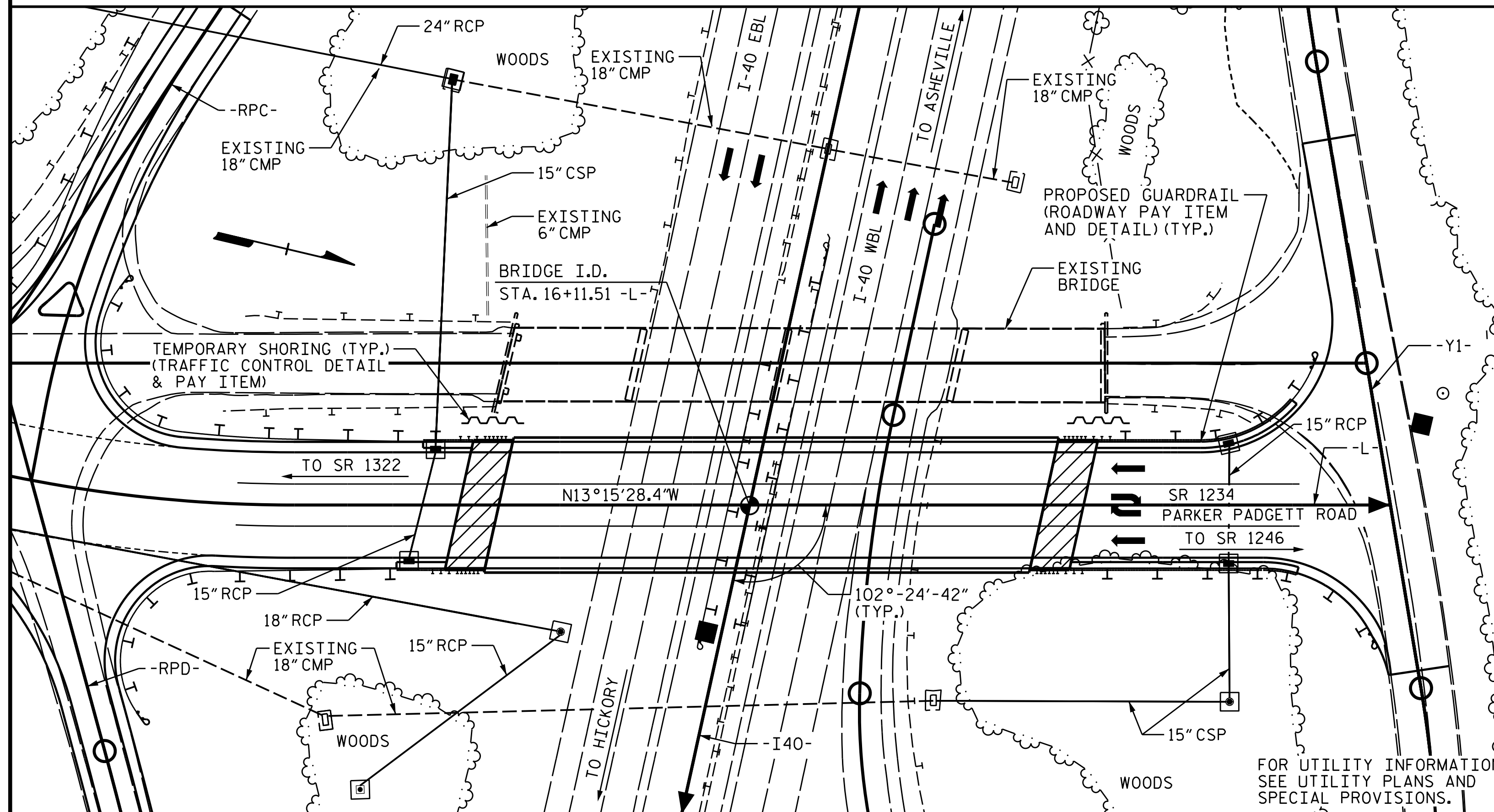


BM #2: R.R. SPIKE IN BASE OF 24" OAK, 33.53' LEFT OF STA. 15+61.27 -Y1-, ELEV. 1395.50



LOCATION SKETCH

NOTES

ASSUMED LIVE LOAD = HL-93 OR ALTERNATE LOADING

THIS BRIDGE HAS BEEN DESIGNED IN ACCORDANCE WITH THE REQUIREMENTS OF THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 8TH EDITION.

THIS BRIDGE IS LOCATED IN SEISMIC ZONE 1.

FOR OTHER DESIGN DATA AND GENERAL NOTES, SEE SHEET SN.

FOR SUBMITTAL OF WORKING DRAWINGS, SEE SPECIAL PROVISIONS.

FOR FALSEWORK AND FORMWORK, SEE SPECIAL PROVISIONS.

FOR CRANE SAFETY, SEE SPECIAL PROVISIONS.

