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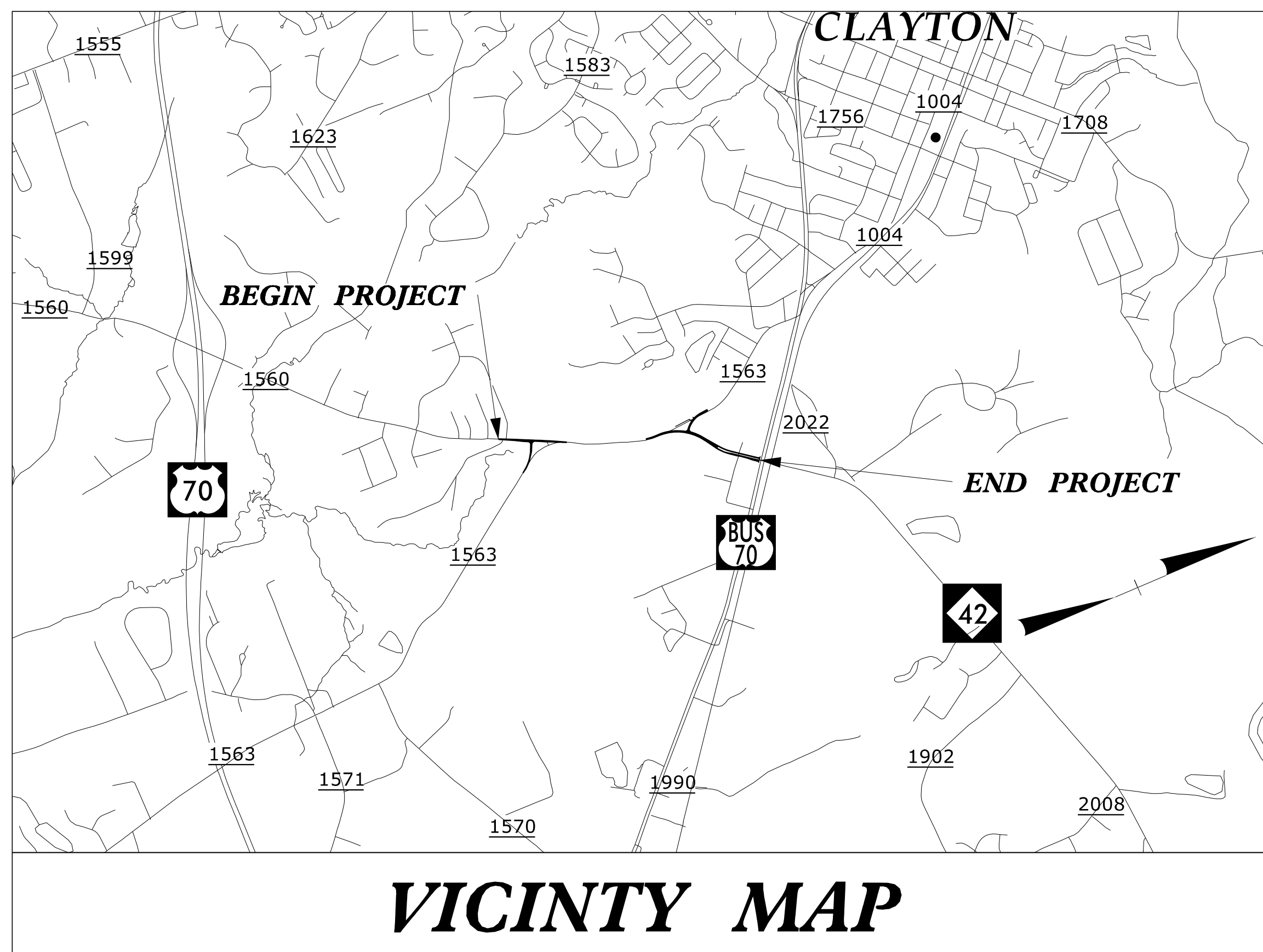
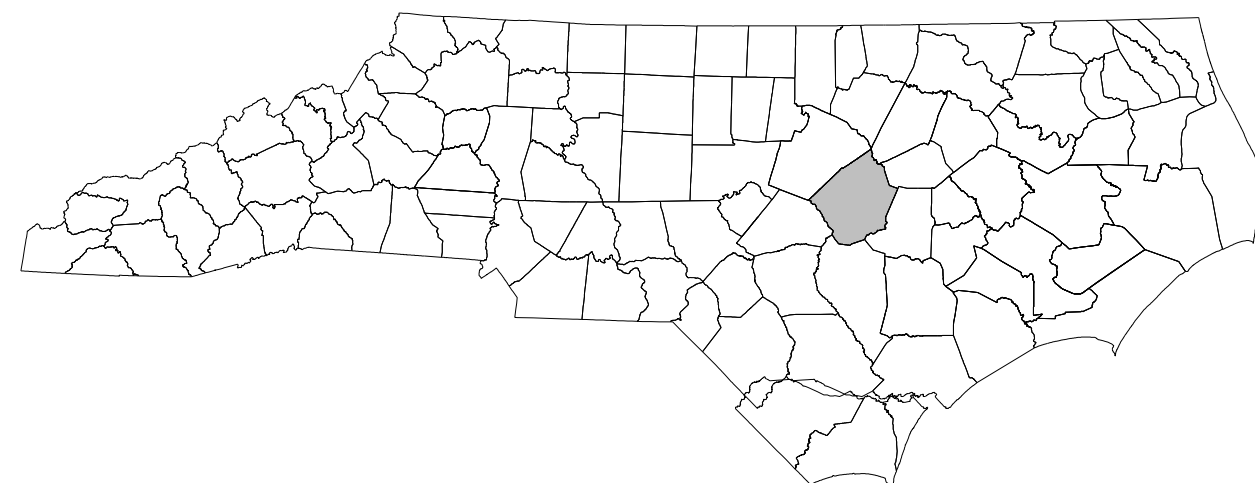
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STATE OF NORTH CAROLINA  
DIVISION OF HIGHWAYS

**TRANSPORTATION MANAGEMENT PLAN**

**JOHNSTON COUNTY**



**VICINTY MAP**  
LOCATION: NEW LOCATION FROM SR 1560 (RANCH ROAD)  
TO THE US 70 BUS & NC 42 INTERCHANGE  
TYPE OF WORK: GRADING, DRAINAGE, PAVING, SIGNALS, AND WALLS

**INDEX OF SHEETS**

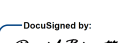
SHEET NO.	TITLE
TMP-1	TITLE SHEET, VICINITY MAP, AND INDEX OF SHEETS
TMP-1A	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS AND LEGEND
TMP-1B	TRANSPORTATION OPERATIONS PLAN: (MANAGEMENT STRATEGIES, GENERAL NOTES, AND LOCAL NOTES)
TMP-2	DETOUR
TMP-3 THRU 3A	TEMPORARY TRAFFIC CONTROL PHASING
TMP-4	TEMPORARY TRAFFIC CONTROL AREA 1 PHASE I
TMP-5 THRU 6A	TEMPORARY TRAFFIC CONTROL AREA 2 PHASE I
TMP-7	TEMPORARY TRAFFIC CONTROL AREA 1 PHASE II
TMP-8 AND 9	TEMPORARY TRAFFIC CONTROL AREA 2 PHASE II

SHEET NO.  
TMP-1

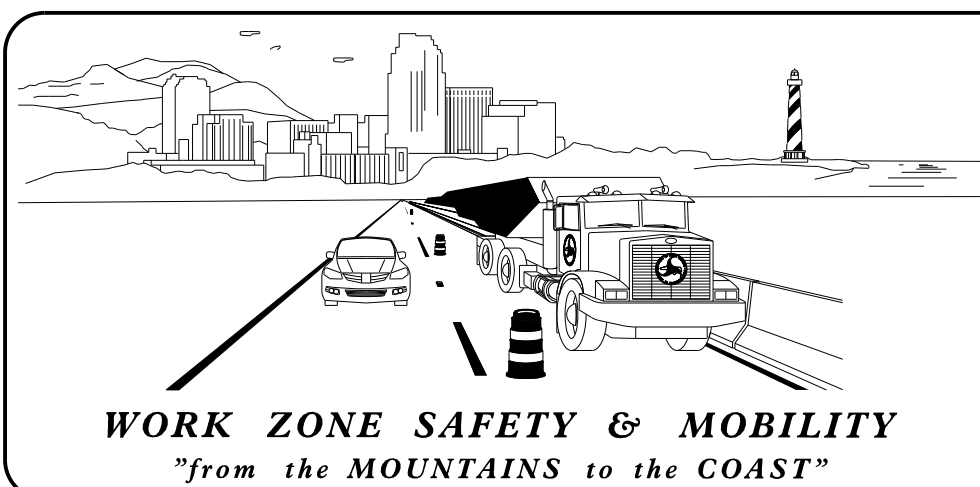
**U-6223**

**TIP PROJECT:**

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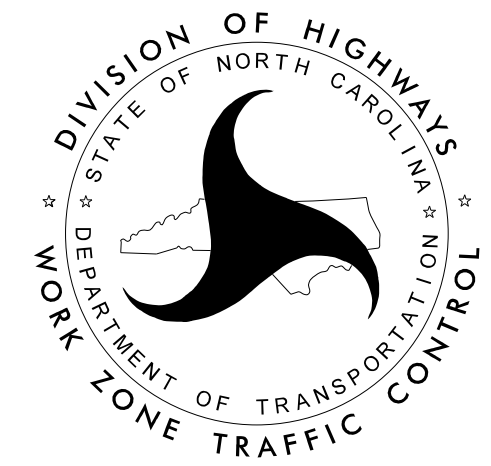
APPROVED:   
DATE: 5/20/2022

SEAL



PLANS PREPARED FOR THE NCDOT BY:  
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NCDOT CONTACTS:  
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PROJECT ENGINEER  
D.R. ETHRIDGE  
PROJECT DESIGN ENGINEER



5/18/2022  
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User: BLS82185

# ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS SHOWN IN "ROADWAY STANDARD DRAWINGS" - N.C. DEPARTMENT OF TRANSPORTATION - RALEIGH, N.C., DATED JANUARY 2018 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.01	WORK ZONE WARNING SIGNS
1101.02	TEMPORARY LANE CLOSURES
1101.03	TEMPORARY ROAD CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.06	WARNING SIGNS FOR BLASTING ZONES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW BOARDS
1130.01	DRUMS
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGING DEVICES
1160.01	TEMPORARY CRASH CUSHION
1165.01	TRUCK MOUNTED ATTENUATOR
1170.01	PORTABLE CONCRETE BARRIER
1180.01	SKINNY - DRUMS
1205.01	PAVEMENT MARKINGS - LINE TYPES AND OFFSETS
1205.02	PAVEMENT MARKINGS - TWO LANE AND MULTILANE ROADWAYS
1205.03	PAVEMENT MARKINGS - EXITS AND ENTRANCE RAMP
1205.04	PAVEMENT MARKINGS - INTERSECTIONS
1205.05	PAVEMENT MARKINGS - TURN LANES
1205.06	PAVEMENT MARKINGS - LANE DROPS
1205.07	PAVEMENT MARKINGS - PEDESTRIAN CROSSWALKS
1205.08	PAVEMENT MARKINGS - SYMBOLS AND WORD MESSAGES
1205.09	PAVEMENT MARKINGS - PAINTED ISLANDS
1205.10	PAVEMENT MARKINGS - SCHOOL AREAS
1205.11	PAVEMENT MARKINGS - RAILROAD CROSSINGS
1205.12	PAVEMENT MARKINGS - BRIDGES
1205.13	PAVEMENT MARKINGS - LANE REDUCTIONS
1205.14	PAVEMENT MARKINGS - ROUNDABOUTS
1205.15	PAVEMENT MARKINGS - SUPERSTREETS
1250.01	RAISED PAVEMENT MARKERS - INSTALLATION SPACING
1251.01	RAISED PAVEMENT MARKERS - (PERMANENT AND TEMPORARY)
1261.01	GUARDRAIL AND BARRIER DELINEATORS - INSTALLATION SPACING
1261.02	GUARDRAIL AND BARRIER DELINEATORS - TYPES AND MOUNTING
1262.01	GUARDRAIL END DELINEATION
1264.01	OBJECT MARKERS - TYPES
1264.02	OBJECT MARKERS - INSTALLATION

