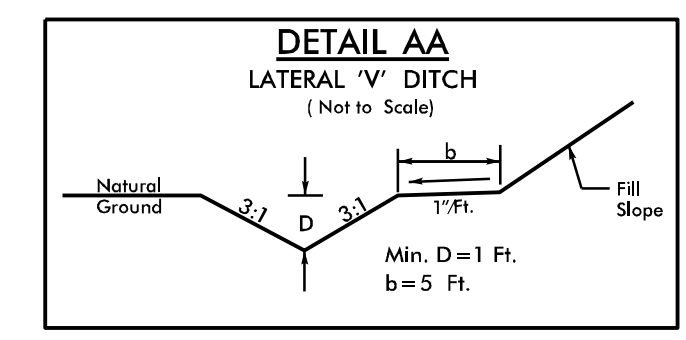
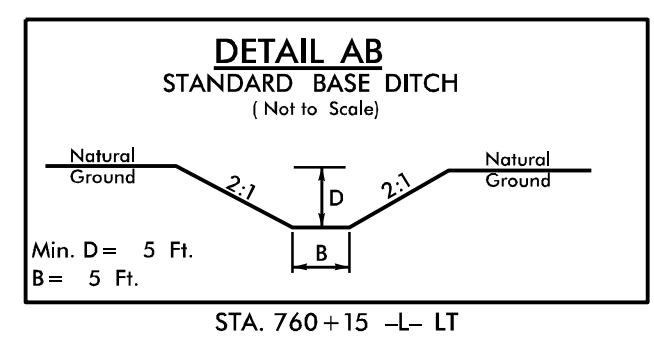


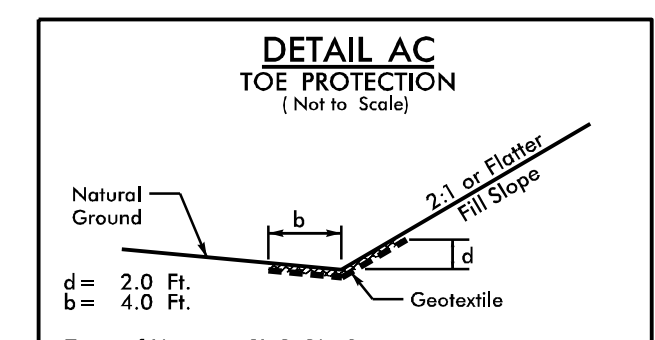
| | |
|--|---|
| PROJECT REFERENCE NO. 1-5897B | SHEET NO. 2D-2 |
| RW SHEET NO. | |
| ROADWAY DESIGN ENGINEER | HYDRAULICS ENGINEER |
| DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED | |
| Prepared in the Office of: | 7621 Purfoy Road, Suite 115 Fuquay-Varina, NC 27526 (919) 552-2253 www.mottmac.com MOTT MACDONALD NC License No. F-0669 |
| | |



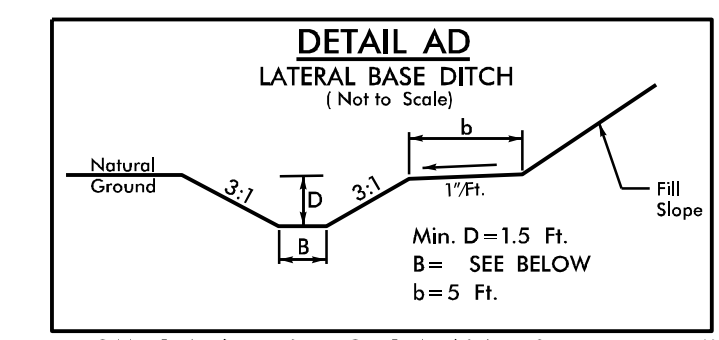
FROM STA. 698+50 TO STA. 700+50 -L- RT
 FROM STA. 703+50 TO STA. 707+50 -L- RT
 FROM STA. 23+09 TO STA. 23+59 -Y1B- LT
 FROM STA. 37+50 TO STA. 38+50 -Y1B- RT
 FROM STA. 19+50 TO STA. 20+93 -Y1BRPC- RT
 FROM STA. 18+45 TO STA. 23+50 -Y1BRPD- RT
 FROM STA. 24+00 TO STA. 28+00 -Y7- LT
 FROM STA. 31+50 TO STA. 32+93 -Y7- RT



