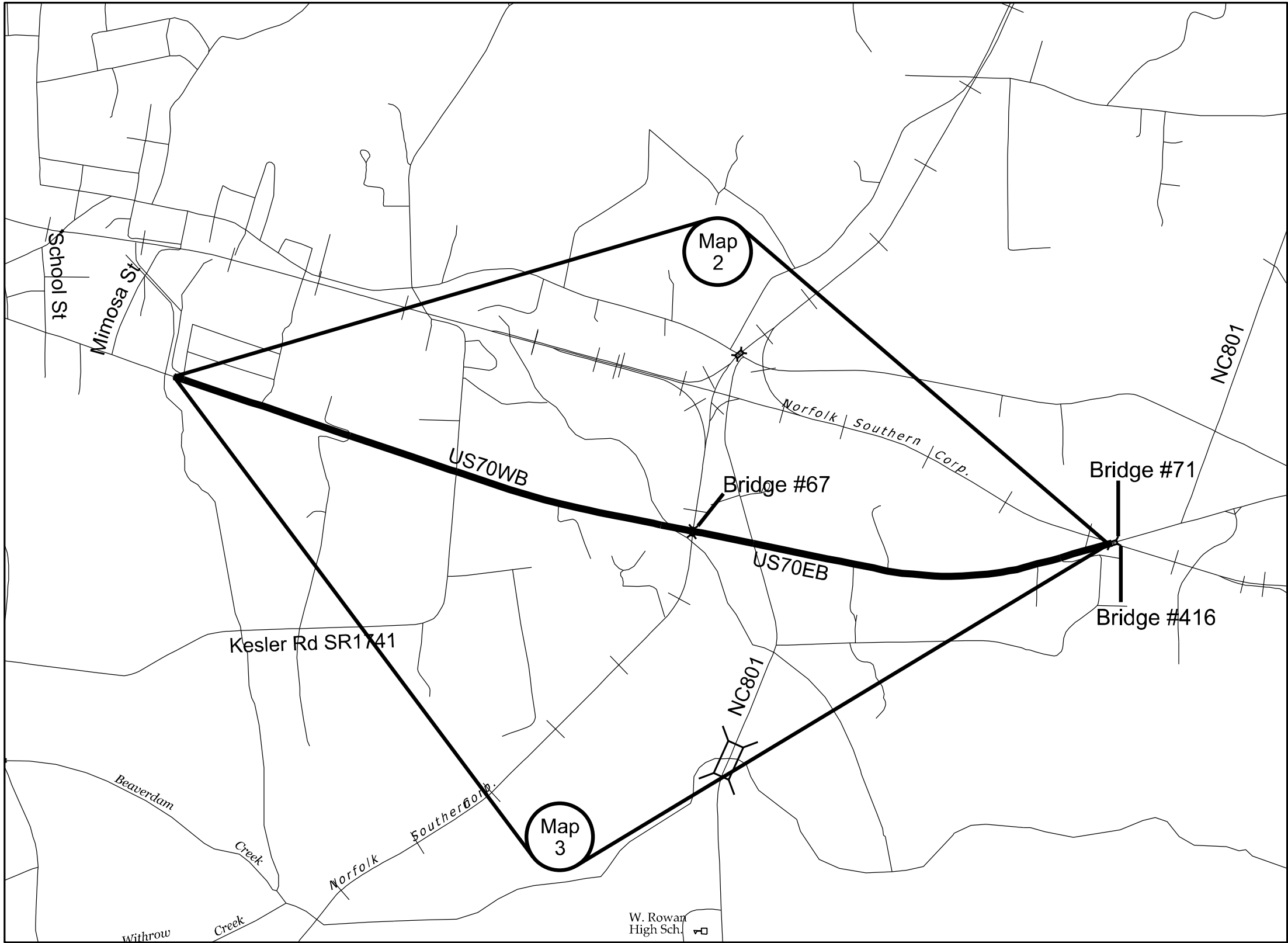
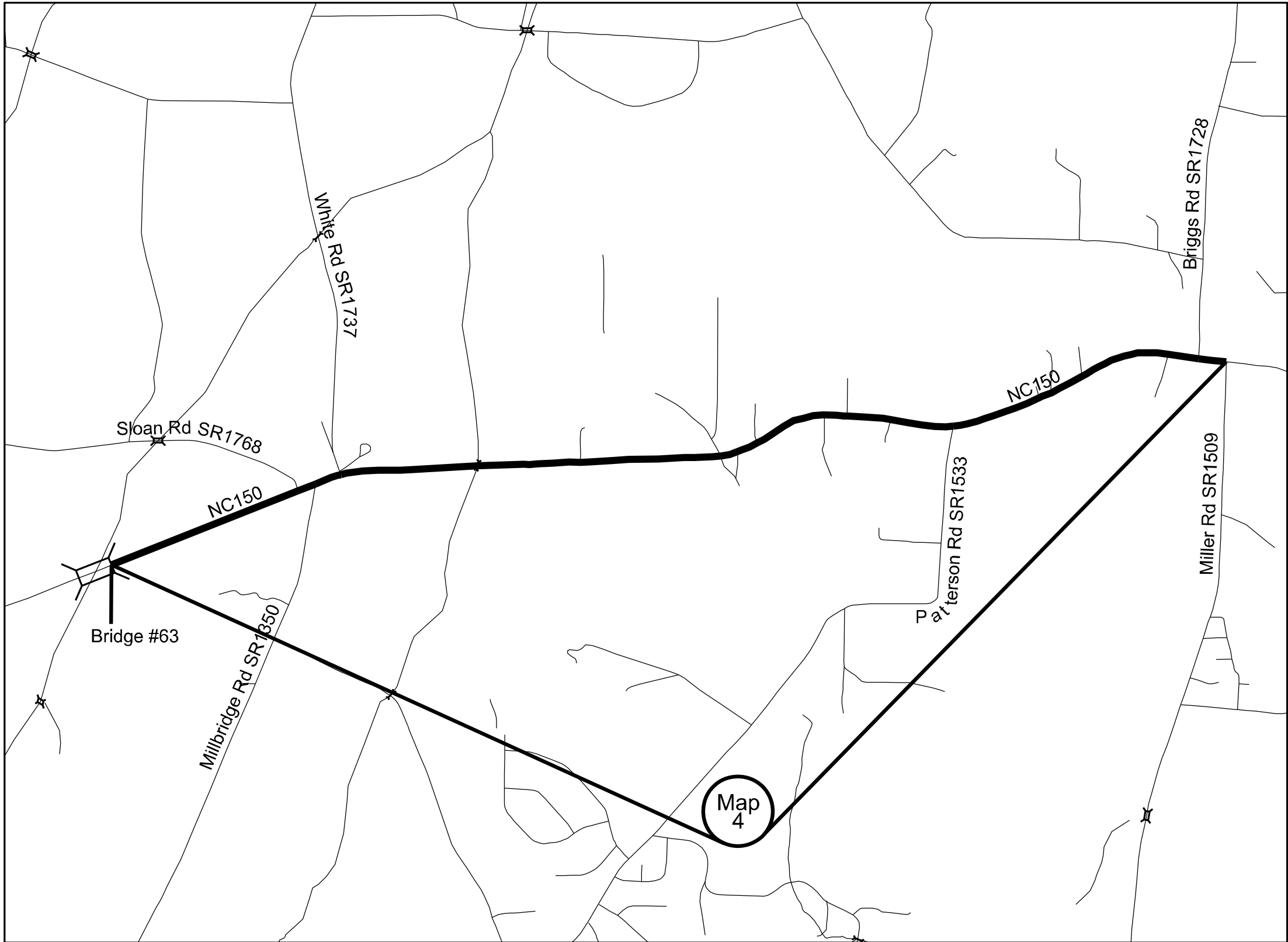

 Map 1 US29 From Long Ferry Rd SR2120 to 16th St
 Mill 1 1/2" entire width
 Mill 1 1/2" incidental at all SR intersections
 Pave 1 1/2" S9.5C



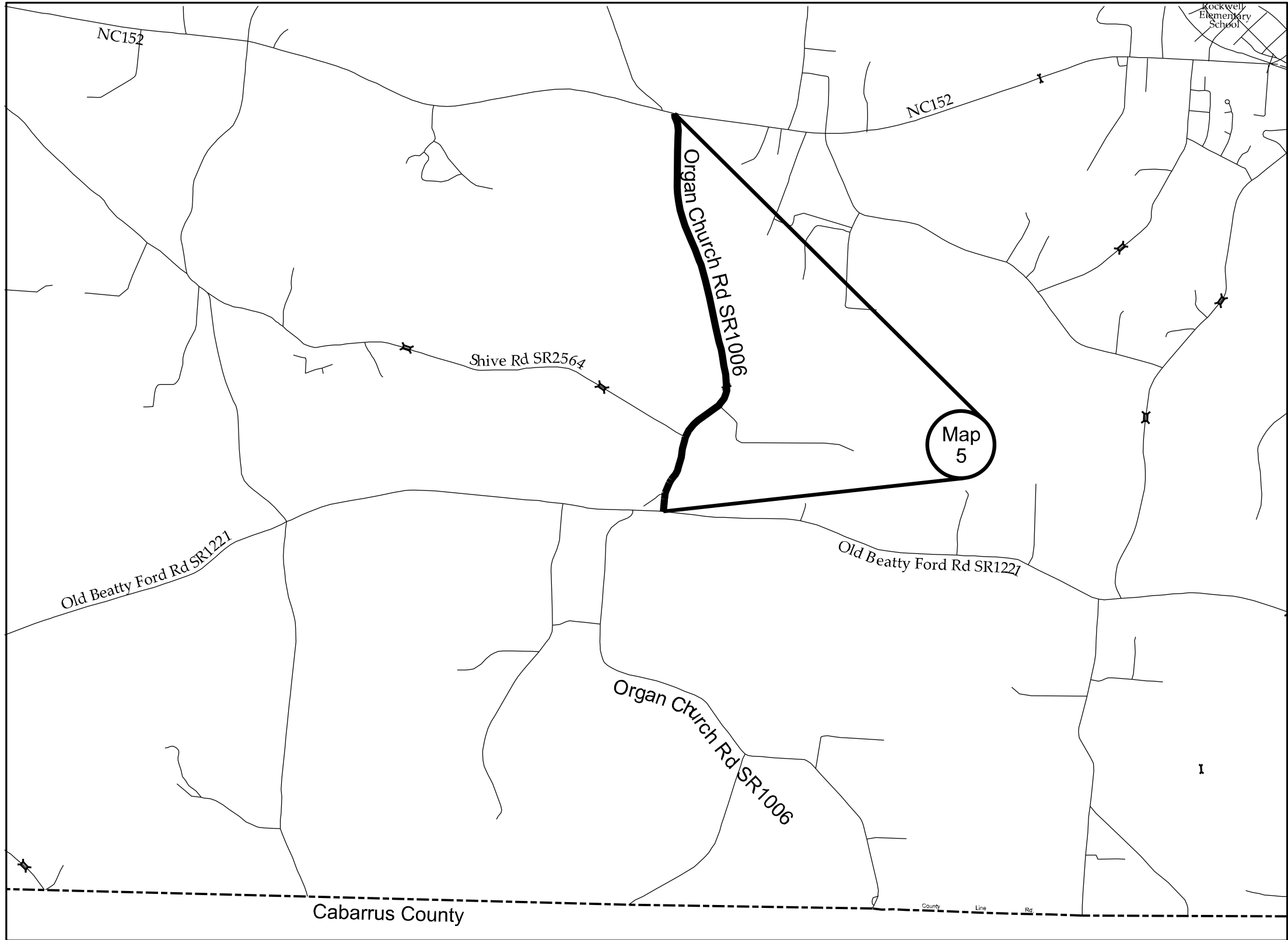
Map 2 US70WB from bridge #71 to beginning of curb and gutter east of Mimosa St.
 Mill 0-11/2 beginning ,end and at all SR intersections
 Pave 11/2" S9.5C