# LEGEND

## GENERAL

- DIRECTION OF TRAFFIC FLOW
- DIRECTION OF PEDESTRIAN TRAFFIC FLOW
- EXIST. PVMT.
- NORTH ARROW
- PROPOSED PVMT.
- TEMP. SHORING (LOCATION PURPOSES ONLY)

- WORK AREA
- REMOVAL
- INCIDENTAL STONE BASE
- USER DEFINED (IF NEEDED)

## SIGNALS

- EXISTING
- PROPOSED
- TEMPORARY
- PORTABLE

## PAVEMENT MARKINGS

- EXISTING LINES
- TEMPORARY LINES

## TRAFFIC CONTROL DEVICES

- BARRICADE (TYPE III)
- CONE
- DRUM
- SKINNY DRUM
- TUBULAR MARKER
- TEMPORARY CRASH CUSHION
- FLASHING ARROW BOARD
- FLAGGER
- LAW ENFORCEMENT
- TRUCK MOUNTED ATTENUATOR (TMA)
- CHANGEABLE MESSAGE SIGN

## TEMPORARY SIGNING

- PORTABLE SIGN
- STATIONARY SIGN
- STATIONARY OR PORTABLE SIGN

## PAVEMENT MARKERS

- CRYSTAL/CRYSTAL
- CRYSTAL/RED
- YELLOW/YELLOW

## PAVEMENT MARKING SYMBOLS

- PAVEMENT MARKING SYMBOLS

## TEMPORARY PAVEMENT MARKING

- |  |  |
|--|--|
| PAINT (4")<br>P1 WHITE EDGELINE<br>P2 WHITE SOLID LANE LINE<br>P3 10 FT. WHITE SKIP<br>P4 3 FT. - 9 FT./SP WHITE MINISKIP<br>P5 2 FT. - 6 FT./SP WHITE MINISKIP<br>P10 YELLOW EDGELINE<br>P13 YELLOW DOUBLE CENTER<br>P14 2 FT. - 6 FT./SP YELLOW MINISKIP | PAINT (24")<br>P61 WHITE STOPBAR<br><br>PAINT MARKING CHARACTERS<br>P100 ALPHANUMERIC CHAR.<br><br>PAINT MARKING SYMBOLS<br>P70 LEFT TURN ARROW<br>P71 RIGHT TURN ARROW<br>P72 STRAIGHT ARROW<br>P73 COMBO LEFT/STRAIGHT ARROW<br>P77 U-TURN ARROW |
|--|--|

- PAINT (8")
- P40 WHITE GORELINE
- P41 WHITE DIAGONAL
- P42 YELLOW DIAGONAL
- P43 WHITE SOLID LANE LINE
- P44 3 FT. - 9 FT./SP WHITE MINISKIP

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APPROVED: DATE: 5/20/2022			<p style="text-align: center;"><b>ROADWAY STANDARD DRAWINGS &amp; LEGEND</b></p>
<p><b>DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED</b></p>			

PLANS PREPARED FOR THE NCDOT BY:

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RALEIGH, NC 27604

**M** MOTT MACDONALD  
NC LICENSE NO. F-0669

# MANAGEMENT STRATEGIES

THE FOLLOWING LISTED WORK ZONE STRATEGIES ARE RECOMMENDED FOR INCLUSION WITHIN THIS TRANSPORTATION MANAGEMENT PLAN (TMP).

## RECOMMENDED STRATEGIES:

### TRAFFIC MANAGEMENT STRATEGIES:

- LANE SHIFTS OR CLOSURES
- SHOULDER CLOSURES
- ONE-LANE, TWO WAY OPERATION (FLAGGING)
- NIGHT WORK
- WEEKEND WORK
- WORK HOUR RESTRICTIONS FOR PEAK TRAVEL
- BUSINESS ACCESS IMPROVEMENTS

### CONTRACTING & INNOVATIVE CONSTRUCTION STRATEGIES: INTERMEDIATE CONTRACT TIMES / LIQUIDATED DAMAGES

# GENERAL NOTES

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS, AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS OR RESULT IN DUPLICATE OR UNDESIRABLE OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING, OR REMOVAL OF DEVICES AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT EXCEPT WHEN OTHERWISE NOTED IN THE PLAN OR DIRECTED BY THE ENGINEER.

## TIME RESTRICTIONS

### A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
SR 1560, SR 1563, & ROSE ST	MONDAY THROUGH FRIDAY 6:00 AM TO 9:00 AM AND 4:00 PM TO 7:00 PM
US 70	MONDAY THROUGH SUNDAY 6:00 AM TO 8:30 PM

### B) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS:

ROAD NAME
SR 1560, SR 1563, ROSE ST, & US 70

## HOLIDAY

- FOR ANY UNEXPECTED OCCURRENCE THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
- FOR NEW YEAR'S, BETWEEN THE HOURS OF 6:00 AM DECEMBER 31st AND 8:30 PM JANUARY 2ND. IF NEW YEAR'S DAY IS ON A FRIDAY, SATURDAY, SUNDAY, OR MONDAY THEN UNTIL 8:30 PM THE FOLLOWING TUESDAY.
- FOR EASTER, BETWEEN THE HOURS OF 6:00 AM THURSDAY AND 8:30 PM MONDAY.
- FOR MEMORIAL DAY, BETWEEN THE HOURS OF 6:00 AM FRIDAY TO 8:30 PM TUESDAY.
- FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 6:00 AM THE DAY BEFORE INDEPENDENCE DAY AND 8:30 PM THE DAY AFTER INDEPENDENCE DAY.

IF INDEPENDENCE DAY IS ON A FRIDAY, SATURDAY, SUNDAY OR MONDAY THEN BETWEEN THE HOURS OF 6:00 AM THE THURSDAY BEFORE INDEPENDENCE DAY AND 8:30 PM THE TUESDAY AFTER INDEPENDENCE DAY.

- FOR LABOR DAY, BETWEEN THE HOURS OF 6:00 AM FRIDAY AND 8:30 PM TUESDAY.
- FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 6:00 AM TUESDAY AND 8:30 PM MONDAY.
- FOR CHRISTMAS, BETWEEN THE HOURS OF 6:00 AM THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 8:30 PM THE FOLLOWING TUESDAY AFTER THE WEEK OF CHRISTMAS.

## LANE AND SHOULDER CLOSURE REQUIREMENTS

- REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED OR AS DIRECTED BY THE ENGINEER.
- WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 15 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR A LANE CLOSURE IS INSTALLED.
- WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS, OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.
- DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY, RAMP, OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.
- DO NOT INSTALL MORE THAN ONE LANE CLOSURE IN ANY ONE DIRECTION ON ANY ROAD.

## PAVEMENT EDGE DROP OFF REQUIREMENTS

- BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS AN EDGE OF PAVEMENT DROP-OFF AS FOLLOWS:
  - BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.
  - BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.
  - BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.
- DO NOT EXCEED A DIFFERENCE OF 2 INCHES IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC FOR NOMINAL LIFTS OF 1.5 INCHES. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) 350 IN ADVANCE AND A MINIMUM OF EVERY HALF MILE THROUGHOUT THE UNEVEN AREA.

## TRAFFIC PATTERN ALTERATIONS

- NOTIFY THE ENGINEER THIRTY (30) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

## SIGNING

- INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 40 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.
- ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.
- INSTALL BLACK ON ORANGE "DIP" SIGNS (W8-2) AND/OR "BUMP" SIGNS (W8-1) 350 FT IN ADVANCE OF THE UNEVEN AREA, OR AS DIRECTED BY THE ENGINEER.

## TRAFFIC CONTROL DEVICES

- WHEN LANE CLOSURES ARE NOT IN EFFECT SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER IN FEET THAN TWICE THE POSTED SPEED LIMIT (MPH) EXCEPT, 10 FT ON-CENTER IN RADII, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY. REFER TO STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES SECTIONS 1130 (DRUMS), 1135 (CONES) AND 1180 (SKINNY DRUMS) FOR ADDITIONAL REQUIREMENTS.
- PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY.
- PLACE ADDITIONAL SETS OF THREE CHANNELIZING DEVICES PERPENDICULAR TO THE EDGE OF TRAVELWAY ON 500 FT CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC.


## PAVEMENT MARKINGS AND MARKERS

- INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:
 

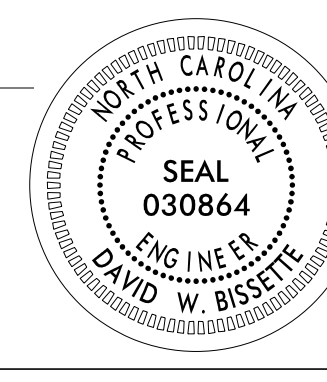
ROAD NAME	MARKING	MARKER
SR 1560, SR 1563, ROSE ST, & US 70	PAINT	RAISED
- PLACE ONE APPLICATION OF PAINT FOR TEMPORARY TRAFFIC PATTERNS. PLACE A SECOND APPLICATION OF PAINT SIX (6) MONTHS AFTER THE INITIAL APPLICATION AND EVERY SIX MONTHS AS DIRECTED BY THE ENGINEER.
- TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.
- REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS BY THE END OF EACH DAY'S OPERATION.

## MISCELLANEOUS

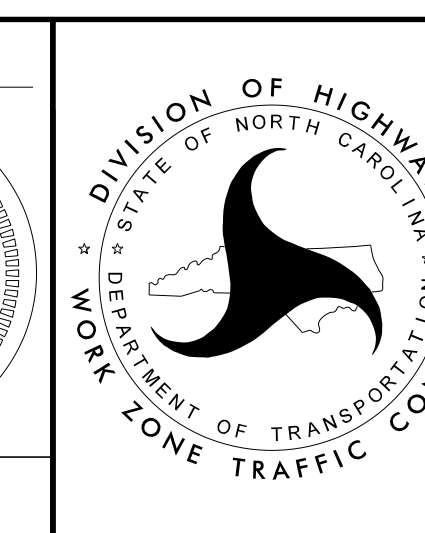
- IN THE EVENT A TIE-IN CANNOT BE MADE IN ONE DAY'S TIME, BRING THE TIE-IN AREA TO AN APPROPRIATE ROADWAY ELEVATION AS DETERMINED BY THE ENGINEER. PLACE BLACK ON ORANGE "LOOSE GRAVEL" SIGNS (W8-7) AND BLACK ON ORANGE "PAVEMENT ENDS" SIGNS (W8-3) 350 FT AND 750 FT RESPECTIVELY IN ADVANCE OF THE UNEVEN AREAS. USE DRUMS TO DELINEATE THE EDGE OF ROADWAY ALONG UNPAVED AREAS.
- ALL CURB RAMP LOCATIONS SHALL BE DERIVED FROM STATIONING SHOWN ON PAVEMENT MARKING PLANS OR AS DIRECTED BY THE ENGINEER IN COORDINATION WITH THE SIGNING AND DELINEATION UNIT.

