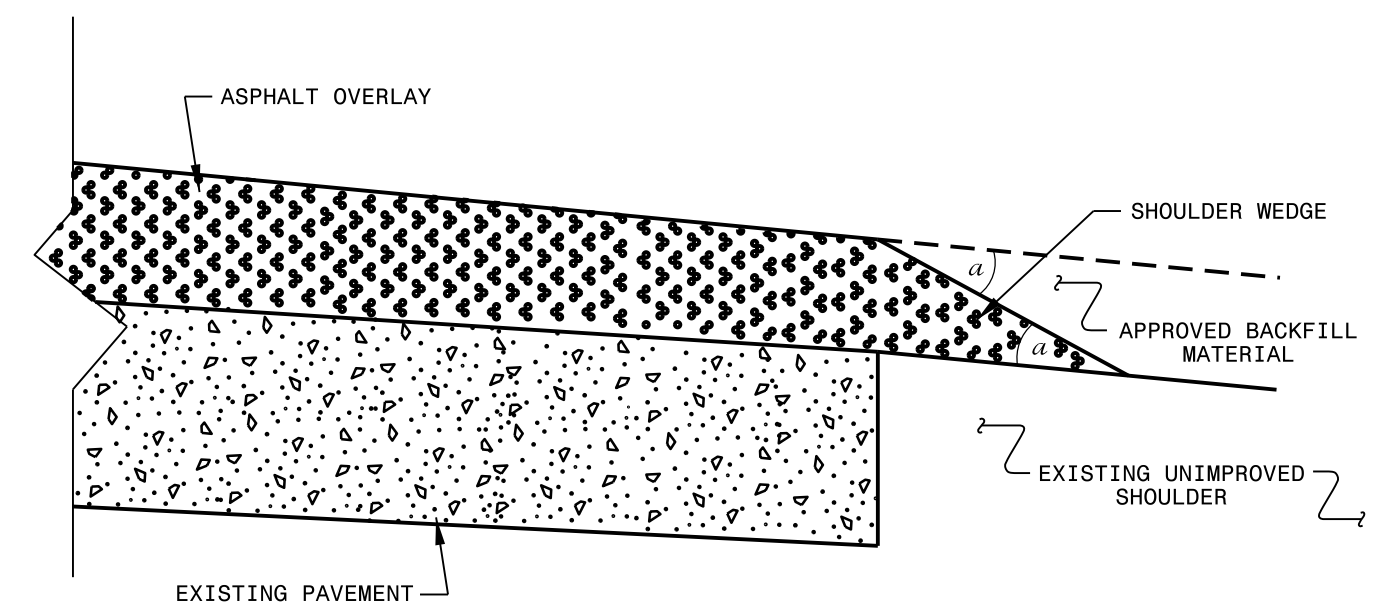
NOTE

1. All milling, S9.5D and OGFC must be completed before plug joint work can begin.
2. OGFC may be placed directly over existing plug joints.
3. Refer to the structure plans and contract special provisions for any necessary bridge deck repairs encountered after milling the AWS, and before placing the new AWS, on Bridges 285, 299, 339 and 340.