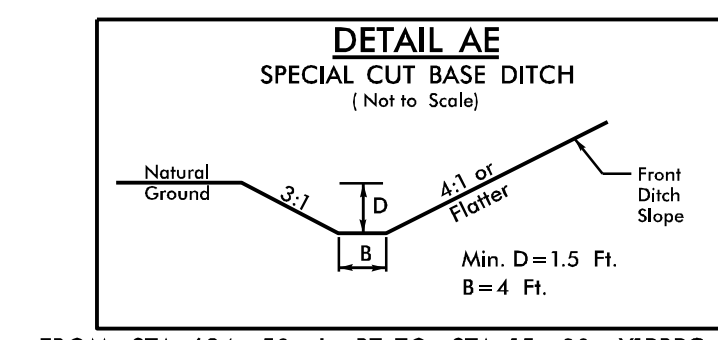
STA. 760+15 -L- LT



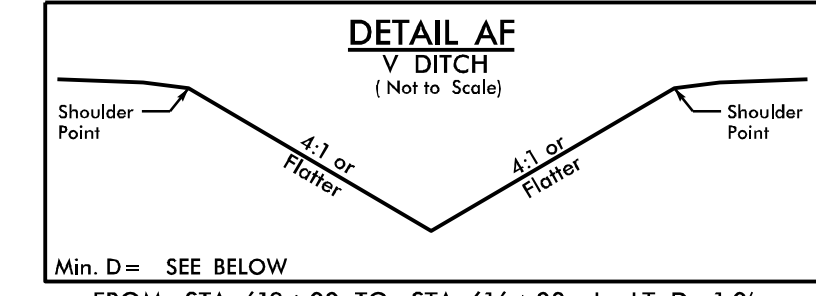
Type of Liner = CL B Rip-Rap
 FROM STA. 592+52 TO STA. 599+00 -L- LT
 FROM STA. 796+91 TO STA. 801+88 -L- LT
 FROM STA. 791+87 TO STA. 801+87 -L- RT
 FROM STA. 804+68 TO STA. 816+77 -L- LT
 FROM STA. 804+17 TO STA. 815+20 -L- RT
 FROM STA. 898+38 TO STA. 902+09 -L- LT
 FROM STA. 902+58 TO STA. 906+96 -L- LT
 FROM STA. 897+99 TO STA. 902+08 -L- RT
 FROM STA. 902+58 TO STA. 906+73 -L- RT
 FROM STA. 18+67 TO STA. 20+87 -SR3- RT
 FROM STA. 16+90 TO STA. 15+15 -Y4- RT
 FROM STA. 29+00 TO STA. 32+40 -Y4- RT



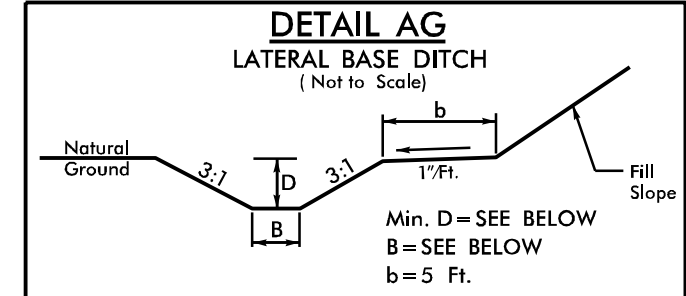
FROM STA. 677+25 TO STA. 684+50 -L- RT, B=4'
 FROM STA. 20+50 TO STA. 22+50 -Y1B- RT, B=3'
 FROM STA. 34+87 TO STA. 37+50 -Y1B- RT, B=2'
 FROM STA. 31+50 TO STA. 37+00 -Y6- LT, B=2'



