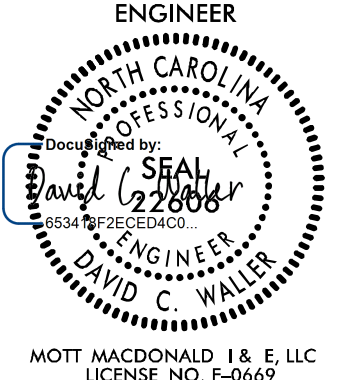
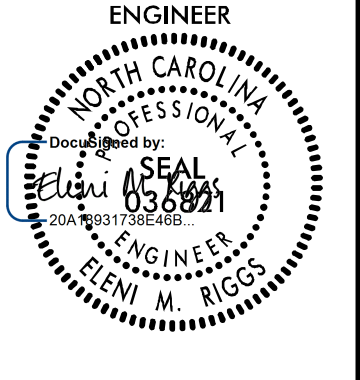


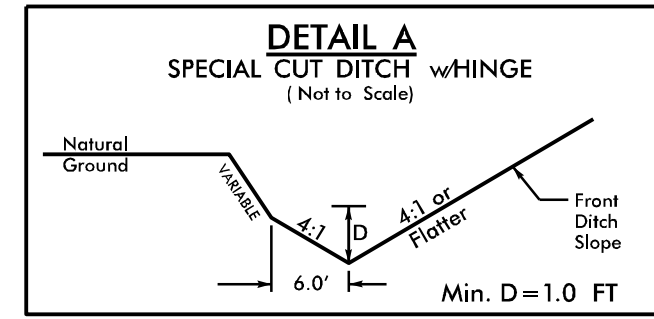
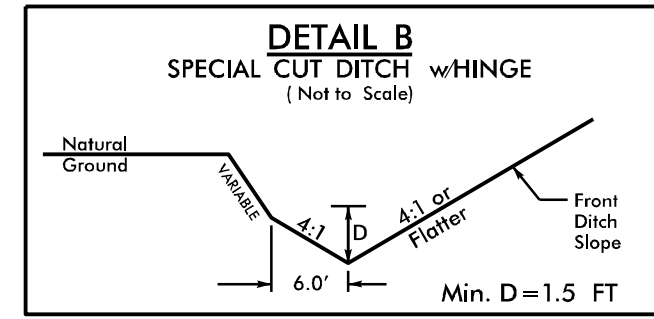


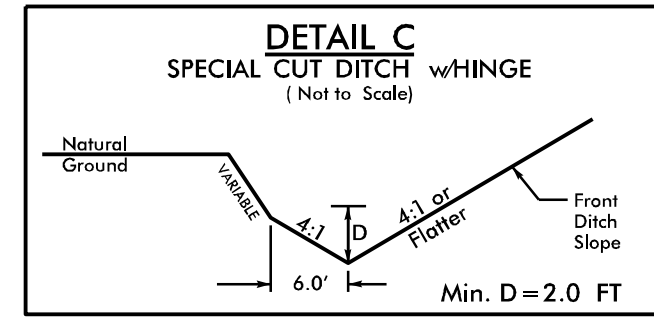
PROJECT REFERENCE NO. <b>1-5897B</b>	SHEET NO. <b>2D-1</b>
RW SHEET NO.	
ROADWAY DESIGN ENGINEER 	HYDRAULICS ENGINEER 
MOTT MACDONALD I & E, LLC LICENSE NO. F-0669	
<b>DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED</b>	
Prepared in the Office of: 	7621 Purfoy Road, Suite 115 Fuquay-Varina, NC 27526 (919) 552-2253 www.mottmac.com MOTT MACDONALD NC License No. F-0669
	