Map 3 US70EB from end of curb and gutter east of Mimosa St to Bridge #416
 Mill 0-11/2" beginning, end and at all SR Intersections
 Pave 11/2" S9.5C



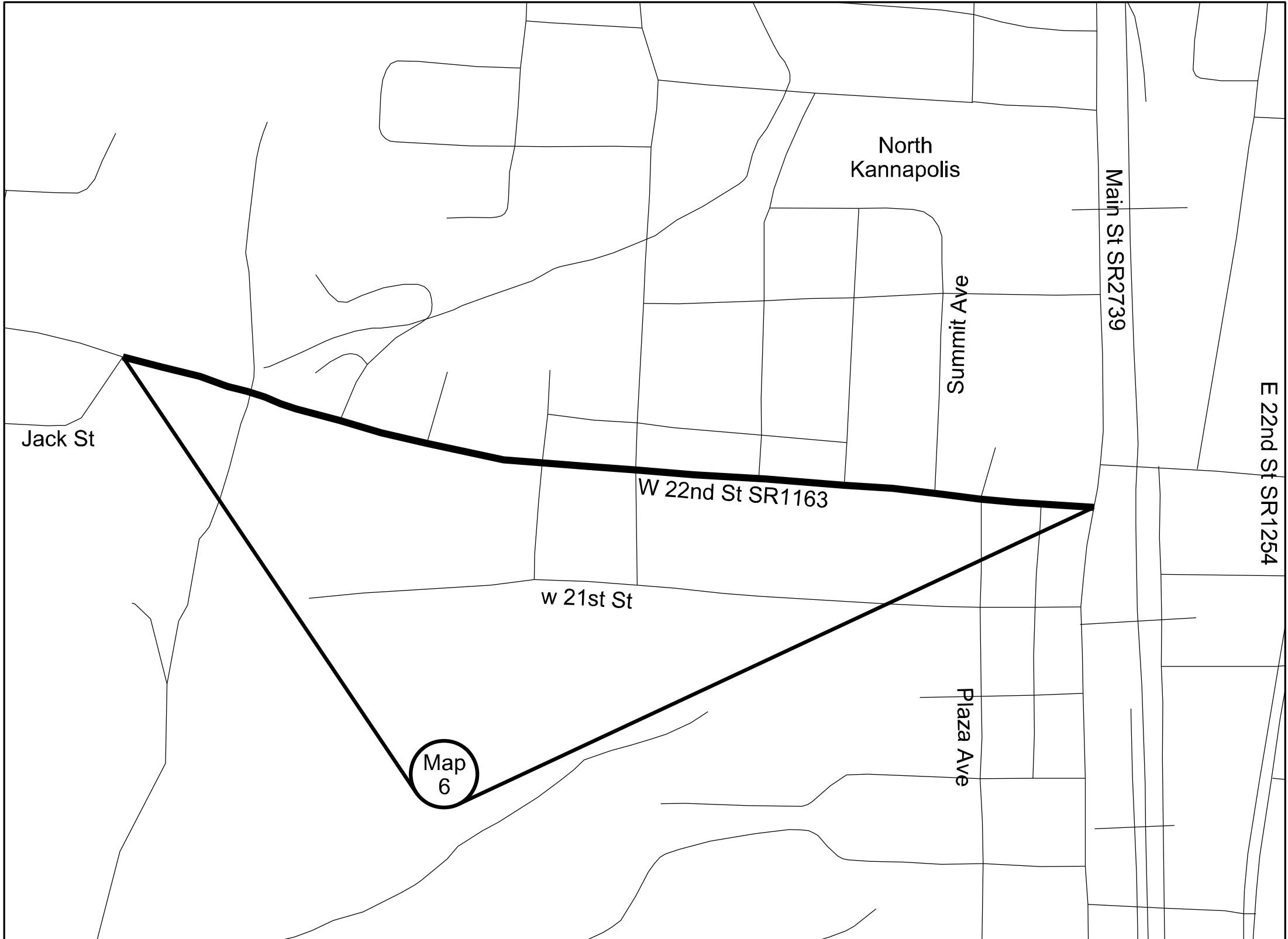
- Map 4 NC150 from bridge #63 to Miller Rd SR1509
- Mill 1 1/2" entire width
- Mill 1 1/2" incidental at all SR intersections
- Asphalt surface treatment, Matcoat
- #67 stone
- Pave 1 1/2" S9.5B

PROJECT REFERENCE NO.	SHEET NO.
2023CPT.09.07.10801 2023CPT.09.08.20801	4



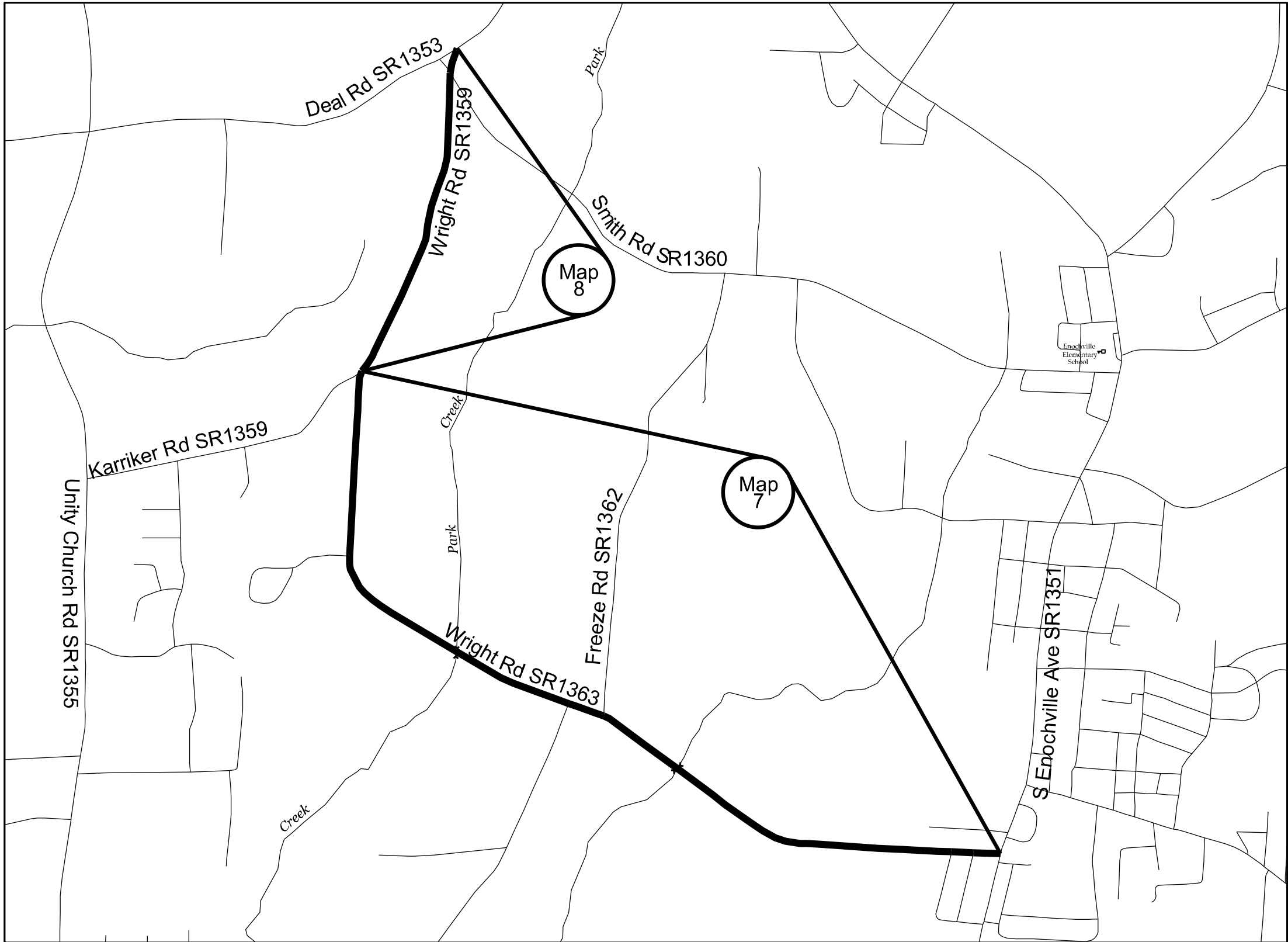
Map 5 Organ Church Rd SR1006 from NC152 to Old Beatty Ford Rd SR1221 Mill 0-11/2" incidental mill beginning, end and at all SR intersections Pave 11/2" S9.5B

ROWAN COUNTY
NORTH CAROLINA



Map 6 W 22nd St SR1163 from Main
St SR2739 to EOM (Jack St)
Mill 1 1/2" entire width
Pave 1 1/2" S9.5B