FOR GROUT FOR STRUCTURES, SEE SPECIAL PROVISIONS.

THE ELEVATIONS AND CLEARANCES SHOWN ON THE PLANS AT THE POINTS OF MINIMUM VERTICAL CLEARANCE ARE FROM THE BEST INFORMATION AVAILABLE. PRIOR TO BEGINNING BRIDGE CONSTRUCTION, VERIFY THE ELEVATIONS ON THE EXISTING PAVEMENT AND CHECK THE CLEARANCE. REPORT ANY VARIATIONS TO THE ENGINEER. ANY PLAN REVISIONS NECESSARY TO ACHIEVE THE REQUIRED MINIMUM VERTICAL CLEARANCE WILL BE PROVIDED BY THE DEPARTMENT.

FOR MAINTENANCE AND PROTECTION OF TRAFFIC BENEATH PROPOSED STRUCTURE, SEE SPECIAL PROVISIONS.

REMOVABLE FORMS MAY BE USED IN LIEU OF METAL STAY-IN-PLACE FORMS IN ACCORDANCE WITH ARTICLE 420-3 OF THE STANDARD SPECIFICATIONS.

NEEDLE BEAMS WILL NOT BE ALLOWED UNLESS OTHERWISE CALLED FOR ON THE PLANS OR APPROVED BY THE ENGINEER.

ALL STRUCTURAL STEEL SHALL BE AASHTO M270 GRADE 50W AND PAINTED IN ACCORDANCE WITH SYSTEM 5 OR SYSTEM 6 OF THE STRUCTURAL STEEL COATINGS PROGRAM AND SECTION 442-8 OF THE STANDARD SPECIFICATIONS UNLESS OTHERWISE NOTED ON THE PLANS.

INASMUCH AS THE PAINT SYSTEM ON THE EXISTING STRUCTURAL STEEL CONTAINS LEAD, THE CONTRACTOR'S ATTENTION IS DIRECTED TO ARTICLE 107-1 OF THE STANDARD SPECIFICATIONS. ANY COSTS RESULTING FROM COMPLIANCE WITH APPLICABLE STATE OR FEDERAL REGULATIONS PERTAINING TO HANDLING OF MATERIALS CONTAINING LEAD BASED PAINT SHALL BE INCLUDED IN THE BID PRICE FOR "REMOVAL OF EXISTING STRUCTURE AT STATION 16+11.51 -L-".

THE CLASS AA CONCRETE IN THE BRIDGE DECK SHALL CONTAIN FLY ASH OR GROUND GRANULATED BLAST FURNACE SLAG AT THE SUBSTITUTION RATE SPECIFIED IN ARTICLE 1024-1 AND IN ACCORDANCE WITH ARTICLES 1024-5 AND 1024-6 OF THE STANDARD SPECIFICATIONS. NO PAYMENT WILL BE MADE FOR THIS SUBSTITUTION AS IT IS CONSIDERED INCIDENTAL TO THE COST OF THE REINFORCED CONCRETE DECK SLAB.

THE MATERIAL SHOWN IN THE CROSS-HATCHED AREA SHALL BE EXCAVATED FOR A DISTANCE OF APPROXIMATELY 30 FT LEFT OF CENTERLINE ROADWAY AND 40 FT RIGHT OF CENTERLINE ROADWAY AS DIRECTED BY THE ENGINEER. THIS WORK WILL BE PAID FOR AT THE CONTRACT LUMP SUM PRICE FOR UNCLASSIFIED STRUCTURE EXCAVATION. SEE SECTION 412 OF THE STANDARD SPECIFICATIONS.

TEMPORARY SHORING WILL BE REQUIRED IN THE AREAS INDICATED IN THE PLAN VIEW.

FOR LIMITS OF TEMPORARY SHORING FOR MAINTENANCE OF TRAFFIC, SEE TRAFFIC CONTROL PLANS. FOR PAY ITEM FOR TEMPORARY SHORING FOR MAINTENANCE OF TRAFFIC, SEE ROADWAY PLANS.

THE EXISTING STRUCTURE CONSISTING OF 4 SPANS (1 @ 50'-6"±, 1 @ 55'-0"±, 1 @ 67'-0"±, 1 @ 56'-9"±) WITH A CLEAR ROADWAY OF APPROXIMATELY 25'-0" AND REINFORCED CONCRETE FLOOR SUPPORTED BY STEEL GIRDERS ON REINFORCED CONCRETE END BENTS AND INTERIOR BENTS LOCATED APPROXIMATELY 14'-0" FROM THE PROPOSED STRUCTURE SHALL BE REMOVED. THE EXISTING BRIDGE IS PRESENTLY NOT POSTED FOR LOAD LIMIT. SHOULD THE STRUCTURAL INTEGRITY OF THE BRIDGE DETERIORATE DURING CONSTRUCTION OF THE PROPOSED BRIDGE, A LOAD LIMIT MAY BE POSTED AND MAY BE REDUCED AS FOUND NECESSARY DURING THE LIFE OF THE PROJECT.

