



LOCATION SKETCH

INFORMATION INDICATED ON THE LOCATION SKETCH SHALL BE CONSIDERED GENERAL INFORMATION ONLY. THE CONTRACTOR SHALL CONFIRM, THROUGH OTHER SOURCES, SPECIFIC INFORMATION REGARDING BRIDGES, ROADWAYS, UTILITIES, THE SURROUNDING AREA, AND ANY OTHER ASPECTS THAT MAY BE NECESSARY TO PERFORM AND COMPLETE THE PROJECT.

BRIDGE COORDINATES

LAT: 35.84786°
LONG: -82.52542°

NOTES

- EXISTING DIMENSIONS AND BRIDGE CONDITION ARE FROM THE BEST INFORMATION AVAILABLE.
- THE CONTRACTOR SHALL FIELD VERIFY THE INFORMATION SHOWN ON THE PLANS AND NOTIFY THE ENGINEER IF ACTUAL DIMENSIONS AND CONDITIONS DIFFER.
- THE CONTRACTOR SHALL HAVE NO CLAIM WHATSOEVER AGAINST THE DEPARTMENT OF TRANSPORTATION FOR ANY DELAYS OR ADDITIONAL COST INCURRED BASED ON DIFFERENCES BETWEEN THAT SHOWN ON THE PLANS AND THE ACTUAL CONDITIONS AT THE PROJECT SITE.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO FOLLOW ALL STATE AND FEDERAL SAFETY REQUIREMENTS.
- WORK ON THE BRIDGES SHALL BE PERFORMED SO AS NOT TO ALLOW DEBRIS TO FALL BELOW. THE CONTRACTOR SHALL SUBMIT PLANS FOR CONSTRUCTION IN ACCORDANCE WITH ARTICLE 402-2 OF THE STANDARD SPECIFICATIONS AND THE PROJECT SPECIAL PROVISIONS.
- ANY DAMAGE TO EXISTING REINFORCING STEEL, DURING CONTRACTOR'S OPERATIONS, SHALL BE REPAIRED AS DIRECTED BY THE ENGINEER AND PERFORMED AT NO ADDITIONAL COST TO THE DEPARTMENT.
- PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL SUBMIT FOR REVIEW AND APPROVAL A COMPLETE SEQUENCE OF TASK FOR EACH OPERATION AFFECTING THE BRIDGE SURFACE AND/OR TRAFFIC.
- FOR SUBMITTAL OF WORKING DRAWINGS, SEE SPECIAL PROVISIONS.
- FOR FALSEWORK AND FORMWORK, SEE SPECIAL PROVISIONS.
- FOR CRANE SAFETY, SEE SPECIAL PROVISIONS.
- FOR GROUT FOR STRUCTURES, SEE SPECIAL PROVISIONS.
- FOR TRAFFIC CONTROL AND LIMITS OF PHASING OF CONSTRUCTION, SEE TRAFFIC CONTROL SPECIAL PROVISIONS.
- EXISTING JOINTS AND DECK DRAINS SHALL BE SEALED PRIOR TO BEGINNING SURFACE PREPARATION OF BRIDGE DECK.
- FOR SCARIFYING BRIDGE DECK, HYDRO-DEMOLITION OF BRIDGE DECK, AND CLASS II SURFACE PREPARATION, SEE OVERLAY SURFACE PREPARATION SPECIAL PROVISION.
- THE CONTRACTOR SHALL PROVIDE A METHOD OF HANDLING UNEXPECTED BLOW THROUGH OF THE DECK DURING HYDRO-DEMOLITION.
- FOR PLACING AND FINISHING LATEX MODIFIED CONCRETE -EARLY STRENGTH (LMC-ES), SEE LATEX MODIFIED CONCRETE-EARLY STRENGTH SPECIAL PROVISIONS.
- LONGITUDINAL CONSTRUCTION JOINTS SHALL BE LOCATED ALONG THE CENTERLINE OR EDGE OF TRAVEL LANES.
- DURING CONSTRUCTION, BERMS OR APPROPRIATE MEASURES SHALL BE USED TO ENSURE HYDRO-DEMOLITION WATER DOES NOT MIGRATE INTO ACTIVE TRAVEL LANES.
- THE CONTRACTOR SHALL COLLECT, TREAT AND DISPOSE OF RUN-OFF WATER FROM THE HYDRO-DEMOLITION PROCESS, SEE OVERLAY SURFACE PREPARATION SPECIAL PROVISIONS.
- FOR BRIDGE JOINT DEMOLITION, SEE SPECIAL PROVISIONS.
- FOR FOAM JOINT SEAL FOR PRESERVATION, SEE SPECIAL PROVISIONS.
- FOR CONCRETE FOR DECK REPAIRS, SEE SPECIAL PROVISIONS.
- FOR VOLUMETRIC MIXER, SEE SPECIAL PROVISIONS.
- FOR ELASTOMERIC CONCRETE FOR PRESERVATION, SEE SPECIAL PROVISIONS.

PROJ. NO. I-5831A
MADISON COUNTY
 BRIDGE NO. 560550

SHEET 2 OF 2



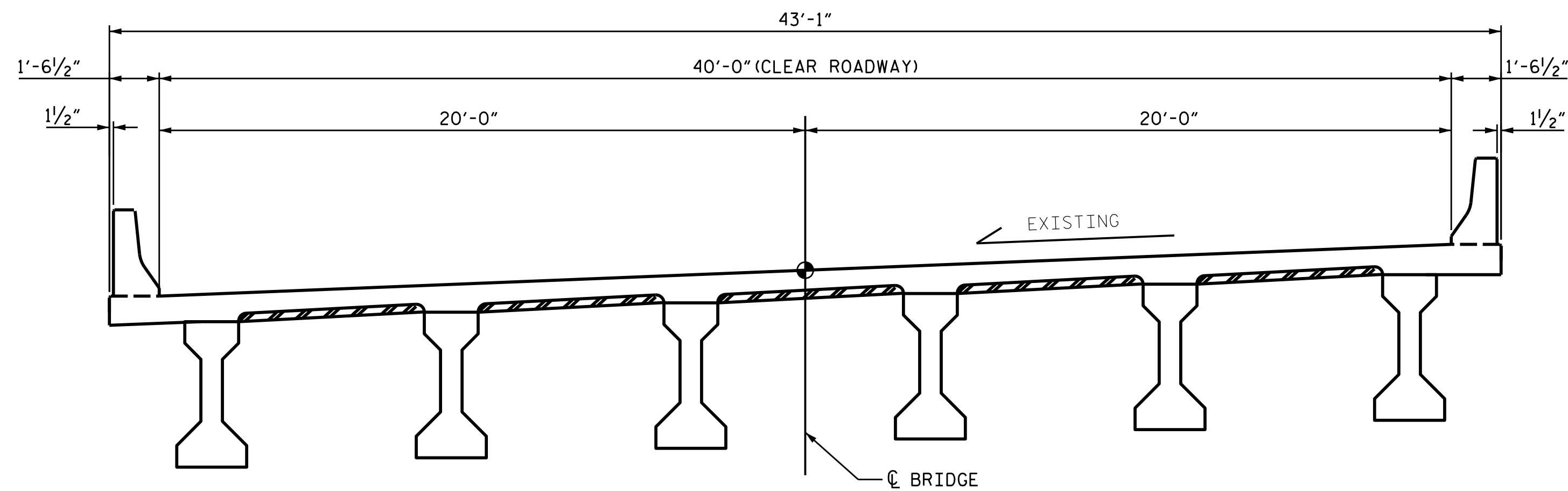
STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

GENERAL DRAWING
 FOR BRIDGE ON
 INTERSTATE 26
 EASTBOUND
 OVER SR 1352
 (HIGGINS BRANCH RD.)

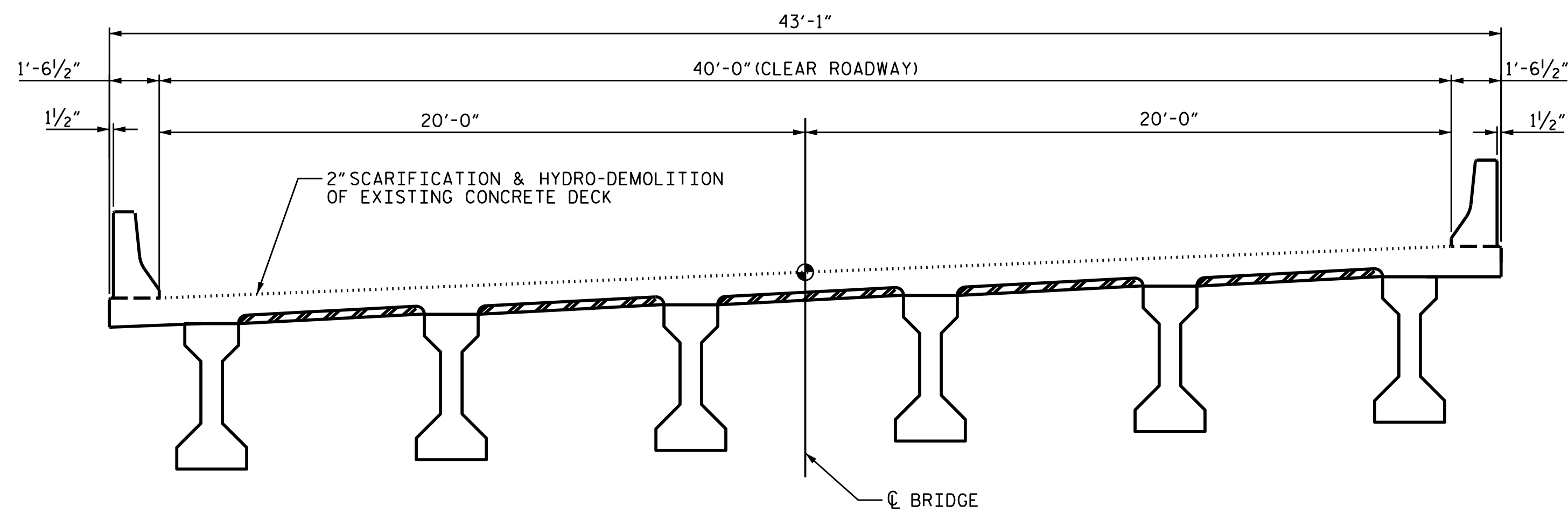
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DRAWN BY : M.ALINAGHIAN DATE : 6/2019
 CHECKED BY : GHOLAMREZA KOUCHEKI DATE : 2/2022