PROJECT REFERENCE NO.	SHEET NO.
2023CPT.09.07.10801 2023CPT.09.08.20801	6



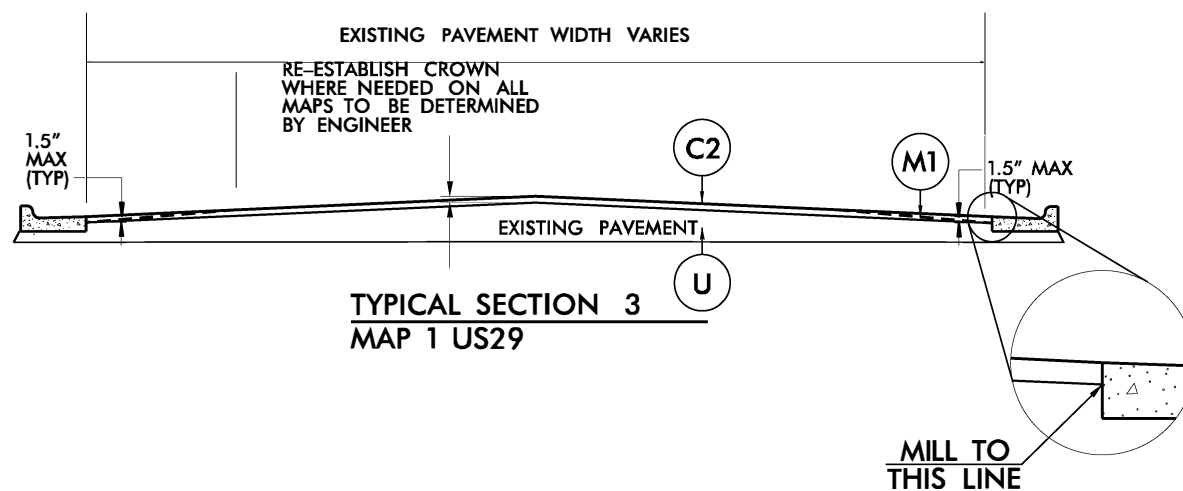
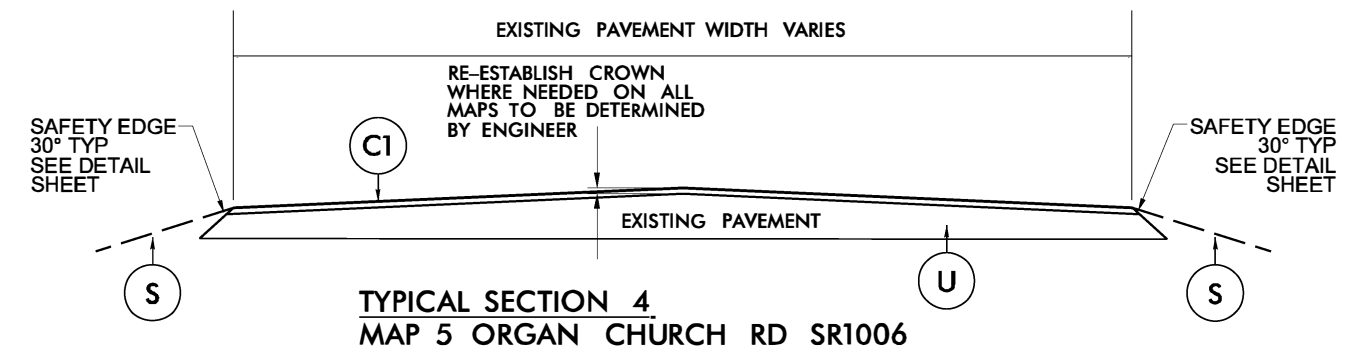
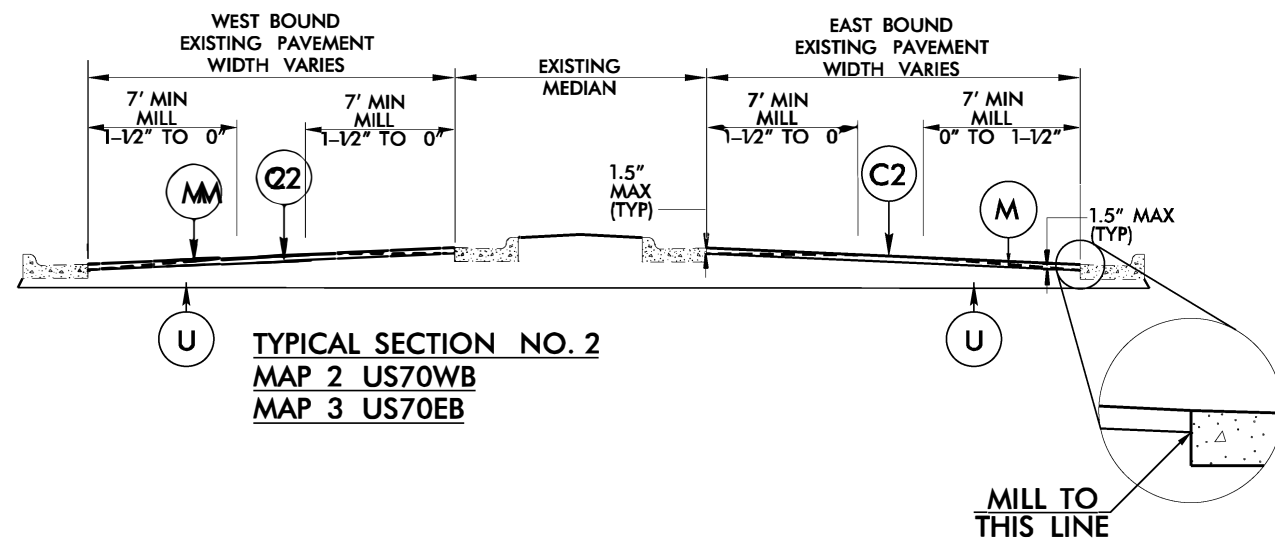
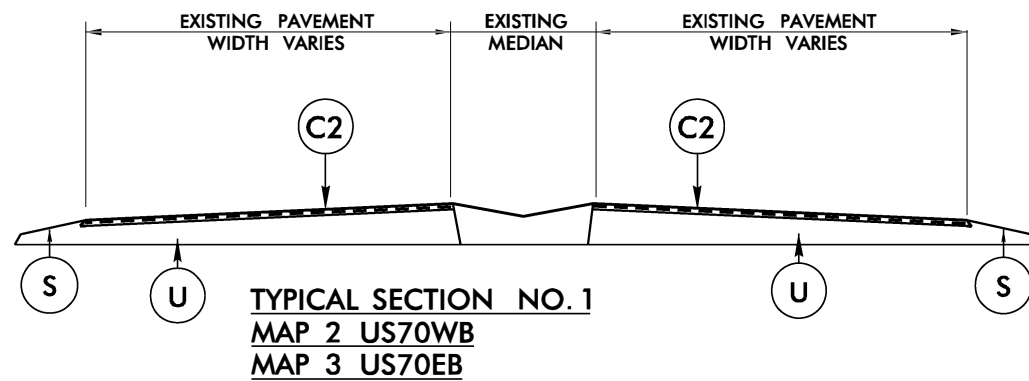
- Map 7 Wright Rd SR1363 from Enochville Ave SR1351 to Karriker Rd SR1359
- Mill 0-11/2" incidental mill beginning and at all SR intersections
- Asphalt surface treatment, Matcoat #67 Stone
- Patching done by contractors
- Pave 11/2" S9.5B

- Map 8 Wright Rd SR1359 from Karriker Rd SR1359 to Deal Rd SR1353
- Mill 0-11/2" incidental mill end and at all SR intersections
- Asphalt surface treatment, Matcoat #67 stone
- Patching done by contractors
- Pave 11/2" S9.5B

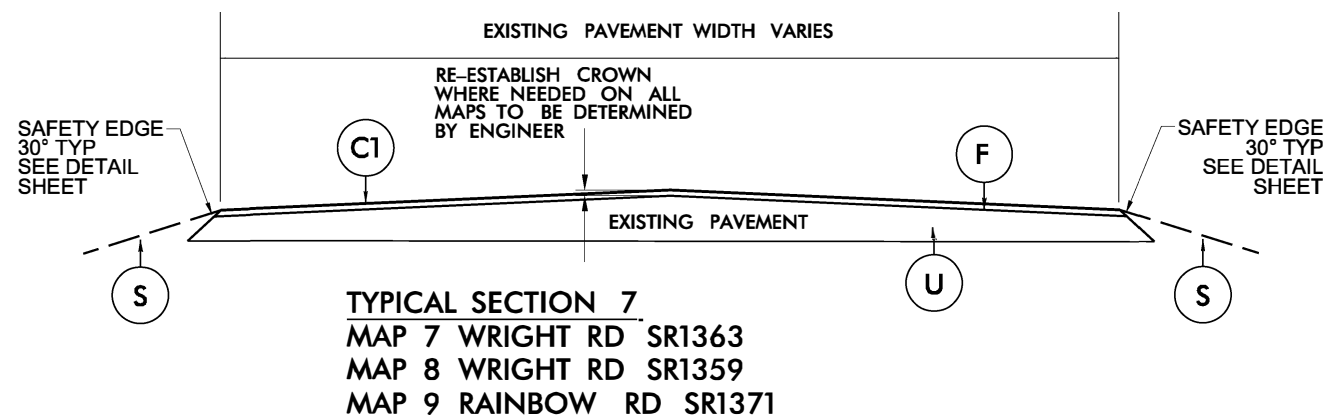
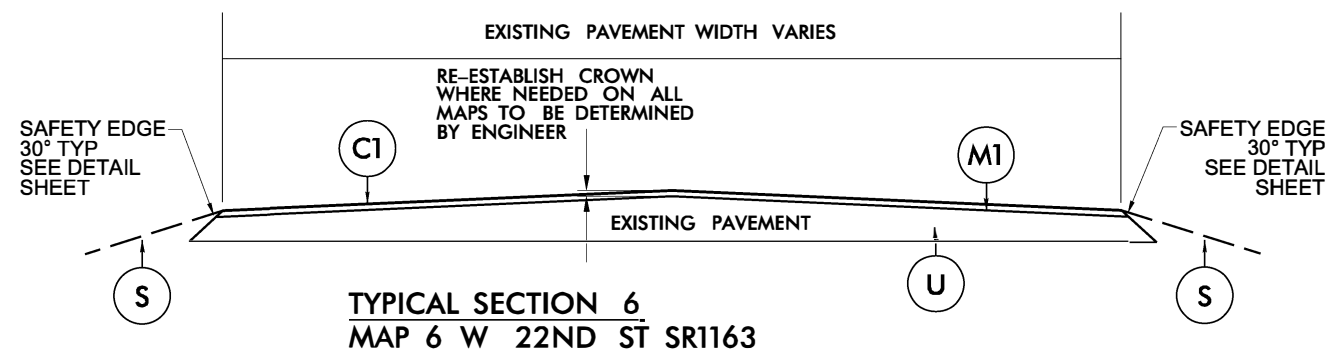
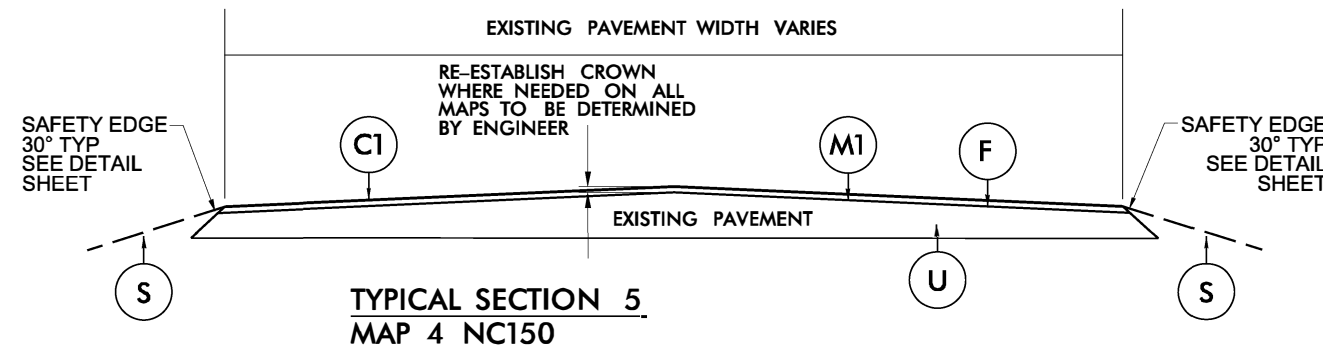
PROJECT REFERENCE NO.	SHEET NO.
2023CPT.09.07.10801 2023CPT.09.08.20801	7



