

NOTES

STIRRUPS IN CAP MAY BE SHIFTED AS NECESSARY TO CLEAR ANCHOR BOLTS.

FOR PILE SPLICE DETAILS, SEE SHEET 3 OF 3.

BACKWALL SHALL BE PLACED BEFORE APPLYING THE EPOXY PROTECTIVE COATING.

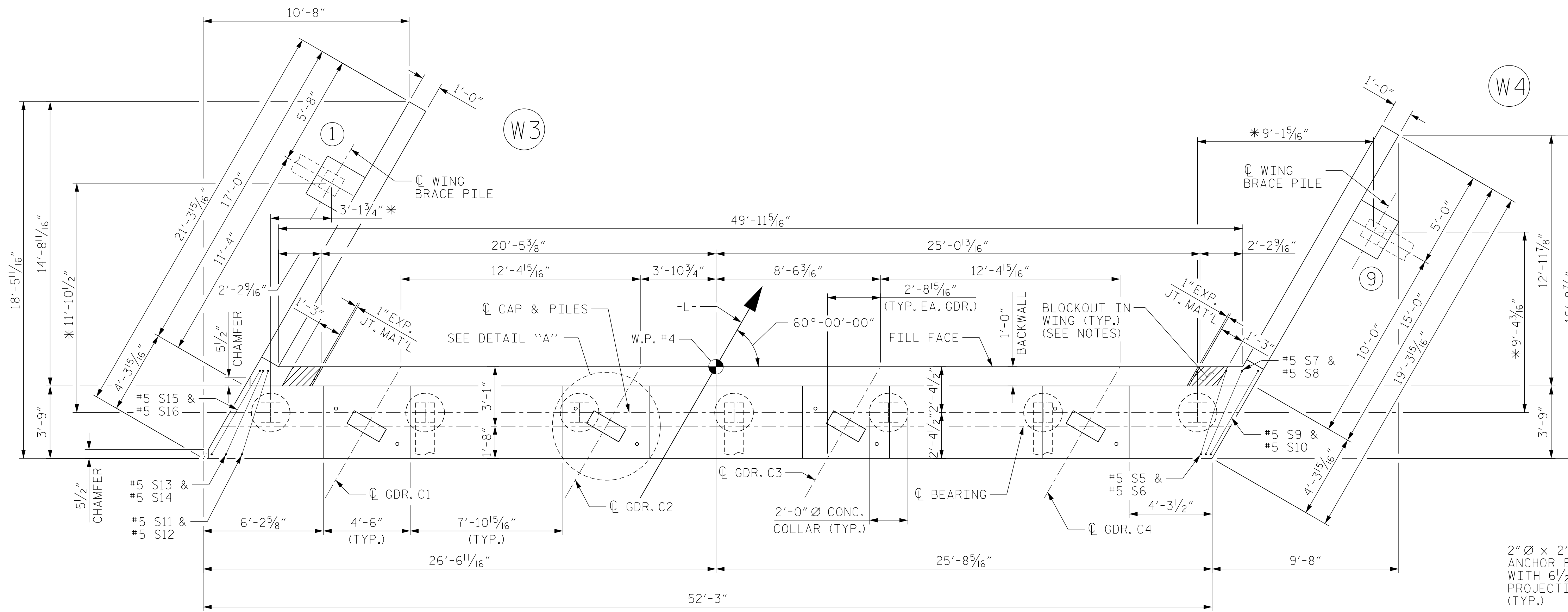
THE TOP SURFACE AREAS OF THE END BENT CAPS SHALL BE CURED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS EXCEPT THE MEMBRANE CURING COMPOUND METHOD SHALL NOT BE USED.

THE TOP SURFACE OF THE CAP EXCEPT THE BRIDGE SEAT BUILDUPS SHALL BE SLOPED TRANSVERSELY FROM THE FILL FACE TO THE BACK FACE AT THE RATE OF 2%.

THE CONCRETE IN THE SHADED AREA OF THE WING SHALL BE POURED AFTER THE BARRIER RAILS ARE CAST IF SLIP FORMING IS USED.