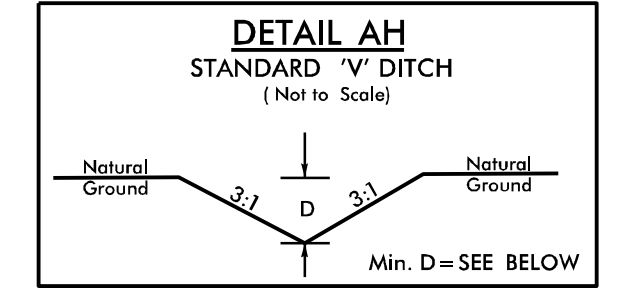
FROM STA. 684+50 -L- RT TO STA. 15+00 -Y1BRPC- RT



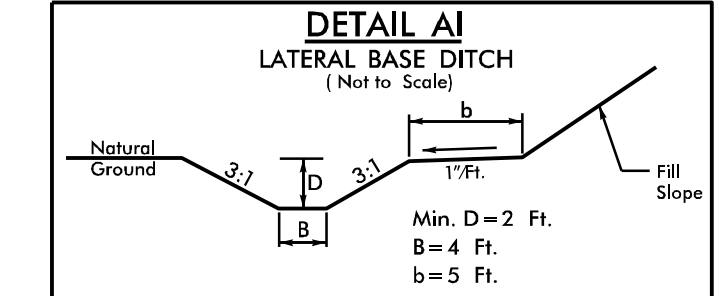
FROM STA. 613+00 TO STA. 616+00 -L- LT, D=1.0'
 FROM STA. 626+50 TO STA. 627+00 -L- RT, D=1.25'
 FROM STA. 627+50 TO STA. 628+00 -L- LT, D=2.0'
 FROM STA. 694+00 TO STA. 698+50 -L- RT, D=1.0'
 FROM STA. 707+50 TO STA. 709+00 -L- RT, D=1.0'
 FROM STA. 706+00 TO STA. 710+50 -L- LT, D=1.0'
 FROM STA. 724+00 TO STA. 726+50 -L- RT, D=1.0'
 TEMP FROM STA. 587+00 TO STA. 592+92 -L- LT, D=1.50'
 BEG. EL.=150.58', END EL.=149.40, 3:1 SLOPES, L=592', S=0.20%
 TEMP FROM STA. 14+06 TO STA. 19+55 -DET L2- LT, D=1.25',
 BEG. EL.=150.48', END EL.=149.75', 3:1 SLOPES, L=549', S=0.13%
 TEMP FROM STA. 19+55 TO STA. 20+67 -DET L2- LT, D=1.0'



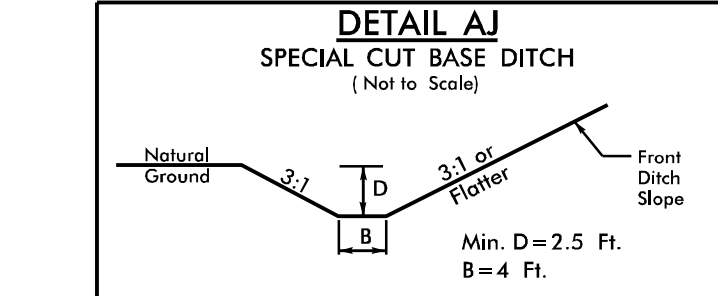
FROM STA. 529+50 TO STA. 530+15 -L- RT, B=4', D=1'
 FROM STA. 530+15 TO STA. 531+00 -L- RT, B=4', D=4'
 FROM STA. 19+86 TO STA. 22+14 -Y1BRPA- RT, B=6', D=3'



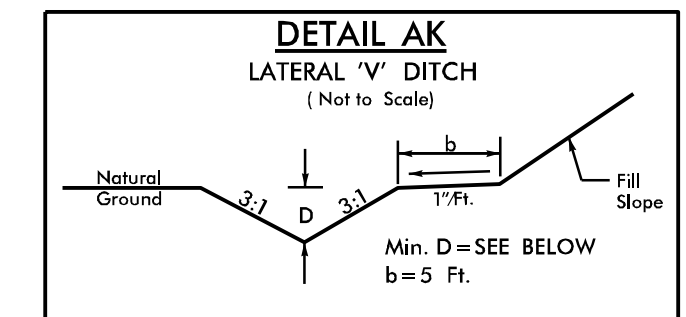
STA. 54+20 TO STA. 55+22 -EY1B- LT, D=1.5'
 FROM STA. 36+12 TO STA. 36+50 -Y1B- LT, D=1.0'