Map 9 Rainbow Rd SR1371 from W C St SR1124 to Carrarrus County Line
 Mill 0-1 1/2" incidental beginning, end and at all SR intersections
 Asphalt surface treatment, Matcoat #67 stone
 Pave 1 1/2" S9.5B

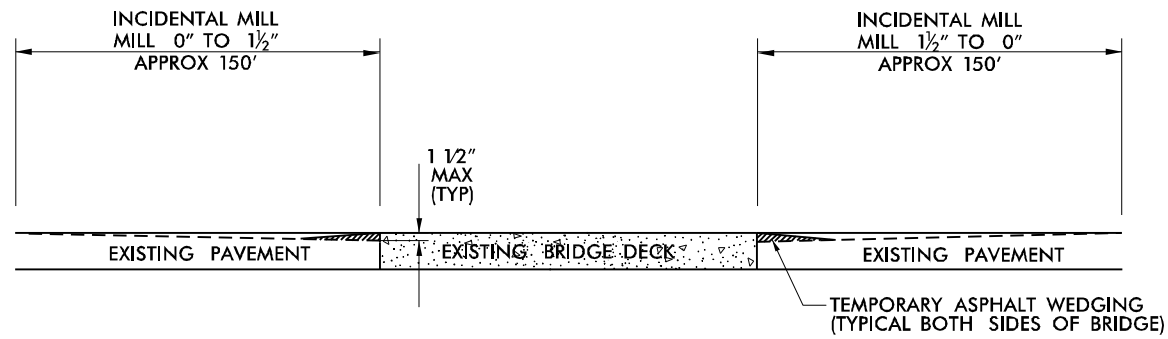


PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, TO BE APPLIED AT AN AVERAGE RATE OF 165 LBS PER SQ YD.
C2	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, TO BE APPLIED AT AN AVERAGE RATE OF 168 LBS PER SQ YD.
F	ASPHALT SURFACE TREATMENT, MATCOAT, #67 STONE TO BE APPLIED AT AN AVERAGE RATE OF 38 LBS PER SY YD, EMULSION RATE OF 0.40 GAL PER SY YD
M	MILL ASPHALT PAVEMENT, 0" TO 1/2"
M1	MILL ASPHALT PAVEMENT, 1/2" DEPTH
S	SHOULDER RECONSTRUCTION (SEE DETAIL)
U	EXISTING PAVEMENT

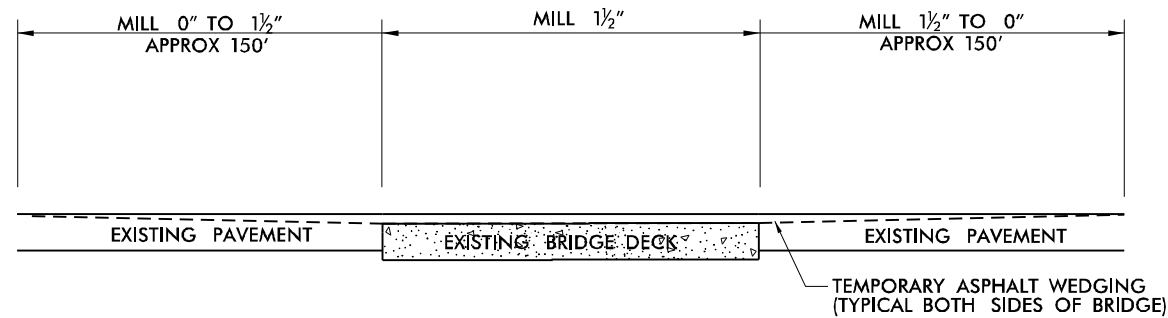


PAVEMENT SCHEDULE

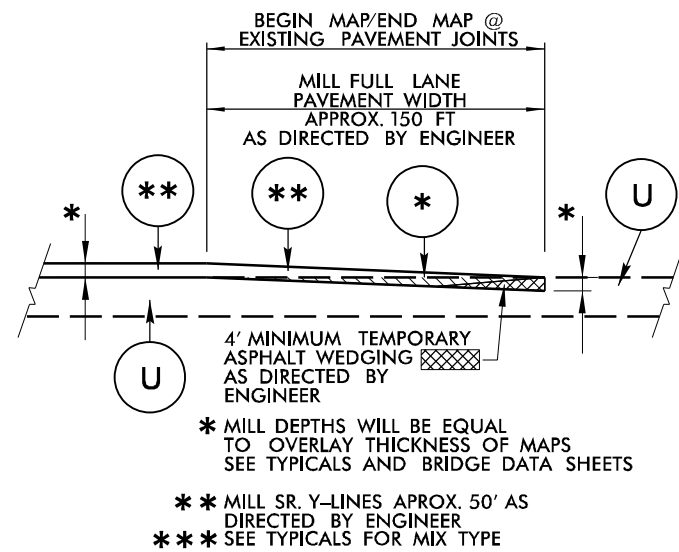
C1	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, TO BE APPLIED AT AN AVERAGE RATE OF 165 LBS PER SQ YD.
C2	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, TO BE APPLIED AT AN AVERAGE RATE OF 168 LBS PER SQ YD.
F	ASPHALT SURFACE TREATMENT, MATCOAT, #67 STONE TO BE APPLIED AT AN AVERAGE RATE OF 38 LBS PER SY YD, EMULSION RATE OF 0.40 GAL PER SY YD
M	MILL ASPHALT PAVEMENT, 0" TO 1½"
M1	MILL ASPHALT PAVEMENT, 1½" DEPTH
S	SHOULDER RECONSTRUCTION (SEE DETAIL)
U	EXISTING PAVEMENT



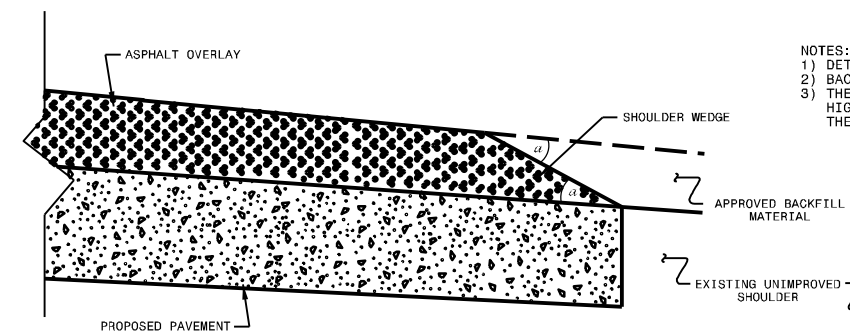
**INCIDENTAL MILLING
BRIDGE APPROACHES**
(SEE BRIDGE DATA SHEET)



**INCIDENTAL MILLING
BRIDGE APPROACHES**
(SEE BRIDGE DATA SHEET)

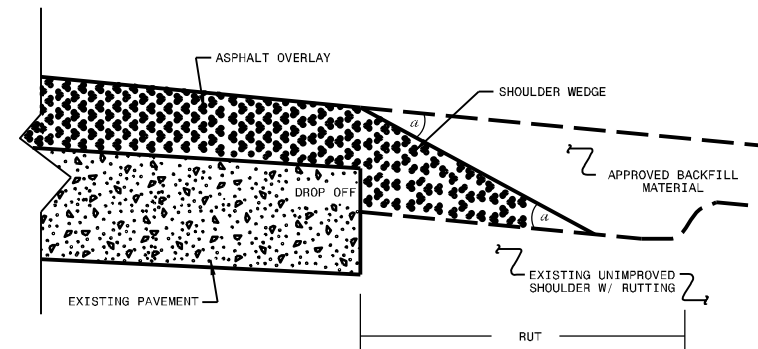


INCIDENTAL TIE-IN MILLING DETAIL

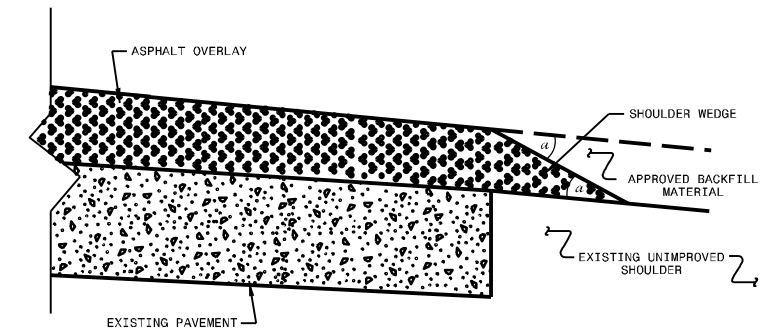


SHOULDER WEDGE DETAIL
(Resurfacing Projects w/ Widening or
with Existing Paved Shoulder having no dropoffs)

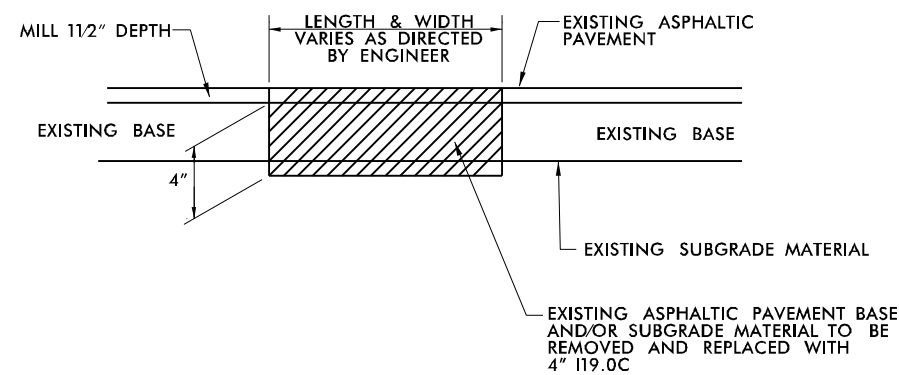
NOTES:
1) DETAIL DOES NOT APPLY TO OGAFCS AND ULTRA-THIN BONDED WEARING COURSE.
2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS DIRECTED BY THE ENGINEER.