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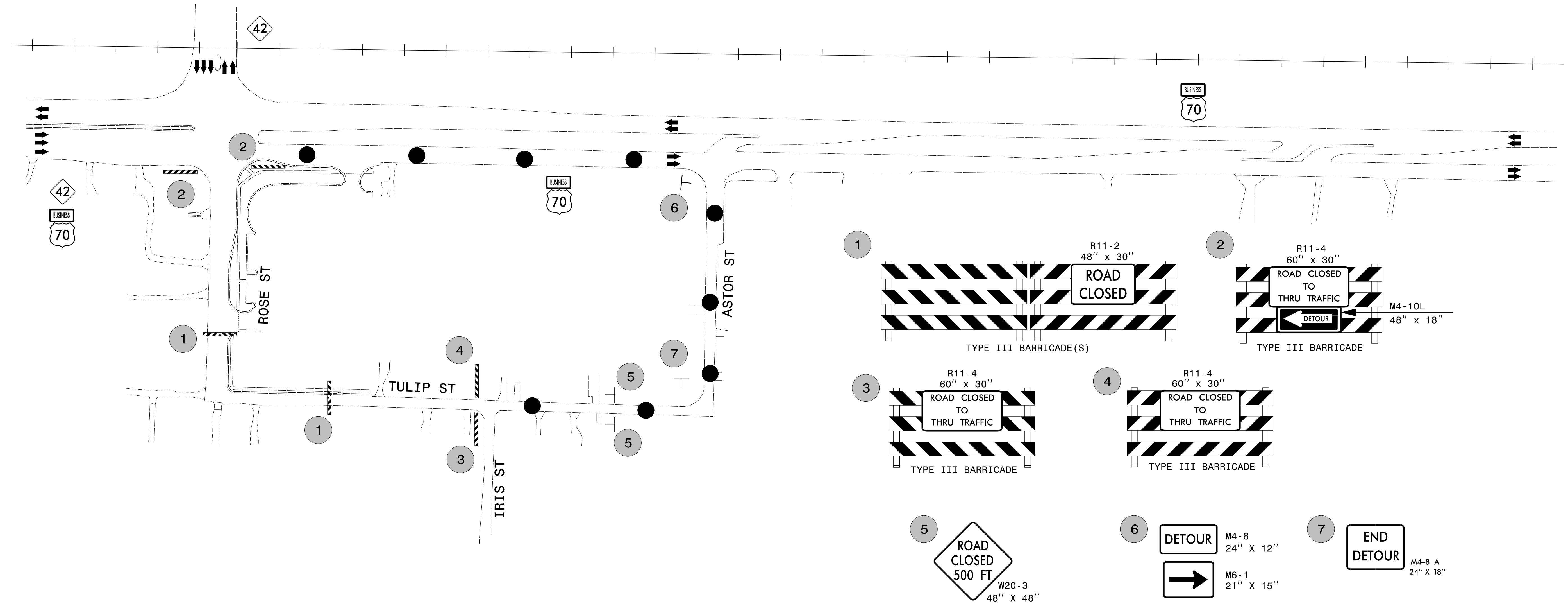
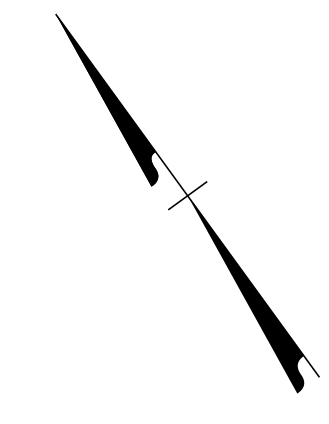
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**TRANSPORTATION  
OPERATIONS  
PLAN**



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PROJ. REFERENCE NO.	SHEET NO.
U-6223	TMP-3

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# TRAFFIC CONTROL PHASING

## AREA 1

### PHASE I

MAINTAIN TRAFFIC IN THE EXISTING PATTERN IN PHASE I UNLESS OTHERWISE STATED

STEP 1. USING ROADWAY STANDARD DRAWING 1101.01 SHEET 3 OF 3 AND ROADWAY STANDARD DRAWING 1101.02 SHEET 1 OF 14 AS NEEDED INSTALL WORK ZONE ADVANCE WARNING SIGNS

STEP 2. USING ROADWAY STANDARD DRAWING 1101.02 SHEET 1 OF 14 AS NEEDED BEGIN CONSTRUCTION OF THE FOLLOWING UP TO THE EXISTING EDGE AND ELEVATION, BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE:

- L- STATION 12+00+/- TO STATION 20+50+/-
- L- LEFT OF STATION 20+50+/- TO STATION 25+00+/- (INCLUDING WEDGING RIGHT OF -L- AS NEEDED TO MAINTAIN DRAINAGE)
- L- RIGHT OF STATION 21+66+/- TO STATION 22+72+/-
- Y1- STATION 13+50+/- TO STATION 16+50+/-

USING ROADWAY STANDARD DRAWING 1101.02 SHEET 1 OF 14 AS NEEDED CONSTRUCT -Y1- STATION 10+50+/- TO STATION 13+50+/- UP TO, BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE (INCLUDING PROPOSED UNDERCUT -Y1- STATION 10+50+/- TO STATION 13+50+/-)

USING ROADWAY STANDARD DRAWING 1101.02 SHEET 1 OF 14 AS NEEDED COMPLETE THE FOLLOWING -L- RIGHT OF STATION 20+85+/- TO STATION 21+46+/-:

- REMOVE ASPHALT ISLAND
- REPAIR ASPHALT PAVEMENT
- REPAIR PAVEMENT MARKINGS

USING ROADWAY STANDARD DRAWING 1101.02 SHEET 1 OF 14 AS NEEDED INSTALL THE PROPOSED TRAFFIC SIGNAL AT INTERSECTION OF -L- STATION 18+22+/- AND -Y1- STATION 10+00+/-

## AREA 2

### PHASE I

MAINTAIN TRAFFIC IN THE EXISTING PATTERN IN PHASE I UNLESS OTHERWISE STATED

STEP 1. USING ROADWAY STANDARD DRAWING 1101.01 SHEETS 2 AND 3 OF 3 AND ROADWAY STANDARD DRAWING 1101.02 SHEETS 1 AND 3 OF 14 AS NEEDED INSTALL WORK ZONE ADVANCE WARNING SIGNS

STEP 2. USING ROADWAY STANDARD DRAWING 1101.02 SHEETS 1 AND 3 OF 14 AS NEEDED BEGIN CONSTRUCTION OF THE FOLLOWING UP TO THE EXISTING EDGE AND ELEVATION, BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE:

- L- STATION 40+10+/- TO STATION 45+50+/-
- L- LEFT OF STATION 62+50+/- TO STATION 63+06+/-
- L- LEFT OF STATION 63+66+/- TO STATION 63+76+/- (CURB AND GUTTER IN MEDIAN OF US 70 BUS)
- Y2- STATION 10+00+/- TO STATION 13+50+/-

USING ROADWAY STANDARD DRAWING 1101.02 SHEETS 1 AND 3 OF 14 AS NEEDED BEGIN CONSTRUCTION OF THE FOLLOWING UP TO, BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE:

- L- STATION 45+50+/- TO STATION 58+50+/- (INCLUDING PROPOSED UNDERCUT -L- STATION 50+00+/- TO STATION 55+00+/-)
- L- LEFT STATION 58+50+/- TO STATION 62+50+/- (INCLUDING PROPOSED SHALLOW UNDERCUT -L- STATION 59+00+/- TO STATION 62+50+/-)
- Y2- STATION 13+50+/- TO STATION 16+00+/- (INCLUDING PROPOSED UNDERCUT -Y2- STATION 13+50+/- TO STATION 14+50+/-)

USING ROADWAY STANDARD DRAWING 1101.02 SHEETS 1 AND 3 OF 14 AS NEEDED BEGIN INSTALLATION OF THE FOLLOWING:

- TRAFFIC SIGNAL AT INTERSECTION OF -L- STATION 48+30+/- AND -Y2- STATION 16+00+/-
- TRAFFIC SIGNAL AT INTERSECTION OF -L- STATION 63+65+/- AND US 70 BUS

STEP 3. USING ROADWAY STANDARD DRAWING 1101.02 SHEETS 1 AND 3 OF 14 AS NEEDED COMPLETE THE FOLLOWING:

- INSTALL TEMPORARY PAVEMENT MARKINGS RIGHT OF -L- STATION 60+00+/- TO STATION 63+17+/- AND CLOSE THE RIGHT TURN LANE USING DRUMS WITH A MAXIMUM SPACING OF 10' (SEE SHEET TMP-6)
- CLOSE THE EXISTING SIDEWALK FROM -L- STATION 60+93+/- TO STATION 63+17+/-

USING ROADWAY STANDARD DRAWING 1101.02 SHEETS 1 AND 3 OF 14 AS NEEDED BEGIN CONSTRUCTION OF THE FOLLOWING:

- RETAINING WALL RIGHT OF -L- STATION 61+25+/- TO STATION 62+55+/-
- CURB AND GUTTER FROM -L- STATION 60+31+/- TO STATION 63+17+/-
- SIDEWALK FROM -L- STATION 60+93+/- TO STATION 62+72+/-

STEP 4. USING ROADWAY STANDARD DRAWING 1101.02 SHEET 1 OF 14 AS NEEDED CONSTRUCT A TEMPORARY MEDIAN CROSSING WITH INCIDENTAL STONE BETWEEN -L- STATION 60+00+/- AND STATION 60+20+/-

WORK IN A CONTINUOUS MANNER TO COMPLETE STEP 5

STEP 5. USING ROADWAY STANDARD DRAWING 1101.02 SHEET 1 OF 14, ROADWAY STANDARD DRAWING 1101.03 SHEET 1 OF 1, TMP-02 AND TMP-06 AS NEEDED INSTALL TEMPORARY ROAD CLOSURES ON -L- AT STATION 60+00+/- AND ON -Y4- AT STATION 12+30 (MAINTAIN ACCESS TO RESIDENCES)

CLOSE THE EXISTING SIDEWALK FROM -L- STATION 60+00+/- TO -Y4- STATION 12+20+/-

WORKING INSIDE THE ROAD CLOSURE COMPLETE THE FOLLOWING:

- CONSTRUCT THE FOLLOWING UP TO, BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE:
  - L- RIGHT OF STATION 58+50+/- TO STATION 60+00+/-
  - Y4- STATION 10+00+/- TO STATION 12+20+/-
- CONSTRUCT A TEMPORARY MEDIAN CROSSING WITH INCIDENTAL STONE BETWEEN -L- STATION 58+62+/- AND STATION 58+82+/-

OPEN -L- AND -Y4- IN THE EXISTING PATTERN

OPEN SIDEWALK FROM -L- STATION 60+00+/- TO -Y4- STATION 12+20+/-

STEP 6. USING ROADWAY STANDARD DRAWING 1101.02 SHEETS 1 AND 3 OF 14 AS NEEDED COMPLETE CONSTRUCTION OF -L- LEFT OF STATION 58+50+/- TO STATION 63+06+/- UP TO, BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE (INCLUDING PROPOSED SHALLOW UNDERCUT -L- STATION 59+00+/- TO STATION 62+50+/- AND EXCLUDING THE TEMPORARY CROSSING BETWEEN -L- STATION 60+00+/- AND STATION 60+20+/-)

