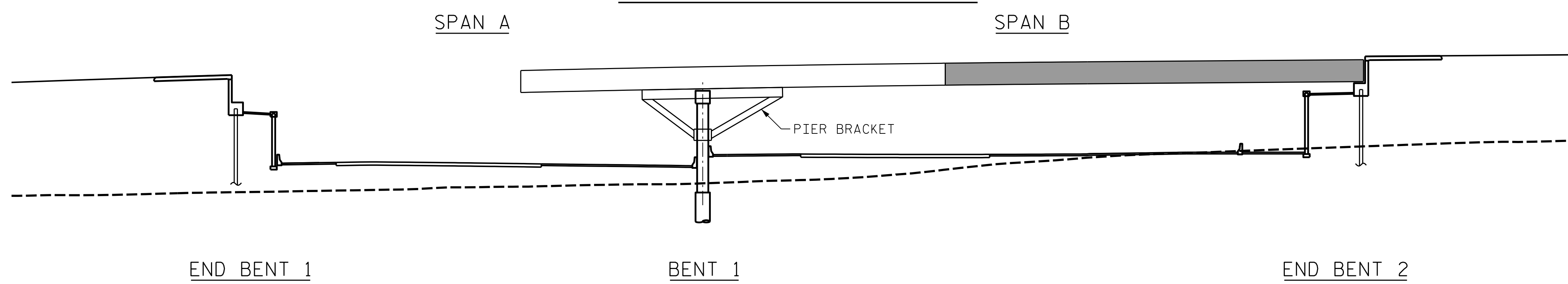
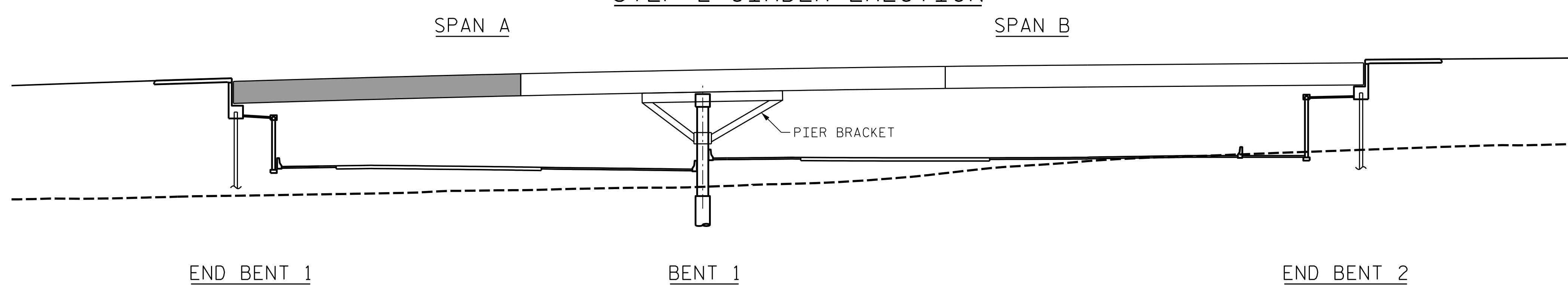


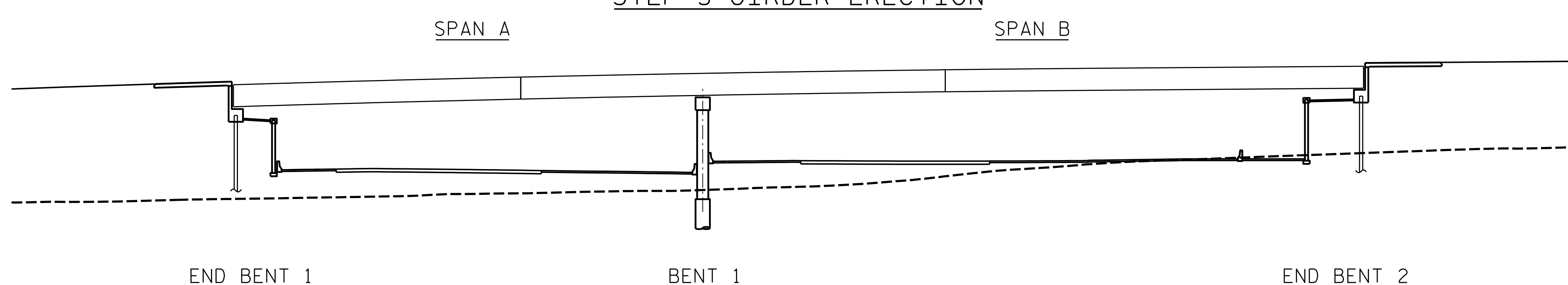
STEP 1 GIRDER ERECTION



STEP 2 GIRDER ERECTION



STEP 3 GIRDER ERECTION



COMPLETED STRUCTURE

NOTES

FOR TEMPORARY BENTS (AND PIER BRACKETS, WHICH ARE CONSIDERED A SUBSET OF TEMPORARY BENTS), SEE SPECIAL PROVISIONS.

