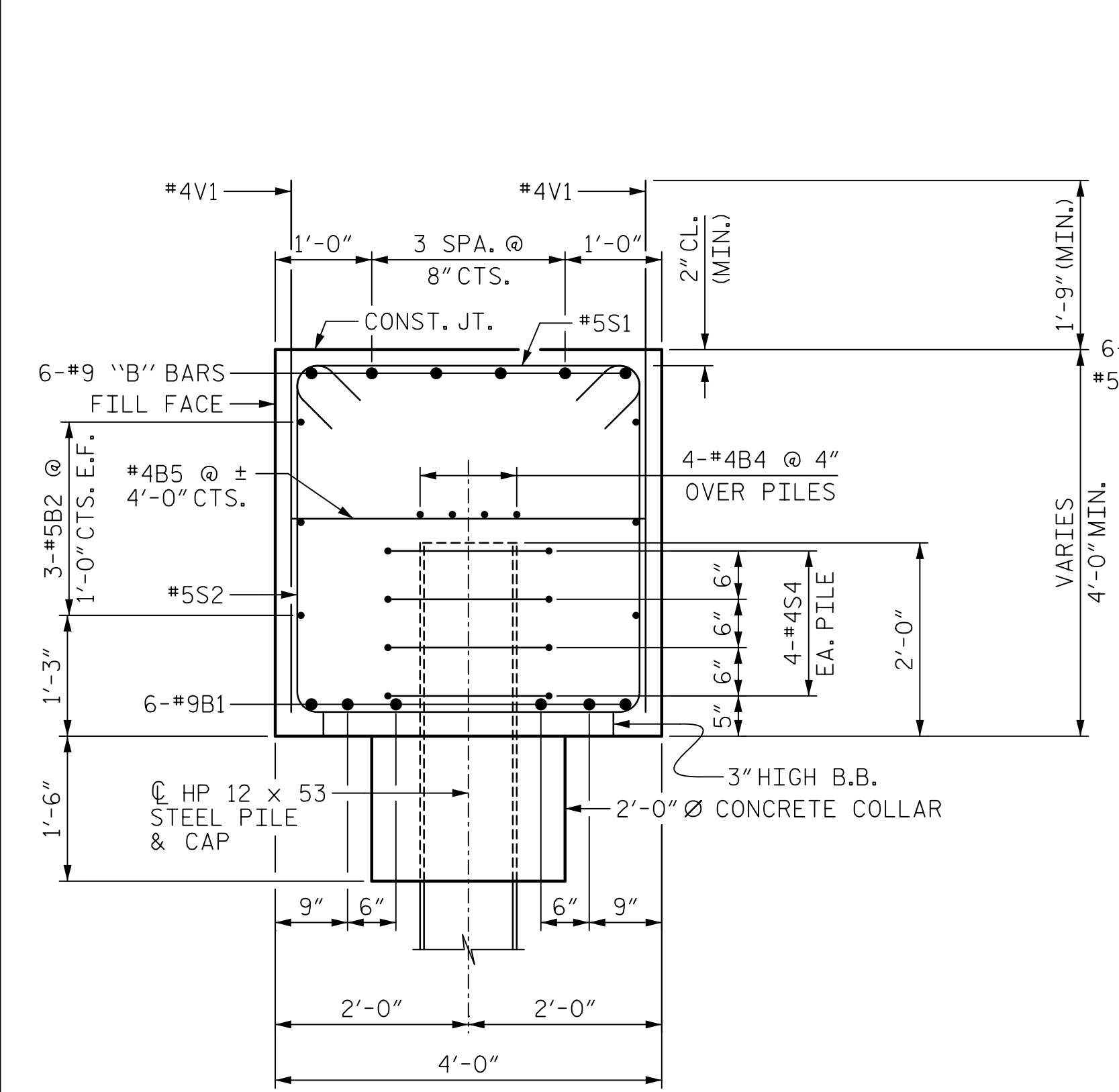