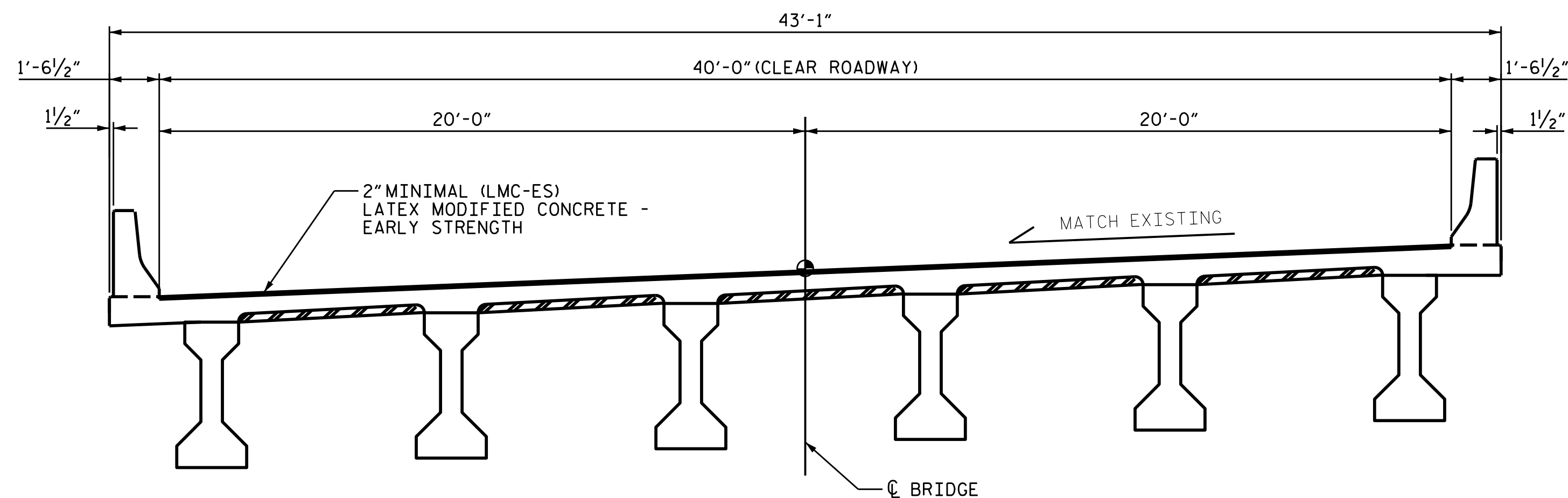
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EXISTING TYPICAL SECTION



PROPOSED TYPICAL SECTION

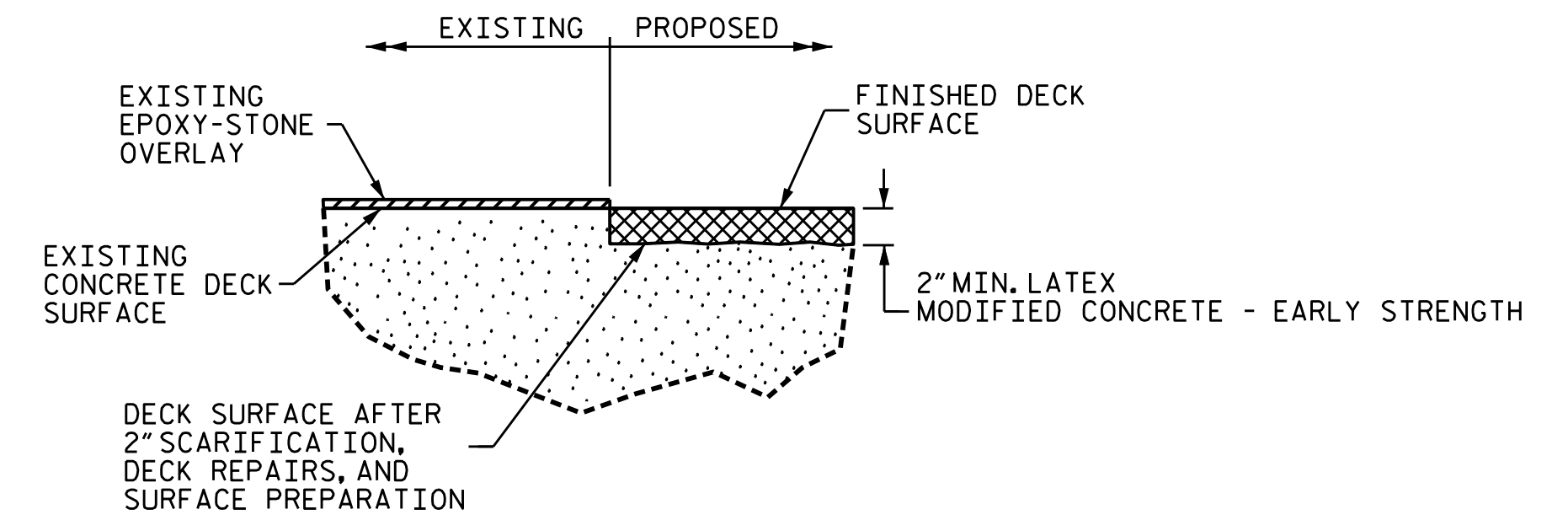


PROPOSED TYPICAL SECTION

NOTE

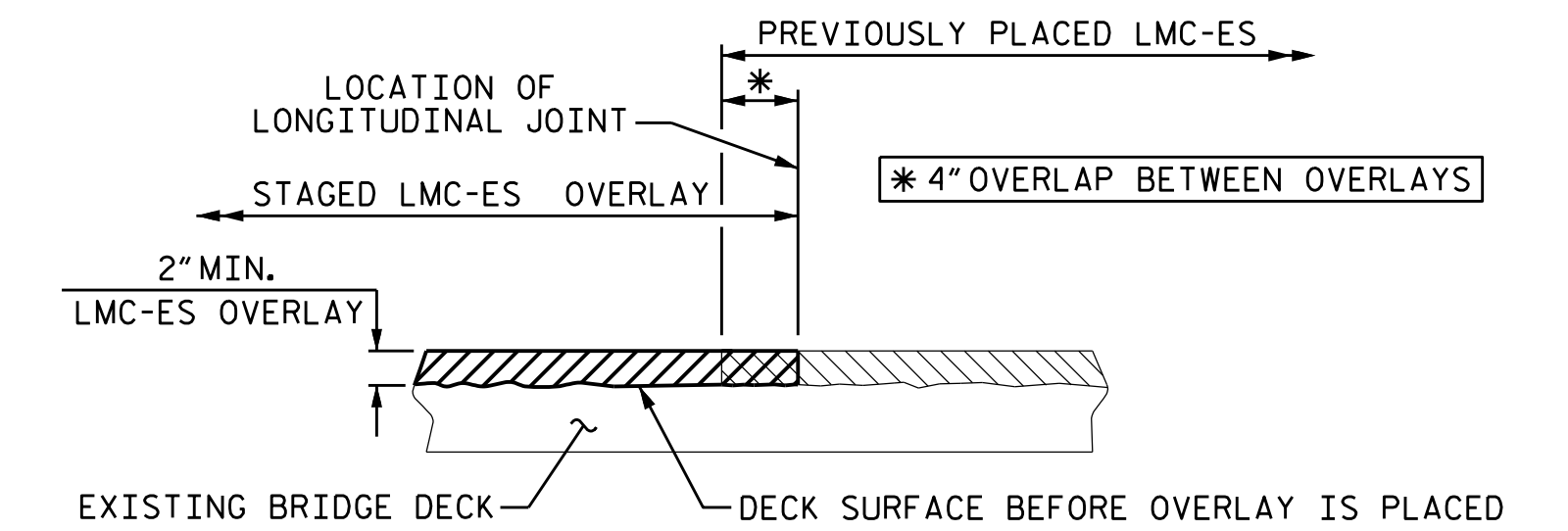
WHEN PREPARING THE SURFACE FOR LMC-ES OVERLAY ADJACENT TO THE PREVIOUSLY PLACED LMC-ES STAGE, THE PREVIOUSLY PLACED LMC-ES SHALL BE SAW-CUT TO THE FULL DEPTH OF THE LMC-ES AT THE CENTERLINE OF THE BRIDGE AND ALL LMC-ES IN THE 4\"/>

SEE TRAFFIC CONTROL SPECIAL PROVISIONS FOR LANE WIDTHS, SEQUENCING AND OTHER TRAFFIC CONTROL MEASURES FOR STAGGING OF OVERLAY SURFACE PREPARATION AND OVERLAY PLACEMENT.