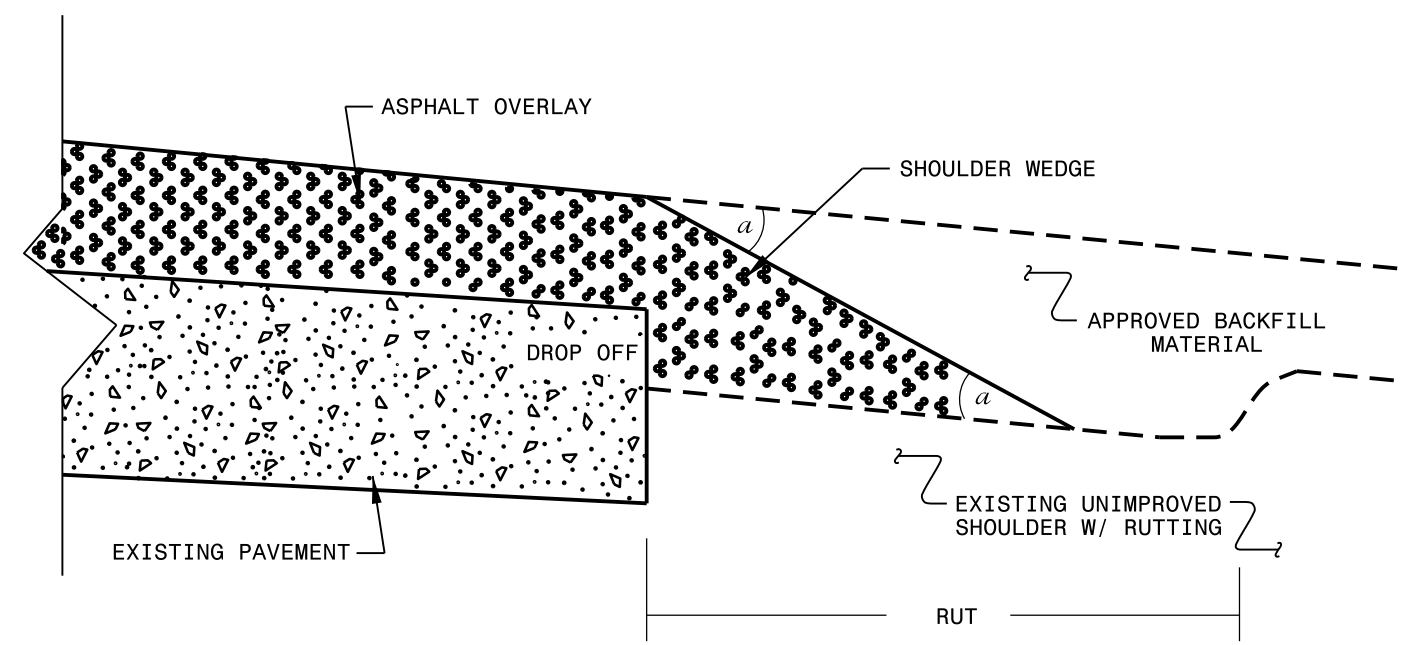
- NOTES:
- 1) DETAIL DOES NOT APPLY TO OGAFc AND ULTRA-THIN BONDED WEARING COURSE.
 - 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
 - 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS, SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS APPROVED BY THE ENGINEER.



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ Widening or
 with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL
 (Resurfacing Adjacent to
 Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

\$\$\$\$SYTIME\$\$\$\$
 \$\$\$USERNAME\$\$\$

| | |
|--|---------------|
| CONTRACT STANDARDS AND DEVELOPMENT UNIT | |
| Office 919-707-6950 FAX 919-250-4119 | |
| SHOULDER WEDGE DETAILS | |
| ORIGINAL BY: T.SPELL | DATE: 7-19-11 |
| MODIFIED BY: | DATE: 2/2/16 |
| CHECKED BY: | DATE: |
| FILE SPEC.: s:\usr\detatl1\stand\shoulderwedgedeta11.dgn | |