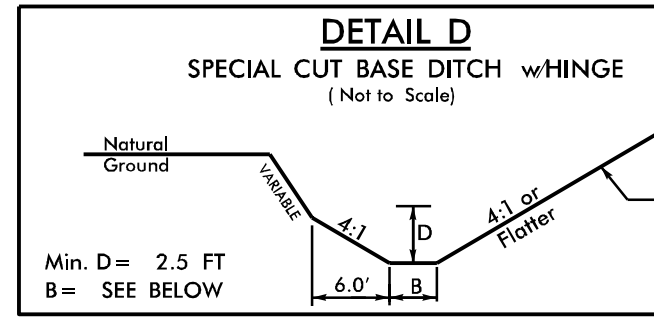
FROM STA. 522+00 TO STA. 527+00 -L- RT  
 FROM STA. 558+00 TO STA. 564+00 -L- LT  
 FROM STA. 562+00 TO STA. 566+00 -L- RT  
 FROM STA. 875+00 TO STA. 878+00 -L- LT  
 FROM STA. 20+50 TO STA. 27+00 -Y1BRPB- RT  
 FROM STA. 15+00 TO STA. 18+00 -Y1BRPB- RT  
 FROM STA. 19+50 TO STA. 21+25 -Y7- RT



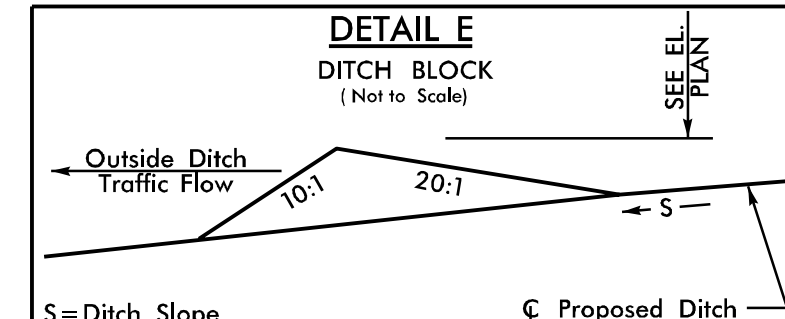
FROM STA. 497+80 TO STA. 506+00 -L- RT  
 FROM STA. 571+50 TO STA. 572+50 -L- LT  
 FROM STA. 772+50 TO STA. 789+00 -L- RT  
 FROM STA. 839+00 TO STA. 851+00 -L- LT  
 FROM STA. 851+00 TO STA. 864+83 -L- RT  
 FROM STA. 892+00 TO STA. 895+00 -L- LT



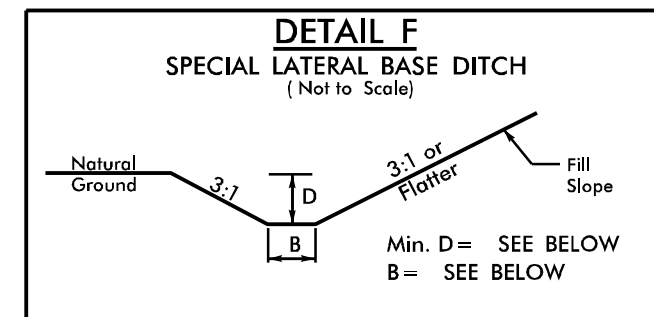
FROM STA. 543+50 TO STA. 551+50 -L- RT  
 FROM STA. 851+00 TO STA. 863+00 -L- LT  
 FROM STA. 868+00 TO STA. 878+00 -L- LT



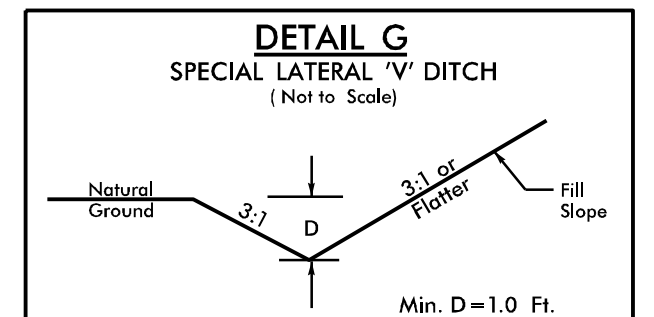
FROM STA. 816+50 TO STA. 851+00 -L- RT, B=4'