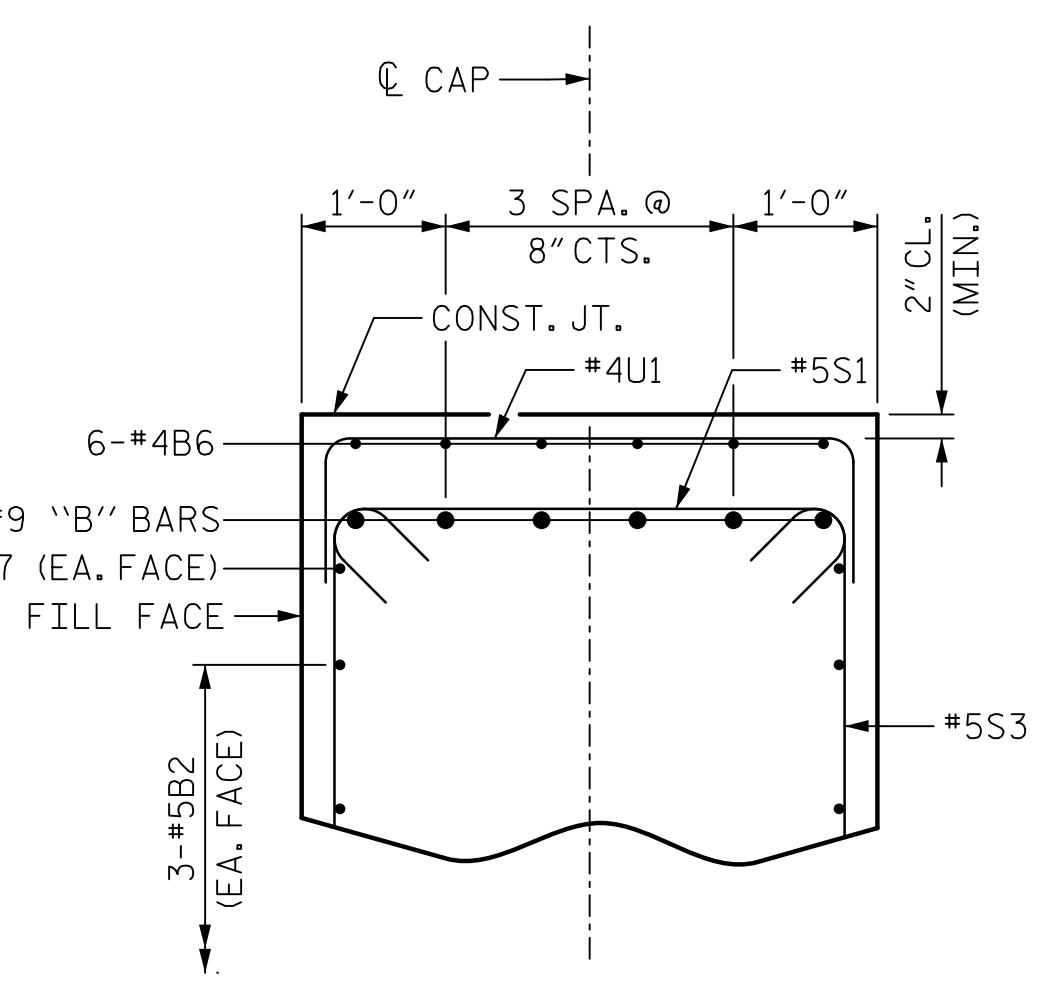