DOCUMENT NOT CONSIDERED FINAL
UNLESS ALL SIGNATURES COMPLETED



For OGFC
Incidental Mill
0" to 3/4"
Over 75'
←-----

For OGFC
Incidental Mill
0" to 3/4"
Over 75'
----->

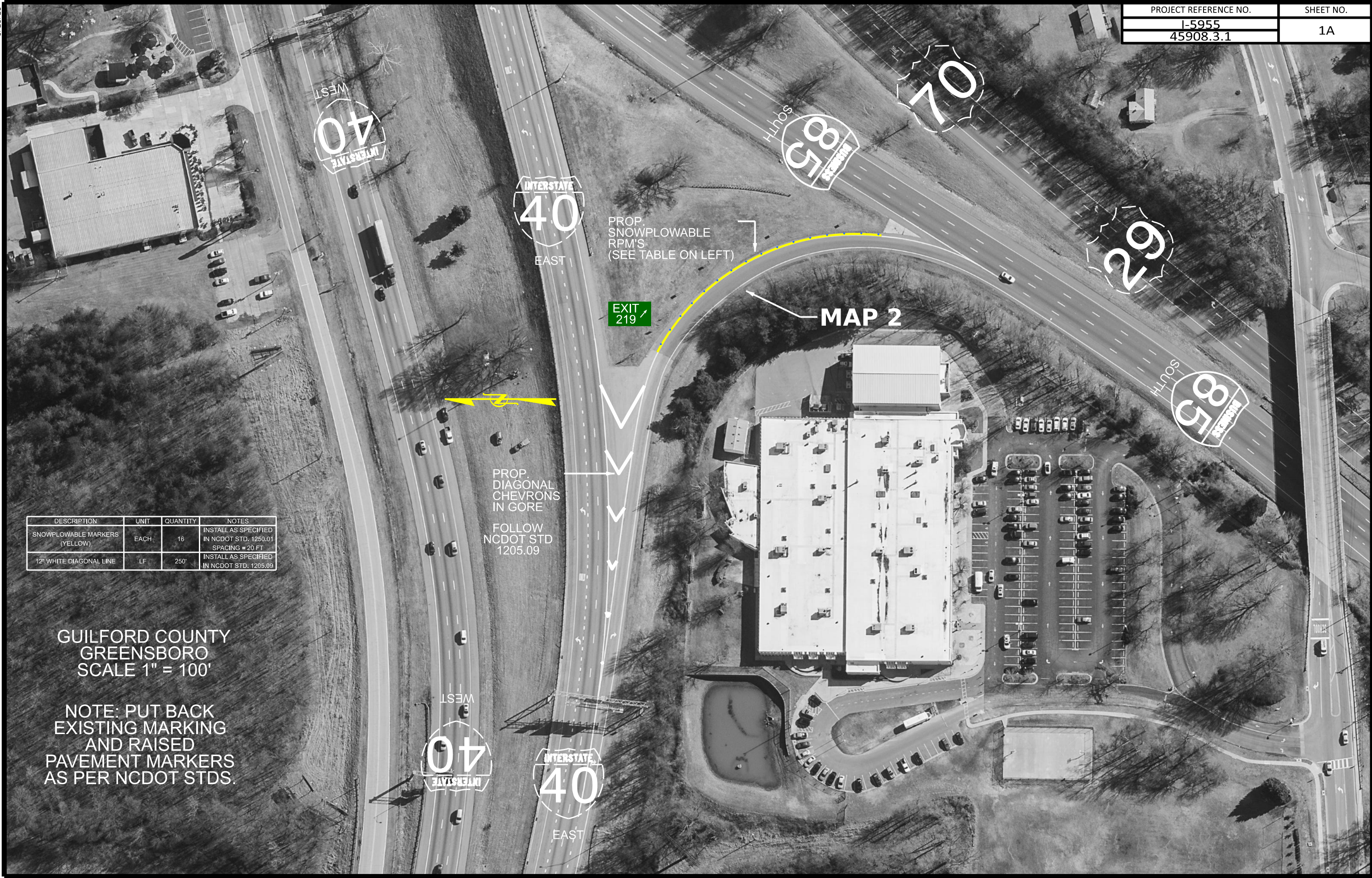
Note: Incidental mill both shoulders 0" to 3/4"
over the last 15' for drainage



Map 18 Incidental Milling
Detail for OGFC
on Bridge #285

5/26/20

| | |
|-----------------------|-----------|
| PROJECT REFERENCE NO. | SHEET NO. |
| I-5955 45908.3.1 | 1A |



PROP.
SNOWPLOWABLE
RPM'S
(SEE TABLE ON LEFT)