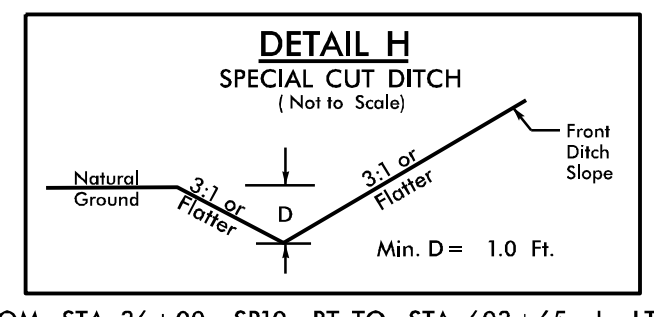
STA. 534+88 -L- LT  
 STA. 536+70 -L- LT  
 STA. 563+62 -L- RT  
 STA. 563+62 -L- LT  
 STA. 568+12 -L- RT  
 STA. 568+12 -L- LT  
 STA. 592+62 -L- LT  
 STA. 595+13 -L- LT  
 STA. 599+88 -L- LT  
 STA. 610+12 -L- LT  
 STA. 610+12 -L- RT  
 STA. 612+37 -L- LT  
 STA. 613+12 -L- LT  
 STA. 615+12 -L- RT  
 STA. 619+80 -L- RT  
 STA. 622+37 -L- LT  
 STA. 771+62 -L- RT  
 STA. 839+00 -L- LT  
 STA. 35+09 -SR10- LT



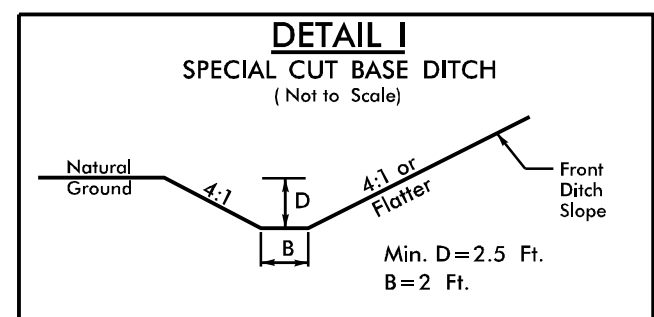
FROM STA. 495+00 TO STA. 506+50 -L- RT, D=1.5', B=6'  
 FROM STA. 495+00 TO STA. 497+80 -L- LT, D=1.0', B=6'  
 FROM STA. 497+80 TO STA. 502+00 -L- LT, D=1.0', B=4'  
 FROM STA. 507+00 TO STA. 513+95 -L- RT, D=1.0', B=4'  
 FROM STA. 514+13 TO STA. 518+33 -L- RT, D=5.0', B=8'  
 FROM STA. 518+33 TO STA. 520+00 -L- RT, D=1.0', B=6'  
 FROM STA. 536+85 TO STA. 543+50 -L- RT, D=1.0', B=4'  
 FROM STA. 556+00 TO STA. 556+10 -L- RT, D=1.5', B=4'  
 FROM STA. 577+00 TO STA. 585+00 -L- LT, D=3.0', B=6'  
 FROM STA. 593+30 TO STA. 595+00 -L- RT, D=2.25', B=6'  
 FROM STA. 601+50 TO STA. 607+00 -L- RT, D=3.0', B=4'  
 FROM STA. 618+50 TO STA. 621+12 -L- RT, D=1.5', B=4'  
 FROM STA. 632+00 TO STA. 634+39 -L- RT, D=1.5', B=6'  
 FROM STA. 634+39 TO STA. 638+00 -L- RT, D=1.0', B=2'  
 FROM STA. 626+00 TO STA. 634+50 -L- LT, D=2.25', B=6'  
 FROM STA. 634+50 TO STA. 639+00 -L- LT, D=1.5', B=4'  
 FROM STA. 639+00 TO STA. 642+25 -L- RT, D=1.0', B=2'  
 FROM STA. 884+50 TO STA. 898+50 -L- RT, D=2.5', B=3'  
 FROM STA. 73+50 TO STA. 79+00 -SR4- LT, D=1.5', B=4'  
 FROM STA. 19+00 TO STA. 21+21 -SR10- RT, D=1.0', B=2'  
 FROM STA. 27+50 TO STA. 30+00 -SR10- LT, D=1.25', B=4'  
 FROM STA. 30+00 TO STA. 33+00 -SR10- LT, D=1.75', B=4'  
 FROM STA. 35+50 TO STA. 36+25 -Y5- RT, D=2.0', B=2'  
 FROM STA. 37+00 TO STA. 41+28 -Y6- LT, D=2.5', B=6'  
 FROM STA. 38+50 TO STA. 45+00 -Y7- RT, D=2.5', B=3'  
 FROM STA. 45+00 -Y7- RT TO XPIPE SR 1904 RT, D=2.5', B=3'