STEPS 1 AND 2 SHALL BE ACCOMPLISHED PRIOR TO SHIFTING ANY WESTBOUND US 74 / US 23 TRAFFIC TO EASTBOUND US 74 / US 23 LANES. SEE TRANSPORTATION MANAGEMENT PLAN (TMP) FOR MORE DETAILS.

ERECT A MINIMUM OF TWO GIRDERS WITH ALL DIAPHRAGMS BETWEEN THE GIRDERS IN PLACE AND THE BOLTS TIGHTENED PRIOR TO RELEASING THE GIRDERS.

ERECT EACH SUBSEQUENT GIRDER WITH DIAPHRAGMS CONNECTING TO THE ADJACENT PREVIOUSLY ERECTED GIRDER AND TIGHTEN ALL BOLTS BEFORE RELEASING.

THE STRUCTURAL STEEL SHALL REMAIN SUPPORTED DURING ERECTION. TEMPORARY SUPPORTS (PIER BRACKETS) AS SHOWN SHALL BE USED.

PIER BRACKETS SHALL REMAIN IN PLACE UNTIL ALL DIAPHRAGMS ARE IN PLACE AND ALL HIGH STRENGTH BOLTS ARE TIGHTENED.

PIER BRACKETS SHALL PROVIDE BEARING AT CONNECTOR PLATE LOCATIONS. WHEN CONNECTOR PLATES ARE USED AS TEMPORARY BEARING STIFFENERS, DIAPHRAGMS MUST BE ATTACHED.

THE CONTRACTOR'S ERECTION PLANS SHALL INCLUDE A METHOD OF TEMPORARY BENT REMOVAL THAT WILL TRANSFER THE STRUCTURAL WEIGHT TO THE PERMANENT STRUCTURAL STEEL FRAMING SYSTEM SUCH THAT THE GIRDERS WILL DEFLECT GRADUALLY AND UNIFORMLY TO THEIR INTENDED STEEL DEAD LOAD POSITION, WITHOUT EXPERIENCING UPLIFT OR OTHER ADVERSE INTERIM CONDITIONS.

PLANS FOR TEMPORARY BENT AND PIER BRACKET ERECTION AND REMOVAL SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW.

THE CONTRACTOR IS RESPONSIBLE FOR DESIGNING THE TEMPORARY BENTS AND PIER BRACKETS. THE DESIGN SHALL BE COMPLETED BY A PROFESSIONAL ENGINEER REGISTERED IN THE STATE OF NORTH CAROLINA. THE CONTRACTOR SHALL SUBMIT SIGNED AND SEALED WORKING DRAWINGS AND CALCULATIONS FOR APPROVAL BY THE ENGINEER.

DURING THE GIRDER ERECTION PROCEDURE, THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING TEMPORARY SUPPORTS, BLOCKING, LATERAL BRACING, AND/OR OTHER MEANS OF SUPPORT, AS REQUIRED, TO MAINTAIN STABILITY, PREVENT UPLIFT OF THE GIRDERS AT TEMPORARY BENTS, PERMANENT BENTS, AND END BENTS, AND TO MAINTAIN GEOMETRY OF THE GIRDERS.

NO SEPARATE MEASUREMENT OR PAYMENT WILL BE MADE FOR PROVIDING THE TEMPORARY SUPPORTS, TEMPORARY LATERAL BRACING OR OTHER MEANS OF SUPPORT. THE COST FOR ALL MATERIALS, EQUIPMENT, TOOLS, DESIGN, LABOR AND ANY INCIDENTALS NECESSARY TO PROVIDE THE TEMPORARY SUPPORTS SHALL BE CONSIDERED INCIDENTAL TO THE LUMP SUM BID PRICE FOR STRUCTURAL STEEL.

THE CONTRACTOR IS ADVISED THAT THE EXISTING GROUND UNDER THE PROPOSED BRIDGE MAY HAVE STEEP SLOPES, STREAMS, AND/OR HEAVY VEGETATION.

THE CONTRACTOR MAY SUBMIT AN ALTERNATE ERECTION METHOD TO THE ENGINEER FOR REVIEW AND APPROVAL.

PROPOSED FILL MAY NOT BE IN PLACE AT TIME OF GIRDER ERECTION.

PROJECT NO. B-3186/B-5898
HAYWOOD COUNTY
 STATION: 27+54.43 -Y1RT-



W. Brian Watson 1/24/2022

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

GIRDER ERECTION DETAILS

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	502-06
1	--	--	3	--	--	TOTAL SHEETS
2	--	--	4	--	--	48

DES BY: <u>B. WATSON</u>	DATE: <u>07/21</u>	DWG BY: <u>B. PETERSON</u>	DATE: <u>07/21</u>
DES CHK: <u>S. NIFONG</u>	DATE: <u>07/21</u>	CHK BY: <u>S. NIFONG</u>	DATE: <u>07/21</u>

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

PLOT DRIVER: NCDOT_STRUCTURE_DEFAULT_PLOTTER.plt PENTABLE: NCDOT_STRUCTURE_DEFAULT_PEN.tbl
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