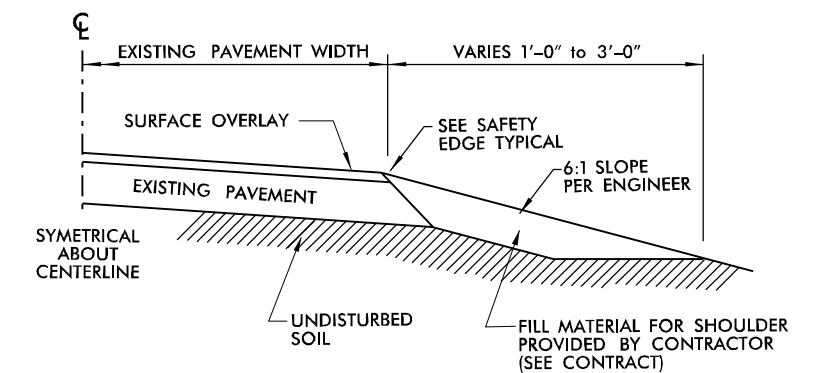
SHOULDER WEDGE DETAIL
(Resurfacing Adjacent to
Rutted Shoulder)



SHOULDER WEDGE DETAIL
(Resurfacing Projects w/ NO Widening)



FULL DEPTH PATCHING



SHOULDER RECONSTRUCTION

CONSTRUCTION NOTES:

1. ALL QUANTITIES ARE "ESTIMATED" AS INDICATED IN THE "SUMMARY OF QUANTITIES".

2. CONSTRUCTION SHALL PROGRESS IN PHASES, IN THE ORDER INDICATED BELOW:

- PHASE 1 - MILLING AND PATCHING (WHEN REQUIRED)
 PHASE 2 - SURFACE OVERLAY
 PHASE 3 - SHOULDER DROP-OFF REPAIR (AS NEEDED AND DIRECTED BY ENGINEER)
 PHASE 4 - UTILITY ADJUSTMENTS (MANHOLE RING/COVER, VALVE/METER BOX RING/COVER, CATCH BASIN GRATE/COVER, DROP INLET GRATE/COVER, ETC.) WHEN REQUIRED.

3. BRIDGES THAT HAVE FLOOR DRAINS, SHALL HAVE ALL FLOOR DRAINS LEFT OPEN. EXTRA CARE SHALL BE EXERCISED IN MILLING (IF REQUIRED) AND IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE.

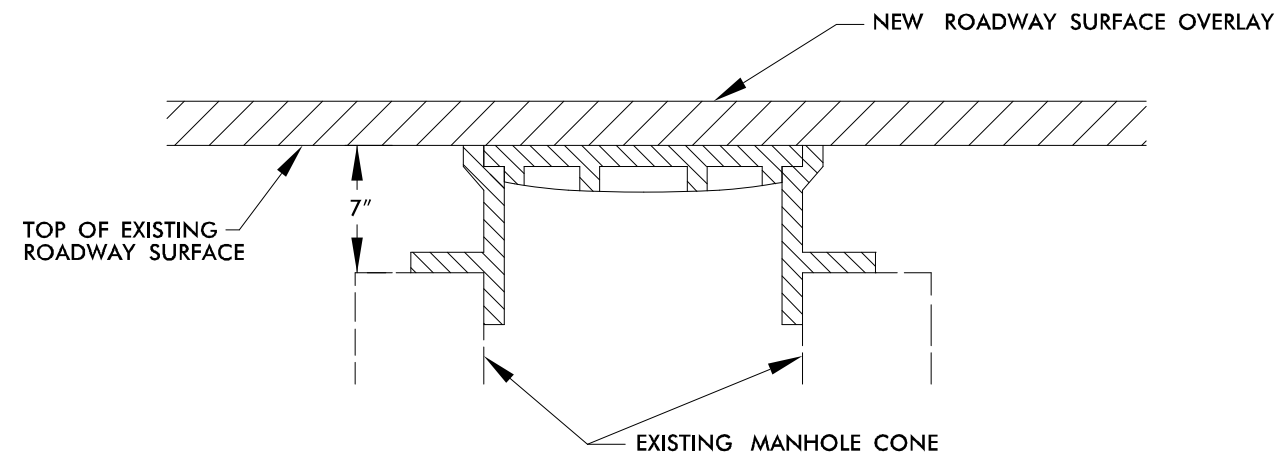
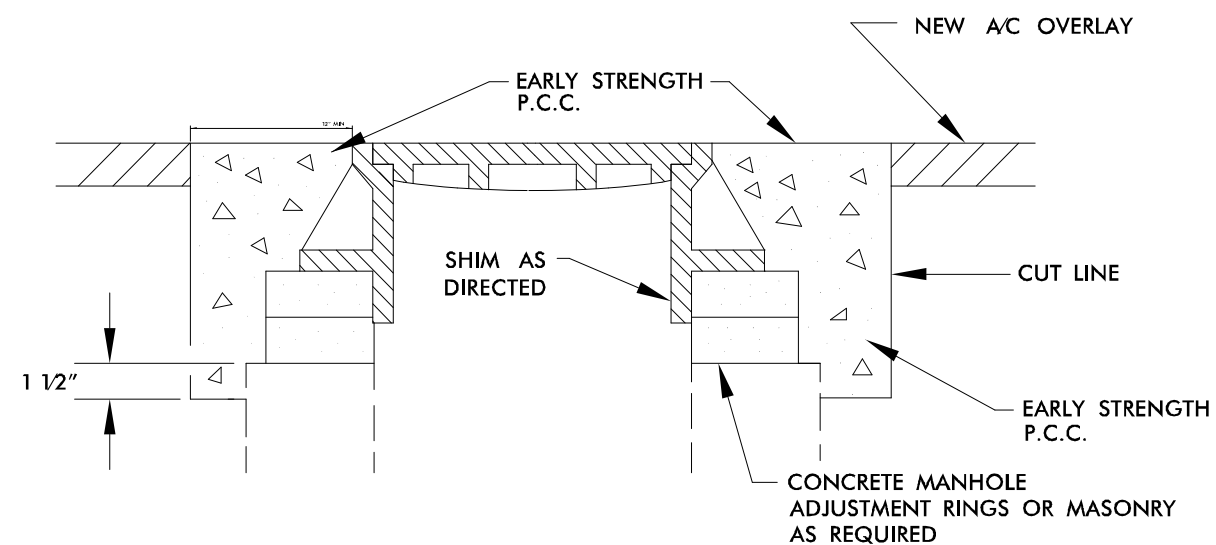
4. TEMPORARY ASPHALT WEDGING SHALL BE PLACED ON THE SAME DAY THAT BRIDGE AND/OR RAILROAD APPROACHES ARE MILLED (AND IF APPROACHES ARE MILLED PRIOR TO BRIDGE DECK).

5. FOR TWO-LANE ROADWAYS - IT SHALL BE UNDERSTOOD THAT TYPICALLY ON A ROADWAY MEASURING 20 FEET OR LESS IN WIDTH, THE CENTER OF THE WHITE EDGELINE SHALL BE LOCATED SIX INCHES FROM THE EDGE OF PAVEMENT ON EITHER SIDE OF THE ROADWAY; ON A ROADWAY MEASURING 22 FEET IN WIDTH, TRAVEL LANES SHALL MEASURE 10 FEET FROM THE EDGE OF PAVEMENT ON EITHER SIDE; ON A ROADWAY MEASURING 24 FEET IN WIDTH, TRAVEL LANES SHALL MEASURE 11 FEET AND THE WHITE EDGELINE SHALL BE LOCATED ONE FOOT FROM THE EDGE OF PAVEMENT ON EITHER SIDE; ON A ROADWAY MEASURING 26 FEET OR MORE IN WIDTH, TRAVEL LANES SHALL MEASURE 12 FEET AND THE WHITE EDGELINE SHALL BE LOCATED NO LESS THAN ONE FOOT FROM THE EDGE OF PAVEMENT ON EITHER SIDE. THIS SHALL BE STANDARD PRACTICE UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

6. PAPER JOINTS ARE TO BE PLACED BETWEEN DAYS OF PAVING OPERATIONS AS SPECIFIED IN THE STANDARD SPECIFICATIONS SECTION 610-11.

7. ALL MILLED AREAS WILL BE PAVED WITHIN 72 HOURS UNLESS APPROVED BY THE ENGINEER.

8. REPLACE ANY PORTION OF STOP BARS AND OTHER PAVEMENT MARKINGS AT ANY INTERSECTION INCLUDING Y-LINES NOT ACTUALLY BEING PAVED OVER, THAT ARE OBLITERATED BY THE PAVING OPERATION EITHER BY HAULING WHEEL TRACKS OR TACK TRUCK BY THE END OF EACH RESURFACING OPERATION

**STEP 1****STEPS 2,3, & 4**

- STEP 1 COVER EXISTING MANHOLE WITH APPROVED MATERIAL AND CONSTRUCT OVERLAY ACROSS TOP OF MANHOLE
- STEP 2 SAW CUT EXCAVATION AROUND MANHOLE 12" MIN. FROM MANHOLE FRAME.
- STEP 3 RAISE MANHOLE FRAME RINGS TO FINISH PAVEMENT PROFILE AND CROSS SLOPE.
- STEP 4 BACKFILL WITH EARLY STRENGTH P.C.C. TO DEPTHS AS DIRECTED.