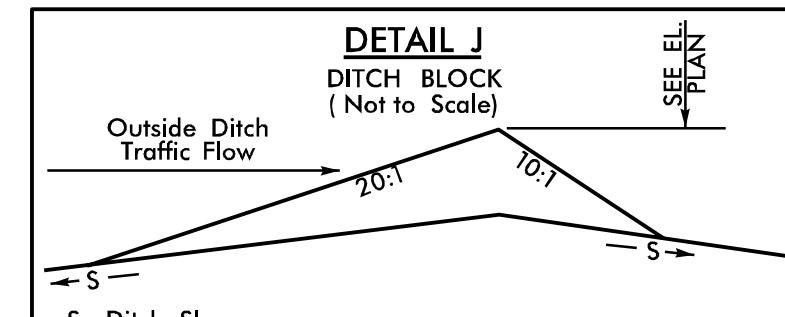
FROM STA. 502+00 TO STA. 503+00 -L- LT  
 FROM STA. 527+50 TO STA. 529+50 -L- RT  
 FROM STA. 531+00 TO STA. 532+00 -L- RT  
 FROM STA. 534+00 TO STA. 536+80 -L- RT  
 FROM STA. 543+50 TO STA. 553+50 -L- RT  
 FROM STA. 555+90 TO STA. 558+00 -L- LT  
 FROM STA. 564+00 TO STA. 565+00 -L- LT  
 FROM STA. 556+10 TO STA. 562+00 -L- RT  
 FROM STA. 599+00 TO STA. 600+00 -L- LT  
 FROM STA. 607+00 TO STA. 610+00 -L- RT  
 FROM STA. 617+15 TO STA. 617+56 -L- RT  
 FROM STA. 638+00 TO STA. 639+50 -L- RT  
 FROM STA. 642+25 TO STA. 650+00 -L- RT  
 FROM STA. 639+50 TO STA. 650+00 -L- LT  
 FROM STA. 650+00 TO STA. 654+98 -L- LT  
 FROM STA. 650+00 TO STA. 655+60 -L- RT  
 FROM STA. 700+50 TO STA. 702+50 -L- LT  
 FROM STA. 74+00 TO STA. 76+50 -SR4- RT  
 FROM STA. 10+50 TO STA. 12+50 -SR10- LT  
 FROM STA. 10+50 TO STA. 11+50 -SR10- RT  
 FROM STA. 12+50 TO STA. 15+00 -SR10- RT  
 FROM STA. 19+50 TO STA. 27+50 -SR10- LT  
 FROM STA. 21+21 TO STA. 36+00 -SR10- RT  
 FROM STA. 19+00 TO STA. 20+15 -Y1BRPB- LT  
 FROM STA. 33+50 TO STA. 36+00 -Y4- RT  
 FROM STA. 36+60 TO STA. 38+00 -Y5- RT  
 FROM STA. 43+08 TO STA. 43+50 -Y5- LT  
 FROM STA. 15+00 TO STA. 23+30 -Y6- LT  
 FROM STA. 36+50 TO STA. 44+00 -Y6- RT  
 FROM STA. 41+50 TO STA. 44+00 -Y6- LT  
 FROM STA. 45+00 TO STA. 46+00 -Y6- LT