USING ROADWAY STANDARD DRAWING 1101.02 SHEETS 1 AND 3 OF 14 AS NEEDED COMPLETE CONSTRUCTION OF THE FOLLOWING:

- RETAINING WALL RIGHT OF -L- STATION 61+25+/- TO STATION 62+55+/-
- CURB AND GUTTER RIGHT OF -L- STATION 60+31+/- TO STATION 63+17+/-
- SIDEWALK FROM -L- STATION 60+93+/- TO STATION 62+72+/-

COMPLETE STEP 7 IN ONE WORK PERIOD

STEP 7. USING ROADWAY STANDARD DRAWING 1101.02 SHEETS 1 AND 3 OF 14 AS NEEDED INSTALL TEMPORARY PAVEMENT MARKINGS FROM -L- STATION 58+82+/- TO STATION 62+97 AND SHIFT -L- TO THE PROPOSED TEMPORARY PATTERN (SEE SHEET TMP-6A)

STEP 8. USING ROADWAY STANDARD DRAWING 1101.02 SHEET 1 OF 14 AS NEEDED REMOVE THE TEMPORARY CROSSING BETWEEN -L- STATION 60+00+/- AND STATION 60+20+/- AND COMPLETE -L- MEDIAN CURB AND GUTTER -L- STATION 60+00+/- TO STATION 60+20+/-

USING ROADWAY STANDARD DRAWING 1101.02 SHEETS 1 AND 3 OF 14 AS NEEDED BEGIN CONSTRUCTION OF -L- RIGHT OF STATION 60+00+/- TO STATION 63+17+/- UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE

STEP 9. USING ROADWAY STANDARD DRAWING 1101.02 SHEET 1 OF 14 AS NEEDED COMPLETE CONSTRUCTION OF THE FOLLOWING UP TO THE EXISTING EDGE AND ELEVATION, BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE:

- L- STATION 40+10+/- TO STATION 45+50+/-
- Y2- STATION 10+00+/- TO STATION 13+50+/-


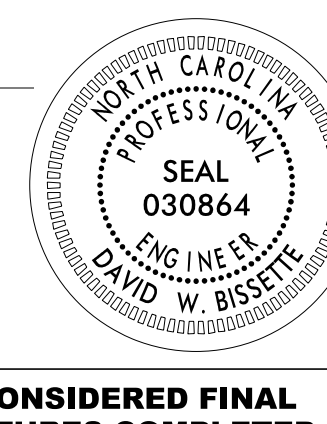

USING ROADWAY STANDARD DRAWING 1101.02 SHEETS 1 AND 3 OF 14 AS NEEDED COMPLETE CONSTRUCTION OF THE FOLLOWING UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE:

- L- STATION 45+50+/- TO STATION 58+50+/-
- L- RIGHT OF STATION 60+00+/- TO STATION 63+17+/-
- L- LEFT OF STATION 63+66+/- TO STATION 63+76+/- (CURB AND GUTTER IN THE MEDIAN OF US 70 BUS)
- Y2- STATION 13+50+/- TO STATION 16+00+/-

USING ROADWAY STANDARD DRAWING 1101.02 SHEETS 1 AND 3 OF 14 AS NEEDED COMPLETE INSTALLATION OF THE FOLLOWING:

- TRAFFIC SIGNAL AT INTERSECTION OF -L- STATION 48+30+/- AND -Y2- STATION 16+00+/-
- TRAFFIC SIGNAL AT INTERSECTION OF -L- STATION 63+65+/- AND US 70 BUS

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<p>APPROVED: </p> <p>DATE: 5/20/2022</p> 		<p><b>TEMPORARY TRAFFIC CONTROL PHASING</b></p>
<p><b>DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED</b></p>		

PROJ. REFERENCE NO.	SHEET NO.
U-6223	TMP-3A

PLANS PREPARED FOR THE NCDOT BY:

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**M** NC LICENSE NO. F-0669  
 MOTT MACDONALD

# TRAFFIC CONTROL PHASING

## AREA 1

### PHASE II

SHIFT TRAFFIC TO THE PROPOSED PATTERN AND MAINTAIN IT THERE IN PHASE II

COMPLETE STEP 1 IN ONE WORK PERIOD

STEP 1. USING ROADWAY STANDARD DRAWING 1101.02 SHEET 1 OF 14 AS NEEDED COMPLETE THE FOLLOWIING:

- COMPLETE CONSTRUCTION OF THE FOLLOWIING UP TO, BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE:
  - L- STATION 12+00+/- TO STATION 20+50+/-
  - L- LEFT OF STATION 20+50+/- TO STATION 25+00+/- (INCLUDING WEDGING RIGHT OF -L- AS NEEDED TO MAINTAIN DRAINAGE)
  - L- RIGHT OF STATION 21+66+/- TO STATION 22+72+/-
  - Y1- STATION 13+50+/- TO STATION 16+50+/-

- INSTALL TEMPORARY PAVEMENT MARKINGS IN THE FINAL PATTERN IN THE FOLLOWING LOCATIONS:
  - L- STATION 12+00+/- TO STATION 25+00+/-
  - Y1- STATION 10+00+/- TO STATION 16+50+/-

- ACTIVATE THE TRAFFIC SIGNAL AT THE INTERSECTION OF -L- STATION 18+22+/- AND -Y1- STATION 10+00+/-

- SHIFT -L- AND -Y1- TRAFFIC TO THE FINAL PATTERN

STEP 2. USING ROADWAY STANDARD DRAWING 1101.02 SHEET 1 OF 14 AS NEEDED COMPLETE THE FOLLOWIING:

- REMOVE THE EXISTING PAVEMENT ON LITTLE CREEK CHURCH ROAD AS SHOWN IN THE ROADWAY PLANS.

- COMPLETE CONSTRUCTION OF THE FOLLOWIING UP TO, BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE:
  - L- RIGHT OF STATION 20+50+/- TO STATION 21+66+/-
  - L- RIGHT OF STATION 22+72+/- TO STATION 25+00+/-

STEP 3. USING ROADWAY STANDARD DRAWING 1101.02 SHEET 1 OF 14 AS NEEDED PLACE THE FINAL LAYER OF SURFACE COURSE ON THE FOLLOWING:

- L- STATION 12+00+/- TO STATION 25+00+/-
- Y1- STATION 10+50+/- TO STATION 16+50+/-

STEP 4. USING ROADWAY STANDARD DRAWING 1101.02 SHEET 1 OF 14 AS NEEDED PLACE THE FINAL PAVEMENT MARKINGS AND PAVEMENT MARKERS ON THE FOLLOWING:

- L- STATION 12+00+/- TO STATION 25+00+/-
- Y1- STATION 10+50+/- TO STATION 16+50+/-

## AREA 2

### PHASE II

SHIFT TRAFFIC TO THE PROPOSED PATTERN AND MAINTAIN IT THERE IN PHASE II

COMPLETE STEP 1 IN ONE WORK PERIOD

STEP 1. USING ROADWAY STANDARD DRAWING 1101.02 SHEET 1 OF 14 AS NEEDED COMPLETE CONSTRUCTION OF THE FOLLOWING UP TO, BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE:

- L- STATION 40+10+/- TO STATION 45+50+/-
- Y2- STATION 10+00+/- TO STATION 13+50+/-

USING ROADWAY STANDARD DRAWING 1101.02 SHEETS 1 AND 3 OF 14 AS NEEDED INSTALL TEMPORARY PAVEMENT MARKINGS IN THE FINAL PATTERN IN THE FOLLOWING LOCATIONS:

- L- STATION 40+10+/- TO STATION 63+17+/-
- Y2- STATION 10+00+/- TO STATION 16+00+/-

ACTIVATE TRAFFIC SIGNAL AT INTERSECTION OF -L- STATION 48+30+/- AND -Y2- STATION 16+00+/-

ACTIVATE TRAFFIC SIGNAL AT INTERSECTION OF -L- STATION 63+65+/- AND BUS 70

SHIFT TRAFFIC TO THE FINAL PATTERN IN THE FOLLOWING LOCATIONS:

- L- STATION 40+10+/- TO STATION 63+17+/-
- Y2- STATION 10+00+/- TO STATION 16+00+/-

STEP 2. USING ROADWAY STANDARD DRAWING 1101.02 SHEET 1 OF 14 AS NEEDED REMOVE THE TEMPORARY MEDIAN CROSSING BETWEEN -L- STATION 58+62+/- AND STATION 58+82+/- AND COMPLETE -L- MEDIAN STATION 58+62+/- TO STATION 58+82+/-


STEP 3. USING ROADWAY STANDARD DRAWING 1101.02 SHEETS 1 AND 3 OF 14 AS NEEDED PLACE THE FINAL LAYER OF SURFACE COURSE ON THE FOLLOWING:


- L- STATION 40+10+/- TO STATION 63+00+/-
- Y2- STATION 10+00+/- TO STATION 16+00+/-
- Y4- STATION 10+00+/- TO STATION 12+50+/-

STEP 4. USING ROADWAY STANDARD DRAWING 1101.02 SHEETS 1 AND 3 OF 14 AS NEEDED PLACE THE FINAL PAVEMENT MARKINGS AND PAVEMENT MARKERS ON THE FOLLOWING:

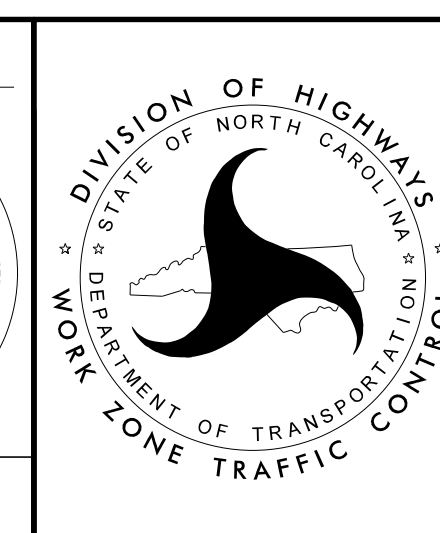
- L- STATION 40+10+/- TO STATION 63+00+/-
- Y2- STATION 10+00+/- TO STATION 16+00+/-

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## TEMPORARY TRAFFIC CONTROL PHASING