DATE: 6/9/2022
TIME: 11:28:54 AM

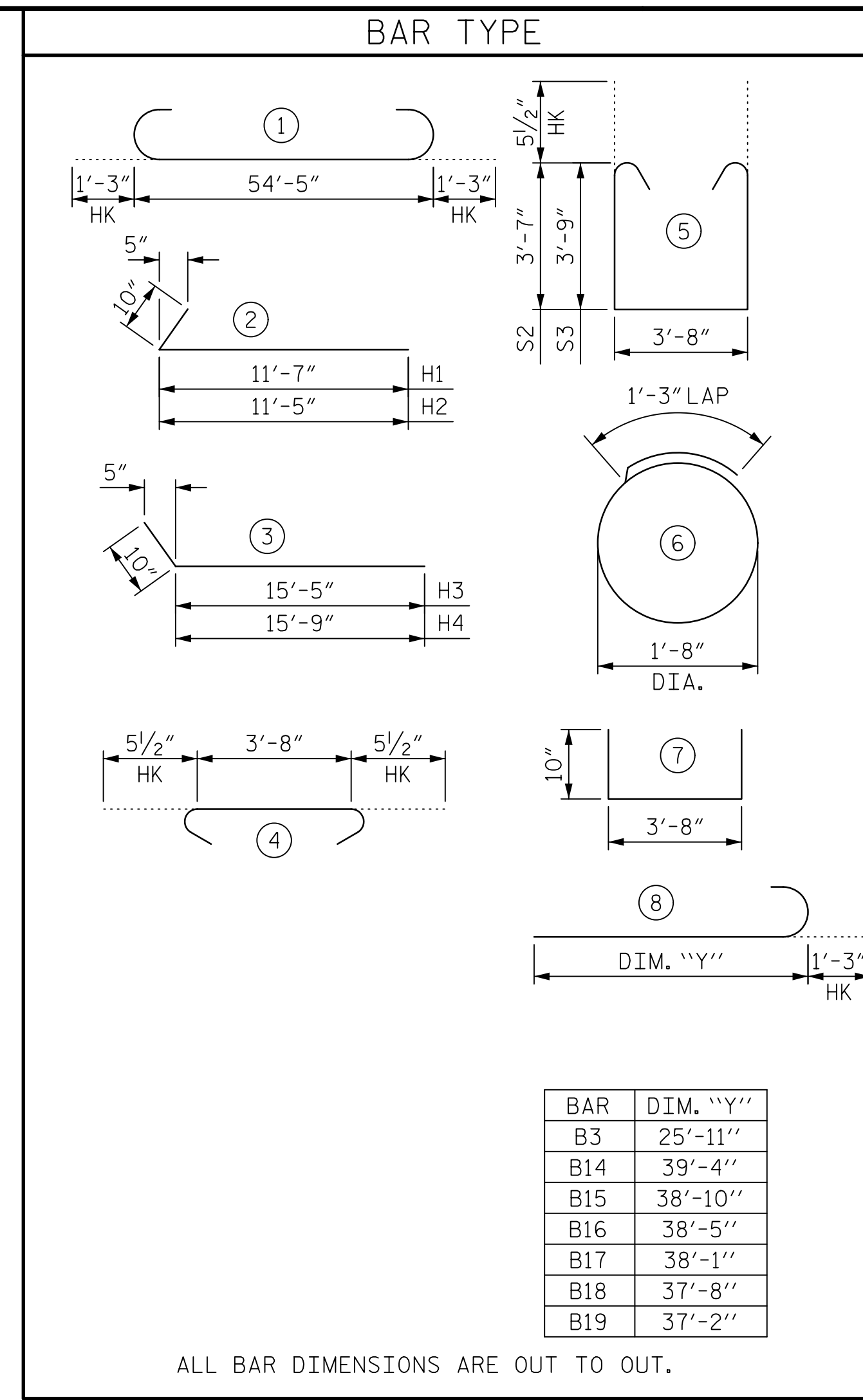
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SECTION A-A



SECTION B-B



BAR	DIM. "Y"
B3	25'-11"
B14	39'-4"
B15	38'-10"
B16	38'-5"
B17	38'-1"
B18	37'-8"
B19	37'-2"

ALL BAR DIMENSIONS ARE OUT TO OUT.

