

Map 1 US 158  
 From Forsyth County Line to NC 65  
 Widen 2 feet  
 1½" S9.5C, #67 Mat.  
 From NC 65 to NC 68  
 Mill 1½" depth  
 pave back 1½" S 9.5C

Map 29 SR 4439 - Lees Glenn Rd  
 Map 30 SR 4441 - Lees Ridge Rd  
 Map 31 SR 4440 - W Glenn Rd  
 Map 32 SR 4482 - Charles Place Dr  
 Map 33 SR 4642 - Misty Meadow Dr

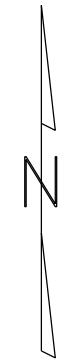
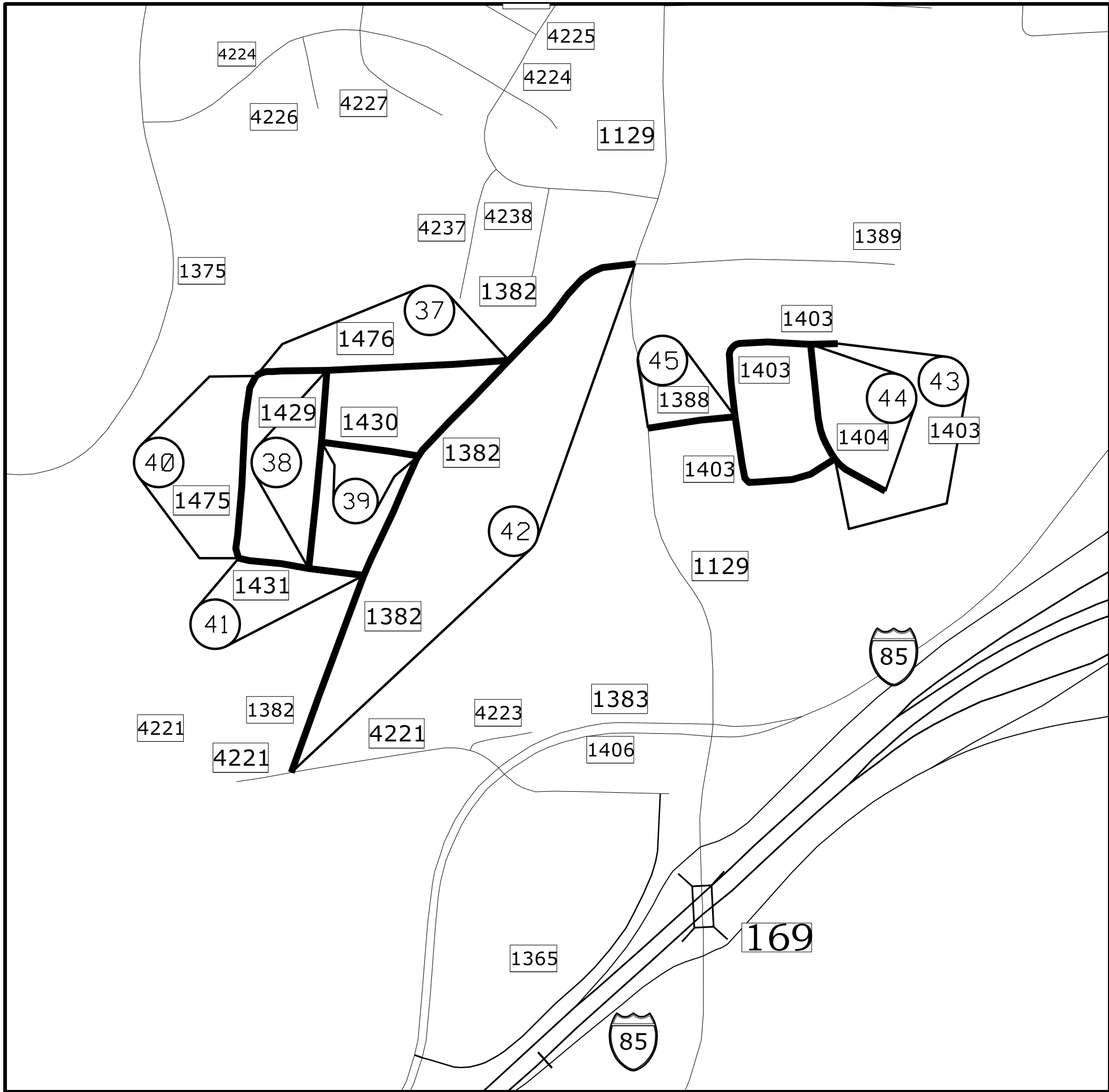
Map 34 SR 2054 - Shilling St / Stokesdale St  
 Skip US 158 tie into new pavement.

Map 35 SR 2053 - Newberry St  
 Tie into new pavement at US 158.