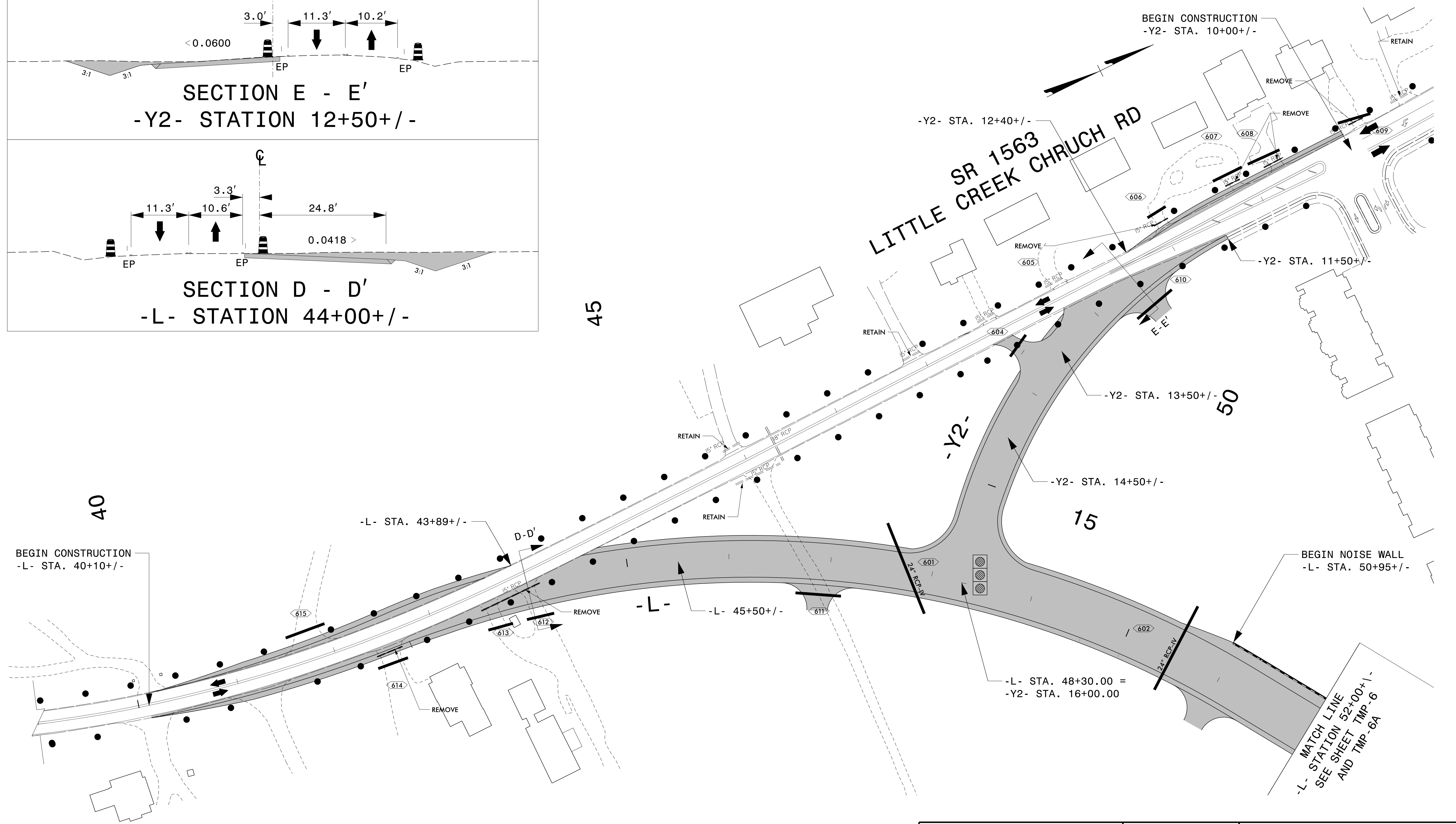
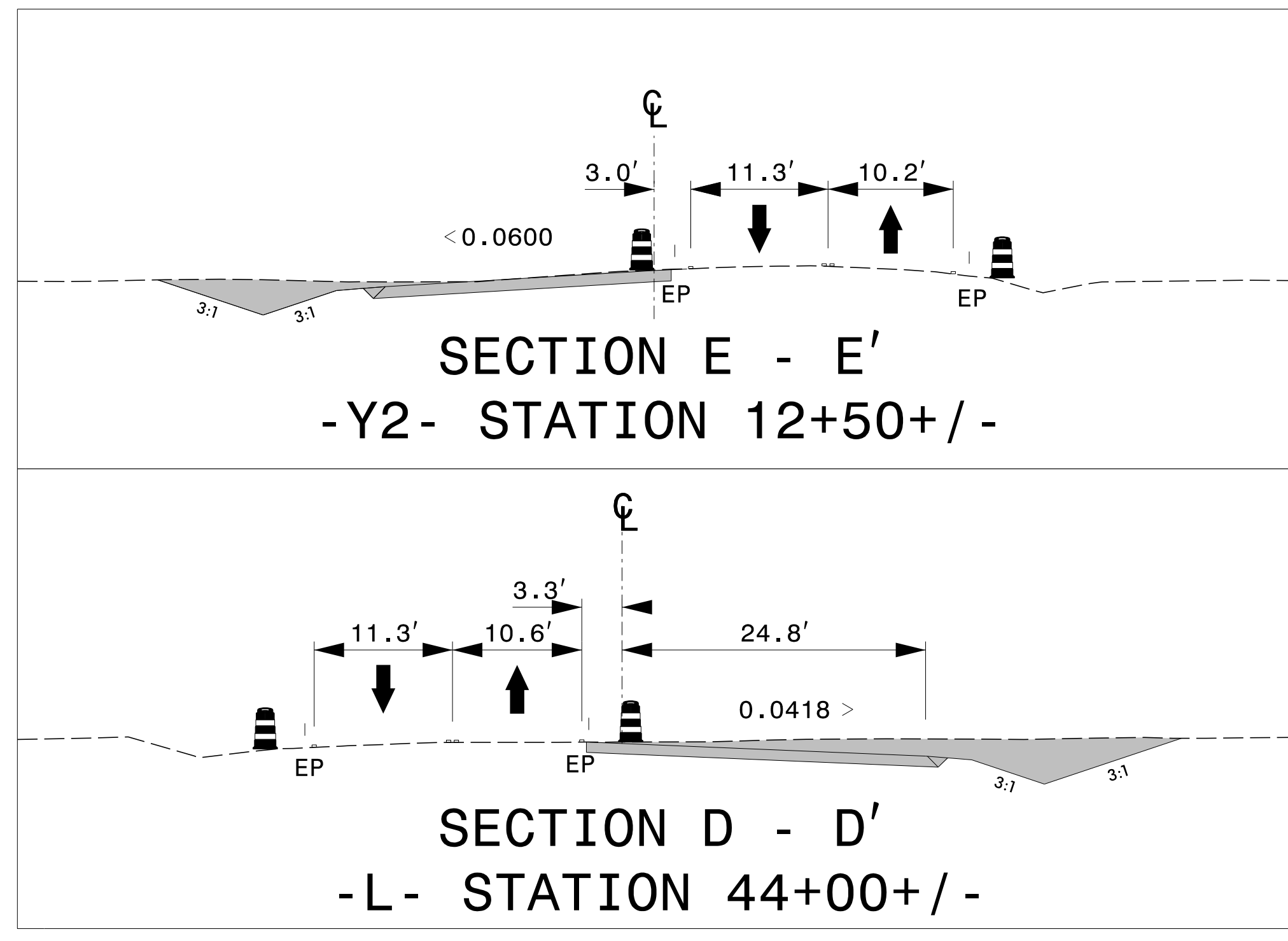