BILL OF MATERIAL					
INTEGRAL END BENT 1					
BAR NO.	NO.	SIZE	TYPE	LENGTH	WEIGHT
B1	6	9	1	56'-11"	1161
B2	6	5	STR	54'-5"	341
B3	6	9	8	27'-2"	554
B4	8	4	STR	28'-6"	152
B5	14	4	STR	3'-8"	34
B6	6	4	STR	9'-11"	40
B7	2	5	STR	31'-5"	66
B8	1	4	STR	9'-1"	6
B9	1	4	STR	8'-7"	6
B10	1	4	STR	8'-2"	5
B11	1	4	STR	7'-10"	5
B12	1	4	STR	7'-5"	5
B13	1	4	STR	6'-11"	5
B14	1	9	8	40'-7"	138
B15	1	9	8	40'-1"	136
B16	1	9	8	39'-8"	135
B17	1	9	8	39'-4"	134
B18	1	9	8	38'-11"	132
B19	1	9	8	38'-5"	131
H1	13	5	2	12'-5"	168
H2	13	5	2	12'-3"	166
H3	12	6	3	16'-3"	293
H4	12	6	3	16'-7"	299
H5	16	5	STR	14'-11"	249
H6	20	6	STR	18'-10"	566
K1	26	4	STR	3'-0"	52
K2	24	4	STR	3'-1"	49
S1	62	5	4	4'-7"	296
S2	19	5	5	11'-9"	233
S3	43	5	5	12'-1"	542
S4	32	4	6	6'-6"	139
U1	12	4	7	5'-4"	43
V1	72	4	STR	6'-5"	309
V2	32	5	STR	9'-11"	331
V3	40	5	STR	10'-4"	431

