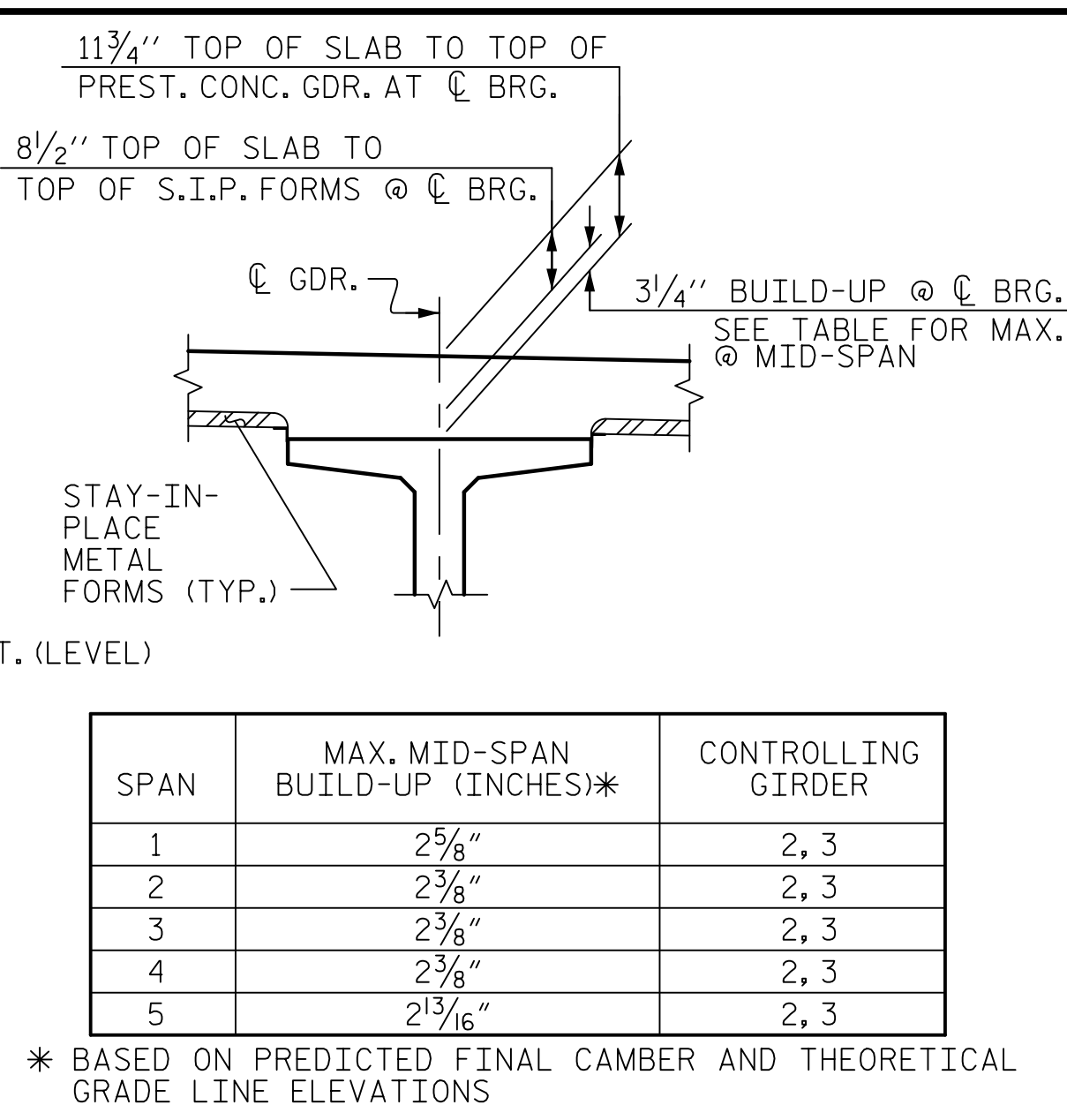
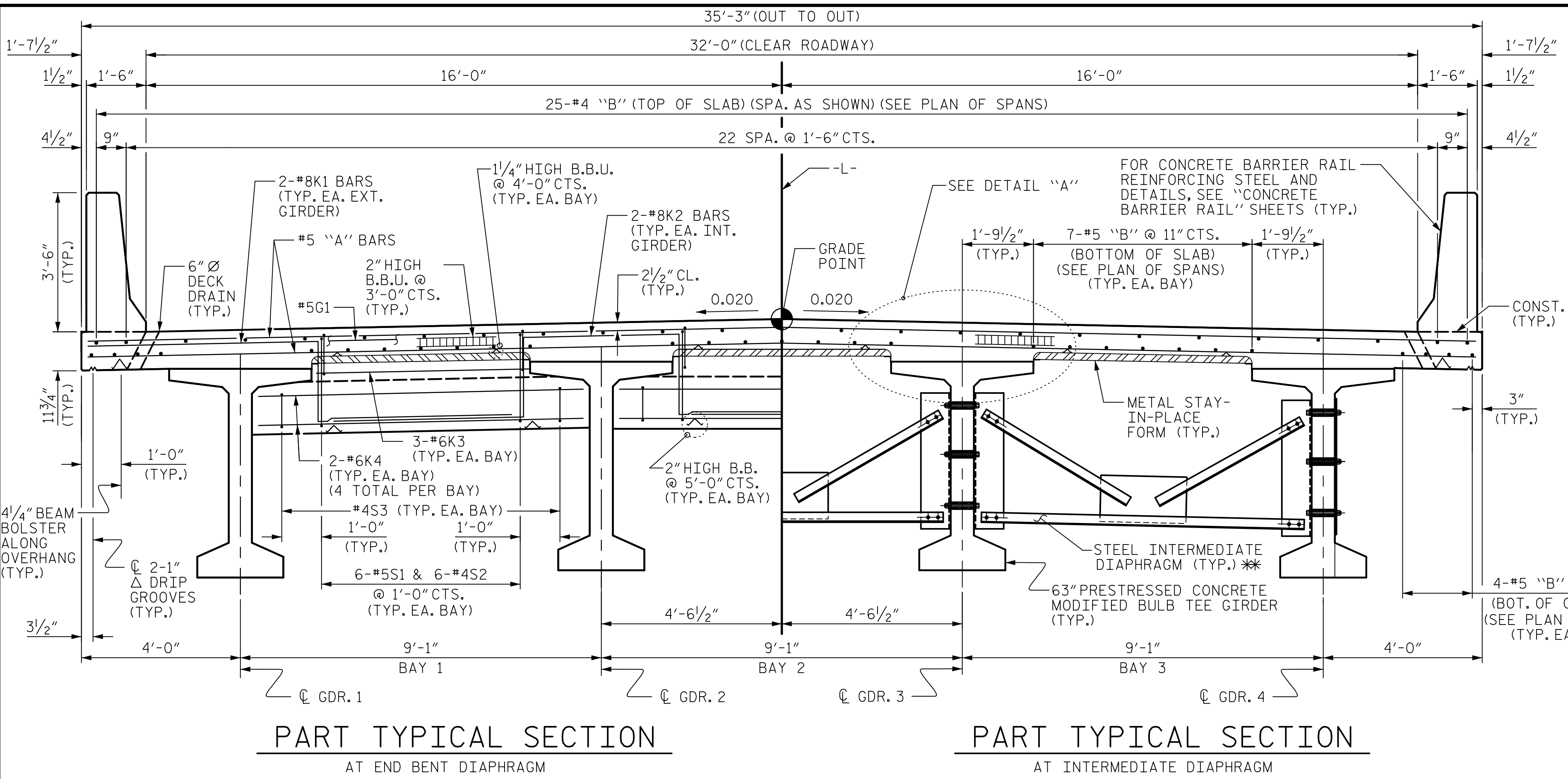