PROJ. REFERENCE NO.	SHEET NO.
U-6223	TMP-5

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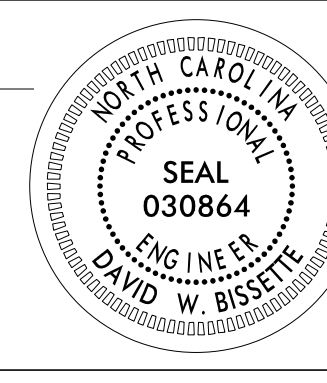
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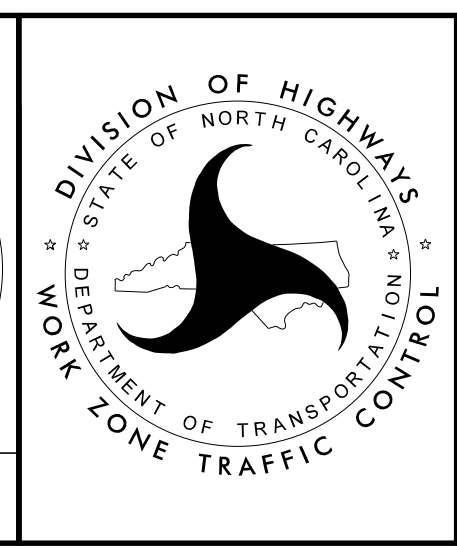
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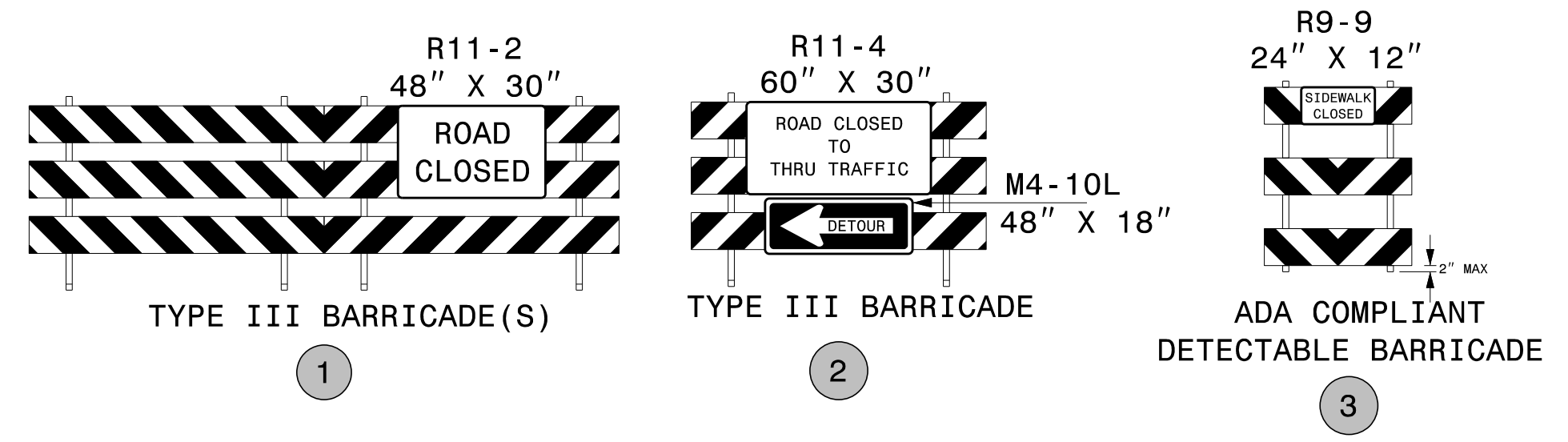
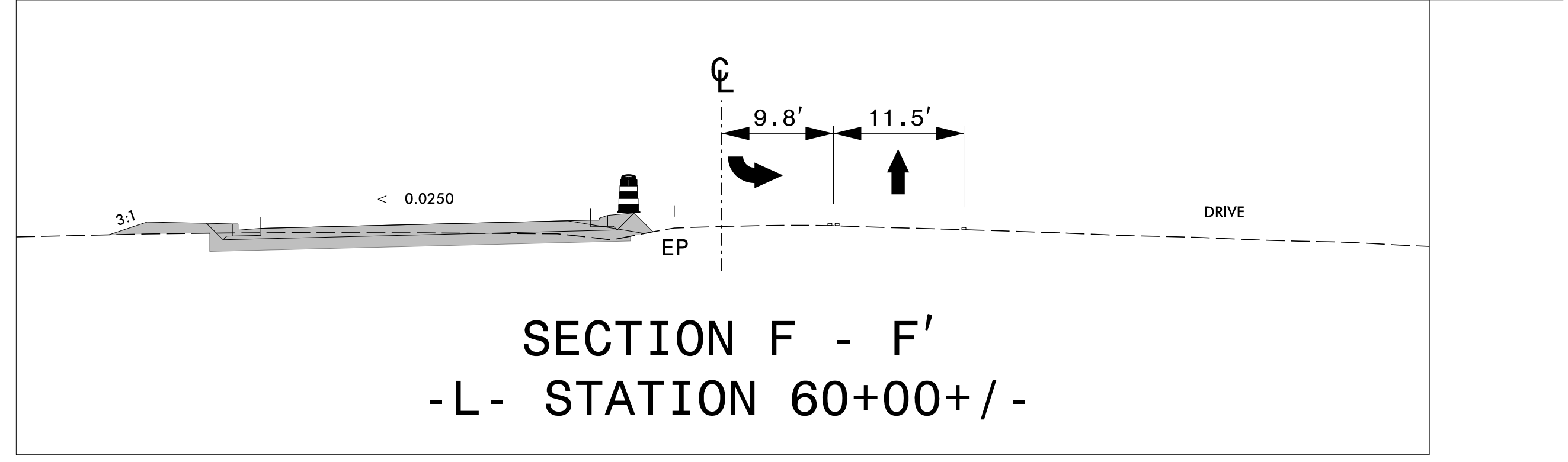
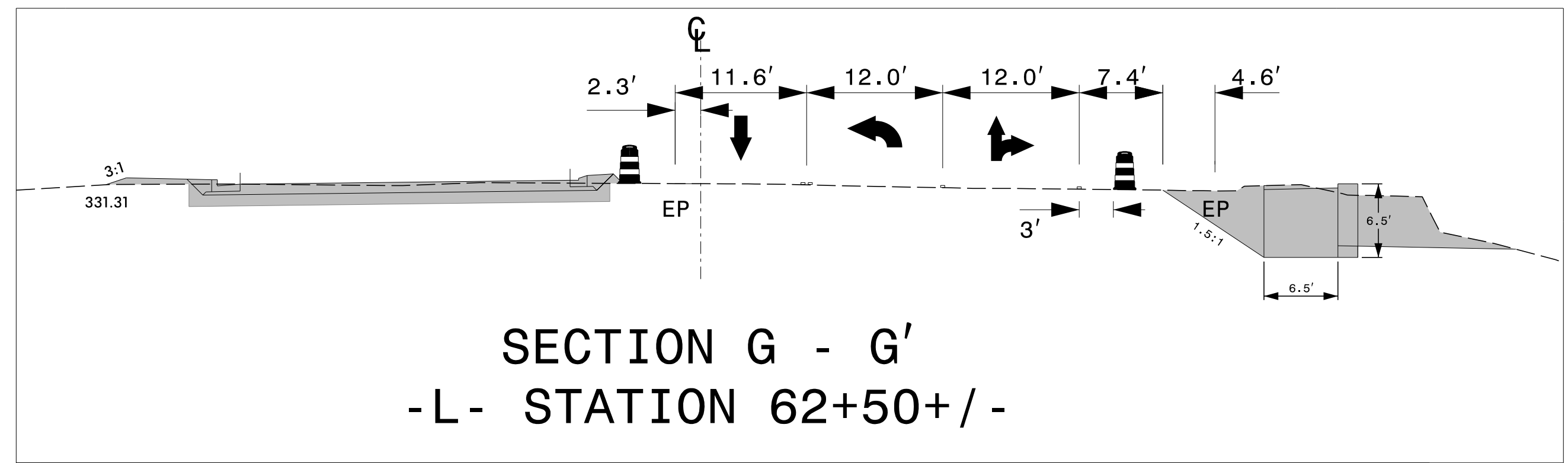
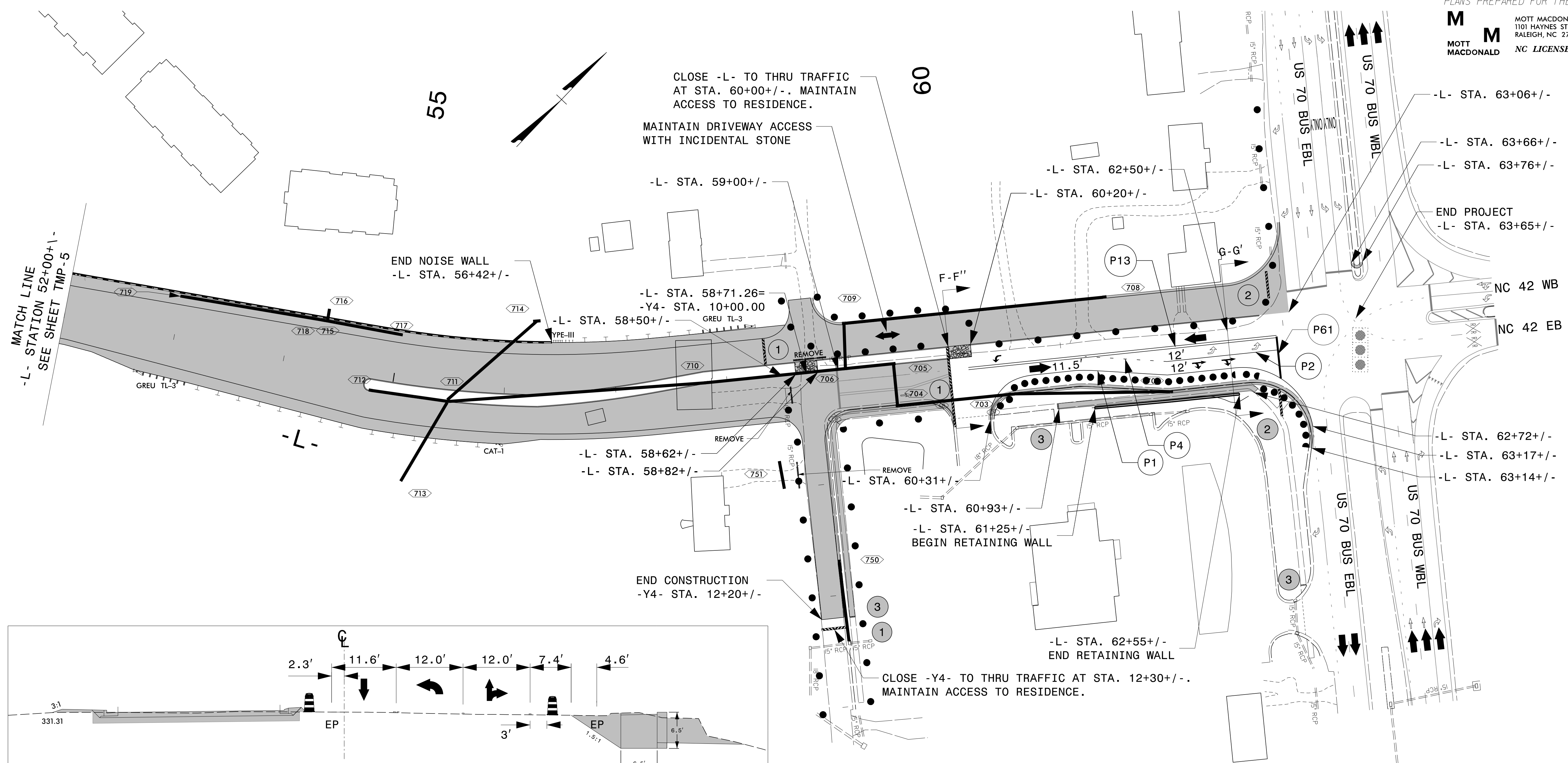
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**TEMPORARY TRAFFIC CONTROL  
 AREA 2  
 PHASE I**

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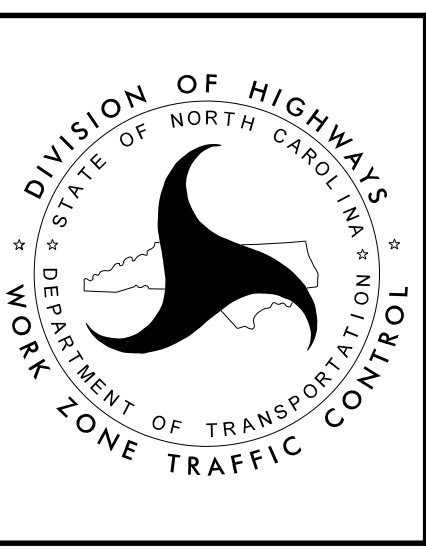
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DATE: 5/20/2022