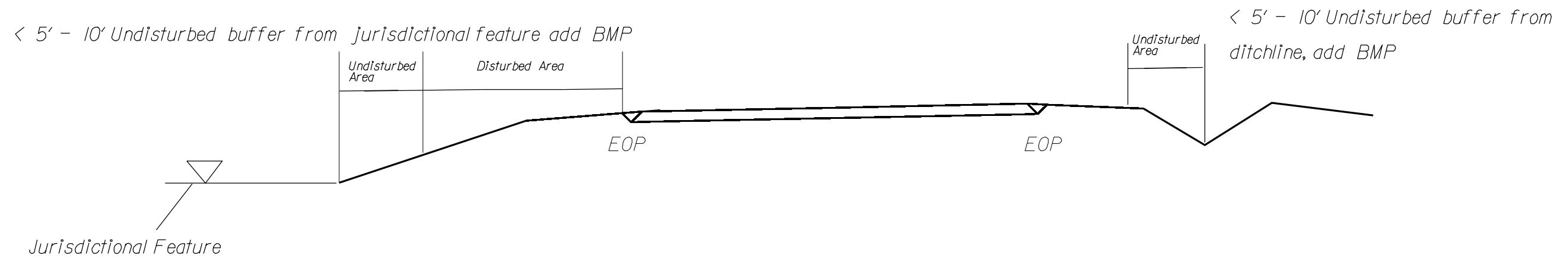
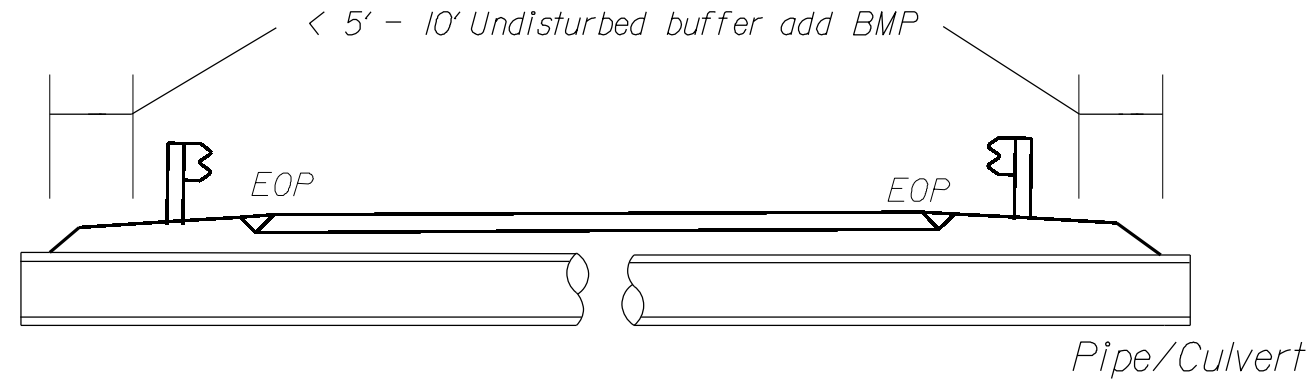
MANHOLE ADJUSTMENT DETAIL

NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

BMP Options: Wattle or Silt Fence

EROSION CONTROL DETAIL

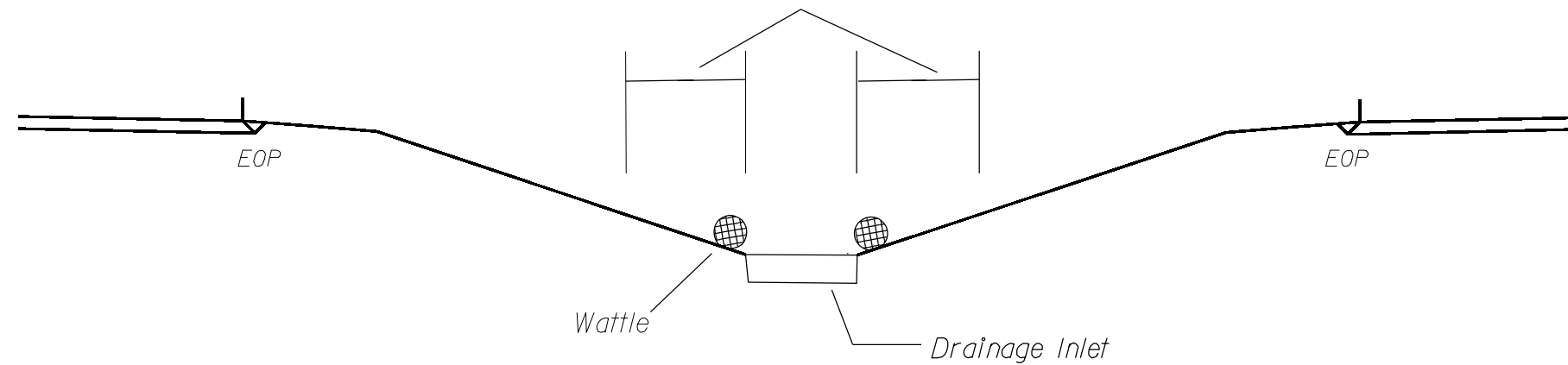
PROJECT REFERENCE NO.	SHEET NO.
2023CPT.09.07.10801 2023CPT.09.08.20801	12



Use BMP's if shoulders and/or frontslopes and/or ditchline and/or backslopes are disturbed



< 5' - 10' Undisturbed buffer from inlet, add wattle



NOT TO SCALE

BRIDGE DATA

										PROJECT NO.	SHEET NO.
										2023CPT.09.07.10801	13
										2023CPT.09.08.20801	
Map No.	Route No.	Route Name	Bridge No.	Feature Intersected	Floor Construction	Clear Roadway Width (Ft)	Horizontal Clearance Under (Ft.)	Vertical Clearance Under	Length (Ft)	Posting	Recommended Treatment, From Bridge Maintenance
x	RxR overpass	US70	67	US70	Timber deck on steel plate girders	0	41	16.8330 EB/ 16.667.0 WB	174	NA	Mill 11/2" pave 11/2" Keep vertical clearance
x	US70WBL	US70	71	SOUTHERN RAILWAY	8 1/4" RC Slab	36	NA	NA	170	NA	Do Not Pave on Bridge
x	US70EBL	US70	416	SOUTHERN RAILWAY	8 1/4" RC Slab	36.13	NA	NA	170	NA	Do Not Pave on Bridge
x	NC150	NC150	63	Kerr Creek	8 1/2" RC Slab	33	NA	NA	125	NA	Do Not Pave on Bridge
x	SR2300	S Main St (Granite Quarry)	170	Cane Creek		NA	NA	NA	87	NA	Mill 11/2" pave 11/2"

PROJECT NO.	SHEET NO.	TOTAL NO.
2023CPT.09.07.10801	14	
2023CPT.09.08.20801		

SUMMARY OF QUANTITIES

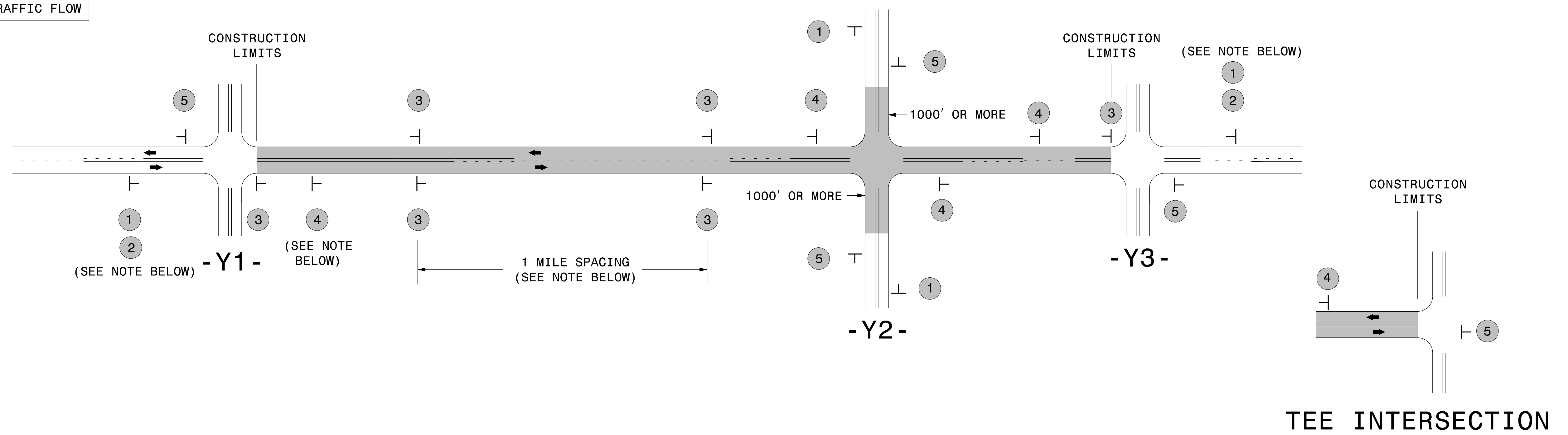
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	0106000000-E	1220000000-E	1245000000-E	1297000000-E	1308000000-E	1330000000-E	1519000000-E	1523000000-E	1575000000-E	1704000000-E	1775500000-E	1838000000-E	2830000000-N	2845000000-N	5255000000-N	6000000000-E	6071010000-E												
												BORROW EXCAVATION	INCIDENTAL STONE BASE	SHOULDER RECONSTRUCTION	MILLING ASPHALT PAVEMENT 1 1/2" DEPTH	MILLING ASPHALT PAVEMENT 0" TO 1 1/2" DEPTH	INCIDENTAL MILLING	SURFACE COURSE, \$9.5B	SURFACE COURSE, \$9.5C	ASPHALT BINDER FOR PLANT MIX	PATCHING EXISTING PAVEMENT	ASPHALT SURFACE TREATMENT, MATCOAT, #67 STONE	EMULSION FOR ASPHALT SURFACE TREATMENT	ADJ. OF MANHOLES	ADJ. OF METER OR VALVE BOX	PORTABLE LIGHTING	TEMPORARY SILT FENCE	WATTLE												
										MI	FT	CY	TONS	SMI	SY	SY	SY	TONS	TONS	TONS	TONS	SY	GAL	EA	EA	LS	LF	LF												
2023CPT.09.07.10801	Rowan	1	US29	FROM LONG FERRY RD SR2120 TO 16TH ST.	3	2	MU - Multi-lane undivided traffic	NO	NO	2.178	51				66,989		870		6,561	395	25			26	12	1														
TOTAL FOR MAP NO. 1																																								
2023CPT.09.07.10801	Rowan	2	US70WB	FROM BRIDGE #71 TO CURB AND GUTTER AT MIMOSA ST	2-Jan	2	MD	NO	NO	3.314	32	398	60	6.63		400	3,375		7,055	424	10						1,326	133												
TOTAL FOR MAP NO. 2																																								
2023CPT.09.07.10801	Rowan	3	US70EB	FROM CURB AND GUTTER AT MIMOSA ST TO BRIDGE #71	2-Jan	2	MD	NO	NO	3.314	32	398	21	6.63		675	4,039		7,119	428	10						1,326	133												
TOTAL FOR MAP NO. 3																																								
2023CPT.09.07.10801	Rowan	4	NC150	FROM BRIDGE #0063 TO MILLER RD SR1509	5	2	2WU	NO	NO	3.864	23	464	120	7.73	54,567		1,845	5,411		365	45	54,835	21,934				1,546	155												
TOTAL FOR MAP NO. 4																																								
TOTAL FOR PROJ NO. 2023CPT.09.07.10801																																								
2023CPT.09.08.20801	Rowan	5	ORGAN CHURCH RD SR1006	FROM NC152 TO OLD BEATTY FORD RD SR1221	4	2	2WU	NO	NO	1.667	23	200	54	3.33			1,252	2,146		144	10							667	67											
TOTAL FOR MAP NO. 5																																								
2023CPT.09.08.20801	Rowan	6	W 22ND ST SR1163	FROM MAIN ST SR2739 TO EOM(JACK ST)	6	2	2WU	NO	NO	0.72	20	86	70	1.44	8,448				807	55	10			9	2		400	40												
TOTAL FOR MAP NO. 6																																								
2023CPT.09.08.20801	Rowan	7	WRIGHT RD SR1363	FROM ENOCHVILLE AVE SR1351 TO KERRIKER RD SR1359	7	2	2WU	NO	NO	2.726	22.5	327	175	5.45			1,386	3,556		250	250	36,637	14,655				1,090	109												
TOTAL FOR MAP NO. 7																																								
2023CPT.09.08.20801	Rowan	8	WRIGHT RD SR1359	FROM KERRIKER RD SR1359 TO DEAL RD SR1353	7	2	2WU	NO	NO	1.023	21.5	123	36	2.05			1,462	1,287		91	100	13,142	5,256				409	41												
TOTAL FOR MAP NO. 8																																								
2023CPT.09.08.20801	Rowan	9	RAINBOW RD SR1371	FROM W.C.ST SR1124 TO CARRARRUS COUNTY LINE	7	2	2WU	NO	NO	0.481	22	58	10	0.96			944	604		41	10	6,332	2,483				192	19												
TOTAL FOR MAP NO. 9																																								
TOTAL FOR PROJ NO. 2023CPT.09.08.20801																																								
GRAND TOTAL																																								

