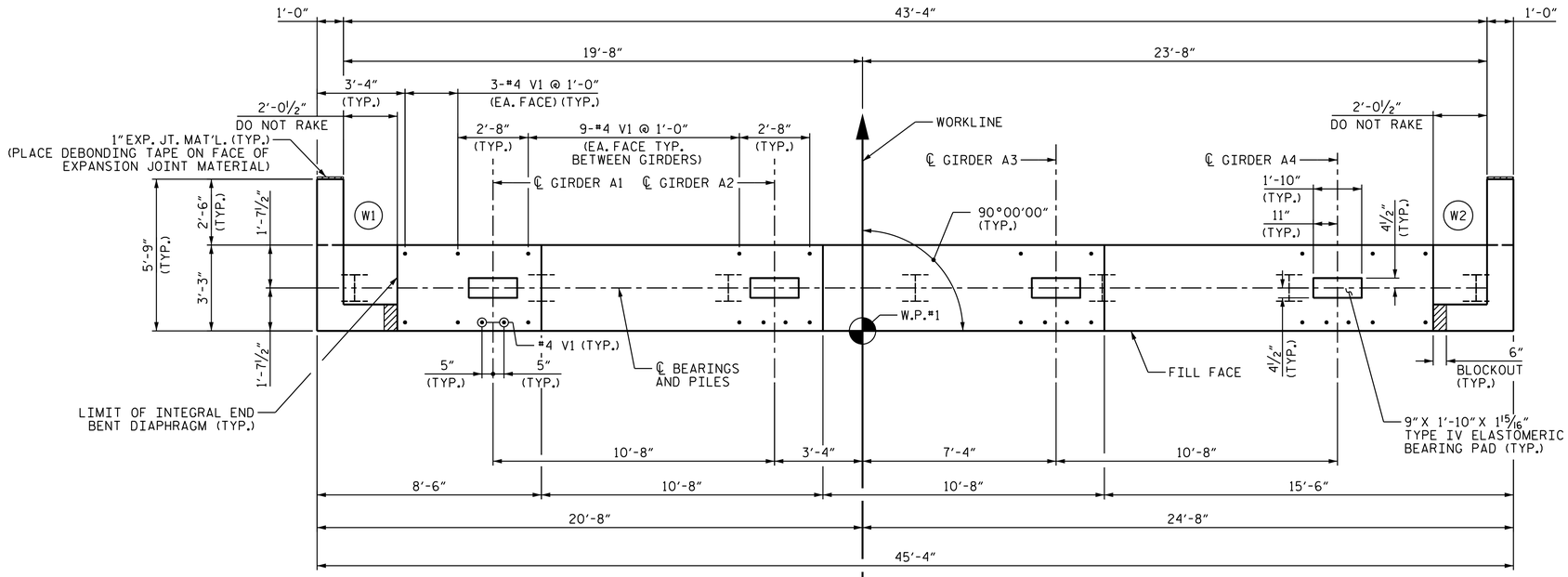
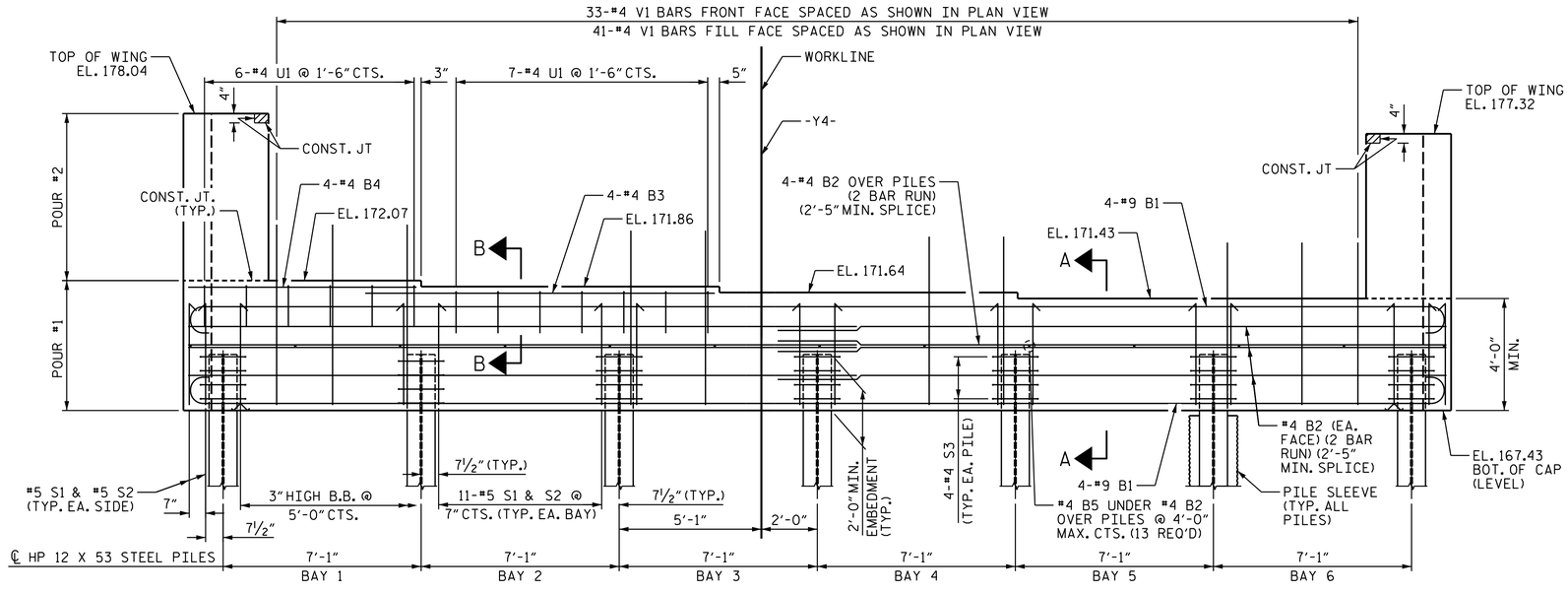