DETAIL FOR LMC-ES OVERLAY

(FINISHED SURFACE OF THE LATEX MODIFIED CONCRETE - EARLY STRENGTH IS APPROXIMATE)



SECTION THRU DECK

STAGED LMC-ES OVERLAY JOINT

(AS NEEDED)

PROJECT NO. I-5831A
MADISON COUNTY
 BRIDGE NO. 560550



DocuSigned by:
 Amber M. Lee
 2022/05/02 10:48:44 AM

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH
 SUPERSTRUCTURE
 TYPICAL SECTION
 AND
 LMC-ES OVERLAY DETAIL

DRAWN BY : GHOLAMREZA KOUICHEKI DATE : 02/2022
 CHECKED BY : H.A. LOCKLEAR DATE : 02/2022

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AS-BUILT REPAIR QUANTITY TABLE

DECK SURFACE REPAIR - SPAN A

	ESTIMATE	ACTUAL
CLASS II SURFACE PREPARATION	0.0 SQ. YDS.	
LATEX MODIFIED CONCRETE-ES	8.2 CU. YDS.	
PLACING & FINISHING LMC-ES OVERLAY	131.0 SQ. YDS.	
SCARIFYING BRIDGE DECK	131.0 SQ. YDS.	
HYDRO-DEMOLITION OF BRIDGE DECK	131.0 SQ. YDS.	
GROOVING BRIDGE FLOORS	1087.0 SQ. FT.	

DECK SURFACE REPAIR - APPROACH SLAB

	ESTIMATE	ACTUAL
CLASS II SURFACE PREPARATION	0.0 SQ. YDS.	
LATEX MODIFIED CONCRETE-ES	4.6 CU. YDS.	
PLACING & FINISHING LMC-ES OVERLAY	73.0 SQ. YDS.	
SCARIFYING BRIDGE DECK	73.0 SQ. YDS.	
HYDRO-DEMOLITION OF BRIDGE DECK	73.0 SQ. YDS.	
GROOVING BRIDGE FLOORS	606.0 SQ. FT.	

NOTES

REPAIR LOCATIONS AND ESTIMATED QUANTITIES ARE BASED ON THE BEST INFORMATION AVAILABLE. IF ADDITIONAL REPAIRS NOT SHOWN ON THE DRAWINGS ARE DEEMED NECESSARY BY THE ENGINEER, THE ENGINEER WILL NOTE ON THE DRAWINGS THE APPROXIMATE LOCATIONS AND DESCRIPTION OF THE REPAIRS AND ENTER THE ACTUAL QUANTITIES INTO THE AS-BUILT REPAIR QUANTITY TABLE.



PAYMENT FOR CLASS II SURFACE PREPARATION IS BASED ON THE SQUARE YARD OF ADDITIONAL DEMOLITION REQUIRED FOLLOWING HYDRO-DEMOLITION OF THE BRIDGE DECK. SEE SPECIAL PROVISIONS.

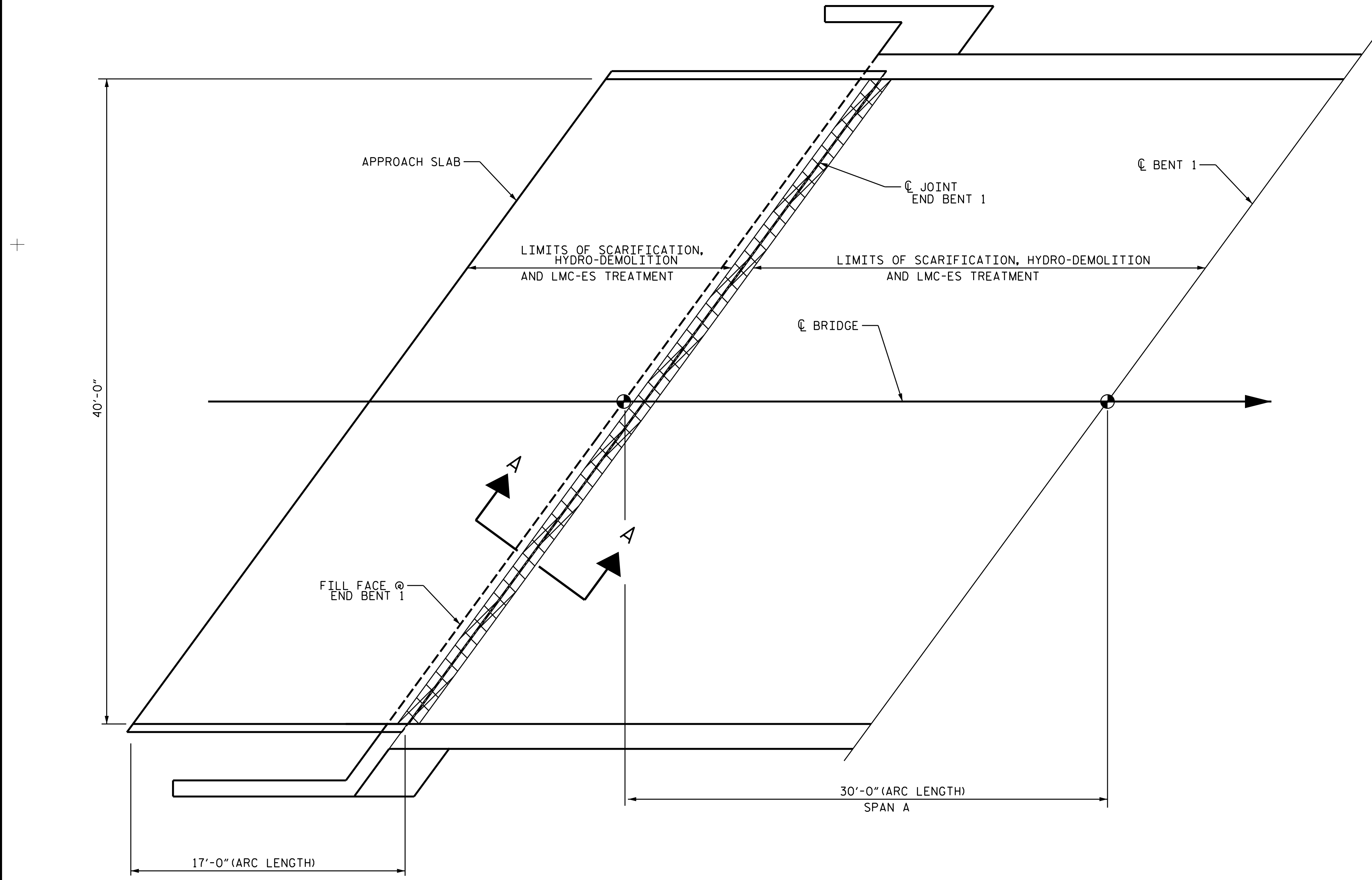
FOR SECTION A-A, SEE JOINT DETAILS, SHEET.

FOR LMC-ES OVERLAY, SEE SPECIAL PROVISION.

FOR BRIDGE JOINT DEMOLITION, SEE SPECIAL PROVISIONS.

FOR BRIDGE JOINT DEMOLITION QUANTITIES, SEE FOAM JOINT DETAIL SHEET.

-  CLASS II SURFACE REPAIR
-  BRIDGE JOINT DEMOLITION



SPAN A

PROJECT NO. I-5831A
MADISON COUNTY
 BRIDGE NO. 560550

SHEET 1 OF 3

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

DECK SURFACE REPAIR
 SPAN A



DocuSigned by:
 Amber M. Lee
 BO485A1P2FAD484
 03/18/2022

DRAWN BY : GHOLAMREZA KOUCHEKI DATE : 02/2022
 CHECKED BY : H.A. LOCKLEAR DATE : 02/2022

3/18/2022
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AS-BUILT REPAIR QUANTITY TABLE

DECK SURFACE REPAIR - SPAN B

	ESTIMATE	ACTUAL
CLASS II SURFACE PREPARATION	0.0 SQ. YDS.	
LATEX MODIFIED CONCRETE-ES	22.9 CU. YDS.	
PLACING & FINISHING LMC-ES OVERLAY	367.0 SQ. YDS.	
HYDRO-DEMOLITION OF BRIDGE DECK	367.0 SQ. YDS.	
SCARIFYING BRIDGE DECK	367.0 SQ. YDS.	
GROOVING BRIDGE FLOORS	3052.0 SQ. FT.	