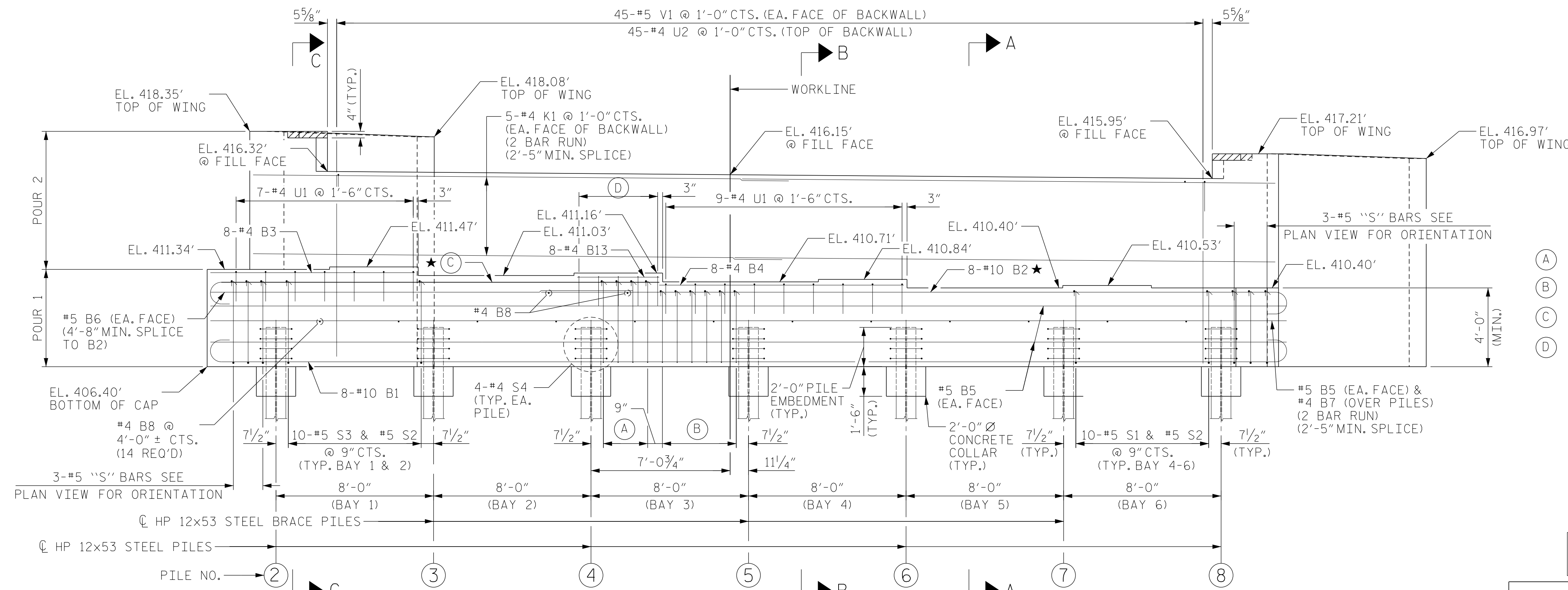
FOR "SECTION A-A", "SECTION B-B", AND "SECTION C-C", SEE SHEET 3 OF 3.

\* DIMENSIONS ARE SHOWN AT BOTTOM OF CAP.