SIGNING FOR RESURFACING PROJECTS

LEGEND

┆ STATIONARY SIGN

← DIRECTION OF TRAFFIC FLOW



MAINLINE (-L-) SIGNING

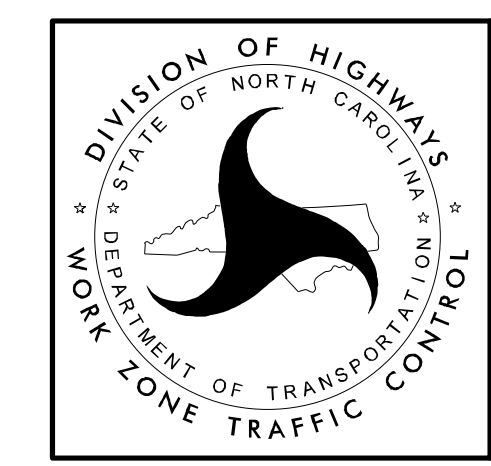
-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	1		PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> LESS THAN 1000' OF RESURFACING ALONG -Y- LINE SUBDIVISION ROADS DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;"> W20-1 48" X 48" PLACED 500' IN ADVANCE OF FLAGGER. </div> <div style="text-align: center;"> W20-7 A 48" X 48" PLACED 250' IN ADVANCE OF FLAGGER. </div> </div>
	2		#2 SIGN ONLY USED WHEN CONSTRUCTION LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)	
	3		- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.	
	4		- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.	
	5		PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.	

THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.

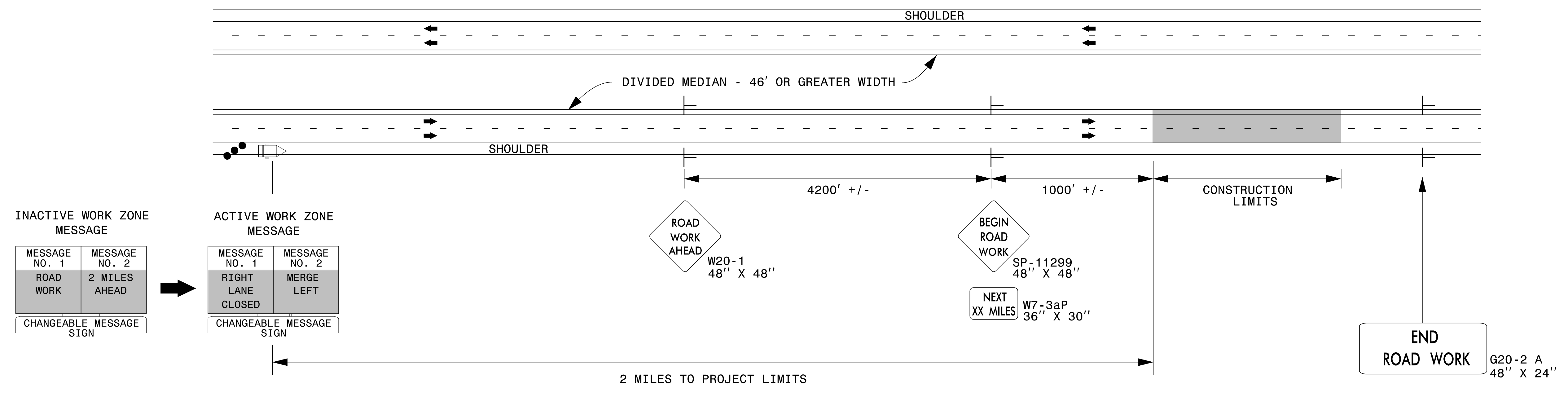
MAPS LESS THAN 2 MILES

FOR RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, NO STATIONARY SIGNS ARE REQUIRED. USE PORTABLE "ROAD UNDER CONSTRUCTION" OR "ROAD WORK AHEAD" SIGNS IN LIEU OF STATIONARY ADVANCE WARNINGS SIGNS.

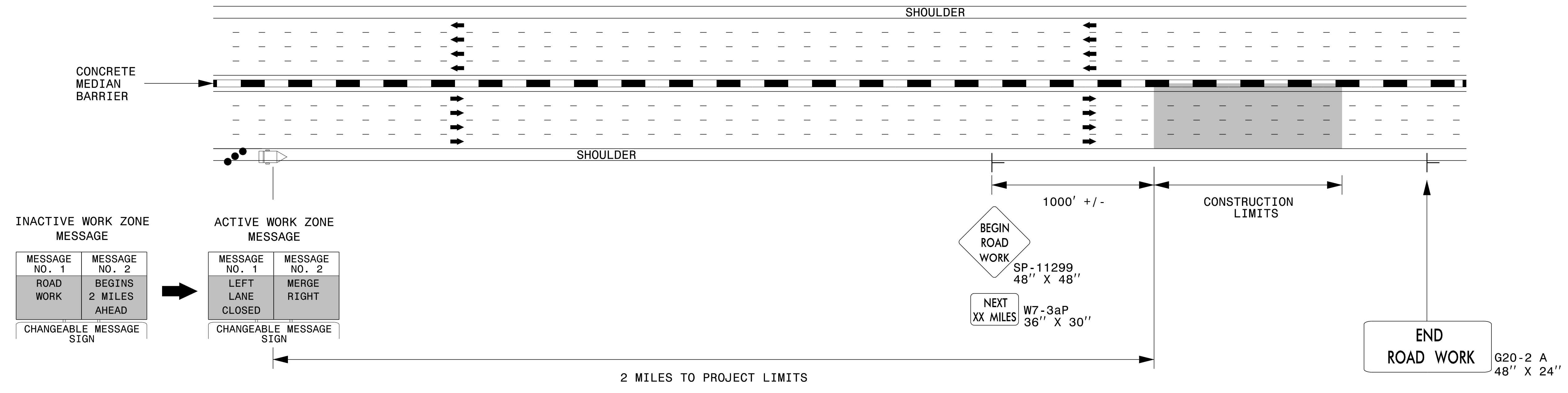


ADVANCE WARNING SIGNS FOR RURAL AND SUBURBAN 2-LANE ROADWAY RESURFACING

DIVIDED MEDIANS WITH WIDTHS 46' OR GREATER



DIVIDED MEDIANS WITH WIDTHS LESS THAN 46' OR WITH PERMANENT MEDIAN BARRIER

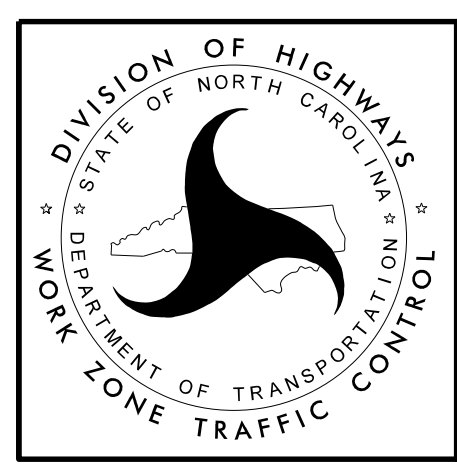


NOTES:

- 1) LATERAL CLEARANCE AT ALL SIGN LOCATIONS SHALL BE 6' AS MEASURED FROM THE EDGE OF PAVEMENT.
- 2) MOUNT SIGNS THAT ARE LARGER THAN 10 SQUARE FEET IN AREA ON TWO OR MORE WOOD OR U-CHANNEL SUPPORTS. PERFORATED SQUARE TUBING SUPPORT SYSTEMS MAY SUPPORT LARGER AREAS ON A SINGLE SUPPORT. FOLLOW MANUFACTURER'S RECOMMENDATIONS. THESE SYSTEMS SHALL BE NCHRP 350 COMPLIANT AND NCDOT APPROVED.
- 3) FOR MEDIAN WIDTHS LESS THAN 46' (MEASURED EDGELINE TO EDGELINE) USE THE BOTTOM DRAWING.
- 4) IF STATIONARY GENERAL WARNING SIGNS ARE USED, THEY WILL BE PAID FOR PER SECTION 104 OF THE NCDOT STANDARD SPECIFICATIONS AS EXTRA WORK.
- 5) INSTALL "ROAD WORK AHEAD" (W20-1) ALONG ENTRANCE RAMP 500' PRIOR TO RAMP TERMINAL, AND "END ROAD WORK" (G20-2a) AT THE END OF EXIT RAMP WITHIN THE WORK ZONE.
- 6) IF MILLED AREAS ARE NOT PAVED BACK BY THE END OF THE WORK DAY, PORTABLE SIGNS SHALL BE USED TO WARN DRIVERS OF THE PRESENT CONDITIONS. THESE ARE TO INCLUDE, BUT NOT LIMITED TO "ROUGH ROAD" W8-8, "UNEVEN LANES" W8-11, "GROOVED PAVEMENT" W8-15 w/MOTORCYCLE PLAQUE MOUNTED BELOW. THESE ARE TO BE DOUBLE INDICATED ON MULTI-LANE ROADWAYS WITH SPEED LIMITS 45 MPH AND GREATER AND WITH DIVIDED MEDIANS OF 46' OR GREATER. THESE PORTABLE SIGNS ARE INCIDENTAL TO THE OTHER ITEMS OF WORK INCLUDED IN THE TEMPORARY TRAFFIC CONTROL (LUMP SUM) PAY ITEM.