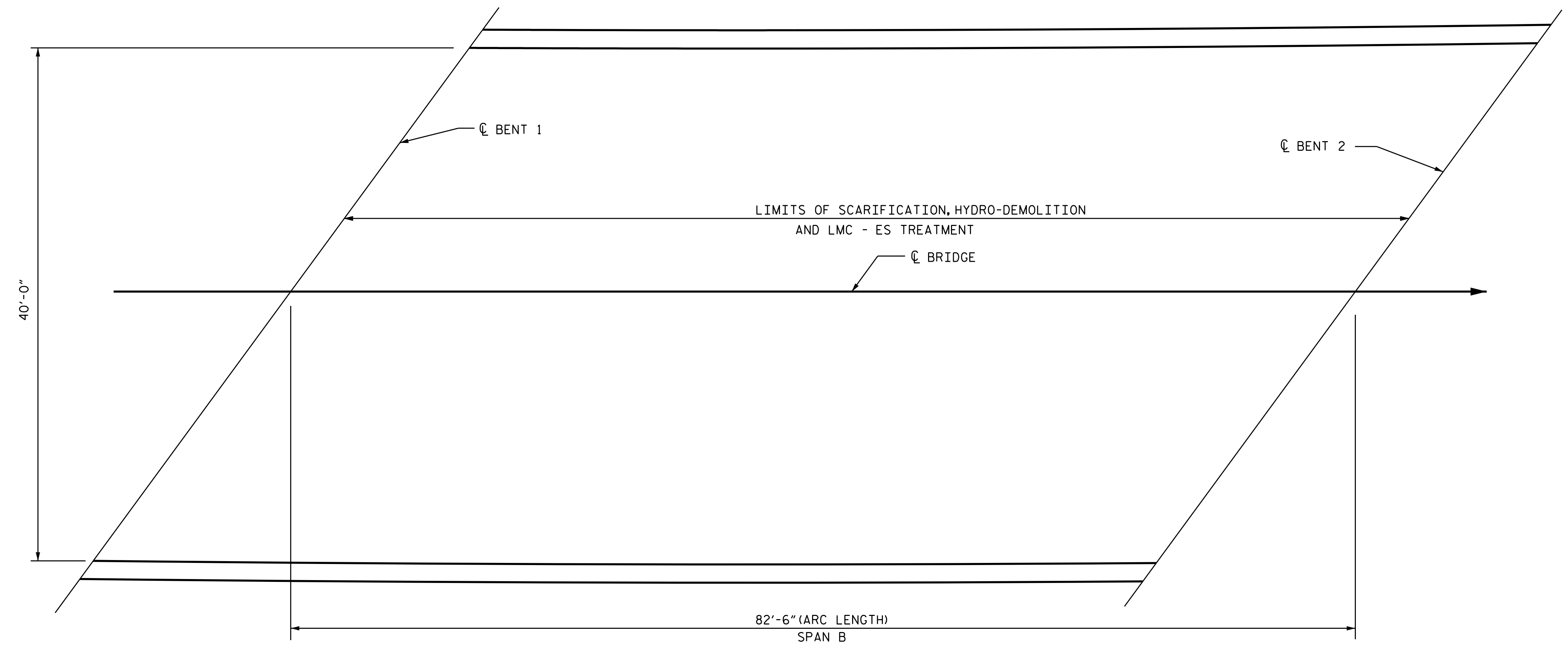
NOTES

REPAIR LOCATIONS AND ESTIMATED QUANTITIES ARE GIVEN WITH THE BEST INFORMATION AVAILABLE. IF ADDITIONAL REPAIRS NOT SHOWN ON THE DRAWINGS ARE DEEMED NECESSARY BY THE ENGINEER, THE ENGINEER WILL NOTE ON THE DRAWINGS THE APPROXIMATE LOCATIONS AND DESCRIPTION OF THE REPAIRS AND ENTER THE ACTUAL QUANTITIES INTO THE AS-BUILT REPAIR QUANTITY TABLE.

PAYMENT FOR CLASS II SURFACE PREPARATION IS BASED ON THE SQUARE YARD OF ADDITIONAL DEMOLITION REQUIRED FOLLOWING HYDRO-DEMOLITION OF THE BRIDGE DECK. SEE SPECIAL PROVISIONS.

FOR LMC-ES MATERIAL, SEE SPECIAL PROVISION.

 CLASS II SURFACE PREPARATION



SPAN B

PROJECT NO. I-5831A
MADISON COUNTY
 BRIDGE NO. 560550

SHEET 2 OF 3

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

DECK SURFACE REPAIR
 SPAN B



DocuSigned by:
 Meher M. Lee
 03/18/2022

DRAWN BY : GHOLAMREZA KOUCHEKI DATE : 02/2022
 CHECKED BY : H.A. LOCKLEAR DATE : 02/2022

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AS-BUILT REPAIR QUANTITY TABLE

DECK SURFACE REPAIR - SPAN C

	ESTIMATE	ACTUAL
CLASS II SURFACE PREPARATION	0.0 SQ. YDS.	
LATEX MODIFIED CONCRETE- ES	5.2 CU. YDS.	
PLACING & FINISHING LMC-ES OVERLAY	94.0 SQ. YDS.	
SCARIFYING BRIDGE DECK	94.0 SQ. YDS.	
HYDRO-DEMOLITION OF BRIDGE DECK	94.0 SQ. YDS.	
GROOVING BRIDGE FLOORS	782.0 SQ. FT.	

DECK SURFACE REPAIR - APPROACH SLAB

	ESTIMATE	ACTUAL
CLASS II SURFACE PREPARATION	0.0 SQ. YDS.	
LATEX MODIFIED CONCRETE- ES	4.1 CU. YDS.	
PLACING & FINISHING LMC-ES OVERLAY	73.0 SQ. YDS.	
SCARIFYING BRIDGE DECK	73.0 SQ. YDS.	
HYDRO-DEMOLITION OF BRIDGE DECK	73.0 SQ. YDS.	
GROOVING BRIDGE FLOORS	606.0 SQ. FT.	

NOTES

REPAIR LOCATIONS AND ESTIMATED QUANTITIES ARE BASED ON THE BEST INFORMATION AVAILABLE. IF ADDITIONAL REPAIRS NOT SHOWN ON THE DRAWINGS ARE DEEMED NECESSARY BY THE ENGINEER, THE ENGINEER WILL NOTE ON THE DRAWINGS THE APPROXIMATE LOCATIONS AND DESCRIPTION OF THE REPAIRS AND ENTER THE ACTUAL QUANTITIES INTO THE AS-BUILT REPAIR QUANTITY TABLE.

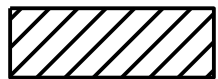

PAYMENT FOR CLASS II SURFACE PREPARATION IS BASED ON THE SQUARE YARD OF ADDITIONAL DEMOLITION REQUIRED FOLLOWING HYDRO-DEMOLITION OF THE BRIDGE DECK. SEE SPECIAL PROVISIONS.

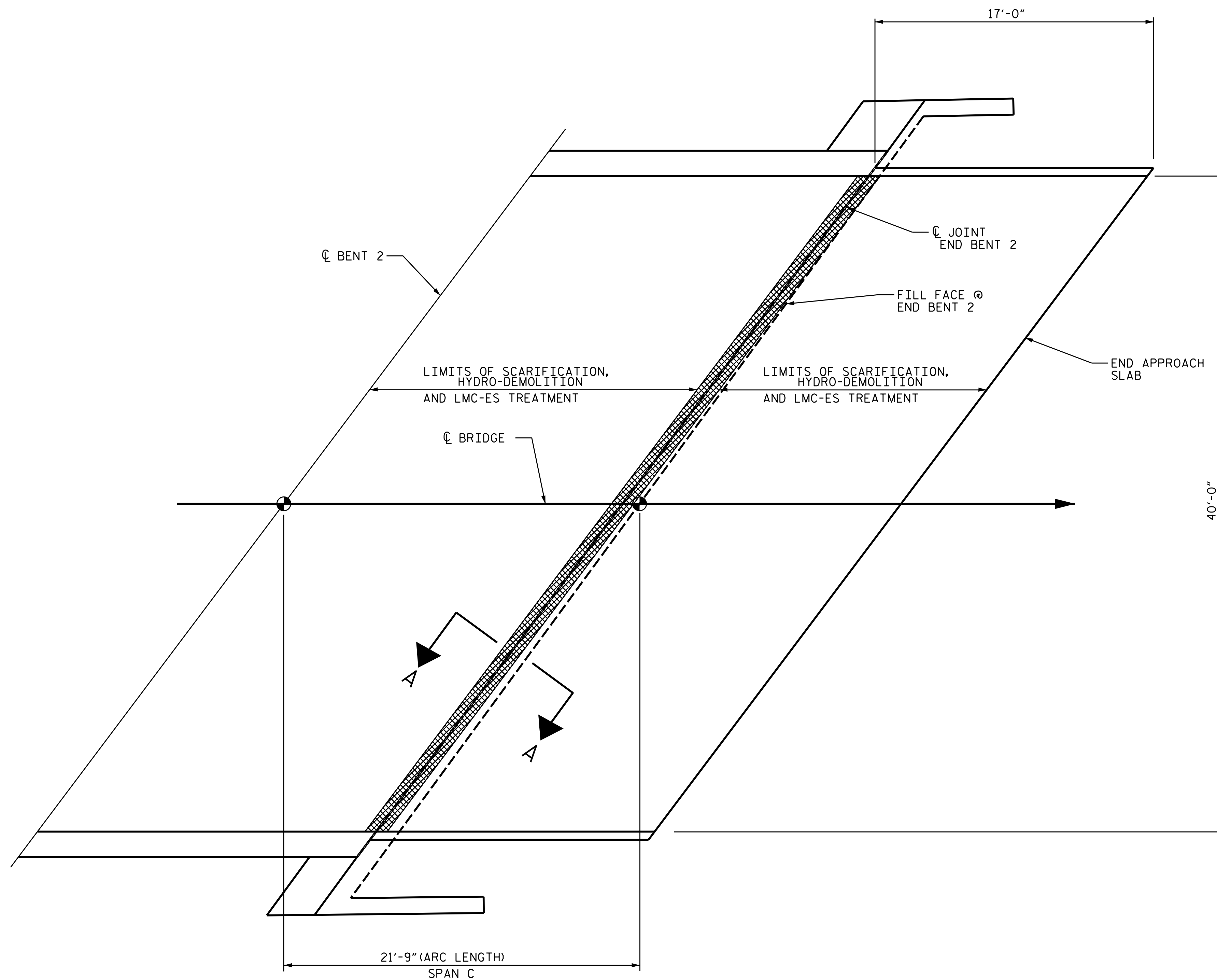
FOR SECTION A-A, SEE JOINT DETAILS, SHEET.