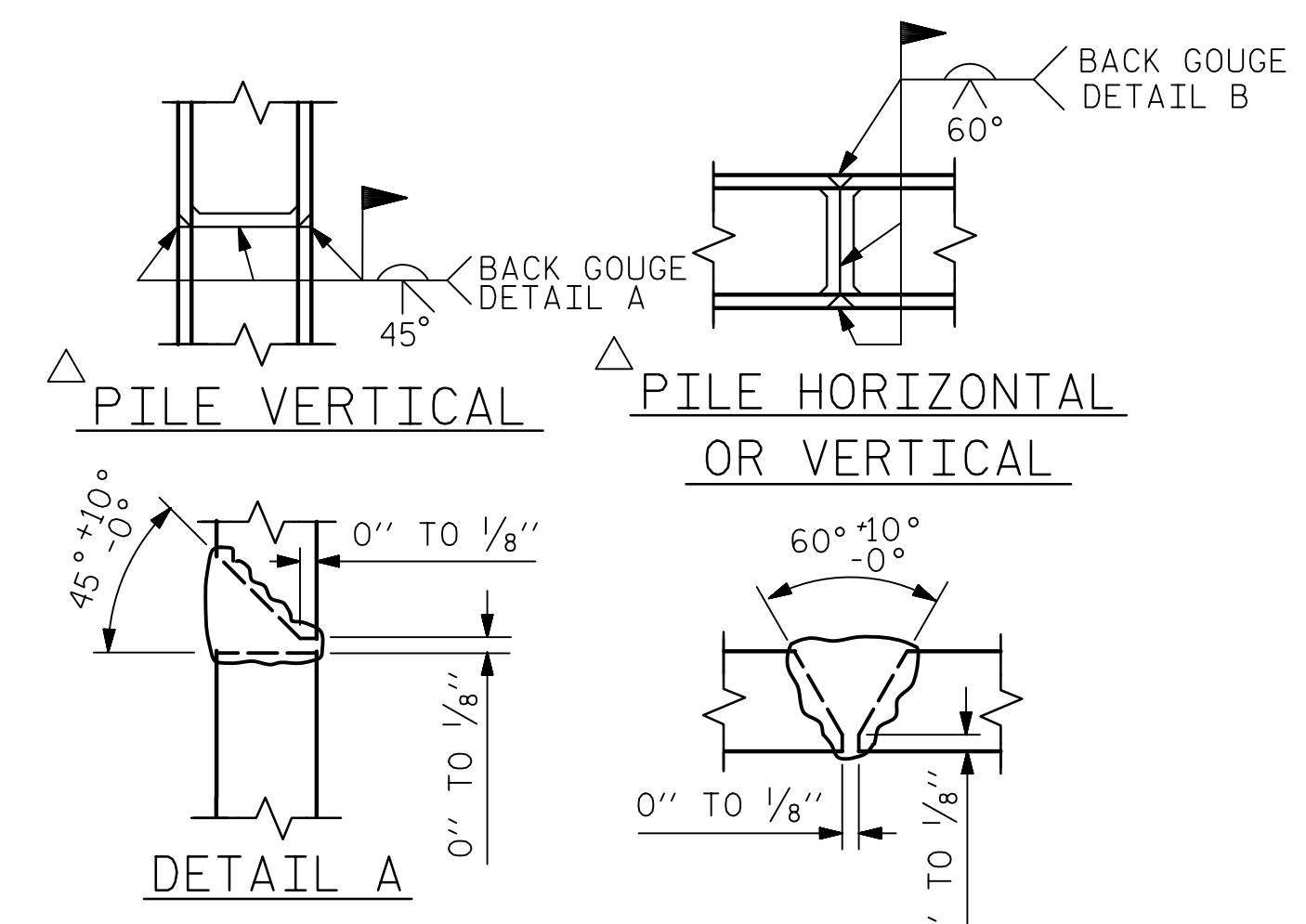
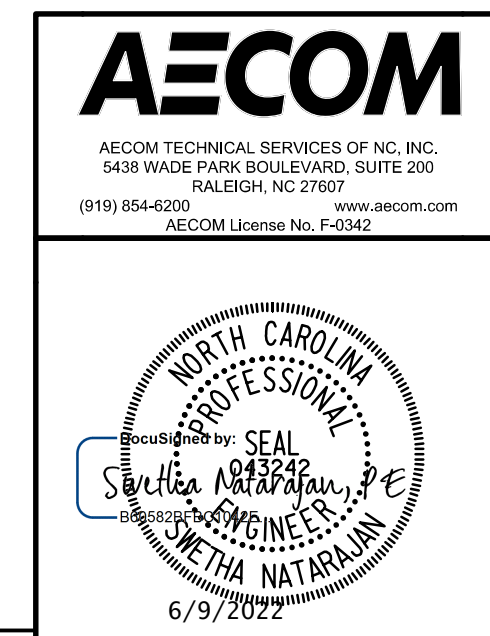
REINFORCING STEEL	7,352 LBS.
CLASS A CONCRETE	
POUR #1 (CAP, COLLARS & LOWER WINGWALLS)	40.8 C.Y.
POUR #2 (UPPER WINGWALL)	7.4 C.Y.
TOTAL = 48.2 C.Y.	
HP 12x53 STEEL PILES:	
NO. = 8	LIN. FT. = 200
PILE DRIVING EQUIPMENT SETUP FOR HP 12x53 STEEL PILES	8 EA.
STEEL PILE POINTS	8 EA.