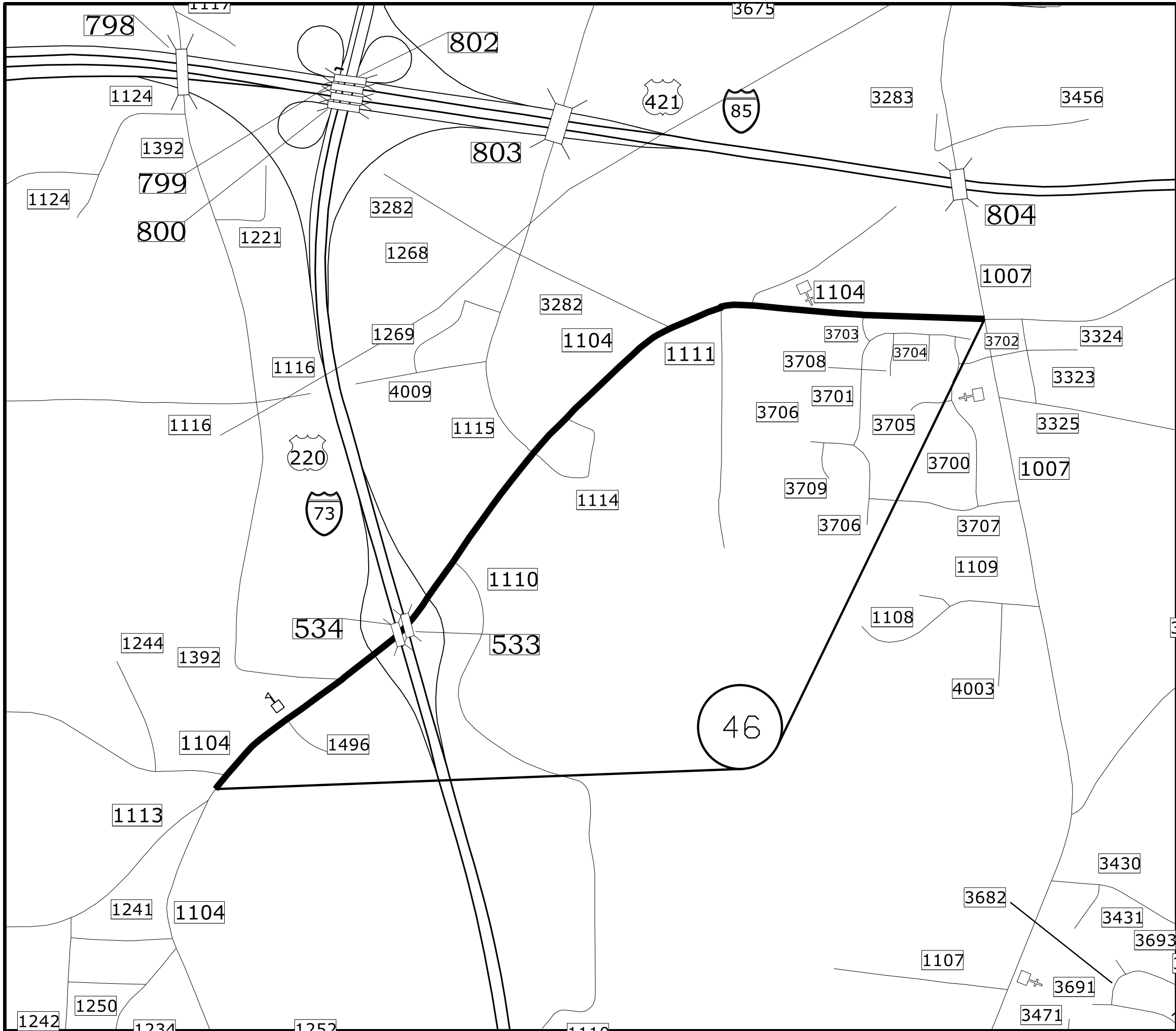
Map 36 SR 2057 - Ivan St  
 Mill 1¼" depth pave back 1¼" S9.5B  
 Tie into new pavement at US 158.







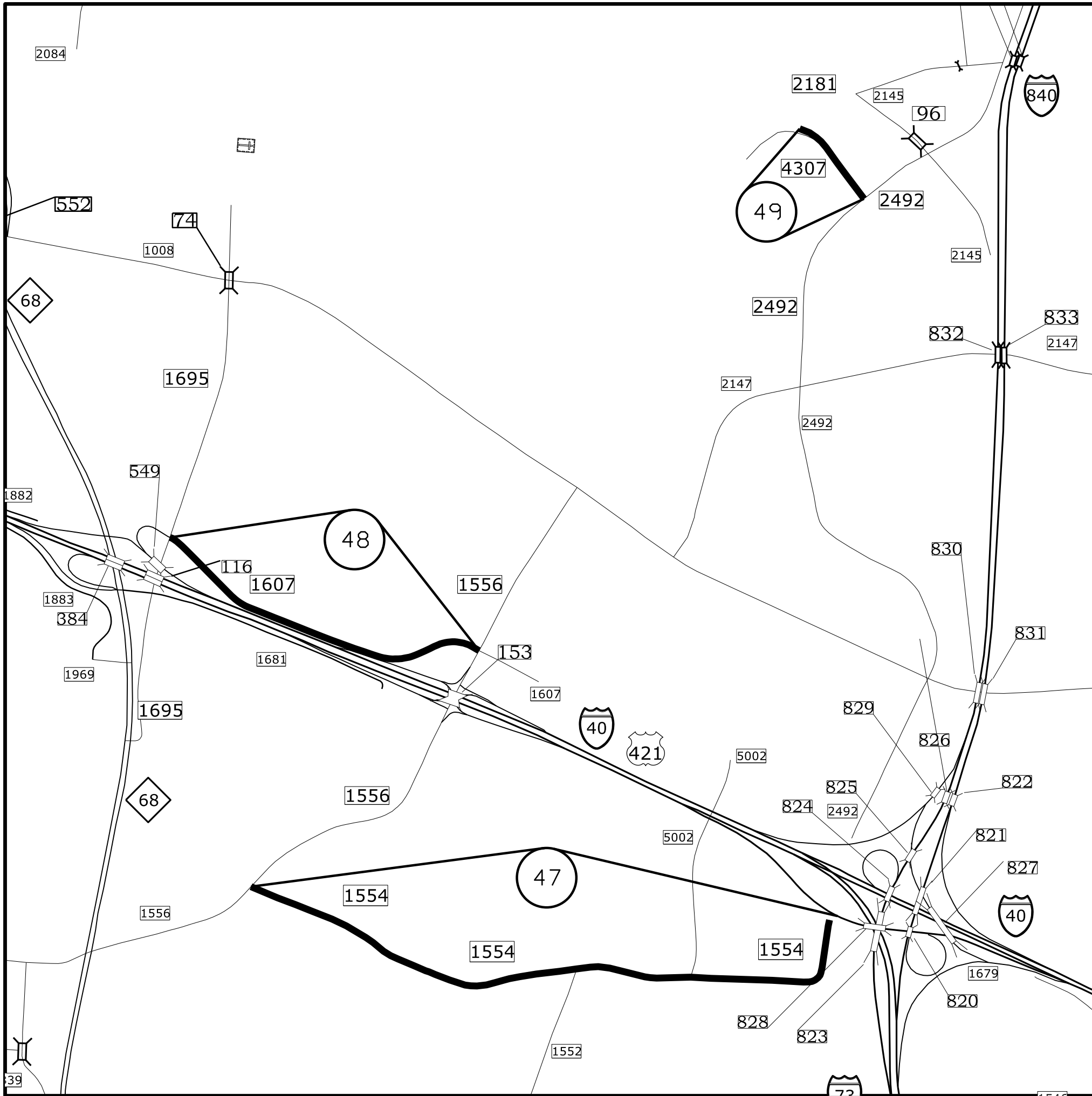
- Map 37 SR 1476 - Farlin Av
- Map 38 SR 1429 - Rose Dr
- Map 39 SR 1430 - Coral Av
- Map 40 SR 1475 - Dewberry Dr
- Map 41 SR 1431 - Crane Av
- Map 42 SR 1382 - Rose Lake Dr
  