PLAN



ELEVATION

NOTES:

SEE SUPERSTRUCTURE SHEETS FOR UPPER PART OF INTEGRAL END BENT DETAILS.

#5 S1, #5 S2, AND #4 U1 BARS MAY BE SHIFTED SLIGHTLY TO CLEAR #4 V1.

FOR PILE SPLICE DETAILS, SEE "END BENT DETAILS", SHEET 3 OF 3.

THE CONCRETE IN THE SHADED AREA OF THE WING SHALL BE POURED AFTER THE JOINT BETWEEN THE DECK AND APPROACH SLAB HAS BEEN SAWED AND BARRIER RAIL IS CAST IF SLIP FORMING IS USED.

THE TOP SURFACE OF THE END BENT CAP AND WINGS, EXCLUDING THE BEARING AREA AND THE AREA DESIGNATED IN THE PLANS, SHALL BE RAKED TO A DEPTH OF ¼".

THE WING WALLS ARE DETAILED TO FIT WITH MSE WALL COPING DETAIL A AS SHOWN ON THE SLOPE PROTECTION SHEET. IF MSE WALL COPING DETAIL B IS USED, WING WALLS SHALL BE SHORTENED TO FIT. COORDINATE WITH THE MSE WALL FABRICATOR FOR COPING DETAIL TO BE USED. "H" BAR LENGTHS AND "V" BAR POSITIONS SHALL BE ADJUSTED TO FIT FINAL WINGWALL LENGTHS. THE WALL THICKNESS AT TIME OF DESIGN WAS ASSUMED TO BE 6".

THE TOP SURFACE AREA OF THE END BENT CAP BETWEEN THE LIMIT OF INTEGRAL END BENT DIAPHRAGM AND THE WING WALL SHALL BE CURED IN ACCORDANCE WITH THE STANDARD SPECIFICATION EXCEPT THE MEMBRANE CURING COMPOUND METHOD SHALL NOT BE USED.

PROJECT NO. I-5987B

ROBESON COUNTY

STATION: 24+76.86 -Y4-

SHEET 1 OF 3

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

SUBSTRUCTURE

END BENT 1
(INTEGRAL)

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S4-19
1			3			TOTAL SHEETS
2			4			29

DRAWN BY: R. L. DICKE DATE: 8-2021
CHECKED BY: J. M. ROBINSON DATE: 8-2021
DESIGN ENGINEER OF RECORD: J. T. WILLIAMS DATE: 8-2021

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

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NORTH CAROLINA PROFESSIONAL SEAL 043301 ENGINEER JONATHAN T. WILLIAMS

DocuSigned by: Jonathan T. Williams
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21 March 2022

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