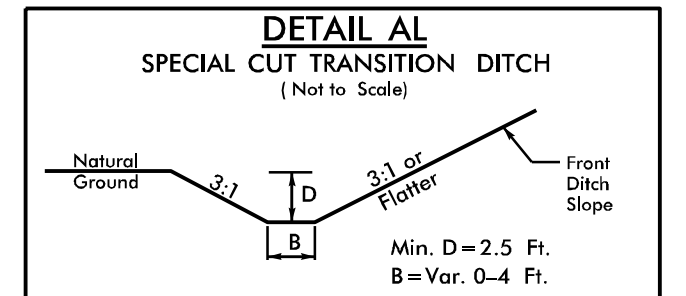
FROM STA. 10+50 TO STA. 12+38 -SR3- RT
 FROM STA. 20+95 TO STA. 25+50 -SR3- RT
 FROM STA. 14+00 TO STA. 16+50 -Y4- RT
 FROM STA. 17+00 TO STA. 18+75 -Y4- LT



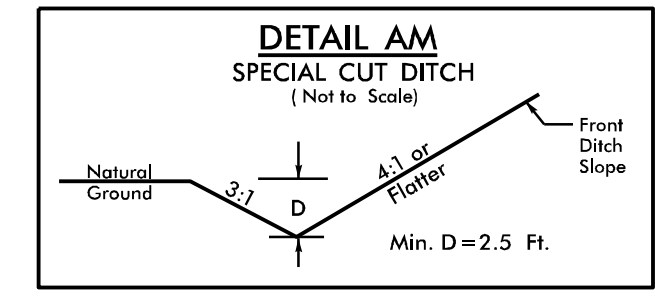
FROM STA. 25+50 TO STA. 37+00 -SR3- RT



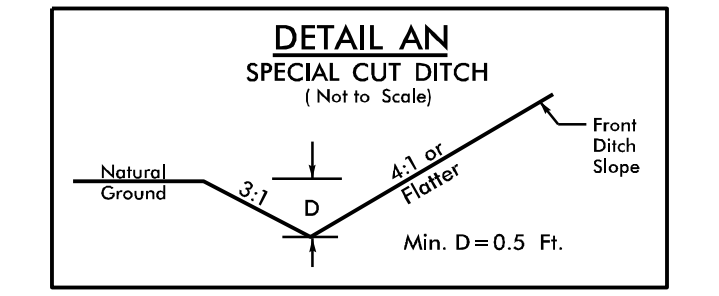
FROM STA. 600+00 TO STA. 604+64 -L- LT, D=2.5'
 FROM STA. 10+00 TO STA. 13+25 -Y1BRPA- RT, D=2.0'



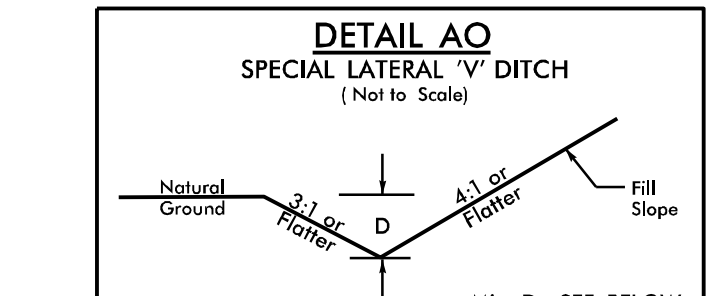
FROM STA. 37+00 TO STA. 38+50 -SR3- RT



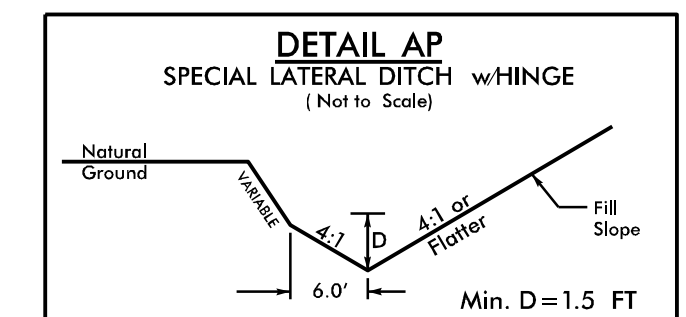
FROM STA. 38+50 TO STA. 43+50 -SR3- RT
 FROM STA. 36+13 TO STA. 36+50 -Y1B- LT