EXIT
219

MAP 2

PROP.
DIAGONAL
CHEVRONS
IN GORE

FOLLOW
NCDOT STD
1205.09

| DESCRIPTION | UNIT | QUANTITY | NOTES |
|-------------------------------|------|----------|--|
| SNOWPLOWABLE MARKERS (YELLOW) | EACH | 16 | INSTALL AS SPECIFIED IN NCDOT STD. 1250.01 SPACING = 20 FT |
| 12" WHITE DIAGONAL LINE | LF | 250' | INSTALL AS SPECIFIED IN NCDOT STD. 1205.09 |

GUILFORD COUNTY
GREENSBORO
SCALE 1" = 100'

NOTE: PUT BACK
EXISTING MARKING
AND RAISED
PAVEMENT MARKERS
AS PER NCDOT STDS.

| | | |
|-------------|-----------|-----------|
| PROJECT NO. | SHEET NO. | TOTAL NO. |
| I-5955 | 7 | |

SUMMARY OF QUANTITIES

| PROJECT NO | COUNTY | MAP NO | ROUTE | DESCRIPTION | TYP NO | LANES | LANE TYPE | FINAL SURFACE TESTING REQUIRED | WARM MIX ASPHALT REQUIRED | LENGTH | WIDTH | 1297000000-E | | | 1330000000-E | 1503000000-E | 1524200000-E | 1575000000-E | 1577000000-E | 1662000000-E | 1840000000-E | 4424000000-N | 4434000000-N | 4600000000-N | 5255000000-N | 7444000000-E |
|-------------------------------------|----------|--------|--|--|--------|-------|-----------|--------------------------------|---------------------------|---------------|-------|------------------------------------|--|------------------------------------|--------------------|--|---|------------------------------|---|------------------------------|----------------------|-----------------------------|------------------------------------|-------------------------------|-------------------|------------------------|
| | | | | | | | | | | | | MILLING ASPHALT PAVEMENT, 2" DEPTH | MILLING ASPHALT PAVEMENT, 1-1/2" DEPTH | MILLING ASPHALT PAVEMENT, 4" DEPTH | INCIDENTAL MILLING | ASPHALT CONC INTERMEDIATE, TYPE 119.0C | ASPHALT CONC SURFACE COURSE, TYPE 59.5D | ASPHALT BINDER FOR PLANT MIX | POLYMER MODIFIED ASPHALT BINDER FOR PLANT MIX | OPEN-GRADED ASPHALT FRICTION | MILLED RUMBLE STRIPS | WORK ZONE PRESENCE LIGHTING | SEQUENTIAL FLASHING WARNING LIGHTS | CONNECTED LANE CLOSURE DEVICE | PORTABLE LIGHTING | INDUCTIVE LOOP SAW CUT |
| | | | | | | | | | | MI | FT | SY | SY | SY | SY | TON | TON | TON | TON | TON | LF | EA | EA | EA | LS | LF |
| 45908.3.1 | Guilford | 1 | I-40 EB | I-40 EB FROM BRIDGE OVER BUFFALO CREEK TO JOINT EAST OF EAST GATE CITY BLVD/E LEE ST | 1 | 3 | | NO | NO | 5.315 | 45 | 175,101 | | 6,522 | | 19,697 | | 1,510 | 5,897 | 53,040 | | | | | | |
| TOTAL FOR MAP NO. 1 | | | | | | | | | | 5.315 | | 175,101 | | 6,522 | | 19,697 | | 1,510 | 5,897 | 53,040 | | | | | | |