- Map 43 SR 1403 - S Fremont Dr  
Mill 2 1/4" depth pave back 1 1/4"  
to match concrete at gutter.
  
- Map 44 SR 1404 - Adamson Rd  
Mill 2 1/4" depth pave back 1 1/4"  
to match concrete at gutter.
  
- Map 45 SR 1388 - Sedgefield Park Dr  
Mill 2 1/4" depth pave back 1 1/4"  
to match concrete at gutter.



Map 46 SR 1104 - Old Randleman Rd  
Begin 1 1/2" Depth Mill and Fill with 1 1/2" S9.5C at the joint near Kivett Dr. and carry it through the I-73 interchange to the west side of Friendly Farms Rd. SR 1110, Tie to the joints on ramps. Maintain Bridge Clearance at # 533 and # 534 at I-73.

Begin 2 foot widening and #78 Seal with 1 1/2" S9.5C at Friendly Farms road SR 1110 and carry it to the curb and gutter near Blackberry Road SR 1111.

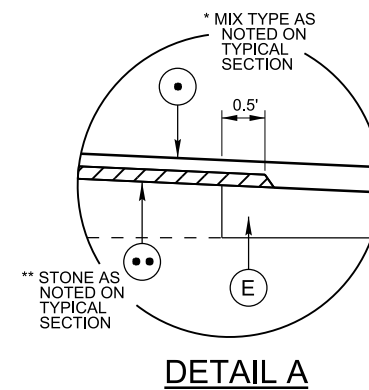
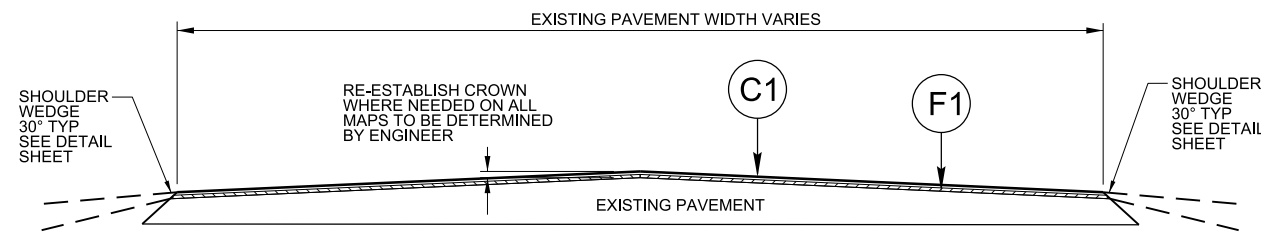
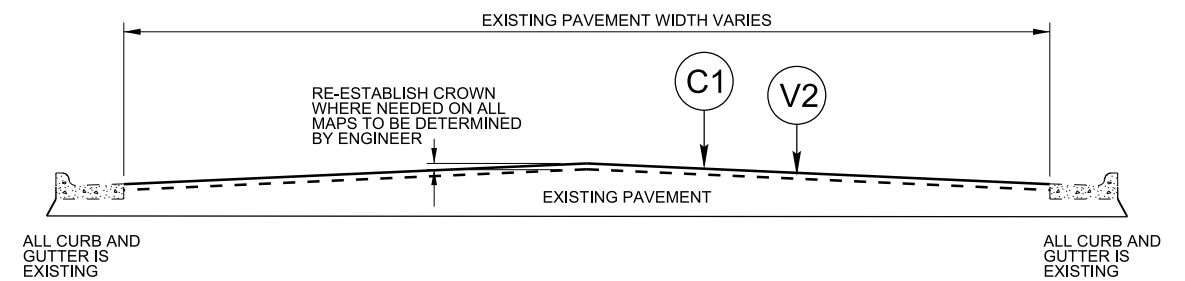
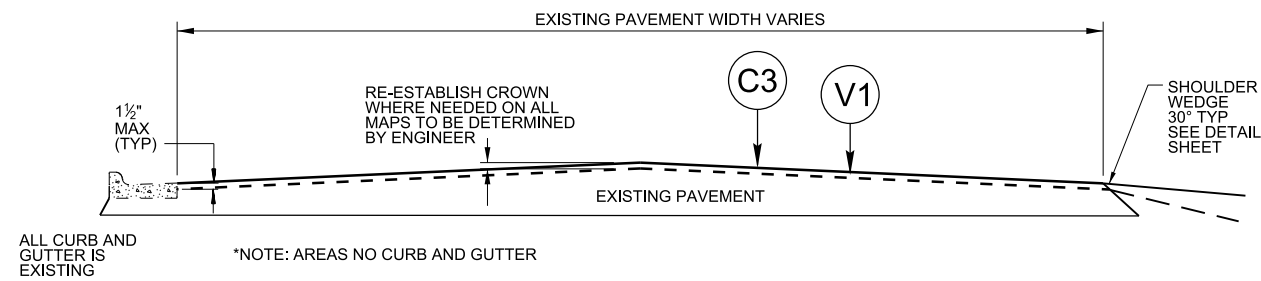
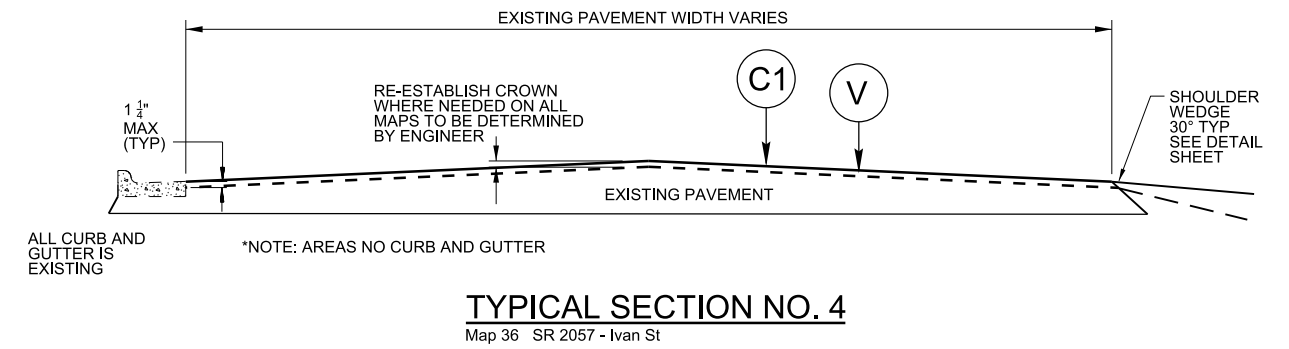
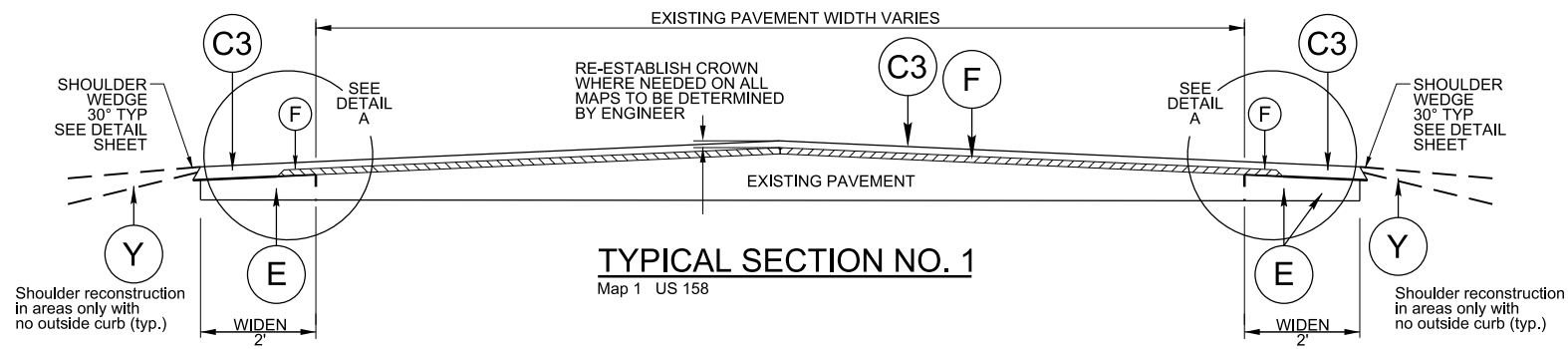
Begin 1 1/2" Mill and Fill at the end of curb and gutter near Blackberry Rd SR 1111 and carry it to the joint at Randalman Rd. SR 1007.