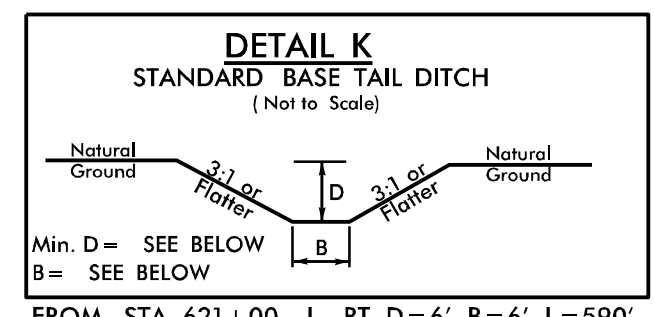
FROM STA. 36+00 -SR10- RT TO STA. 603+65 -L- LT  
 FROM STA. 879+00 TO STA. 883+00 -L- LT  
 FROM STA. 879+00 TO STA. 882+50 -L- RT  
 FROM STA. 36+50 TO STA. 38+50 -Y4- LT  
 FROM STA. 37+00 TO STA. 38+00 -Y4- RT  
 FROM STA. 22+09 TO STA. 26+60 -YSRPB- LT  
 FROM STA. 10+20 -Y21- LT TO STA. 10+25 -Y21- RT



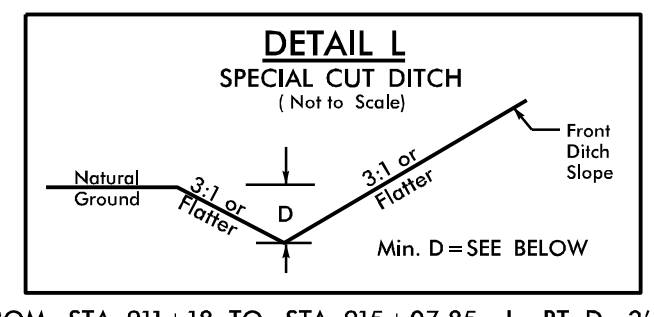
FROM STA. 907+50 TO STA. 913+00 -L- LT  
 FROM STA. 906+77 TO STA. 911+18 -L- RT  
 FROM STA. 10+50 TO STA. 15+50 -Y5- RT



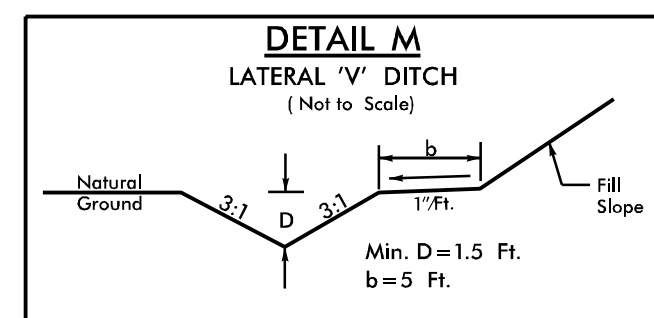
STA. 895+00 -L- LT



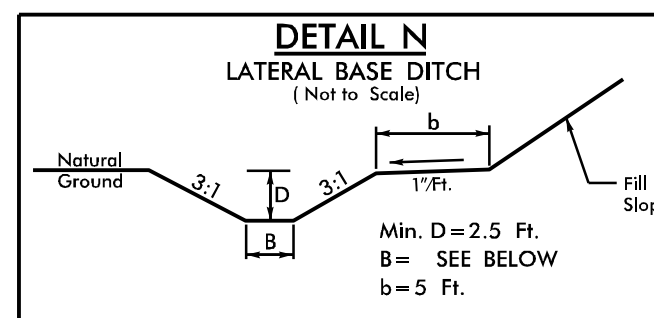
FROM STA. 621+00 -L- RT, D=6', B=6', L=590', S=0.02%, BEG. EL.=153.25', END EL.=153.00'  
 FROM STA. 895+14 TO STA. 895+72 -L- RT, D=2.5', B=3'  
 FROM STA. 15+50 TO STA. 18+00 -Y5- RT, D=1.6', B=4'  
 FROM STA. 48+50 -Y5- LT, D=6', B=6', L=150', S=0.02%, BEG. EL.=154.30', END EL.=154.27'