LEGEND

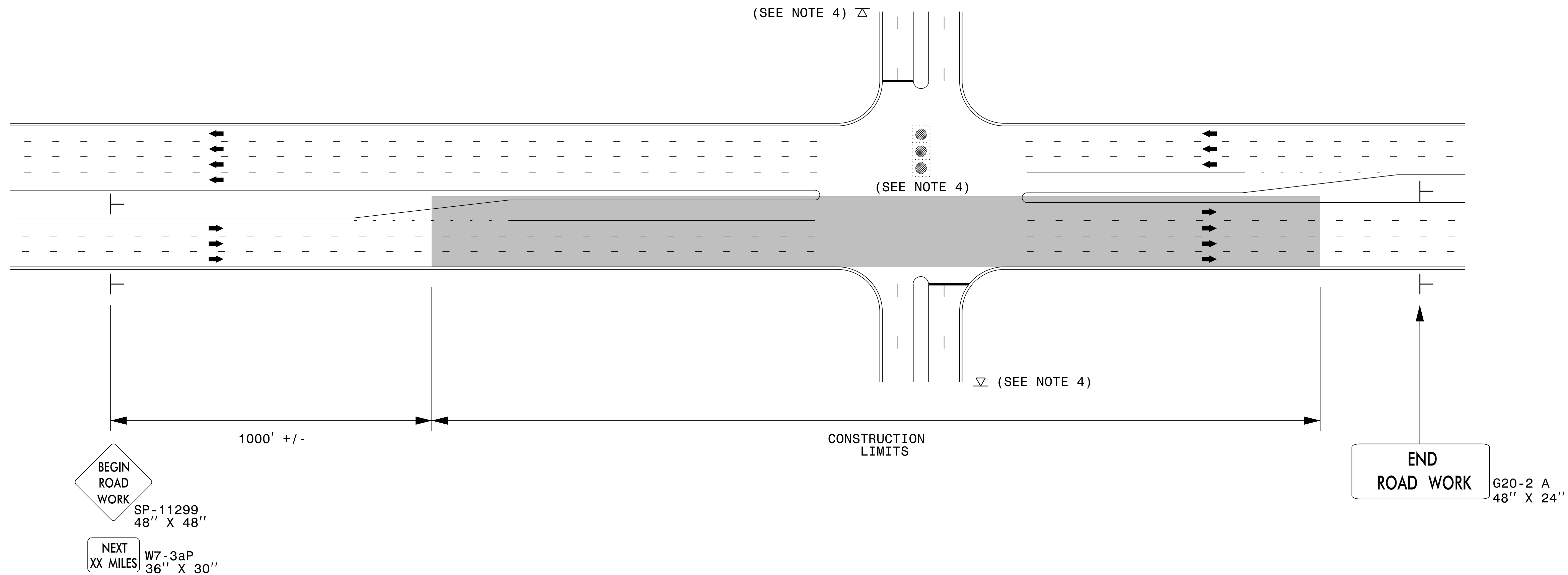
- CHANGEABLE MESSAGE SIGN (CMS)
- STATIONARY SIGN
- DIRECTION OF TRAFFIC FLOW
- TRAFFIC DRUM



**RESURFACING ADVANCE
WARNING SIGNS FOR
HIGH SPEED FACILITIES
≥ 60 MPH**

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URBAN / SUBURBAN WORKZONES

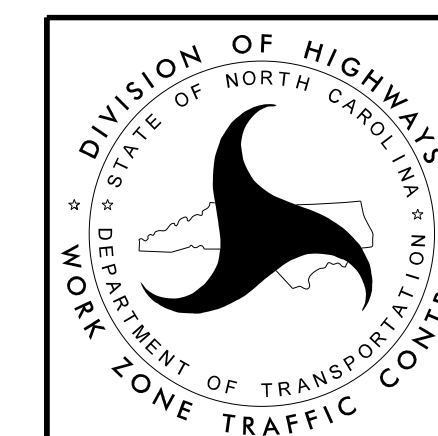


NOTES:

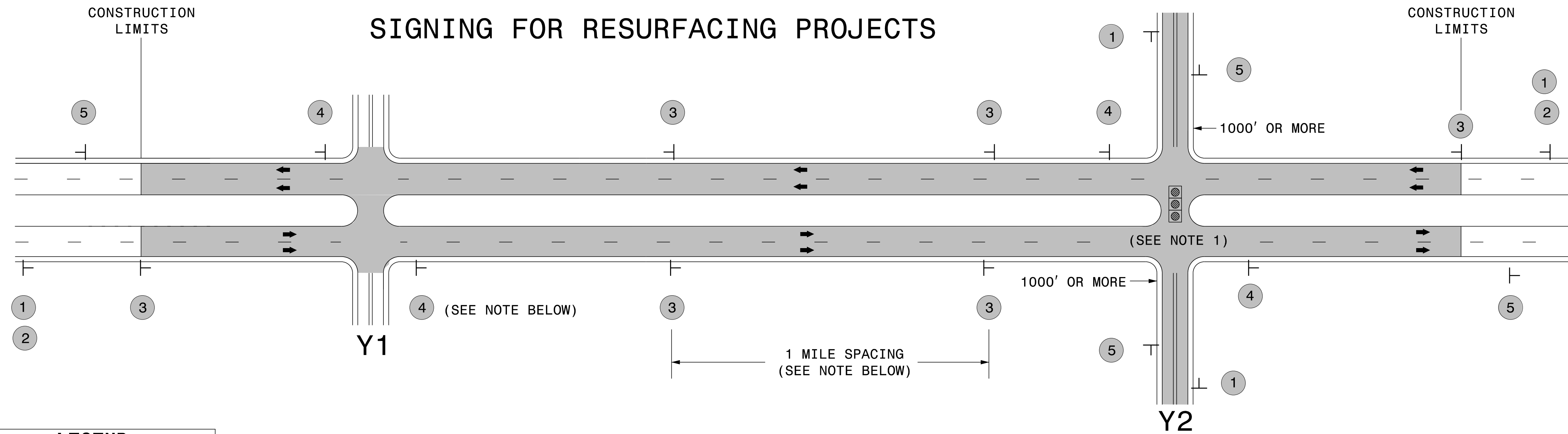
- 1) 48" x 48" SIZED SIGNS (SP- 11299) MAY BE REDUCED TO 36" X 36" ON ROADWAYS WITH SPEED LIMITS OF 40 MPH OR LESS.
- 2) MOUNT SIGNS THAT ARE LARGER THAN 10 SQUARE FEET IN AREA ON TWO OR MORE WOOD OR U-CHANNEL SUPPORTS. PERFORATED SQUARE TUBING SUPPORT SYSTEMS MAY SUPPORT LARGER AREAS ON A SINGLE SUPPORT. FOLLOW MANUFACTURER'S RECOMMENDATIONS. THESE SYSTEMS SHALL BE NCHRP 350 COMPLIANT AND NCDOT APPROVED.
- 3) ADVANCE WARNING SIGNS NOT REQUIRED ON NON-SIGNALIZED SIDE STREETS.
- 4) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION.
- 5) LATERAL CLEARANCE AT ALL SIGN LOCATIONS SHALL BE 2' AS MEASURED FROM THE EDGE OF PAVEMENT OR THE FACE OF THE CURB. WHEN UNABLE TO OBTAIN THE LATERAL CLEARANCE WITHIN THE MEDIAN AREA USE SHOULDER MOUNTS ONLY.
- 6) SIGN MOUNT LOCATIONS SHALL NOT BLOCK SIDEWALKS OR DRIVEWAYS.
- 7) IF STATIONARY GENERAL WARNING SIGNS ARE USED, THEY WILL BE PAID FOR PER SECTION 104 OF THE NCDOT STANDARD SPECIFICATIONS AS EXTRA WORK.
- 8) IF MILLED AREAS ARE NOT PAVED BACK BY THE END OF THE WORK DAY, PORTABLE SIGNS SHALL BE USED TO WARN DRIVERS OF THE PRESENT CONDITIONS. THESE ARE TO INCLUDE, BUT NOT LIMITED TO "ROUGH ROAD" W8-8, "UNEVEN LANES" W8-11, "GROOVED PAVEMENT" W8-15 w/MOTORCYCLE PLAQUE MOUNTED BELOW. THESE ARE TO BE DOUBLE INDICATED ON MULTI-LANE ROADWAYS WITH SPEED LIMITS 45 MPH AND GREATER WHERE LATERAL CLEARANCE CAN BE OBTAINED WITHIN THE MEDIAN AREAS. THESE PORTABLE SIGNS ARE INCIDENTAL TO THE OTHER ITEMS OF WORK INCLUDED IN THE TEMPORARY TRAFFIC CONTROL (LUMP SUM) PAY ITEM.

LEGEND

- ├ STATIONARY SIGN
- ➔ DIRECTION OF TRAFFIC FLOW



**RESURFACING ADVANCE
WARNING SIGNS FOR
URBAN / SUBURBAN
FACILITIES**

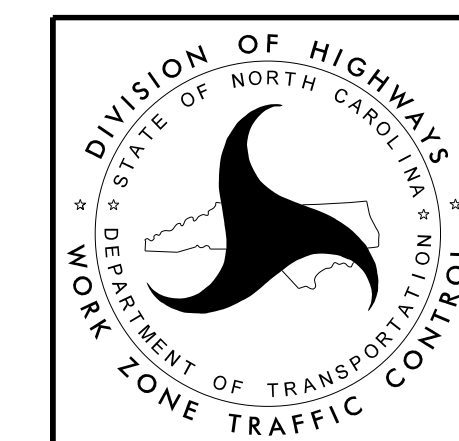


LEGEND	
┆	STATIONARY SIGN
←	DIRECTION OF TRAFFIC FLOW

MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	MAINLINE (-L-) SIGNING		-Y- LINE SIGNING	
	1	 W20-1 48" X 48"	PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;"> W20-1 48" X 48" </div> <div style="text-align: center;"> W20-7 A 48" X 48" </div> </div> <p>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p>
	2	 W7-3aP 24" X 18"	#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)	
	3	 SP 13107 48" X 48"	PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. IF NO -Y- LINES EXIST, PLACE 2ND SET 1/2 MILE FROM THE CONSTRUCTION LIMITS AND THEN SPACE 1 MILE THEREAFTER.	
4	 SP 13106 48" X 48"	THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.		
5	 G20-2 A 48" X 24"	PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.	<p>NOTES:</p> <ol style="list-style-type: none"> 1) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION. 	



RESURFACING
ADVANCE WARNING SIGNS
FOR RURAL AND SUBURBAN
MULTI-LANE ROADWAYS
W/ SHOULDER SECTIONS