Map 47 SR 1554 - S Chimney Rock Rd  
Mill 1 1/2" depth pave back 1 1/2" S9.5C

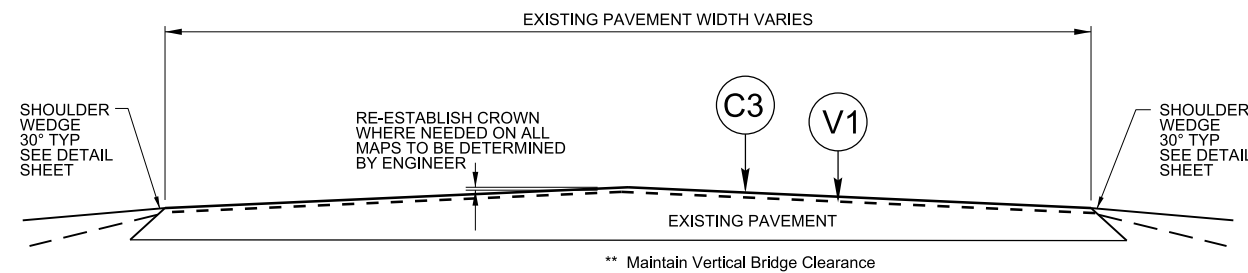
Map 48 SR 1607 - Burnt Poplar Rd  
Mill 1 1/2" depth pave back 1 1/2" S9.5C