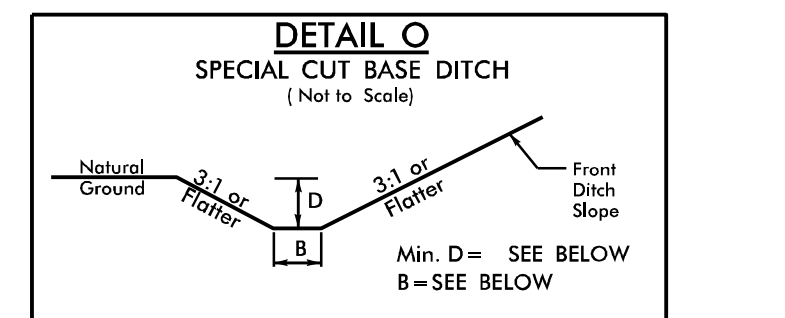
FROM STA. 911+18 TO STA. 915+07.85 -L- RT, D=2'  
 FROM STA. 913+00 TO STA. 915+07.85 -L- LT, D=2'  
 FROM STA. 12+50 TO STA. 14+00 -Y4- LT, D=1.25'  
 FROM STA. 30+50 TO STA. 33+50 -Y5- RT, D=1.5'



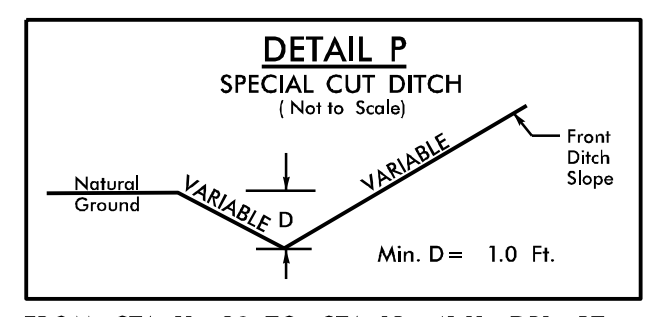
FROM STA. 503+00 TO STA. 507+00 -L- LT  
 FROM STA. 552+00 TO STA. 555+90 -L- LT  
 FROM STA. 721+00 TO STA. 724+50 -L- LT  
 FROM STA. 10+90 TO STA. 16+50 -SR3- LT  
 FROM STA. 76+50 TO STA. 81+00 -SR4- RT  
 FROM STA. 14+00 TO STA. 17+00 -Y4- LT  
 FROM STA. 32+50 TO STA. 36+50 -Y4- LT  
 FROM STA. 21+25 TO STA. 29+00 -Y7- RT



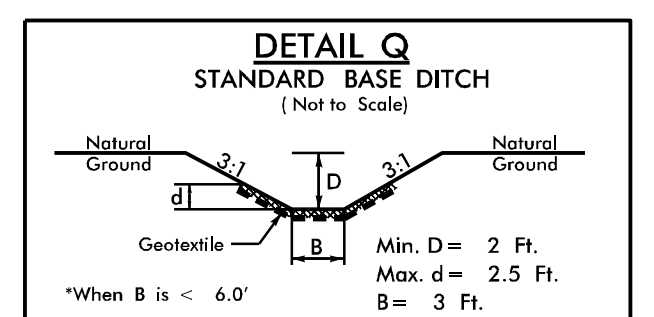
FROM STA. 553+50 TO STA. 556+00 -L- RT, B=4'  
 FROM STA. 574+90 TO STA. 577+00 -L- LT, B=6'  
 FROM STA. 621+00 TO STA. 628+00 -L- RT, B=6'  
 FROM STA. 628+00 TO STA. 632+00 -L- RT, B=6'  
 FROM STA. 79+00 TO STA. 80+88 -SR4- LT, B=4'  
 FROM STA. 13+25 TO STA. 19+80 -Y1BRPB- RT, B=4'



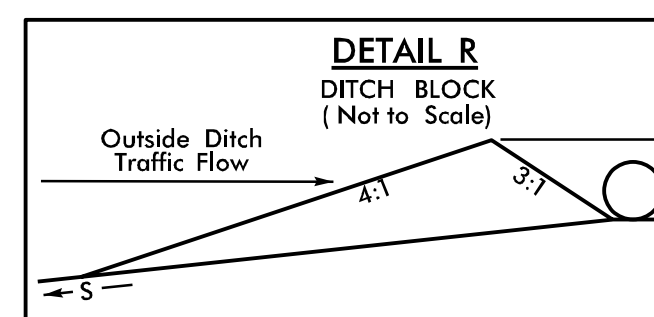
FROM STA. 520+00 TO STA. 522+00 -L- RT, D=1.0', B=4'  
 FROM STA. 10+58.30 TO STA. 15+21 -DR1- LT, D=1.5', B=3'  
 FROM STA. 12+50 TO STA. 14+00 -Y4- RT, D=2.0', B=4'