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TIME: 2:46:43 PM

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NOTES:

PROVIDE 1 1/4" HIGH BEAM BOLSTERS UPPER AT 4'-0" CTS. ATOP THE METAL STAY-IN-PLACE FORMS TO SUPPORT THE BOTTOM MAT OF "A" BARS. WHEN USING REMOVABLE FORMS, PROVIDE CONTINUOUS HIGH CHAIRS FOR METAL DECK (C.H.C.M) @ 4'-0" CTS. WITH A HEIGHT TO SUPPORT THE BOTTOM MAT OF "A" BARS A CLEAR DISTANCE OF 2 1/2" ABOVE THE TOP OF THE REMOVABLE FORM.

LONGITUDINAL STEEL MAY BE SHIFTED SLIGHTLY, AS NECESSARY, TO AVOID INTERFERENCE WITH STIRRUPS IN PRESTRESSED CONCRETE GIRDERS.

PREVIOUSLY CAST CONCRETE IN A CONTINUOUS UNIT SHALL HAVE ATTAINED A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI BEFORE ADDITIONAL CONCRETE IS CAST IN THE UNIT.

BARRIER RAIL IN A CONTINUOUS UNIT SHALL NOT BE CAST UNTIL ALL SLAB CONCRETE IN THE UNIT HAS BEEN CAST AND HAS REACHED A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI.