| 45908.3.1 | Guilford | 2 | RAMP TO BUS 85 SB | I-40 EB RAMP TO BUS 85 SB | 2 | 1 | | NO | NO | 0.058 | 22 | | 832 | | | 72 | | 5 | | | | | | | | |
| TOTAL FOR MAP NO. 2 | | | | | | | | | | 0.058 | | | 832 | | | 72 | | 5 | | | | | | | | |
| 45908.3.1 | Guilford | 3 | BUS 85 NB | BUS 85 NB TO I-40 EB | 3 | 2 | | NO | NO | 0.177 | 31 | 5,516 | | 537 | | 621 | | 50 | 179 | 930 | | | | | | |
| TOTAL FOR MAP NO. 3 | | | | | | | | | | 0.177 | | 5,516 | | 537 | | 621 | | 50 | 179 | 930 | | | | | | |
| 45908.3.1 | Guilford | 4 | BUS 85 NB RAMP TO RANDLEMAN RD | BUS 85 NB RAMP TO RANDLEMAN RD | 2,8,13 | 1 | | NO | NO | 0.08 | 29 | | 1,360 | | | 115 | | 10 | | | | | | | | 450 |
| TOTAL FOR MAP NO. 4 | | | | | | | | | | 0.08 | | | 1,360 | | | 115 | | 10 | | | | | | | | 450 |
| 45908.3.1 | Guilford | 5 | I-40 EB RAMP TO RANDLEMAN RD | I-40 EB RAMP TO RANDLEMAN RD | 2,4 | 1 | | NO | NO | 0.139 | 27 | | 2,426 | | | 205 | | 15 | | | | | | | | 400 |
| TOTAL FOR MAP NO. 5 | | | | | | | | | | 0.139 | | | 2,426 | | | 205 | | 15 | | | | | | | | 400 |
| 45908.3.1 | Guilford | 6 | W PREDDY BLVD/RANDLEMAN RD RAMP TO I-40 EB | W PREDDY BLVD/RANDLEMAN RD RAMP TO I-40 EB | 2,4,6 | 1 | | NO | NO | 0.194 | 19 | | 3,890 | | | 330 | | 20 | | | | | | | | 450 |
| TOTAL FOR MAP NO. 6 | | | | | | | | | | 0.194 | | | 3,890 | | | 330 | | 20 | | | | | | | | 450 |
| 45908.3.1 | Guilford | 6A | I-40 EB RAMP TO S ELM-EUGENE ST | I-40 EB RAMP TO S ELM-EUGENE ST | 7,8 | 1 | | NO | NO | 0.077 | 26 | | 1,655 | | | 140 | | 10 | | | | | | | | 650 |
| TOTAL FOR MAP NO. 6A | | | | | | | | | | 0.077 | | | 1,655 | | | 140 | | 10 | | | | | | | 650 | |
| 45908.3.1 | Guilford | 7 | S ELM-EUGENE ST RAMP TO I-40 EB | S ELM-EUGENE ST RAMP TO I-40 EB | 8 | 1 | | NO | NO | 0.092 | 21 | | 1,296 | | | 110 | | 10 | | | | | | | | |
| TOTAL FOR MAP NO. 7 | | | | | | | | | | 0.092 | | | 1,296 | | | 110 | | 10 | | | | | | | | |
| 45908.3.1 | Guilford | 8 | I-40 EB OFF RAMP TO MLK JR DR | I-40 EB OFF RAMP TO MLK JR DR | 8,9 | 1 | | NO | NO | 0.152 | 28 | | 3,684 | | | 312 | | 20 | | | | | | | | 650 |
| TOTAL FOR MAP NO. 8 | | | | | | | | | | 0.152 | | | 3,684 | | | 312 | | 20 | | | | | | | 650 | |
| 45908.3.1 | Guilford | 9 | MAP DELETED | MAP DELETED | | | | NO | NO | 0 | 0 | | | | | | | | | | | | | | | |
| TOTAL FOR MAP NO. 9 | | | | | | | | | | 0 | | | | | | | | | | | | | | | | |