Map 49 SR 4307 - Radar Rd  
1 1/2" S9.5C #78

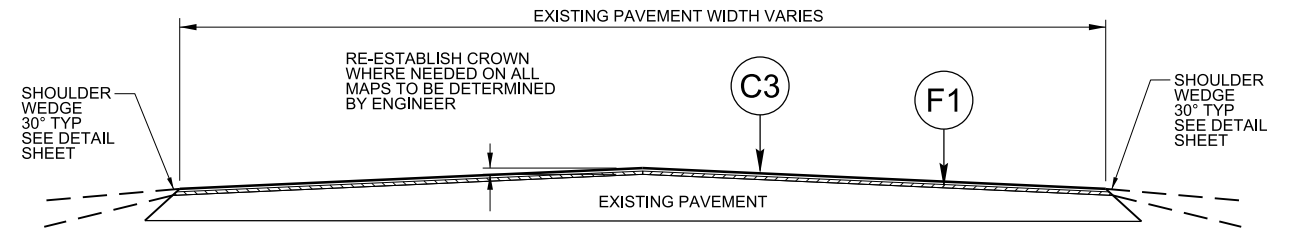


Map 2	SR 4643 - Brotherstwo Rd	Map 22	SR 4035 - Millstaff Dr
Map 3	SR 4429 - Caffey Dr	Map 23	SR 4476 - Robdot Dr
Map 4	SR 4431 - Cross Pond Dr	Map 24	SR 2269 - Alcorn Rd
Map 5	SR 4427 - Dinger Dr		Resurface Bridge #92
Map 6	SR 4428 - Eggers Way		Skip Bridge #1153 DO NOT Pave
Map 7	SR 4432 - Masons Pond Dr	Map 25	SR 5008 - Crutchfield Farm Rd
Map 8	SR 4430 - Quail Creek Ct	Map 26	SR 2299 - Fogleman Wy
Map 9	SR 4426 - Quail Creek Dr	Map 27	SR 2129 - Fogleman Rd
Map 10	SR 4623 - Ballard Farm Rd	Map 28	SR 5027 - Ives Dr
Map 11	SR 4624 - Ballard Farm Ct	Map 29	SR 4439 - Lees Glenn Rd
Map 12	SR 4625 - Barnwood Ct	Map 30	SR 4441 - Lees Ridge Rd
Map 13	SR 4626 - Burchlawn Ct	Map 31	SR 4440 - W Glenn Rd
Map 14	SR 4627 - Berrywood Ct	Map 32	SR 4432 - Charles Place Dr
Map 15	SR 4637 - Ridgeline Dr	Map 33	SR 4642 - Misty Meadow Dr
Map 16	SR 4647 - Sawtooth Ct	Map 34	SR 2054 - Shilling St / Stokesdale Ct
Map 17	SR 4469 - Hollow Hill Rd	Map 35	SR 2053 - Newberry St
Map 18	SR 4666 - Hollow Hill Rd	Map 37	SR 1476 - Farlin Av
Map 19	SR 4667 - Larchwood Dr	Map 38	SR 1429 - Rose Dr
Map 20	SR 4477 - Banager Rd	Map 39	SR 1430 - Coral Av
Map 21	SR 4036 - Leadenhall/Millstaff Dr	Map 40	SR 1475 - Dewberry Dr
		Map 41	SR 1431 - Crane Av
		Map 42	SR 1382 - Rose Lake Dr

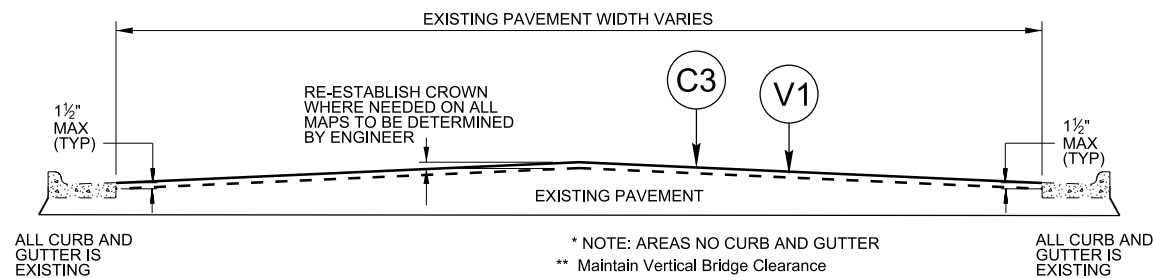
PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B TO BE APPLIED AT AN AVERAGE RATE OF 137.5 LBS PER SQ YD.
C2	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, TO BE APPLIED AT AN AVERAGE RATE OF 165 LBS PER SQ YD.
C3	PROP. APPROX. 1 3/4" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C TO BE APPLIED AT AN AVERAGE RATE OF 168 LBS PER SQ YD.
D	PROP. APPROX. 4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YD.
E	PROP. APPROX. 8" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YD. IN EACH OF TWO LAYERS.
F	AST MAT COAT, #67
F1	AST MAT COAT, #78M
U	EXISTING PAVEMENT
V	MILL ASPHALT PAVEMENT, 1 1/4" DEPTH
V1	MILL ASPHALT PAVEMENT, 1 1/2" DEPTH
V2	MILL ASPHALT PAVEMENT, 2 1/4" DEPTH
V3	MILL ASPHALT PAVEMENT, 4" DEPTH
Y	SHOULDER RECONSTRUCTION (SEE DETAIL)



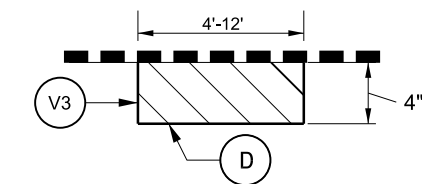
**TYPICAL SECTION NO. 6**  
 \*\* Map 46 SR 1104 - Old Randleman Rd  
 Map 47 SR 1554 - Chimney Rock Rd  
 Map 48 SR 1607 - Burnt Poplar Rd



**TYPICAL SECTION NO. 10**  
 Map 49 SR 4307 - Radar Rd

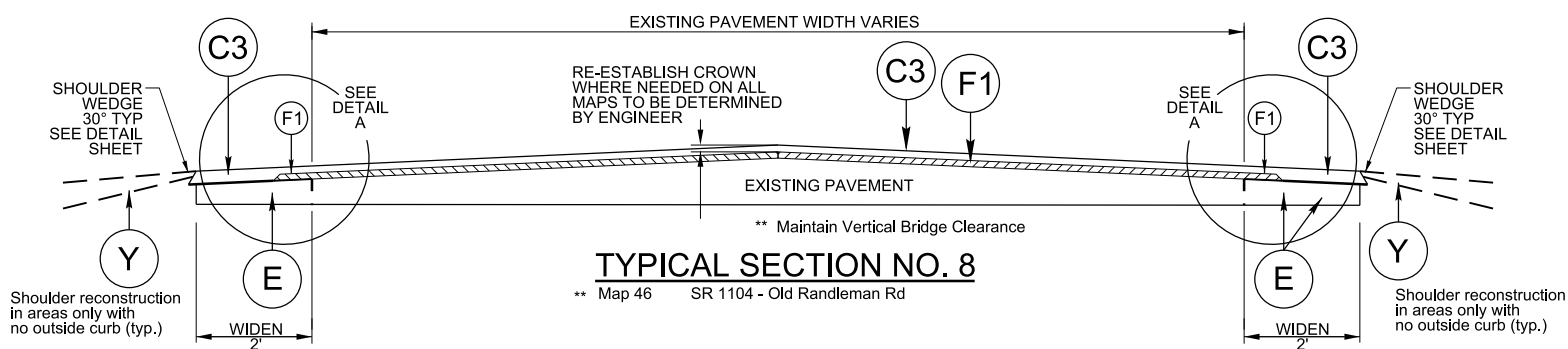


**TYPICAL SECTION NO. 7**  
 \*\* Map 46 SR 1104 - Old Randleman Rd  
 Map 47 SR 1554 - Chimney Rock Rd  
 Map 48 SR 1607 - Burnt Poplar Rd