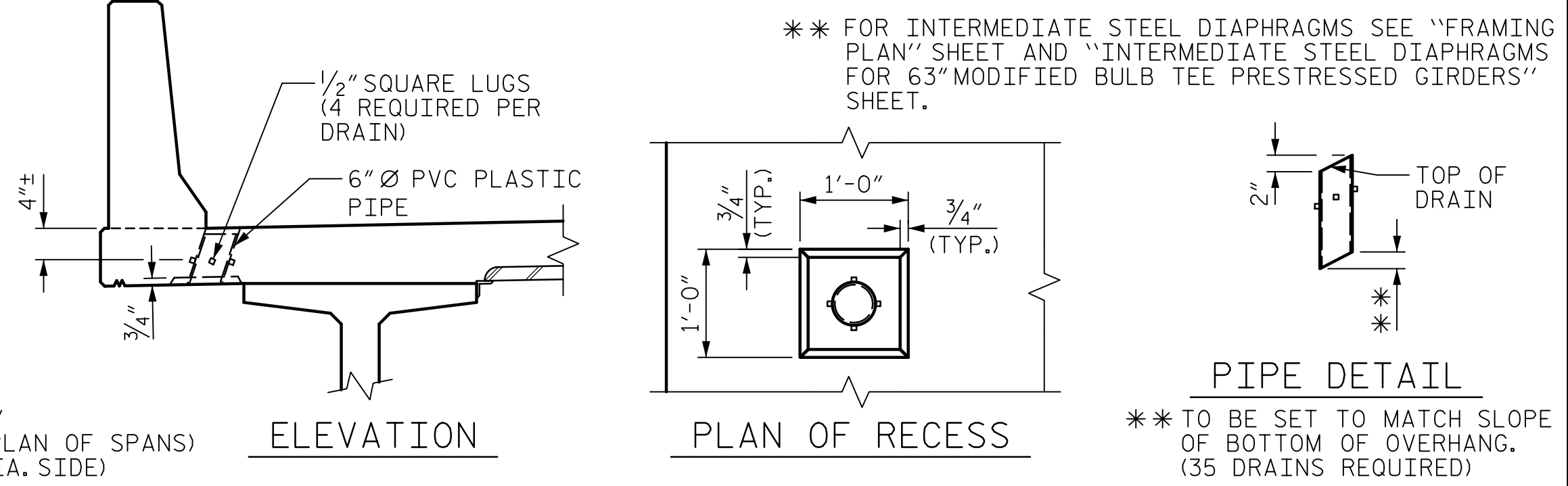
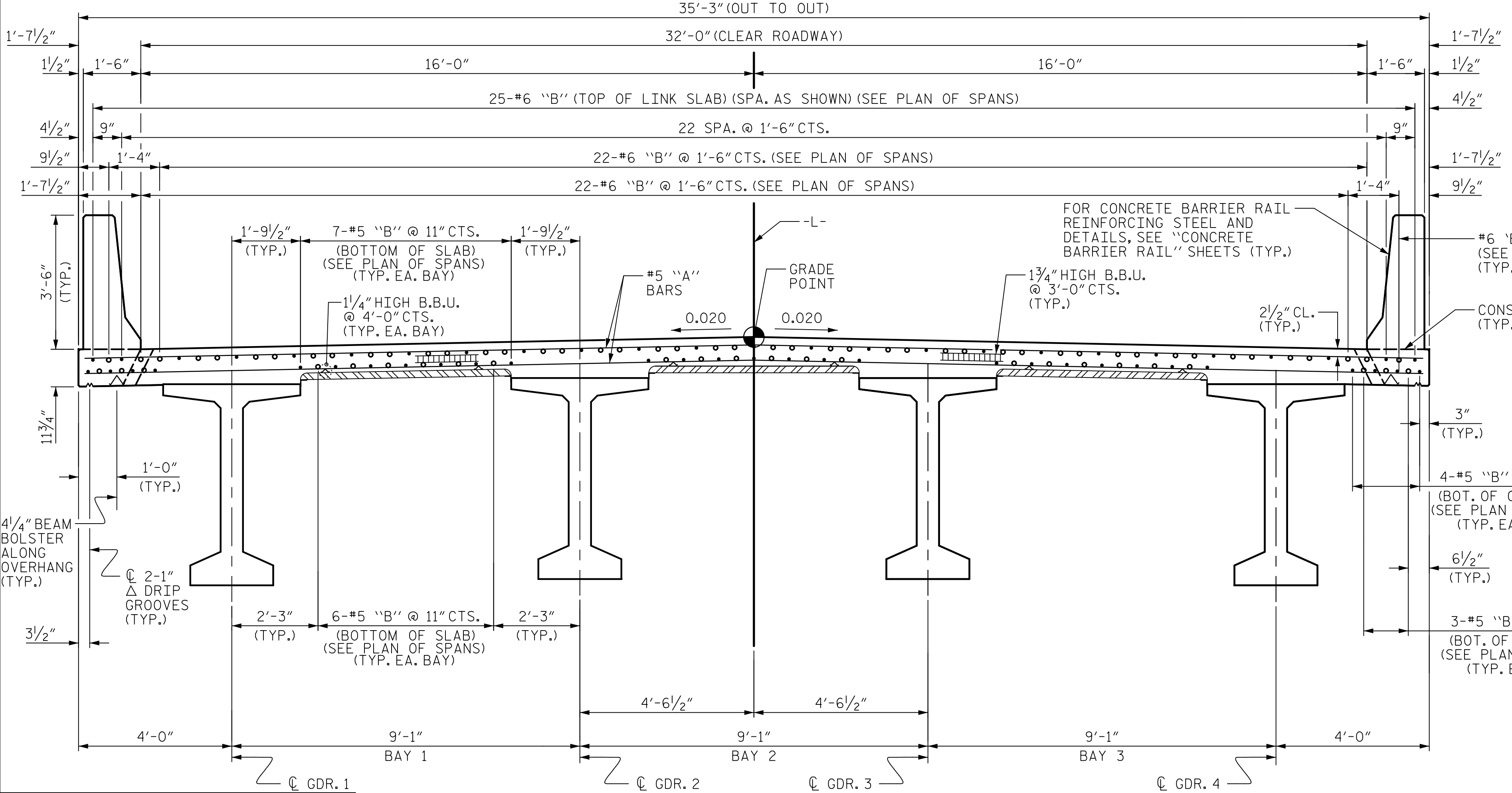
FOR CONCRETE BARRIER RAIL REINFORCING AND DETAILS, SEE "CONCRETE BARRIER RAIL" SHEET.