PROJECT NO. B-5717
 GUILFORD COUNTY
 STATION: 21+22.00 -L-

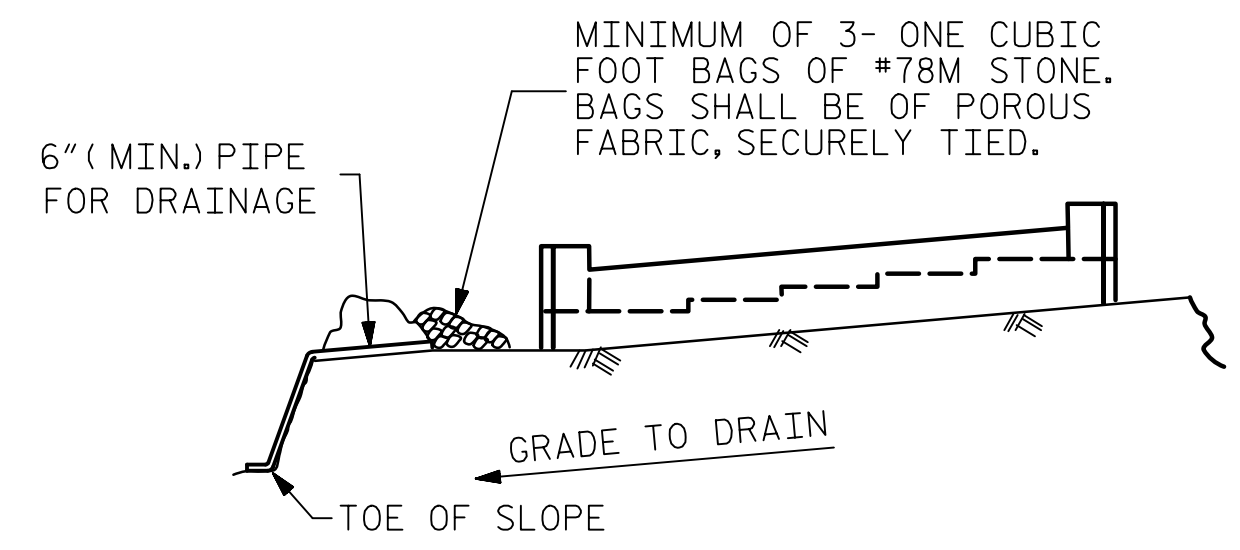
SHEET 3 OF 3

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

SUBSTRUCTURE
 INTEGRAL END BENT 1
 (LEFT LANE)