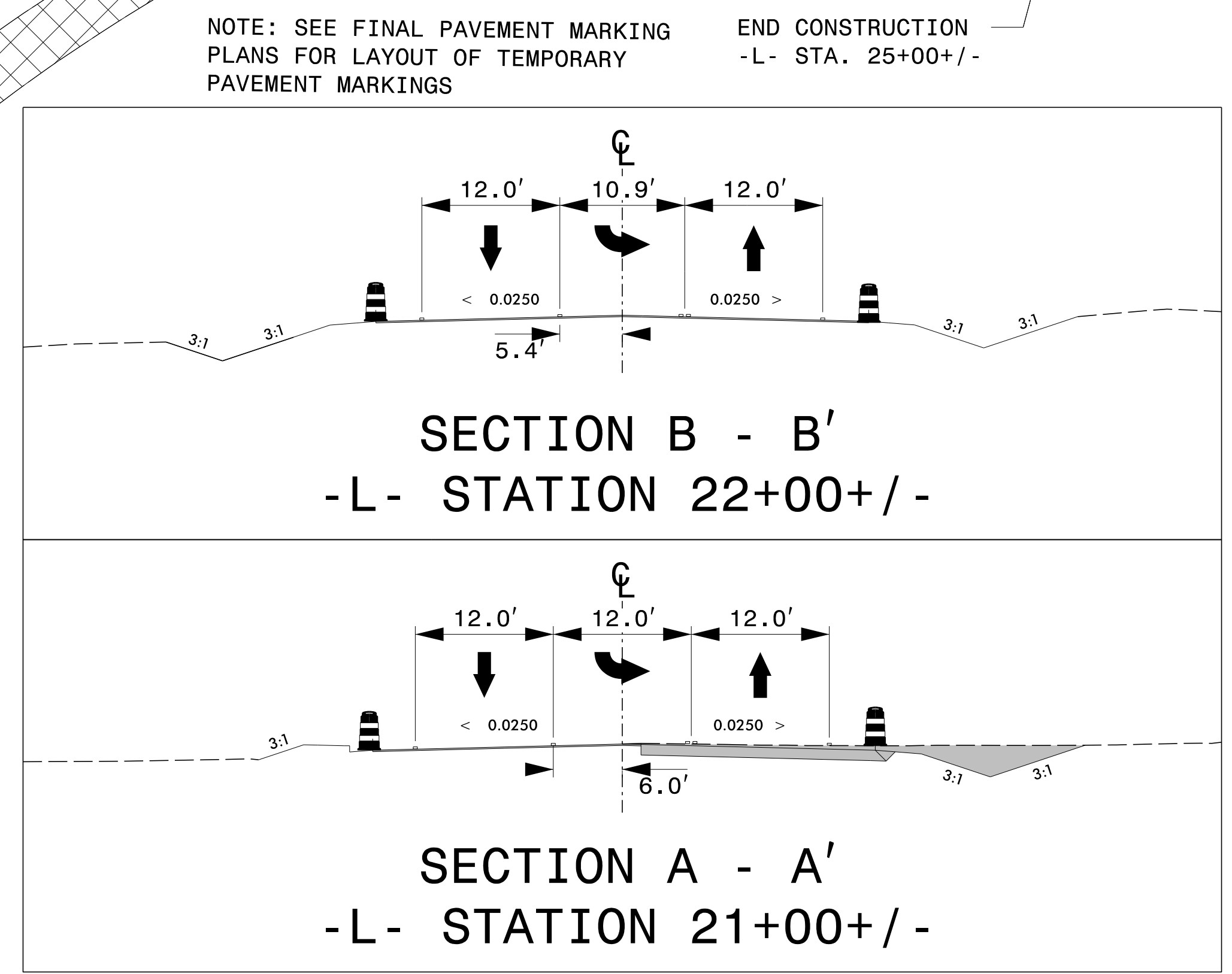
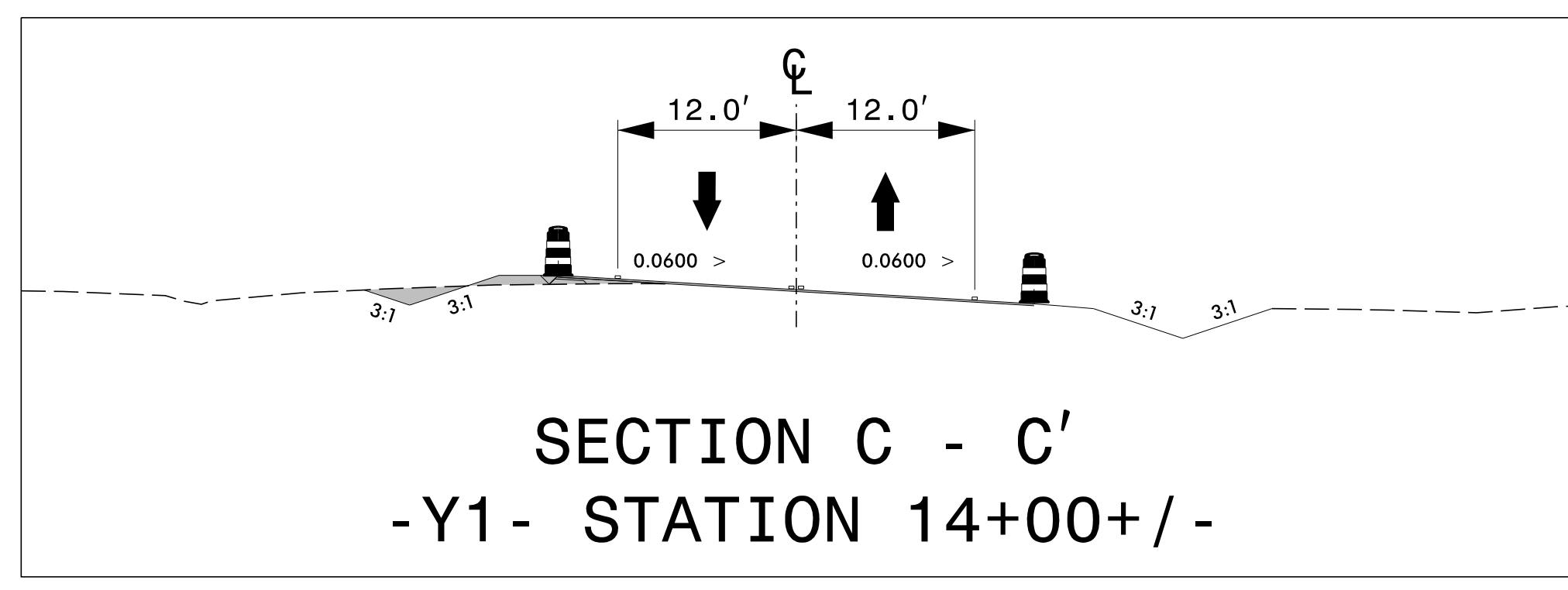
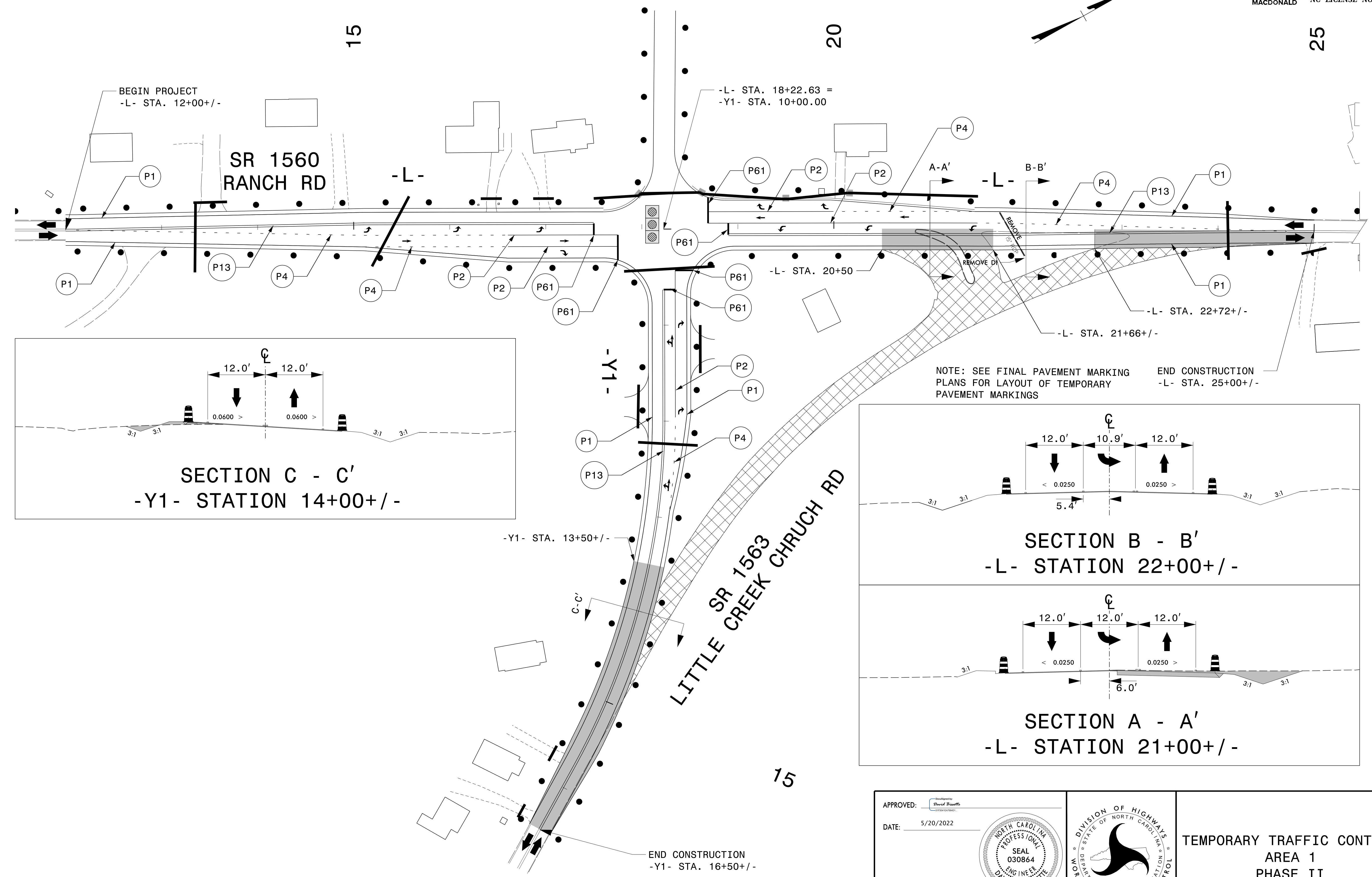
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**TEMPORARY TRAFFIC CONTROL  
AREA 2  
PHASE I**

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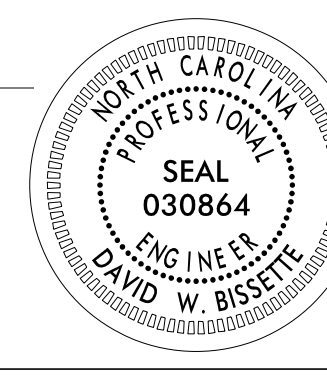