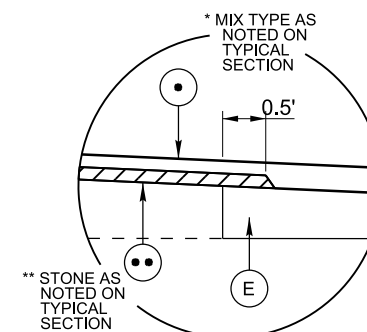


MILL FILL WITH INTERMEDIATE COURSE, TYPE I19.0C AT LOCATIONS AS DIRECTED BY THE ENGINEER.

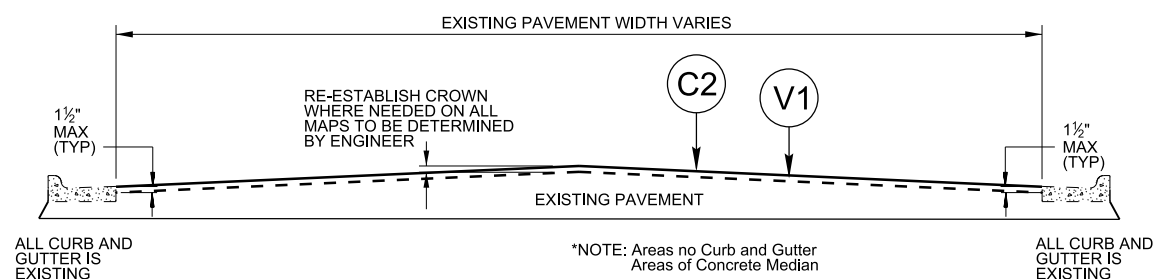
**PATCHING EXISTING PAVEMENT DETAIL**



**TYPICAL SECTION NO. 8**  
 \*\* Map 46 SR 1104 - Old Randleman Rd



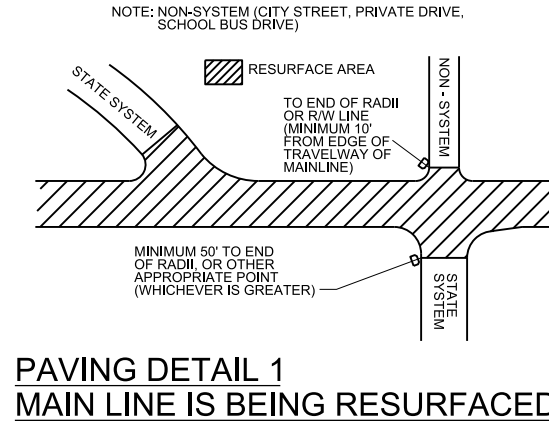
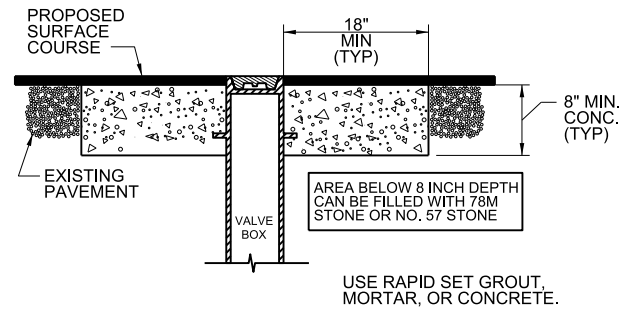
**DETAIL A**



**TYPICAL SECTION NO. 9**  
 Map 50 SR 2137 - Old Oak Ridge Rd

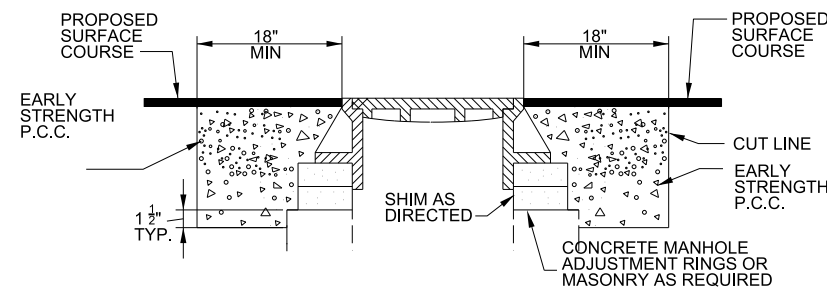
PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B TO BE APPLIED AT AN AVERAGE RATE OF 137.5 LBS PER SQ YD.
C2	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, TO BE APPLIED AT AN AVERAGE RATE OF 165 LBS PER SQ YD.
C3	PROP. APPROX. 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C TO BE APPLIED AT AN AVERAGE RATE OF 168 LBS PER SQ YD.
D	PROP. APPROX. 4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YD.
E	PROP. APPROX. 8" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YD. IN EACH OF TWO LAYERS.
F	AST MAT COAT, #67
F1	AST MAT COAT, #78M
U	EXISTING PAVEMENT
V	MILL ASPHALT PAVEMENT, 1 1/4" DEPTH
V1	MILL ASPHALT PAVEMENT, 1 1/2" DEPTH
V2	MILL ASPHALT PAVEMENT, 2 1/4" DEPTH
V3	MILL ASPHALT PAVEMENT, 4" DEPTH
Y	SHOULDER RECONSTRUCTION (SEE DETAIL)