THE SUBSTRUCTURE OF THE EXISTING BRIDGE INDICATED ON THE PLANS IS FROM THE BEST INFORMATION AVAILABLE. THIS INFORMATION IS SHOWN FOR THE CONVENIENCE OF THE CONTRACTOR. THE CONTRACTOR SHALL HAVE NO CLAIM WHATSOEVER AGAINST THE DEPARTMENT OF TRANSPORTATION FOR ANY DELAYS OR ADDITIONAL COST INCURRED BASED ON DIFFERENCES BETWEEN THE EXISTING BRIDGE SUBSTRUCTURE SHOWN ON THE PLANS AND THE ACTUAL CONDITIONS AT THE PROJECT SITE.

FOR EROSION CONTROL MEASURES, SEE EROSION CONTROL PLANS.

THE LOCATION OF THE CONSTRUCTION JOINT IN THE DRILLED PIERS IS BASED ON AN APPROXIMATE GROUND LINE ELEVATION. IF THE CONSTRUCTION JOINT IS ABOVE THE ACTUAL GROUND ELEVATION, THE CONTRACTOR SHALL PLACE THE CONSTRUCTION JOINT 1 FT. BELOW THE GROUND LINE.

FOR ASBESTOS ASSESSMENT FOR BRIDGE DEMOLITION AND RENOVATION ACTIVITIES, SEE SPECIAL PROVISIONS.

TOTAL BILL OF MATERIAL

	REMOVAL OF EXISTING STRUCTURE AT STA. 16+11.51 -L-	ASBESTOS ASSESSMENT	4'-0" Ø DRILLED PIERS IN SOIL	4'-0" Ø DRILLED PIERS NOT IN SOIL	SID INSPECTION	CSL TESTING	UNCLASSIFIED STRUCTURE EXCAVATION AT 16+11.51 -L-	REINFORCED CONCRETE DECK SLAB
	LUMP SUM	LUMP SUM	LIN. FT.	LIN. FT.	EACH	EACH	LUMP SUM	SQ.FT.
SUPERSTRUCTURE								10,598
END BENT NO.1							LUMP SUM	
BENT NO.1			67.5	26.0	1	1		
END BENT NO.2								
TOTAL	LUMP SUM	LUMP SUM	67.5	26.0	1	1	LUMP SUM	10,598

TOTAL BILL OF MATERIAL

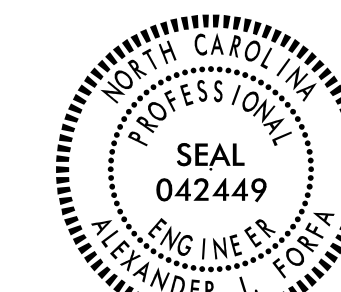
	GROOVING BRIDGE FLOORS	CLASS A CONCRETE	BRIDGE APPROACH SLABS	REINFORCING STEEL	SPIRAL COLUMN REINFORCING STEEL	APPROX. 307,000 LBS. STRUCTURAL STEEL	PILE DRIVING EQUIPMENT SETUP FOR HP12X53 STEEL PILES	HP12X53 STEEL PILES		CONCRETE BARRIER RAIL	4" SLOPE PROTECTION	DISC BEARINGS	ELASTOMERIC BEARINGS	FOAM JOINT SEALS
	SQ.FT.	CU.YDS.	LUMP SUM	LBS.	LBS.	LUMP SUM	EACH	NO.	LIN.FT.	LIN.FT.	SO. YDS.	LUMP SUM	LUMP SUM	LUMP SUM
SUPERSTRUCTURE	10,580		LUMP SUM			LUMP SUM				413.6		LUMP SUM	LUMP SUM	LUMP SUM
END BENT NO.1		48.5		7,566			9	9	383		260			
BENT NO.1		59.3		15,873	2,302									
END BENT NO.2		46.0		7,291			9	9	248		410			
TOTAL	10,580	153.8	LUMP SUM	30,730	2,302	LUMP SUM	18	18	631	413.6	670	LUMP SUM	LUMP SUM	LUMP SUM

PROJECT NO. BR-0033
McDOWELL COUNTY
 STATION: 16+11.51 -L-

SHEET 3 OF 3

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

GENERAL DRAWING
 LOCATION SKETCH



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DOCUMENT NOT CONSIDERED
 FINAL UNLESS ALL
 SIGNATURES COMPLETED

DRAWN BY : T. STUMP DATE : 01/2021
 CHECKED BY : A. FORFA DATE : 06/2021
 DESIGN ENGINEER OF RECORD : A. FORFA DATE : 09/2021

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S-3
1			3			TOTAL SHEETS
2			4			32