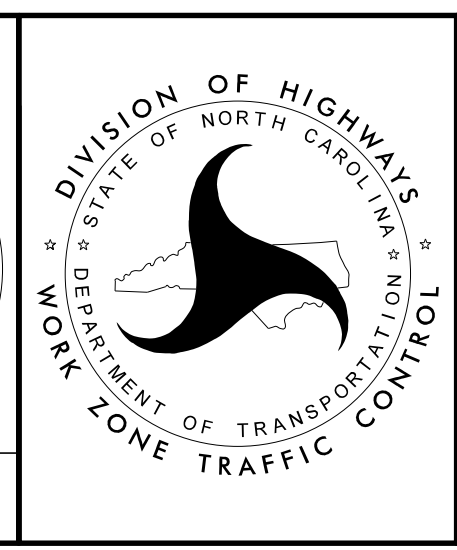
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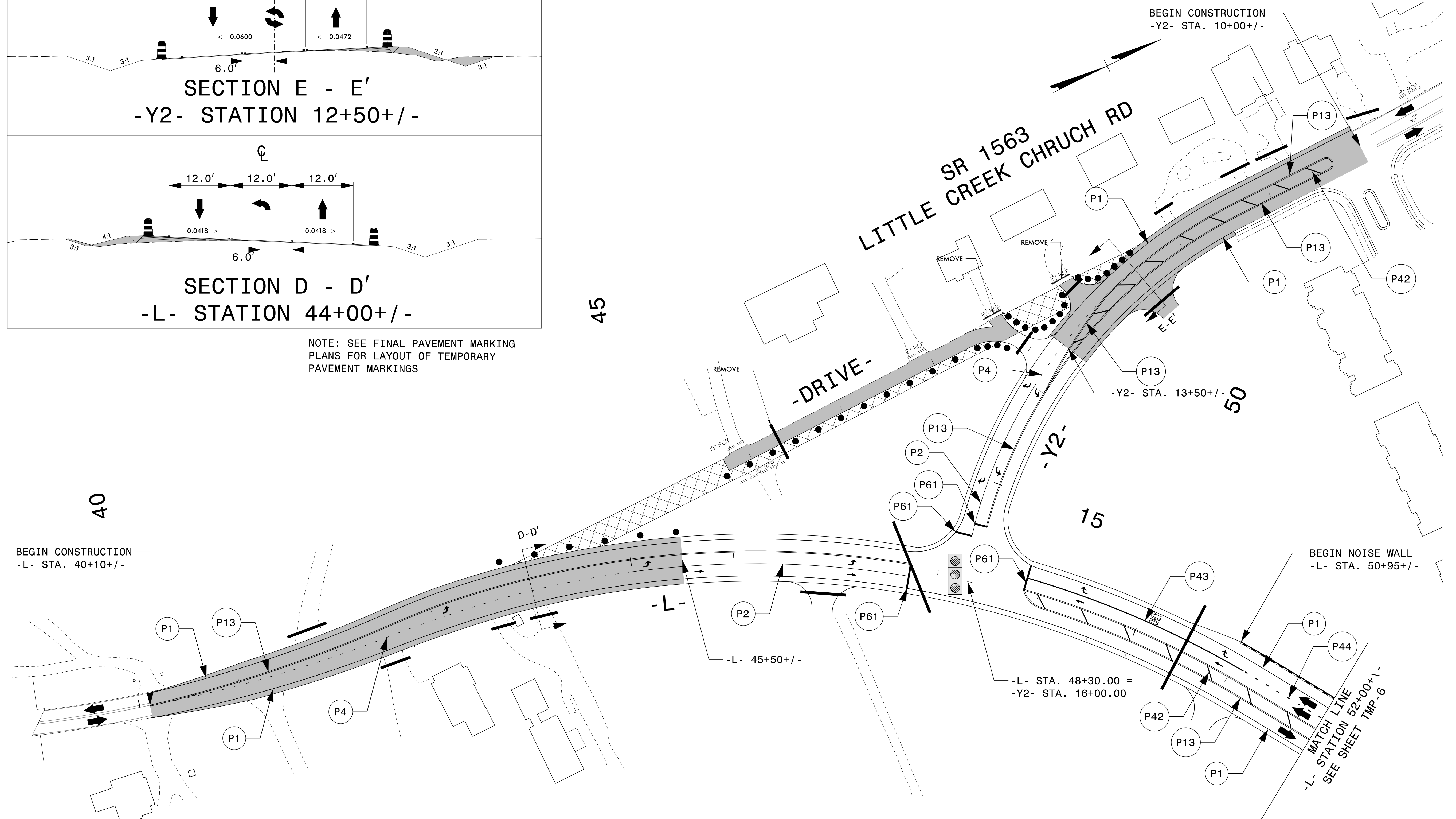
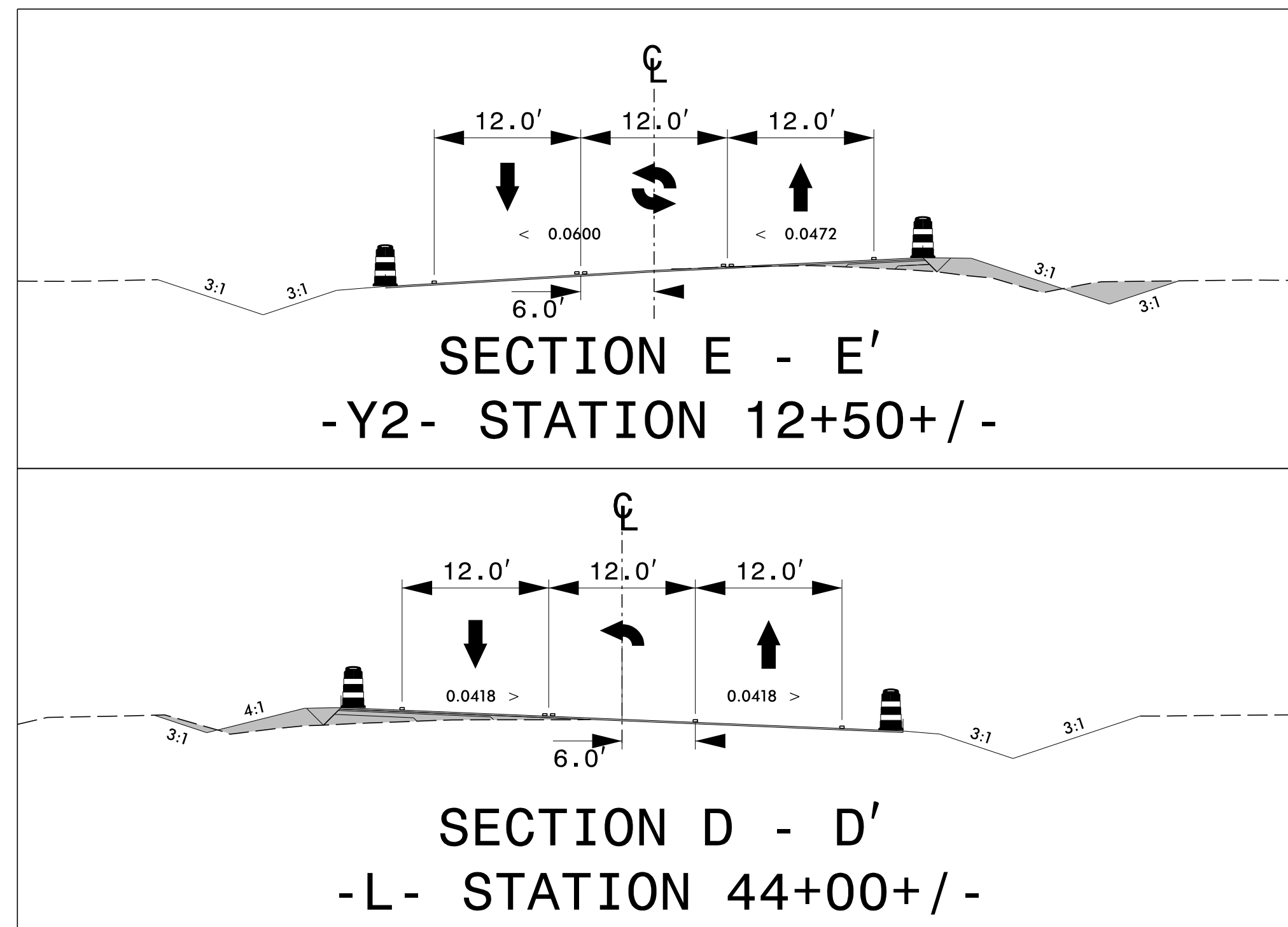


**TEMPORARY TRAFFIC CONTROL  
AREA 1  
PHASE II**

PROJ. REFERENCE NO.	SHEET NO.
U-6223	TMP-8

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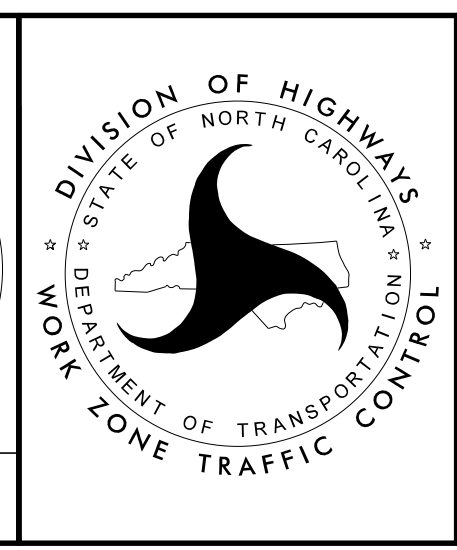


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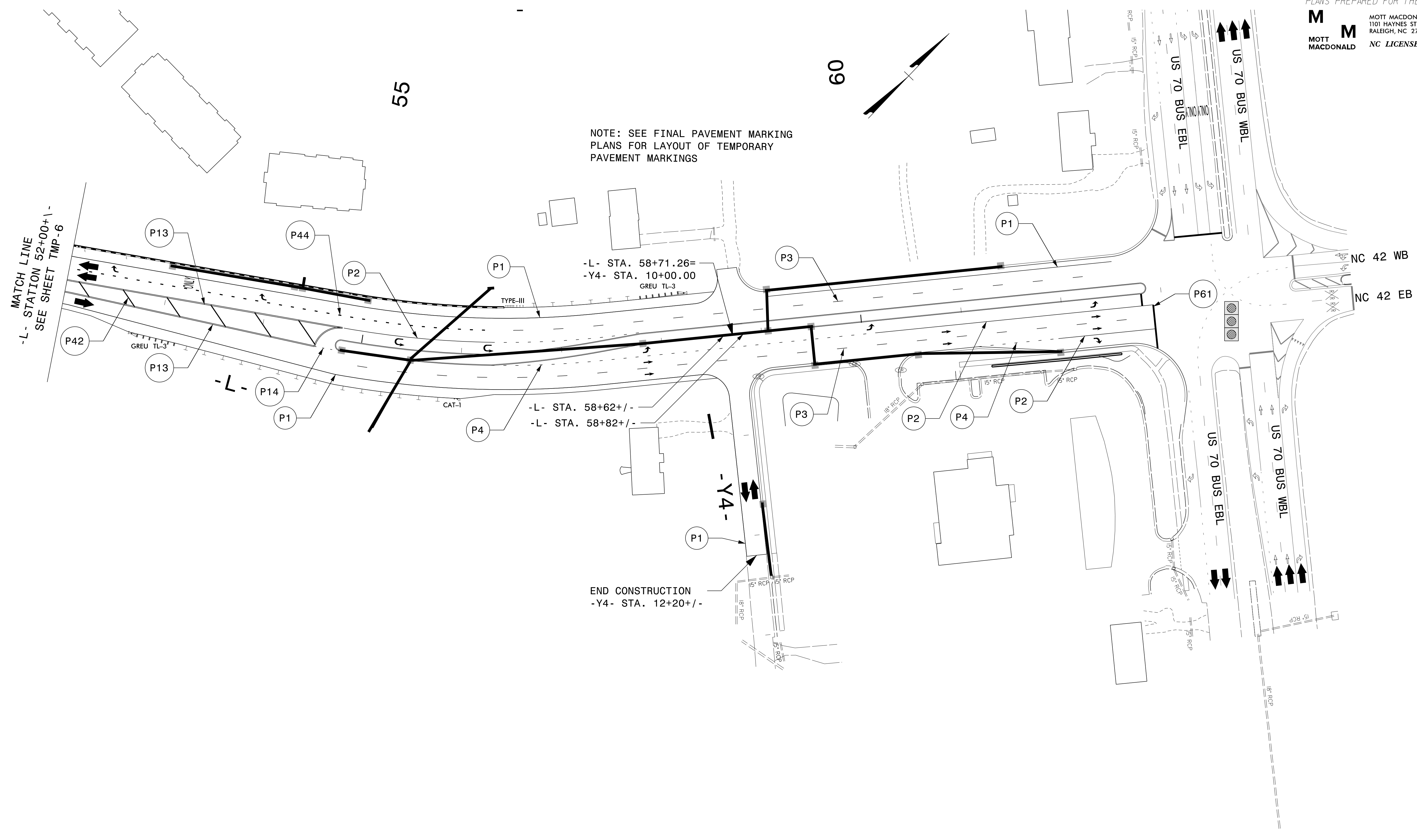


**TEMPORARY TRAFFIC CONTROL  
 AREA 2  
 PHASE II**


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U-6223	TMP-9

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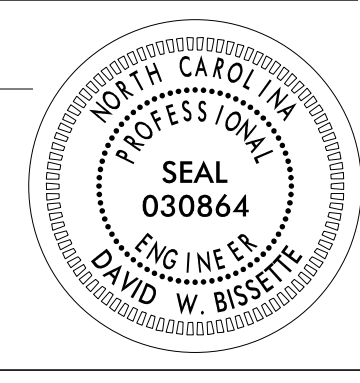
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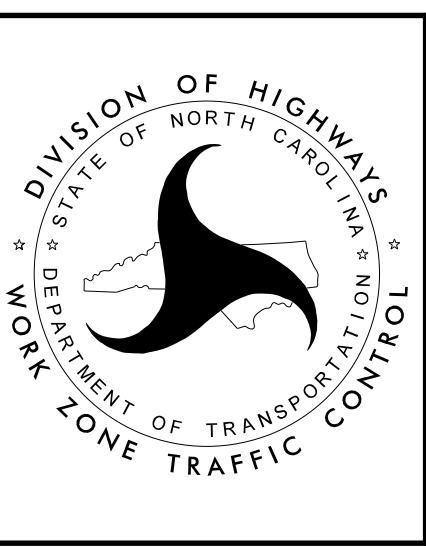
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**TEMPORARY TRAFFIC CONTROL  
 AREA 2  
 PHASE II**