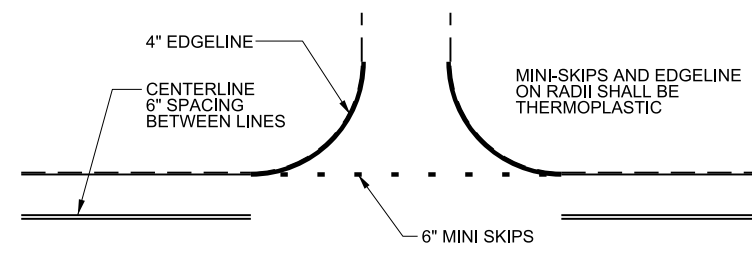
**PAVING DETAIL 2  
MAIN LINE NOT BEING RESURFACED**

**STANDARD CONCRETE ENCASEMENT FOR VALVE CASTINGS IN PAVEMENT**

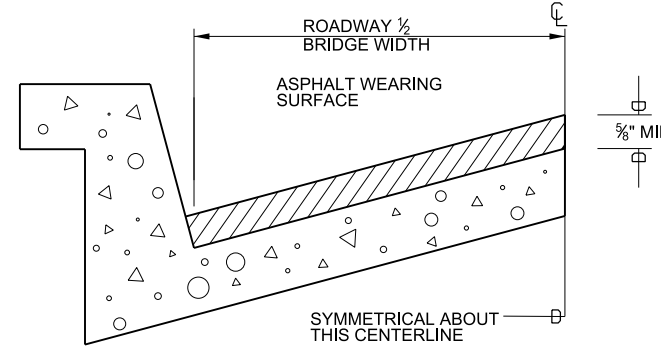


- NOTES:
- MORTAR SHALL BE MIXED TO NCDOT SPECIFICATIONS.
  - ALL FAULTY EXISTING BRICKWORK TO BE REMOVED AND REPLACED WITH NEW BRICK MASONRY.
  - EXCAVATION FOR THE ADJUSTMENT SHALL BE SHEER CUT ON ALL SIDES.
  - RAPID SET GROUT, MORTAR, OR CONCRETE SHALL BE USED CLASS B CONCRETE MAY BE USED WHEN ADJUSTMENTS ARE NOT IN THE TRAVEL LANE.

**STANDARD CONCRETE ENCASEMENT FOR MANHOLE CASTINGS IN PAVEMENT**

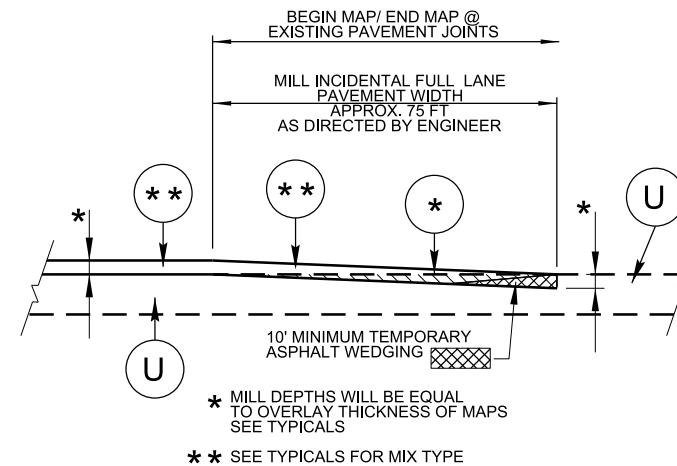


**TO BE USED AT ALL  
NON-SIGNALIZED INTERSECTIONS  
(NOT TO SCALE)**

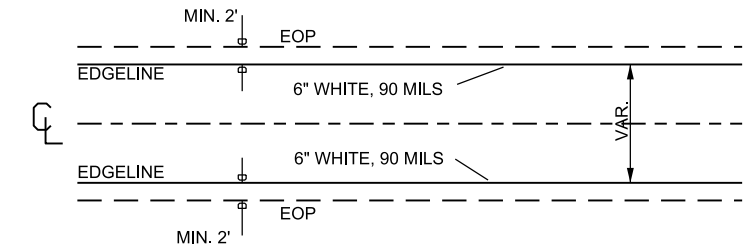


**BRIDGE HALF TYPICAL SECTION**

FOR BRIDGES WITH FLOOR DRAINS, CARE SHALL BE EXERCISED IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE. ALL DRAINS SHALL BE LEFT OPEN. THE PROPOSED WEARING SURFACE SHALL VARY IN THICKNESS AS NECESSARY TO PROVIDE A SMOOTH RIDING SURFACE. A THICKNESS OF NOT LESS THAN 5/8" SHALL BE PROVIDED. THE MAXIMUM THICKNESS SHALL PREFERABLY BE 1-1/2" UNLESS IT IS IMPRACTICAL TO PROVIDE A SMOOTH RIDING SURFACE OTHERWISE.