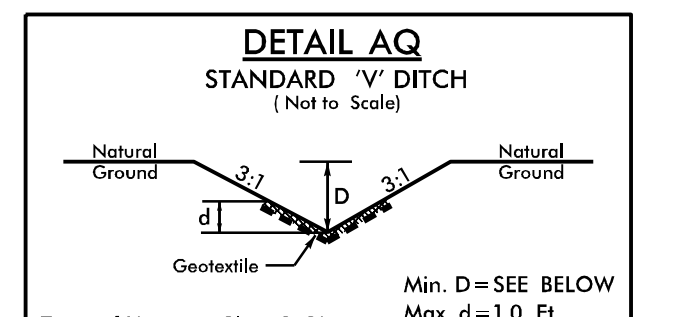
FROM STA. 42+00 TO STA. 43+29 -SR3- LT
 FROM STA. 45+00 TO STA. 46+50 -SR3- LT



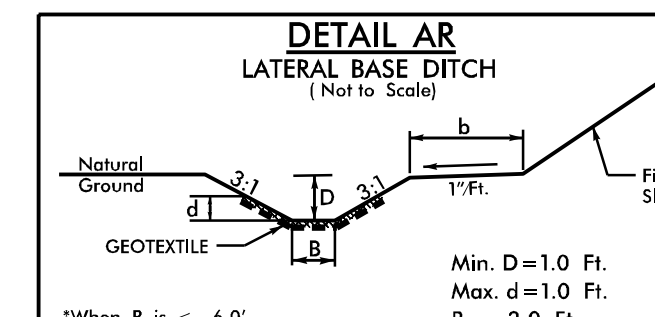
FROM STA. 696+50 TO STA. 702+50 -L- LT, D=1.5'
 FROM STA. 34+00 TO STA. 35+00 -Y5- RT, D=1.5'



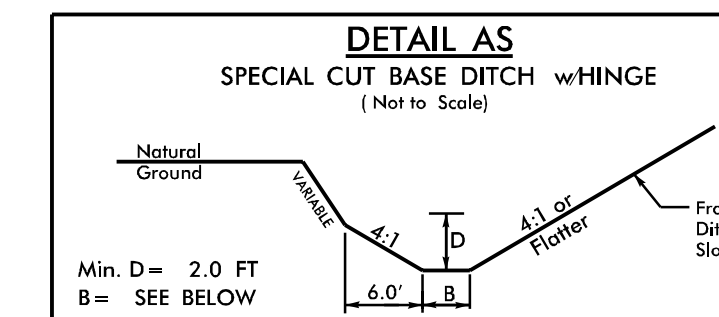
FROM STA. 755+50 TO STA. 759+00 -L- RT
 FROM STA. 762+00 TO STA. 771+60 -L- RT
 FROM STA. 762+50 TO STA. 771+65 -L- LT
 FROM STA. 864+83 TO STA. 871+00 -L- RT



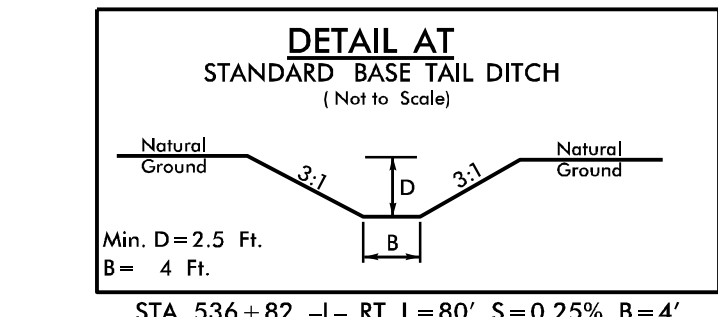
Type of Liner = Class B Rip-Rap
 STA. 771+52 TO STA. 771+72 -L- LT, D=1.5'
 STA. 47+08 TO STA. 48+59 -EY1B- LT, D=1.25'
 STA. 28+80 TO STA. 29+20 -Y5- RT, D=1.25'
 STA. 43+85 -Y5- LT, L=20', S=8.5%, D=1.00'
 BEG. EL. = 162.99, END EL. = 160.69
 STA. 10+20 -Y21- LT TO STA. 599+50 -L- RT, D=1.25'