FOR STRIP SEAL EXPANSION JOINTS, SEE SPECIAL PROVISIONS.

ALL DIMENSIONS ARE HORIZONTAL OR VERTICAL UNLESS OTHERWISE NOTED.

FOR 6" Ø DECK DRAINS, SEE DETAIL THIS SHEET. FOR LOCATIONS AND SPACING OF DECK DRAINS, SEE "PLAN OF SPANS" SHEETS.

REINFORCING STEEL MAY BE SHIFTED, AS NECESSARY, TO AVOID INTERFERENCE WITH DECK DRAINS. AT THE CONTRACTORS OPTION, WITHIN THE DECK POUR OVER THE BENTS WHERE 7 #5 "B" BARS ARE PRESENT IN THE BOTTOM OF EACH OVERHANG, A MAXIMUM OF 1 BOTTOM "B" BAR IN EACH OVERHANG MAY BE CUT AROUND THE DRAIN. SPLICE EQUIVALENT SUPPLEMENTAL REINFORCING WITHIN THE OVERHANG AT EACH CUT LOCATION AT THE MINIMUM SPLICE LENGTHS REQUIRED ON THE PLANS. CUTTING AND SPLICING SUPPLEMENTAL REINFORCING SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE "REINFORCED CONCRETE SLAB".



DRAIN DETAILS

TOP OF FLOOR DRAINS TO BE SET 3/8" BELOW SURFACE OF SLAB.

4 - 1/2" SQUARE LUGS TO BE GLUED TO THE P.V.C. PLASTIC PIPE AT EQUAL SPACES AROUND THE PIPE DRAIN APPROXIMATELY 4" FROM THE TOP OF THE PIPE.

THE 6" Ø PVC PLASTIC PIPE AND FITTINGS SHALL BE SCHEDULE 40 AND CONFORM TO ASTM D1785.

PROJECT NO. BR-0044
 ROCKINGHAM COUNTY
 STATION: 21+97.00 -L-

DRAWN BY : B.D. HODACK
 CHECKED BY : G.R. COLS
 DESIGNED BY : B.D. HODACK
 DESIGN CHECKED BY : G.R. COLS

DATE : 02/2022
 DATE : 03/2022
 DATE : 02/2022
 DATE : 03/2022

TYPICAL SECTION
 LINK SLAB AT BENTS

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED



REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S-06
1			3			TOTAL SHEETS
2			4			39