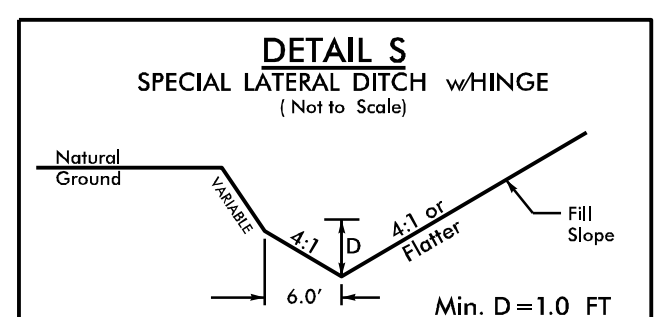
FROM STA. 11+30 TO STA. 12+41.11 -DR1- RT



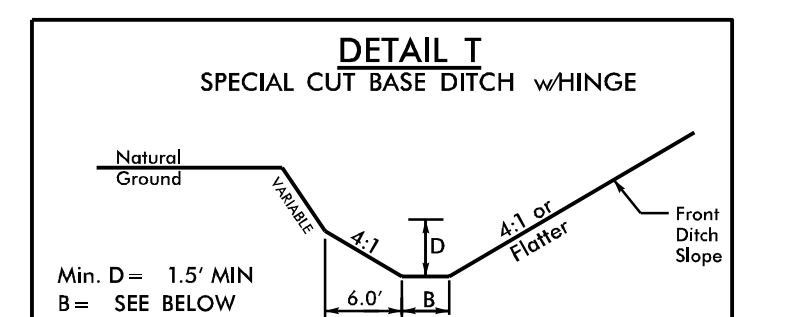
Type of Liner = Class I Rip-Rap  
 STA. 30+55 TO STA. 31+06 -Y7- LT, L=51', S=5.39%  
 BEG. EL.=1565.10', END EL.=162.35'



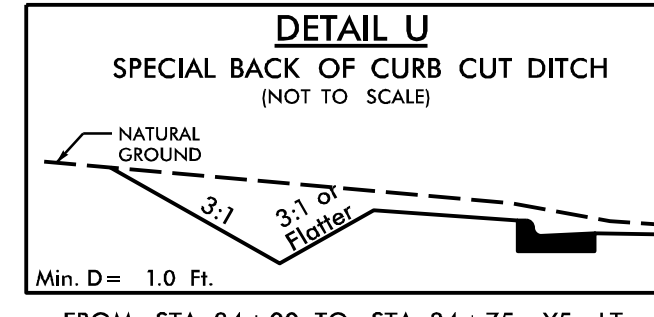
FROM STA. 914+99 -L- RT



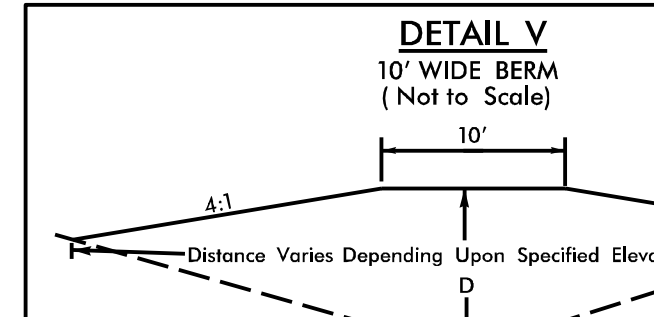
FROM STA. 565+00 TO STA. 566+00 -L- LT  
 FROM STA. 727+00 TO STA. 730+00 -L- LT  
 FROM STA. 726+50 TO STA. 730+00 -L- RT