PILE SPLICE DETAILS



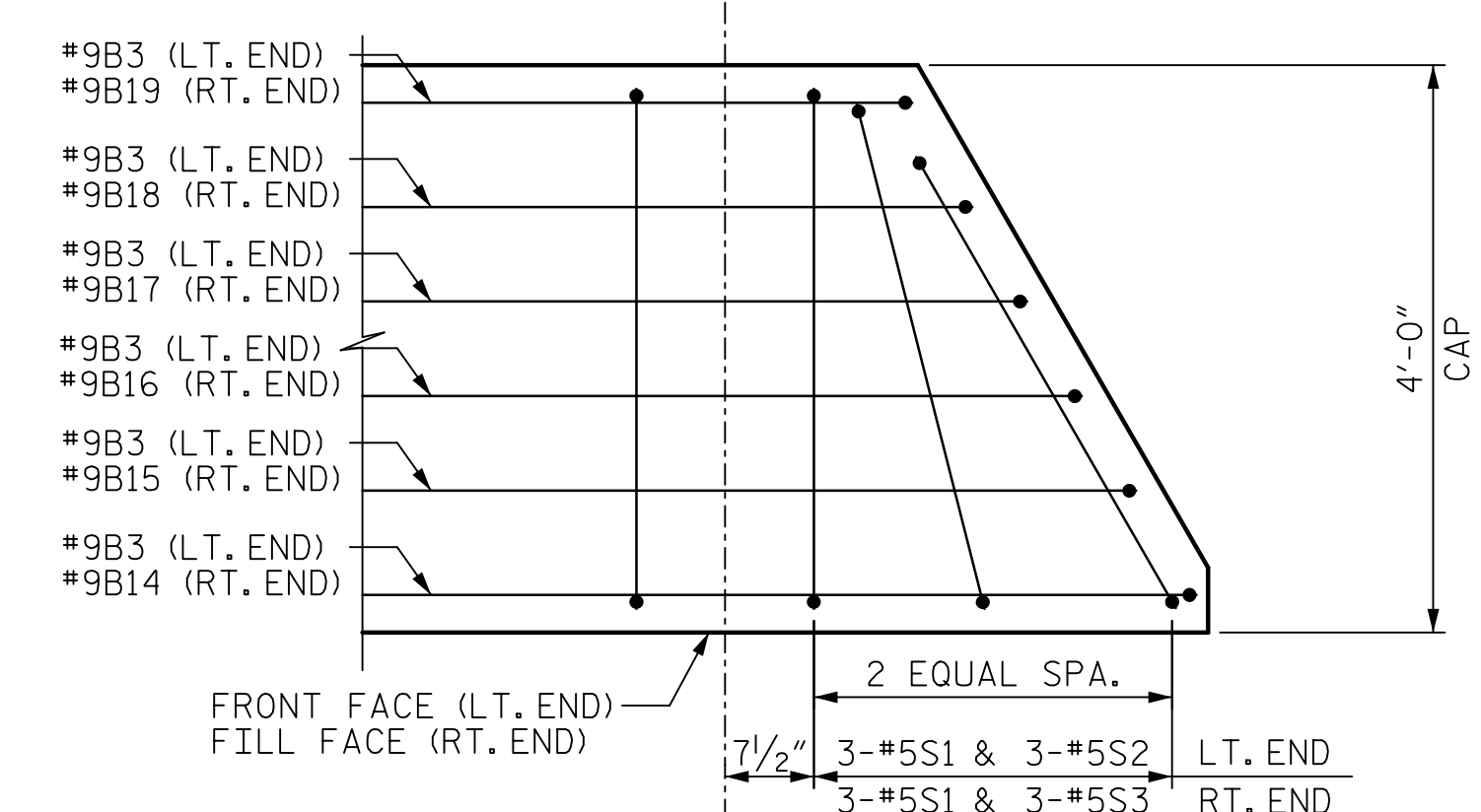
MINIMUM OF 3- ONE CUBIC FOOT BAGS OF #78M STONE. BAGS SHALL BE OF POROUS FABRIC, SECURELY TIED.

BAGGED STONE AND PIPE SHALL BE PLACED IMMEDIATELY AFTER COMPLETION OF END BENT EXCAVATION. PIPE MAY BE EITHER CONCRETE, CORRUGATED STEEL, CORRUGATED ALUMINUM ALLOY, OR CORRUGATED PLASTIC. PERFORATED PIPE WILL NOT BE ALLOWED.

BAGGED STONE SHALL REMAIN IN PLACE UNTIL THE ENGINEER DIRECTS THAT IT BE REMOVED. THE CONTRACTOR SHALL REMOVE AND DISPOSE OF SILT ACCUMULATIONS AT BAGGED STONE WHEN SO DIRECTED BY THE ENGINEER. BAGS SHALL BE REMOVED AND REPLACED WHENEVER THE ENGINEER DETERMINES THAT THEY HAVE DETERIORATED AND LOST THEIR EFFECTIVENESS.

NO SEPARATE PAYMENT WILL BE MADE FOR THIS WORK AND THE ENTIRE COST OF THIS WORK SHALL BE INCLUDED IN THE UNIT CONTRACT PRICE BID FOR THE SEVERAL PAY ITEMS.

TEMPORARY DRAINAGE AT END BENT



SPLAYED BAR DETAIL

PLAN VIEW SHOWN AT LEFT END, RIGHT END SIMILAR BY ROTATION.

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

DRAWN BY : D.R. DRUM	DATE : 05/2021
CHECKED BY : S. NATARAJAN	DATE : 08/2021
DESIGNED BY : D.R. DRUM	DATE : 05/2021
DESIGN CHECKED BY : S. NATARAJAN	DATE : 05/2021

REVISION #1:
 REMOVED PREDRILLING FOR PILES
 PAY ITEM.

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S2-28
1	SN	6-9-22	3			TOTAL SHEETS
2			4			38