| 45908.3.1 | Guilford | 10 | MLK JR DR RAMP TO I-40 EB | MLK JR DR RAMP TO I-40 EB | 10 | 2 | | NO | NO | 0.365 | 27 | 6,974 | | | | 789 | | 50 | | | | | | | | |
| TOTAL FOR MAP NO. 10 | | | | | | | | | | 0.365 | | 6,974 | | | | 789 | | 50 | | | | | | | | |
| 45908.3.1 | Guilford | 11 | MAP DELETED | MAP DELETED | | | | NO | NO | 0 | 0 | | | | | | | | | | | | | | | |
| TOTAL FOR MAP NO. 11 | | | | | | | | | | 0 | | | | | | | | | | | | | | | | |
| 45908.3.1 | Guilford | 12 | I-40 EB RAMP TO E GATE CITY BLVD/E LEE ST | I-40 EB RAMP TO E GATE CITY BLVD/E LEE ST | 2 | 1 | | NO | NO | 0.258 | 24 | | 3,806 | | | 322 | | 20 | | | | | | | | 425 |
| TOTAL FOR MAP NO. 12 | | | | | | | | | | 0.258 | | | 3,806 | | | 322 | | 20 | | | | | | | 425 | |
| 45908.3.1 | Guilford | 13 | E GATE CITY BLVD/E LEE ST RAMP TO I-40 EB | E GATE CITY BLVD/E LEE ST RAMP TO I-40 EB | 5 | 1 | | NO | NO | 0.316 | 33 | | | 3,054 | 436 | 258 | 25 | 15 | | | | | | | | |
| TOTAL FOR MAP NO. 13 | | | | | | | | | | 0.316 | | | | 3,054 | 436 | 258 | 25 | 15 | | | | | | | | |
| 45908.3.1 | Guilford | 14 | I-40 WB | I-40 WB FROM JOINT EAST OF GATE CITY BLVD TO BRIDGE OVER BUFFALO CREEK | 1 | 3 | | NO | NO | 4.9 | 35 | 176,605 | | 4,625 | | 19,864 | | 1,495 | 5,466 | 53,790 | | | | | | |
| TOTAL FOR MAP NO. 14 | | | | | | | | | | 4.9 | | 176,605 | | 4,625 | | 19,864 | | 1,495 | 5,466 | 53,790 | | | | | | |
| 45908.3.1 | Guilford | 15 | I-40 WB RAMP TO E GATE CITY BLVD | I-40 WB RAMP TO E GATE CITY BLVD | 2,11 | 1 | | NO | NO | 0.128 | 29 | | 2,768 | | | 234 | | 15 | | | | | | | | |
| TOTAL FOR MAP NO. 15 | | | | | | | | | | 0.128 | | | 2,768 | | | 234 | | 15 | | | | | | | | |
| 45908.3.1 | Guilford | 16 | E GATE CITY BLVD RAMP TO I-40 WB | E GATE CITY BLVD RAMP TO I-40 WB | 2 | 1 | | NO | NO | 0.068 | 25 | | 1,131 | | | 96 | | 10 | | | | | | | | |
| TOTAL FOR MAP NO. 16 | | | | | | | | | | 0.068 | | | 1,131 | | | 96 | | 10 | | | | | | | | |
| 45908.3.1 | Guilford | 17 | I-40 WB RAMP TO MLK JR DR | I-40 WB RAMP TO MLK JR DR | 2,8,12 | 1 | | NO | NO | 0.259 | 35 | | 4,976 | | | 422 | | 25 | | | | | | | | 640 |
| TOTAL FOR MAP NO. 17 | | | | | | | | | | 0.259 | | | 4,976 | | | 422 | | 25 | | | | | | | 640 | |
| 45908.3.1 | Guilford | 18 | US 29 SB TO I-40 WB | US 29 SB TO I-40 WB | 10 | 2 | | NO | NO | 0.12 | 34 | 3,068 | | 675 | | 347 | | 25 | 63 | 760 | | | | | | |
| TOTAL FOR MAP NO. 18 | | | | | | | | | | 0.12 | | 3,068 | | 675 | | 347 | | 25 | 63 | 760 | | | | | | |