FOR LMC-ES OVERLAY, SEE SPECIAL PROVISION.

FOR BRIDGE JOINT DEMOLITION, SEE SPECIAL PROVISIONS.

FOR BRIDGE JOINT DEMOLITION QUANTITIES, SEE FOAM JOINT DETAIL SHEET.

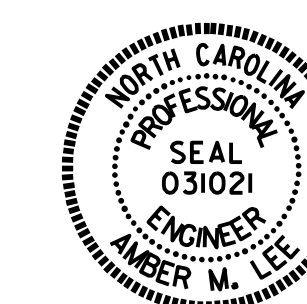
-  CLASS II SURFACE REPAIR
-  BRIDGE JOINT DEMOLITION



SPAN C

PROJECT NO. I-5831A
MADISON COUNTY
 BRIDGE NO. 560550

SHEET 3 OF 3



DocuSign by:
 03/18/2022

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

DECK SURFACE REPAIR
 SPAN C

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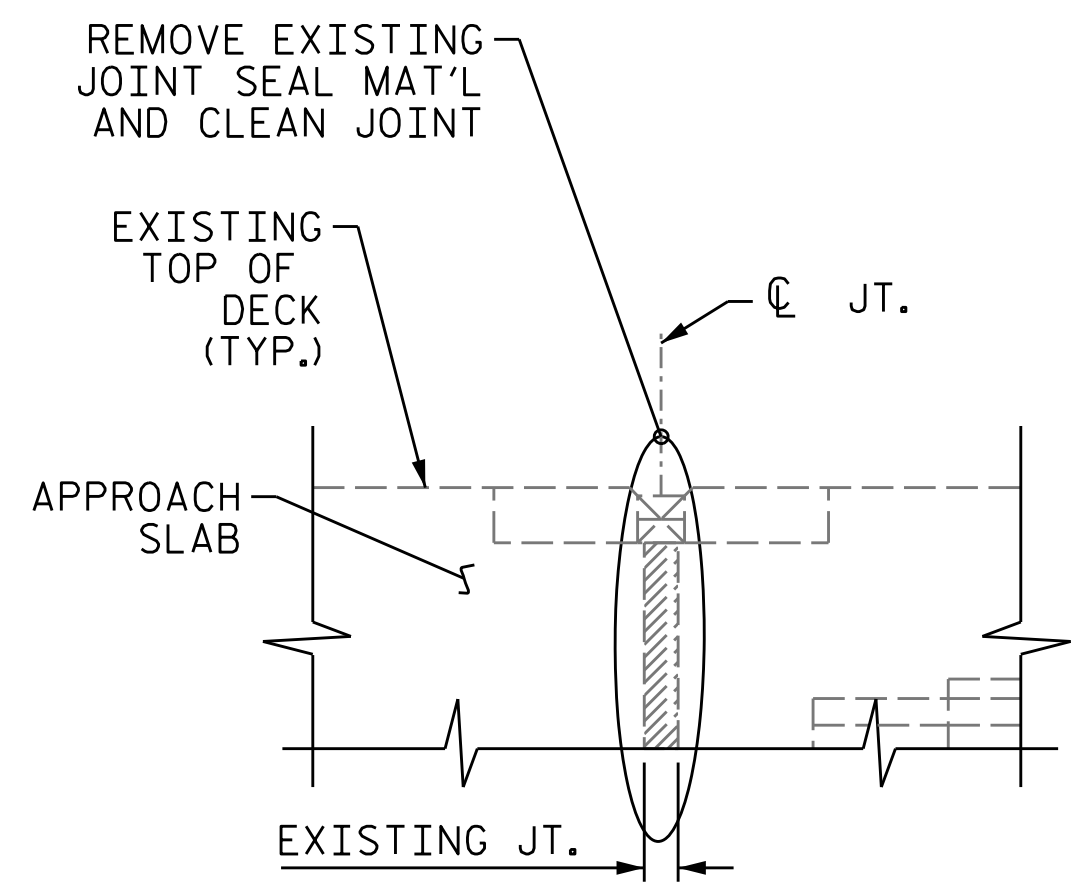
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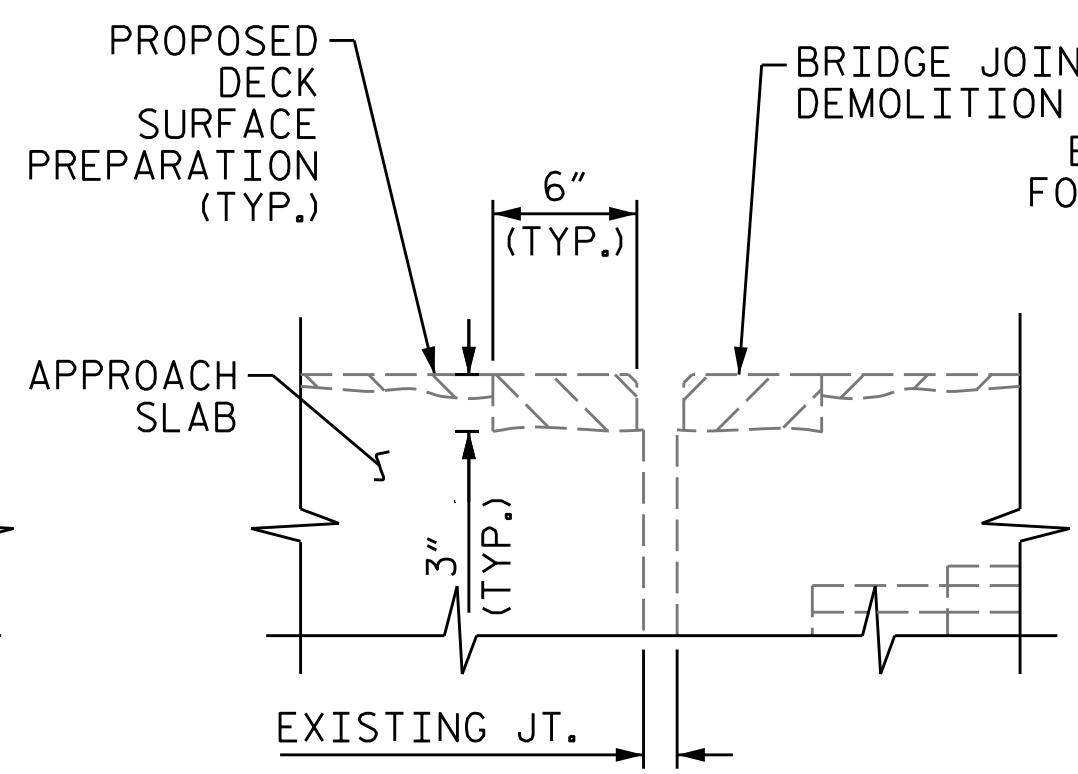
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DRAWN BY : GHOLAMREZA KOUCHEKI DATE : 02/2022
 CHECKED BY : H.A. LOCKLEAR DATE : 02/2022