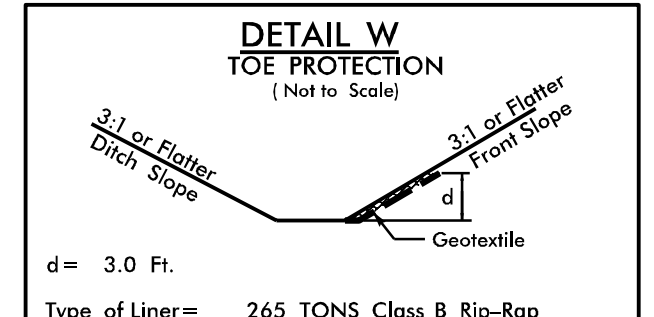
FROM STA. 595+00 TO STA. 597+00 -L- RT, B=6'



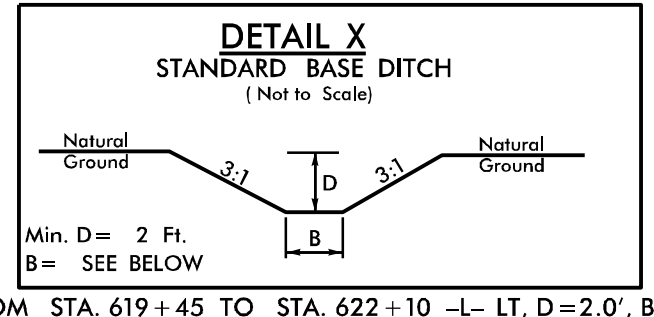
FROM STA. 34+00 TO STA. 34+75 -Y5- LT  
 FROM STA. 24+27.14 TO STA. 25+27.35 -YSRPC- RT



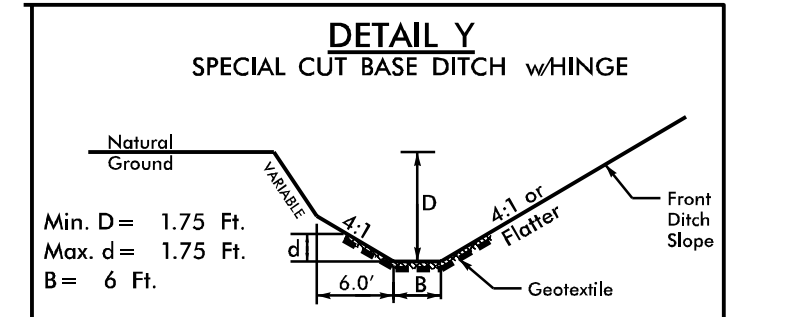
D=Varies; Distance from Top of Berm to lowest control point  
 -L- STA. 639+50 RT, TOP ELEV.=164.0'  
 -L- STA. 667+00 LT, TOP ELEV.=168.0'  
 -Y4- STA. 21+71 RT, TOP ELEV.=158.0'



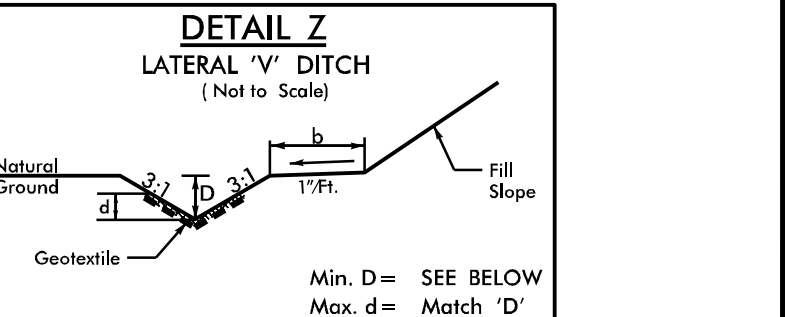
Type of Liner = 265 TONS Class B Rip-Rap  
 560 SY Geotextile  
 FROM STA. 577+00 TO STA. 585+00 -L- LT



FROM STA. 619+45 TO STA. 622+10 -L- LT, D=2.0', B=8'  
 FROM STA. 622+10 TO STA. 626+00 -L- LT, D=2.25', B=8'



Type of Liner = Class B Rip-Rap  
 FROM STA. 597+00 TO STA. 601+50 -L- RT



Type of Liner = VARIES  
 FROM STA. 22+14 -Y1B- LT TO STA. 23+14 -Y1BRPB- RT, D=1.0 FT., Class B Rip-Rap

4/28/2022 R:\Drawings\15987B-hyd\_psh-20-Ditch-Detail.dwg