| 45908.3.1 | Guilford | 19 | US 29 SB RAMP TO MLK JR DR | US 29 SB RAMP TO MLK JR DR | 2,4 | 1 | | NO | NO | 0.403 | 25 | | 7,658 | | | 648 | | 40 | | | | | | | | 425 |
| TOTAL FOR MAP NO. 19 | | | | | | | | | | 0.403 | | | 7,658 | | | 648 | | 40 | | | | | | 425 | | |
| 45908.3.1 | Guilford | 20 | MLK JR DR RAMP TO I-40 WB | MLK JR DR RAMP TO I-40 WB | 2,13 | 1 | | NO | NO | 0.098 | 20 | | 1,266 | | | 108 | | 10 | | | | | | | | |
| TOTAL FOR MAP NO. 20 | | | | | | | | | | 0.098 | | | 1,266 | | | 108 | | 10 | | | | | | | | |
| 45908.3.1 | Guilford | 21 | I-40 WB RAMP TO S ELM-EUGENE ST | I-40 WB RAMP TO S ELM-EUGENE ST | 2,13 | 1 | | NO | NO | 0.121 | 21 | | 1,479 | | | 159 | | 10 | | | | | | | | 300 |
| TOTAL FOR MAP NO. 21 | | | | | | | | | | 0.121 | | | 1,479 | | | 159 | | 10 | | | | | | 300 | | |
| 45908.3.1 | Guilford | 22 | S ELM-EUGENE ST RAMP TO I-40 WB | S ELM-EUGENE ST RAMP TO I-40 WB | 2,13 | 1 | | NO | NO | 0.108 | 26 | | 1,995 | | | 169 | | 10 | | | | | | | | |
| TOTAL FOR MAP NO. 22 | | | | | | | | | | 0.108 | | | 1,995 | | | 169 | | 10 | | | | | | | | |
| 45908.3.1 | Guilford | 22A | S ELM-EUGENE ST RAMP TO I-40 WB | I-40 WB RAMP TO RANDLEMAN RD | 7 | 1 | | NO | NO | 0.103 | 20 | | 1,273 | | | 108 | | 10 | | | | | | | | 1,000 |
| TOTAL FOR MAP NO. 22A | | | | | | | | | | 0.103 | | | 1,273 | | | 108 | | 10 | | | | | | 1,000 | | |
| 45908.3.1 | Guilford | 23 | RANDLEMAN RD RAMP TO I-40 WB | RANDLEMAN RD RAMP TO I-40 WB | 2,14 | 1 | | NO | NO | 0.286 | 22 | | 4,206 | | | 357 | | 25 | | | | | | | | |
| TOTAL FOR MAP NO. 23 | | | | | | | | | | 0.286 | | | 4,206 | | | 357 | | 25 | | | | | | | | |
| 45908.3.1 | Guilford | 24 | RANDLEMAN RD RAMP TO BUS 85 SB | RANDLEMAN RD RAMP TO BUS 85 SB | 2,15 | 1 | | NO | NO | 0.239 | 18 | | 3,130 | | | 266 | | 20 | | | | | | | | |
| TOTAL FOR MAP NO. 24 | | | | | | | | | | 0.239 | | | 3,130 | | | 266 | | 20 | | | | | | | | |
| 45908.3.1 | Guilford | 25 | BUS 85 SB | I-40 WB TO BUS 85 SB | 3 | 2 | | NO | NO | 0.066 | 38 | 1,892 | | 493 | | 213 | | 20 | 54 | 525 | | | | | | |
| TOTAL FOR MAP NO. 25 | | | | | | | | | | 0.066 | | 1,892 | | 493 | | 213 | | 20 | 54 | 525 | | | | | | |
| TOTAL FOR PROJ NO. 45908.1.1 | | | | | | | | | | 14.122 | | 369,156 | 48,831 | 3,054 | 12,852 | 436 | 45,962 | 25 | 3,450 | 11,659 | 109,685 | 28 | 24 | 4 | 1 | 4,750 |
| GRAND TOTAL | | | | | | | | | | 14.122 | | 369,156 | 48,831 | 3,054 | 12,852 | 436 | 45,962 | 25 | 3,450 | 11,659 | 109,685 | 28 | 24 | 4 | 1 | 4,750 |