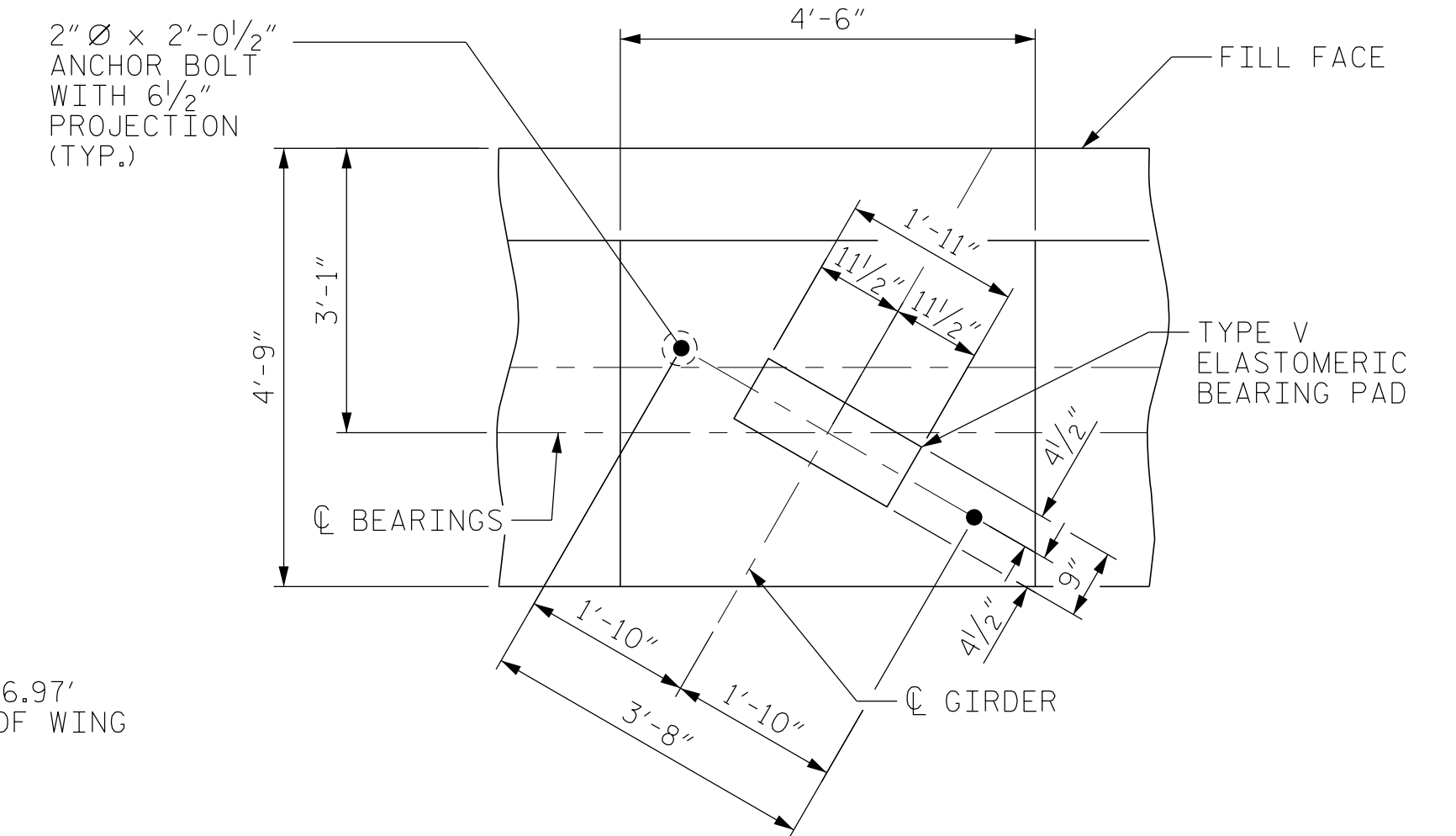
PLAN

★ 6'-0" MIN. NON-CONTACT LAP SPLICE BETWEEN B2 BARS AND (C)



ELEVATION

WINGS PILES NOT SHOWN FOR CLARITY.

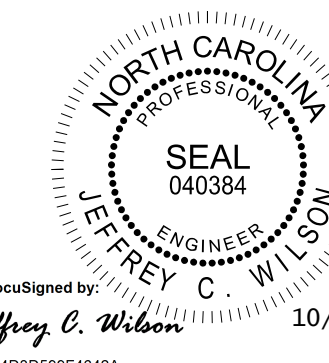


DETAIL "A"

- (A) 4-#5 S3 & #5 S2 @ 9" CTS.
- (B) 6-#5 S1 & #5 S2 @ 9" CTS.
- (C) 8-#10 'B' BARS (2-B9, 2-B10, 2-B11, 2-B12)
- (D) 5-#4 U1 @ 1'-0" CTS.

PROJECT NO. BR-0070  
CASWELL COUNTY  
STATION: 30+57.00 -L-

SHEET 1 OF 3



DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

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STATE OF NORTH CAROLINA  
DEPARTMENT OF TRANSPORTATION  
RALEIGH  
SUBSTRUCTURE  
END BENT 2  
PLAN AND ELEVATION

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S-34
1			3			TOTAL SHEETS
2			4			39

DRAWN BY: J. WILSON DATE: 10/22  
CHECKED BY: D. RUGGLES DATE: 10/22  
DESIGN ENGINEER OF RECORD: J. WILSON DATE: 10/22

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10/10/2022  
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