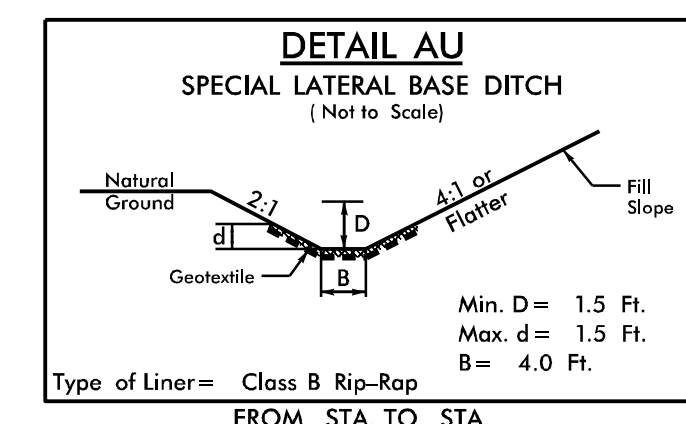
FROM STA. 26+25 TO STA. 28+89 -Y1B- RT
 FROM STA. 30+75 TO STA. 31+00 -Y4- LT



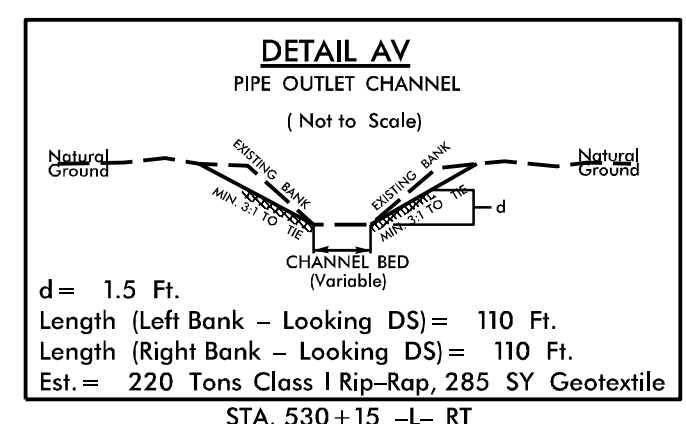
FROM STA. 775+00 TO STA. 797+00 -L- LT, B=4'
 FROM STA. 817+50 TO STA. 838+00 -L- LT, B=4'



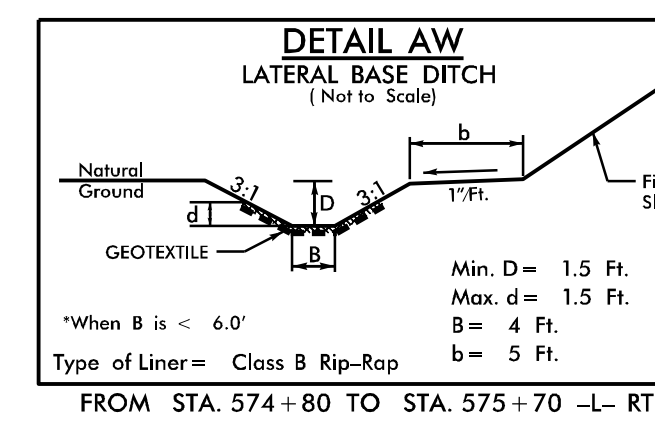
STA. 536+82 -L- RT, L=80', S=0.25%, B=4'
 BEG. EL.=158.95', END EL.=158.75'