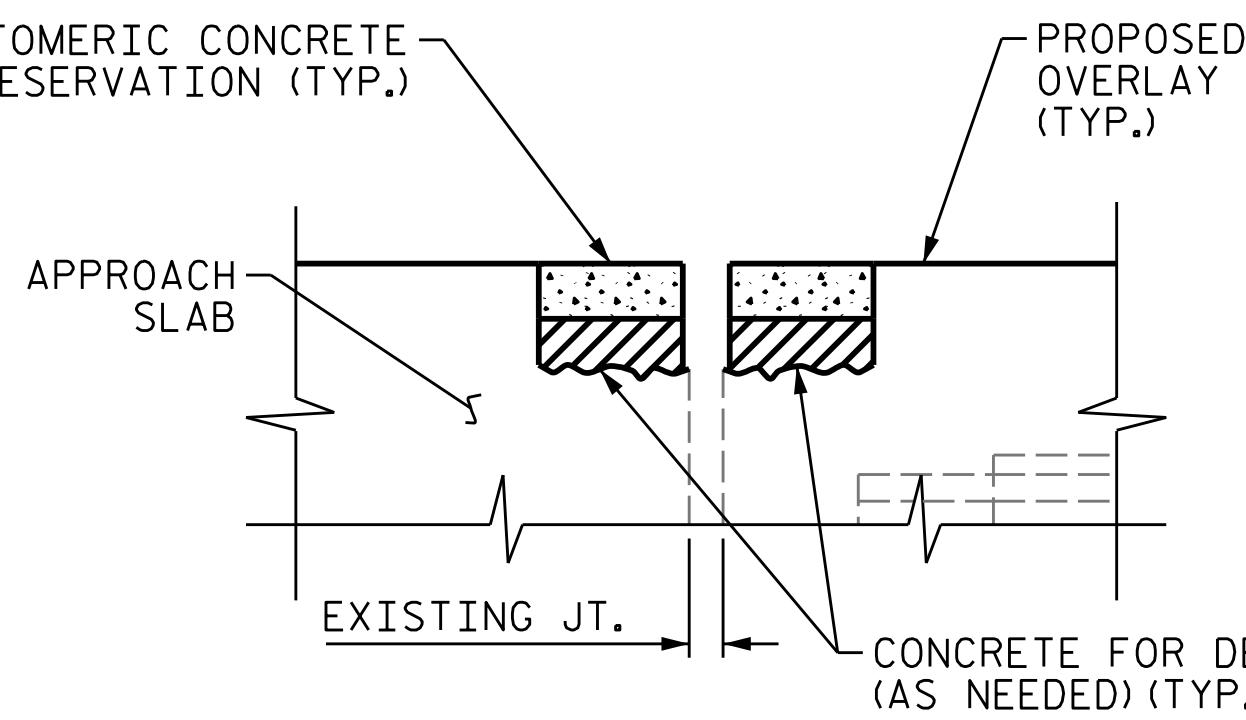
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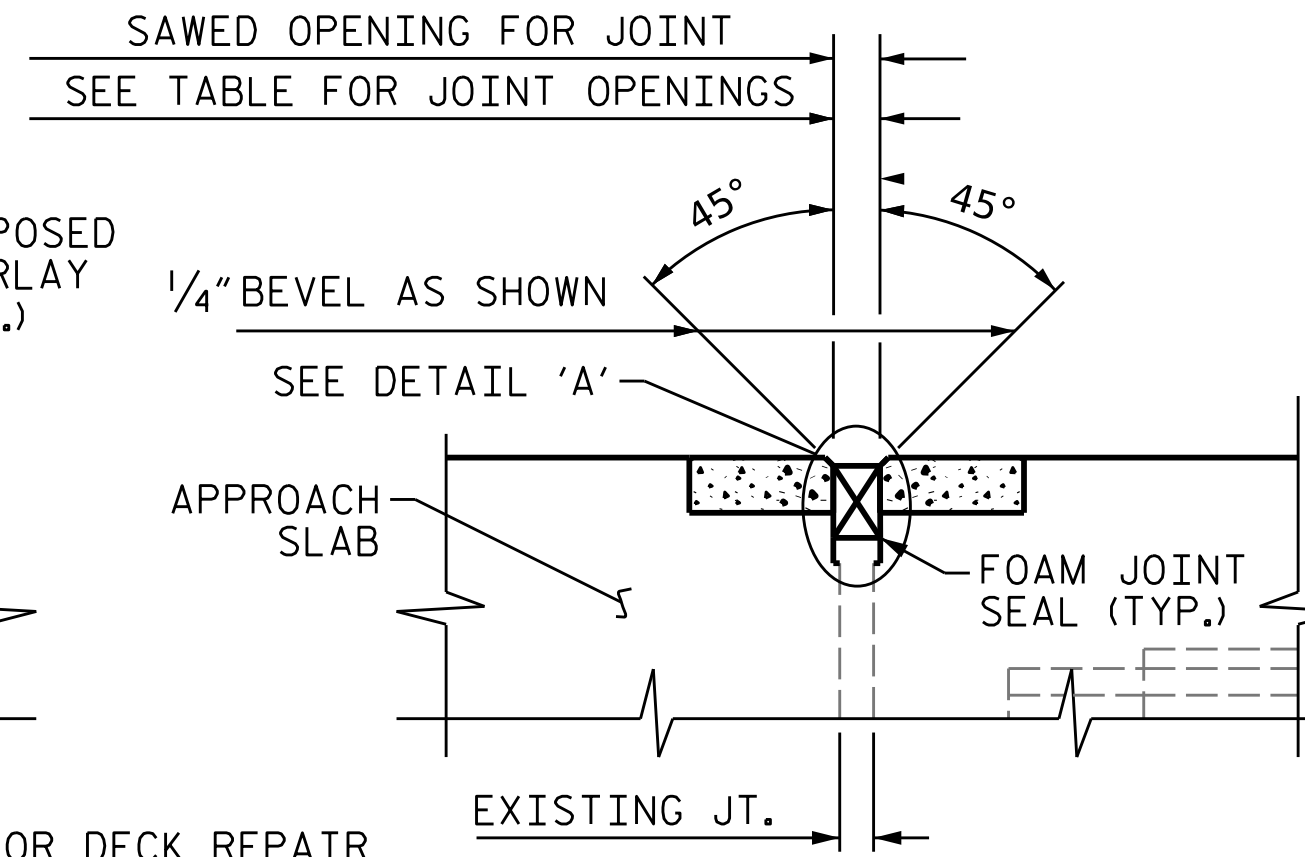
EXISTING JOINT SEAL



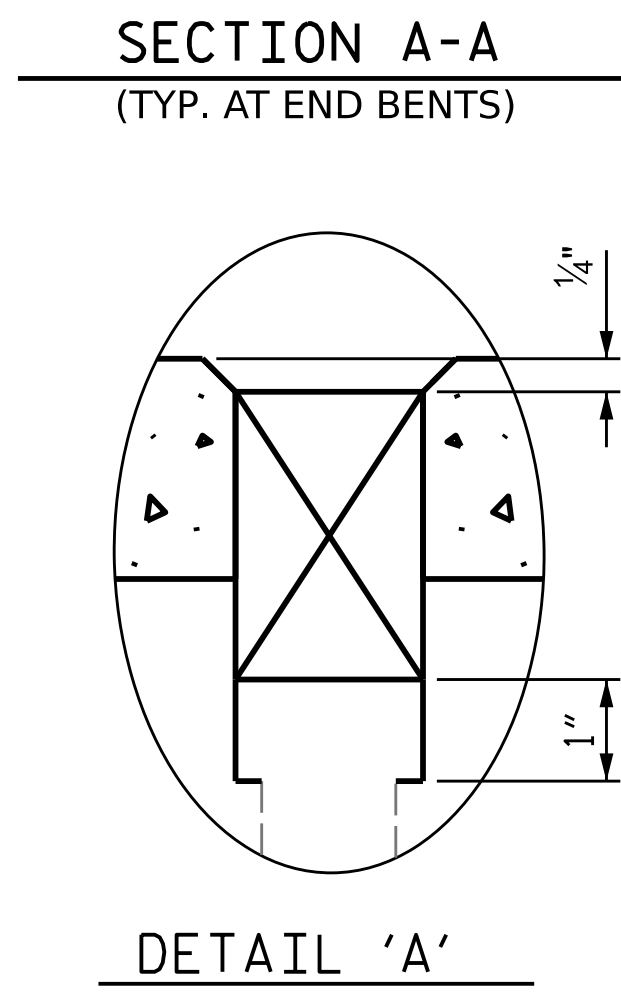
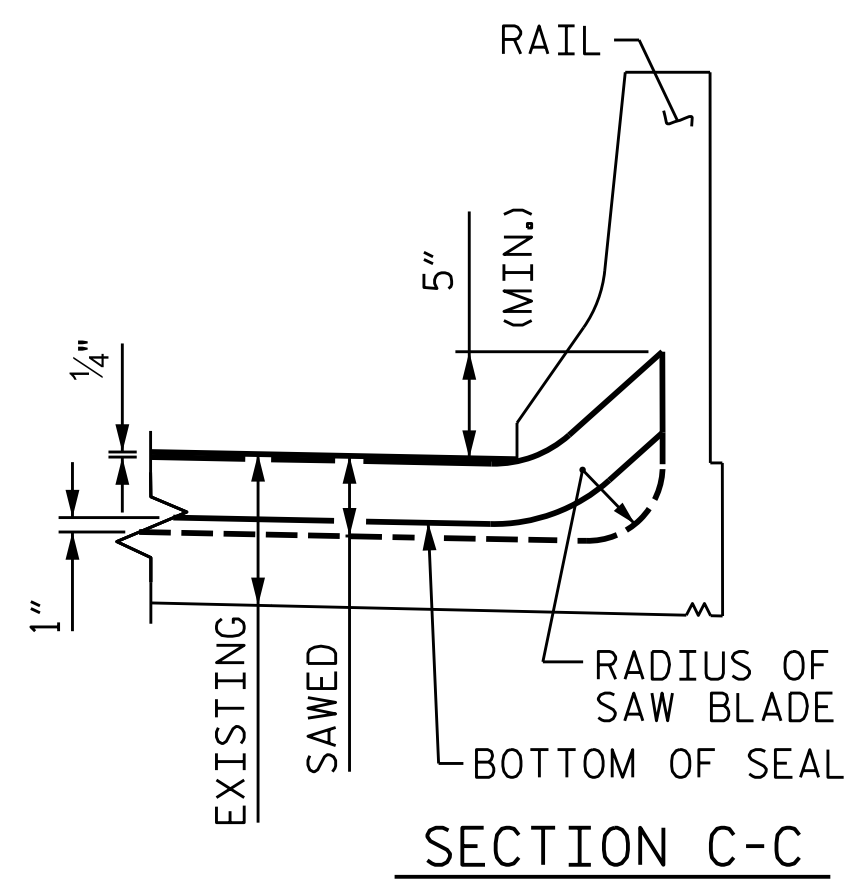
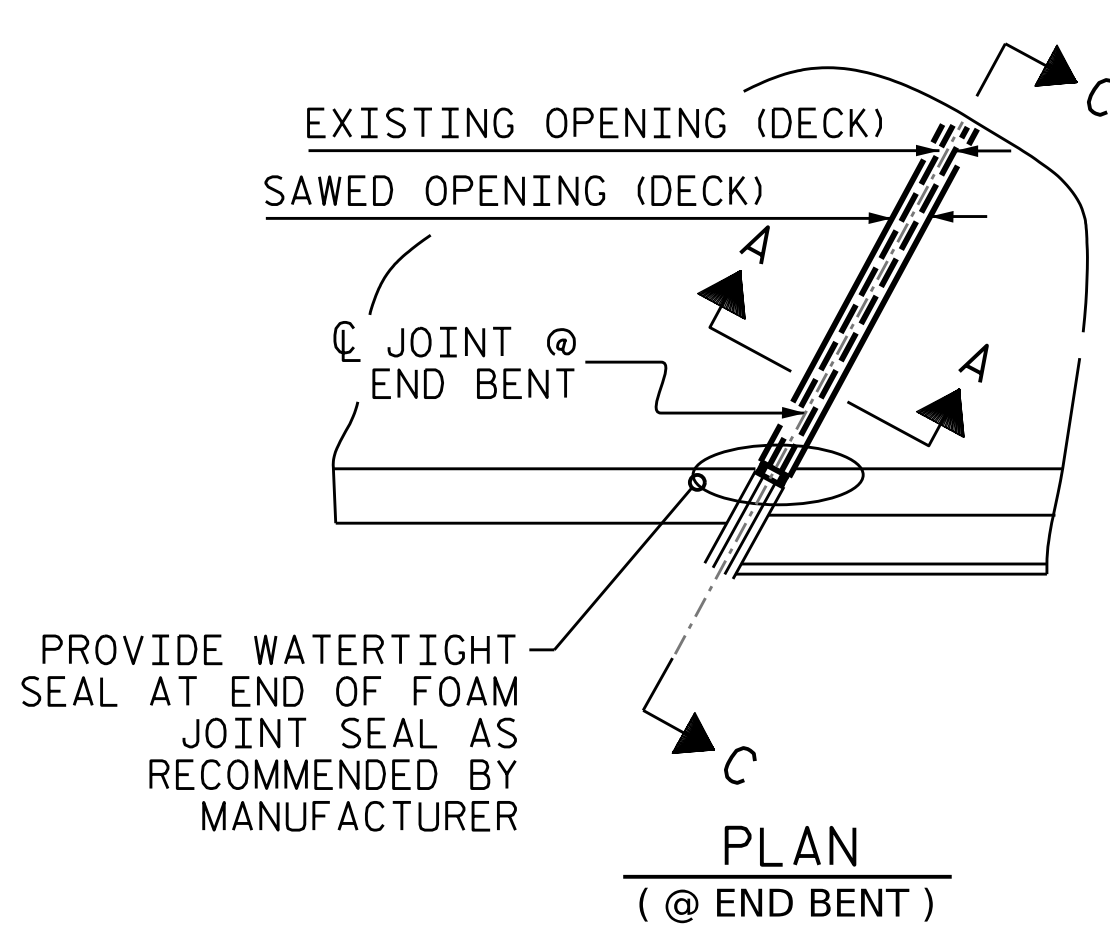
EXISTING JOINT AFTER JOINT DEMOLITION



PROPOSED JOINT PRIOR TO SAWING



PROPOSED FOAM JOINT SEAL



SAWED JOINT OPENING TABLE

LOCATION	SAWED JT. OPENING (PERPENDICULAR TO JT.)		
	AT 45°	AT 60°	AT 90°
END BENT 1	1 5/8"	1 1/6"	1 7/16"
END BENT 2	1 5/8"	1 1/6"	1 7/16"

ELASTOMERIC CONCRETE FOR PRESERVATION

LOCATION	ESTIMATED (CU.FT.)	ACTUAL (CU.FT.)
END BENT 1	12.5	
END BENT 2	12.8	
TOTAL	25.3	

JOINT REPAIR QUANTITY TABLE

	ESTIMATED	ACTUAL
FOAM JOINT SEALS FOR PRESERVATION	101.6 FT.	

BRIDGE JOINT DEMOLITION

LOCATION	ESTIMATE	ACTUAL
END BENT 1	50.0 SQ. FT.	
END BENT 2	51.2 SQ. FT.	
TOTAL	101.2 SQ. FT.	

NOTES

FINAL JOINT SEALS SHALL NOT BE INSTALLED UNTIL THE OVERLAY OR SEALANT WORK IS COMPLETE.

THE CONTRACTOR SHALL FIELD VERIFY THE EXISTING JOINT OPENING PRIOR TO ORDERING JOINT SEAL MATERIAL. IF THE ACTUAL JOINT OPENING VARIES FROM THE OPENING INDICATED IN THE DETAILS BY MORE THAN 1/4", NOTIFY THE ENGINEER.

THE MANUFACTURER IS TO PROVIDE THE NOMINAL UNCOMPRESSED SEAL WIDTH OF THE FOAM JOINT SEAL FOR THE SIZE OF THE OPENING ON THE PLANS AND ACCOMODATE THE MINIMUM EXPANSION SHOWN ON THE PLANS.

FOAM JOINTS SHALL BE INSTALLED AS PER THE MANUFACTURER'S RECOMMENDATIONS.

THE CONTRACTOR SHALL TAKE CARE DURING JOINT REHAB OPERATIONS NOT TO DROP ANY MATERIAL BELOW THE BRIDGE, WITHOUT PROTECTIVE DEVICES BELOW TO CATCH THE MATERIAL. ANY MATERIAL THAT FALLS BELOW THE BRIDGE SHALL BE CONTAINED, REMOVED AND DISPOSED OF BY THE CONTRACTOR AT NO EXTRA COST TO THE DEPARTMENT. IF THE ENGINEER DETERMINES THAT THE PROTECTIVE DEVICES ARE NOT ADEQUATE OR NOT BEING EMPLOYED, THE WORK SHALL BE SUSPENDED UNTIL ADEQUATE PROTECTION IS PROVIDED.

THE CONTRACTOR WILL NOT BE PERMITTED TO FORM THE JOINTS IN LIEU OF SAWING THE JOINT.

THE INSTALLED FOAM JOINTS SHALL BE WATER TIGHT.

FOR FOAM JOINT SEALS FOR PRESERVATION, SEE SPECIAL PROVISIONS.

THE CONTRACTOR SHALL SAW CUT TO A NOMINAL DEPTH OF 1/2" BUT REINFORCING STEEL SHALL NOT BE DAMAGED. CONTRACTOR SHALL REMOVE SURFACE CONCRETE TO VERIFY THAT SAWCUT DEPTH WILL NOT DAMAGE EXISTING REINFORCING STEEL.

QUANTITIES SHOWN IN THE ELASTOMERIC CONCRETE FOR PRESERVATION TABLE ARE BASED ON THE MINIMUM JOINT DEMOLITION SHOWN.

FOR EXCAVATION BELOW THE BOTTOM OF PLANNED JOINT DEMOLITION, CONCRETE FOR DECK REPAIR SHALL BE PLACED IN THE EXCAVATED AREA TO THE ELEVATION AT THE BOTTOM OF THE PROPOSED ELASTOMERIC CONCRETE FOR PRESERVATION HEADERS SHOWN.

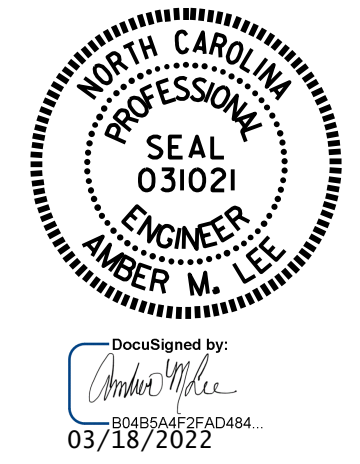
FINAL SURFACE OF THE JOINT DEMOLITION AREA PRIOR TO PLACEMENT OF CONCRETE REPAIR MATERIAL OR ELASTOMERIC CONCRETE SHOULD BE REASONABLY FLAT AND LEVEL. ENGINEER SHALL DETERMINE THE ACCEPTABILITY OF THE SURFACE PRIOR TO PLACEMENT OF REPAIR CONCRETE OR ELASTOMERIC CONCRETE.

FOR BRIDGE JOINT DEMOLITION, SEE SPECIAL PROVISIONS.

FOR ELASTOMERIC CONCRETE FOR PRESERVATION, SEE SPECIAL PROVISIONS.

FOR CONCRETE FOR DECK REPAIR, SEE SPECIAL PROVISIONS.