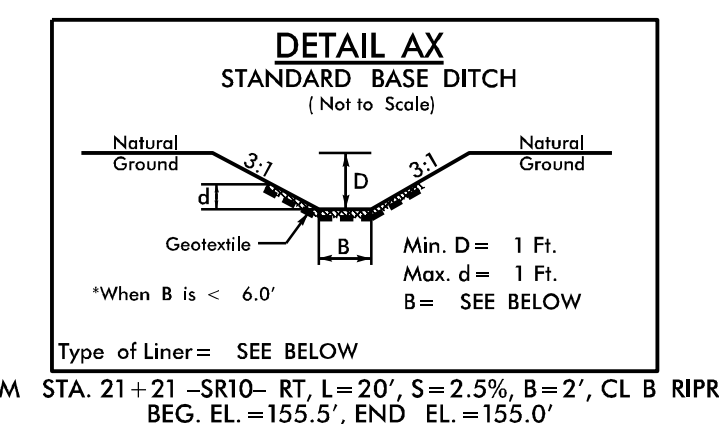
FROM STA. TO STA.



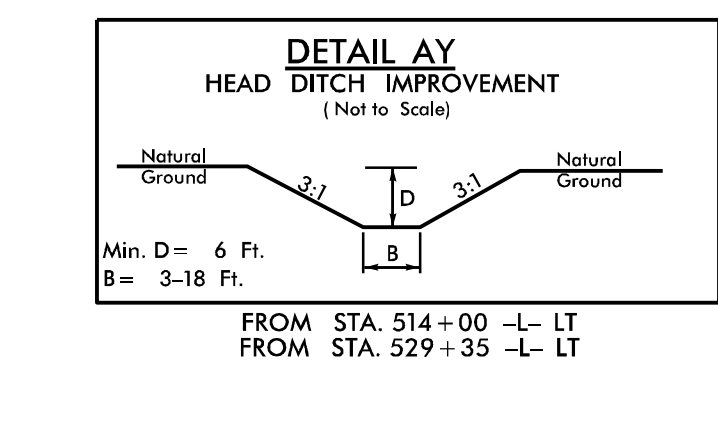
STA. 530+15 -L- RT



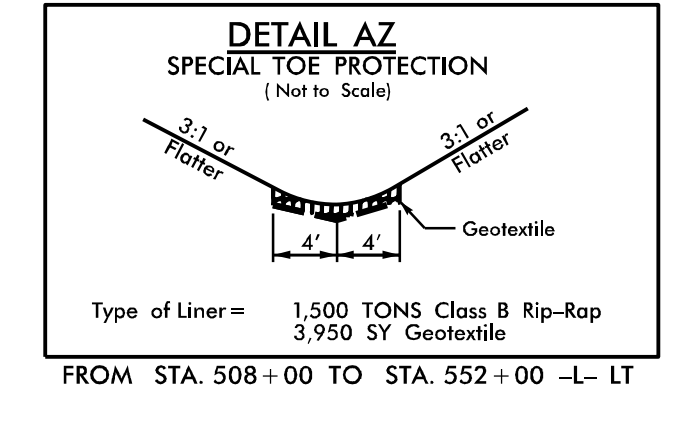
FROM STA. 574+80 TO STA. 575+70 -L- RT



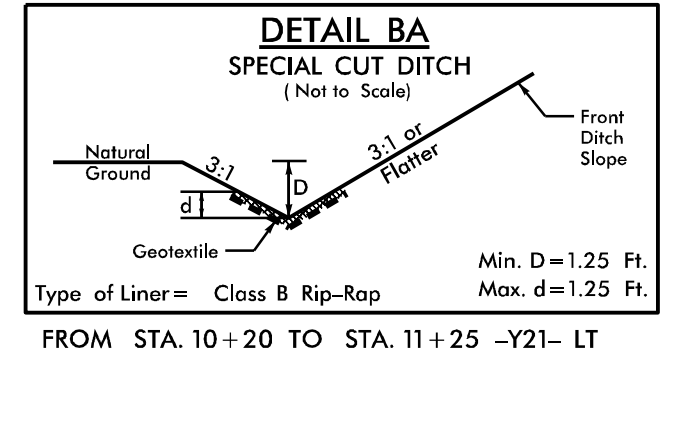
FROM STA. 21+21 -SR10- RT, L=20', S=2.5%, B=2', CL B RIPRAP
 BEG. EL.=155.5', END EL.=155.0'
 STA. 20+50 -Y1B RPB- RT, D=1.0', B=2', CL I RIPRAP
 FROM STA. 18+75 TO STA. 19+50 -Y4- RT, B=4', CL B RIPRAP



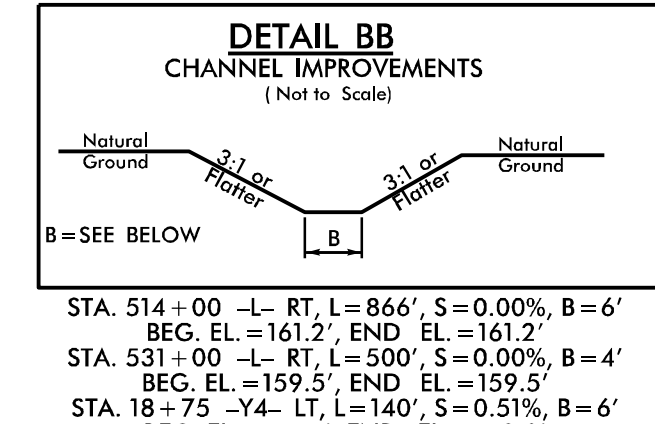
FROM STA. 514+00 -L- LT
 FROM STA. 529+35 -L- LT



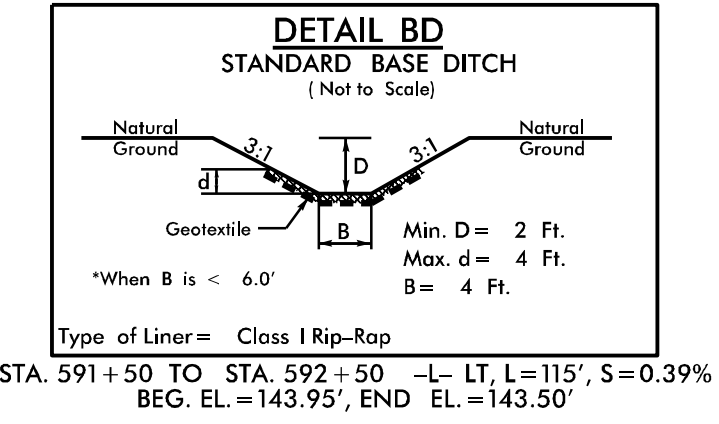
FROM STA. 508+00 TO STA. 552+00 -L- LT



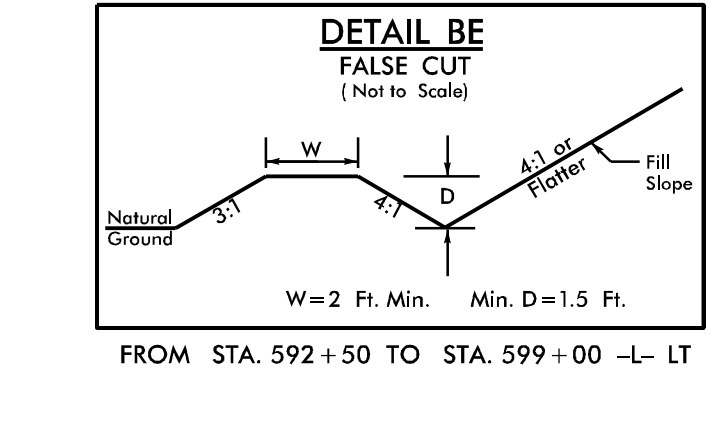
FROM STA. 10+20 TO STA. 11+25 -Y21- LT



STA. 514+00 -L- RT, L=866', S=0.00%, B=6'
 BEG. EL.=161.2', END EL.=161.2'
 STA. 531+00 -L- RT, L=500', S=0.00%, B=4'
 BEG. EL.=159.5', END EL.=159.5'
 STA. 18+75 -Y4- LT, L=140', S=0.51%, B=6'
 BEG. EL.=151.5', END EL.=150.8'



FROM STA. 591+50 TO STA. 592+50 -L- LT, L=115', S=0.39%
 BEG. EL.=143.95', END EL.=143.50'



FROM STA. 592+50 TO STA. 599+00 -L- LT