PROJECT NO. I-5831A
MADISON COUNTY
 BRIDGE NO. 560550



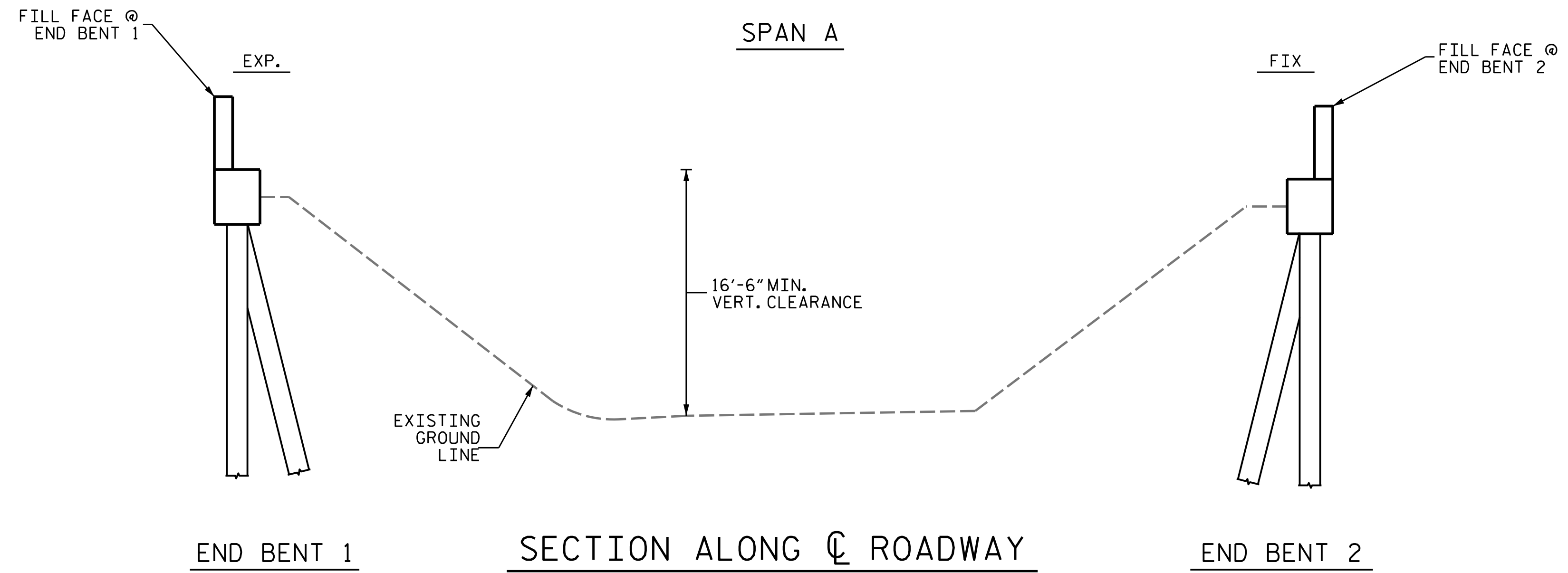
STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH
 STANDARDS

FOAM JOINT SEALS FOR PRESERVATION DETAILS

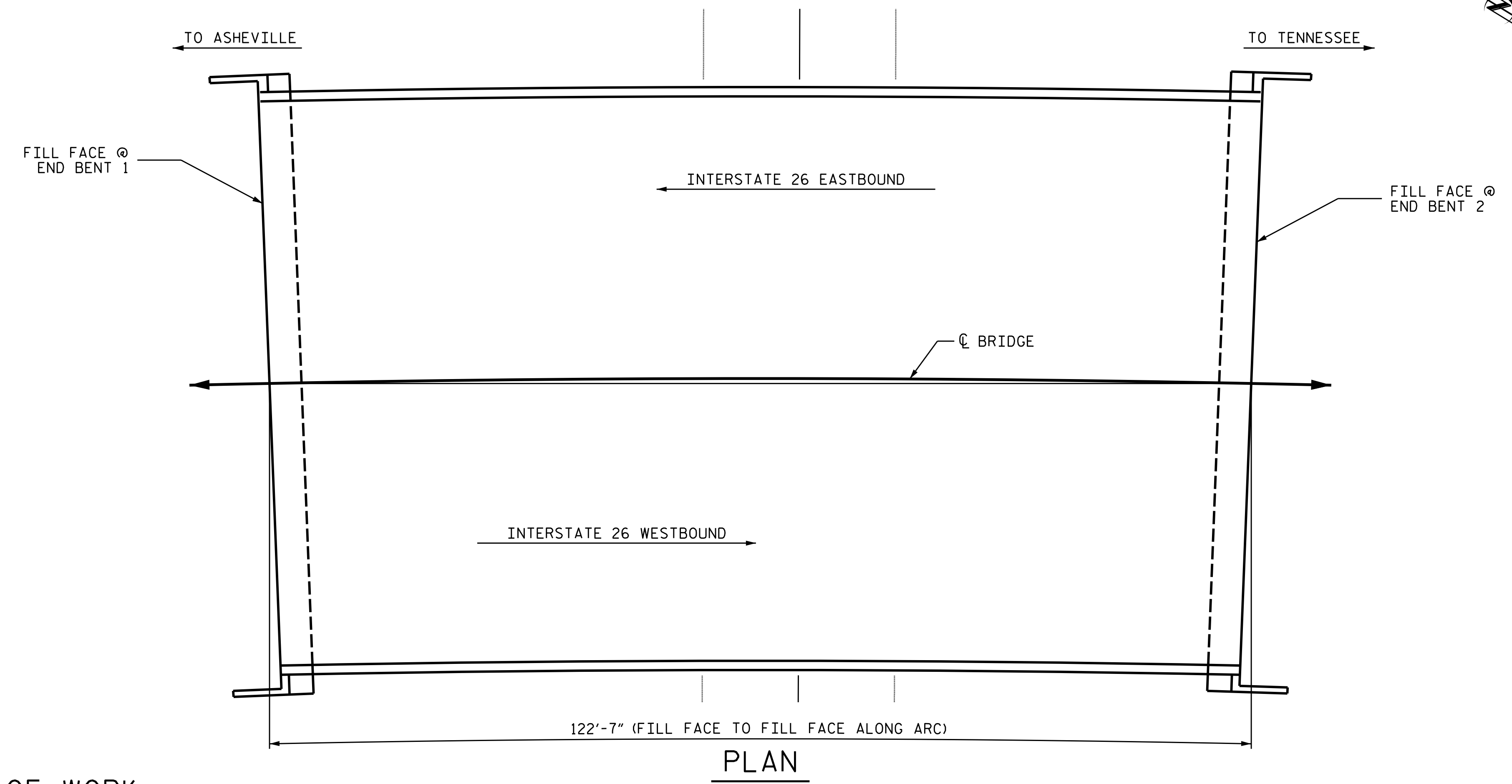
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ASSEMBLED BY : A. M. LEE DATE : 2/2022
 CHECKED BY : H. A. LOCKLEAR DATE : 2/2022
 DRAWN BY : NAP 08/2018
 CHECKED BY : -

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED



NOTES
 GENERAL DRAWING INFORMATION IS TAKEN FROM THE ORIGINAL PLANS AND THE ROUTINE INSPECTION REPORT DATED 08/05/2020.
 BRIDGE ORIENTATION CONFORMS TO THE ORIGINAL BRIDGE PLANS.



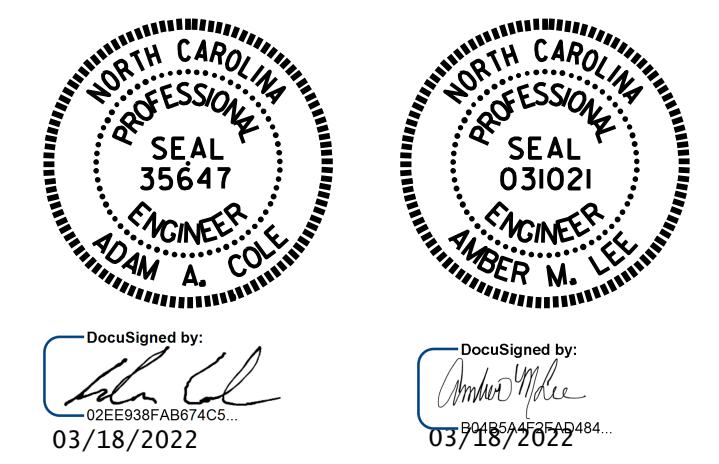
I HEREBY CERTIFY THAT THIS STRUCTURE WAS REHABILITATED ACCORDING TO THESE PLANS OR AS NOTED HEREIN.

 RESIDENT ENGINEER DATE

SCOPE OF WORK

- PARTIALLY REMOVE BRIDGE DECK CONCRETE BY SCARIFICATION AND HYDRO-DEMOLITION METHOD.
- OVERLAY PREPARED TOP OF BRIDGE DECK WITH LATEX MODIFIED CONCRETE - EARLY STRENGTH (LMC-ES)
- REMOVE EXISTING JOINT MATERIALS AND INSTALL FOAM JOINTS.
- GROOVE LMC-ES BRIDGE DECK.
- CLEAN, AND PAINT WEATHERING STEEL BEAM ENDS.
- REMOVE DEBRIS FROM TOP OF END BENT CAPS, AND APPLY EPOXY COATING.
- EPOXY RESIN INJECTION OF CONCRETE CRACKS.
- REMOVE UNSOUND CONCRETE AND PROPERLY PREPARE EXISTING END BENT AREAS FOR SHOTCRETE AND CONCRETE REPAIRS.
- PREFORM SHOTCRETE AND CONCRETE REPAIRS.
- REMOVE UNSOUND CONCRETE AND REBUILD EXISTING APPROACH SLAB CURB.

DRAWN BY : M.ALINAGHIAN DATE : .06/2019
 CHECKED BY : H.A. LOCKLEAR DATE : .02/2022



PROJECT NO. I-5831A
 MADISON COUNTY
 BRIDGE NO. 560551

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH
GENERAL DRAWING
 FOR BRIDGE ON
 INTERSTATE 26
 OVER SR 1346
 (BEAR BRANCH ROAD)

REVISIONS						SHEET NO.
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