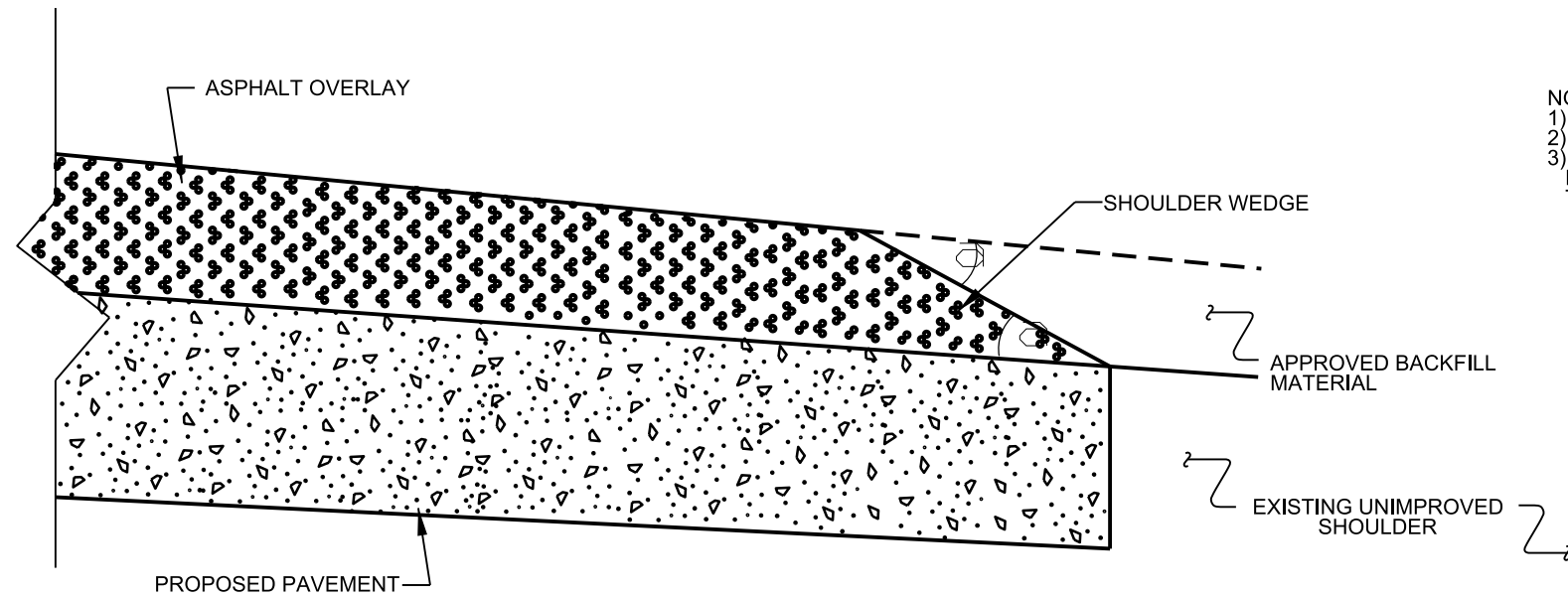
**INCIDENTAL MILLING AT TIE-IN DETAIL**



- NOTE:
- TO BE USED IN CONJUNCTION WITH MAP 3 NC 150.
  - USE IN CONJUNCTION WITH THE EXISTING PAVEMENT MARKINGS TO ESTABLISH THE STRIPING.
  - USE IN CONJUNCTION WITH THE NCDOT STANDARD DRAWINGS.

**STRIPING DETAIL 1  
GENERAL STRIPING DETAIL FOR ENTIRE PROJECT**

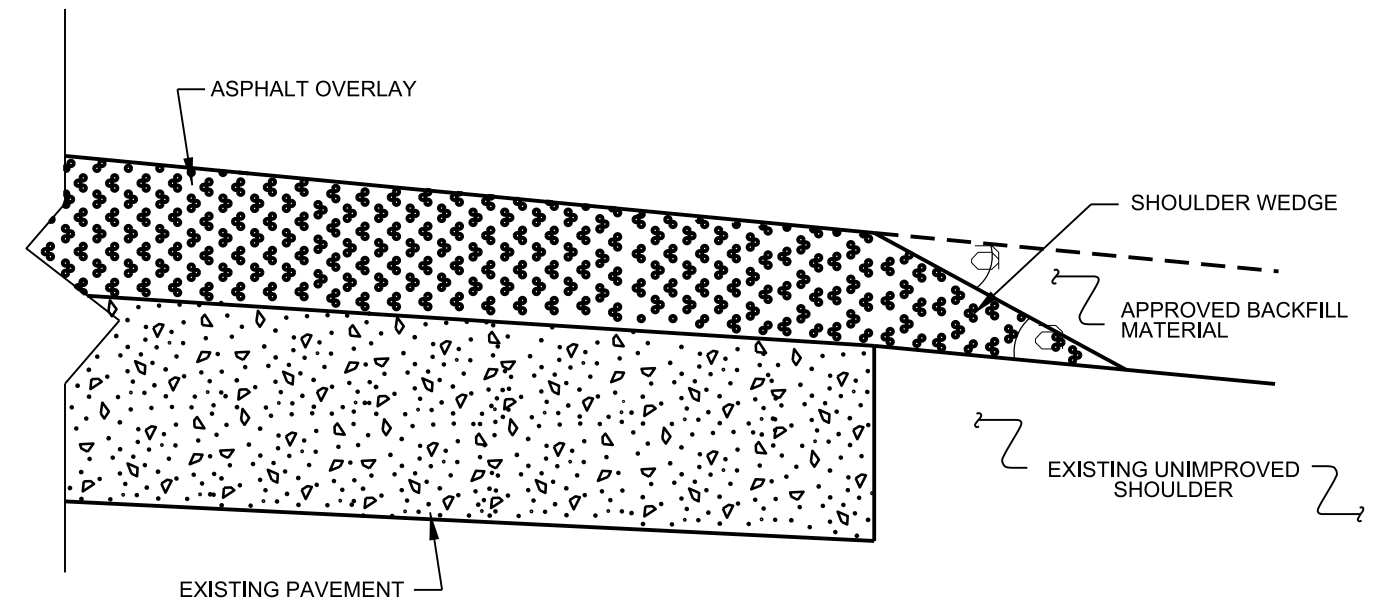
PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B TO BE APPLIED AT AN AVERAGE RATE OF 137.5 LBS PER SQ YD.
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F	AST MAT COAT, #67
F1	AST MAT COAT, #78M
U	EXISTING PAVEMENT
V	MILL ASPHALT PAVEMENT, 1 1/4" DEPTH
V1	MILL ASPHALT PAVEMENT, 1 1/2" DEPTH
V2	MILL ASPHALT PAVEMENT, 2 1/4" DEPTH
V3	MILL ASPHALT PAVEMENT, 4" DEPTH
Y	SHOULDER RECONSTRUCTION (SEE DETAIL)



NOTES:  
 1) DETAIL DOES NOT APPLY TO OGAFC AND ULTRA-THIN BONDED WEARING COURSE.  
 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.  
 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS DIRECTED BY THE ENGINEER.

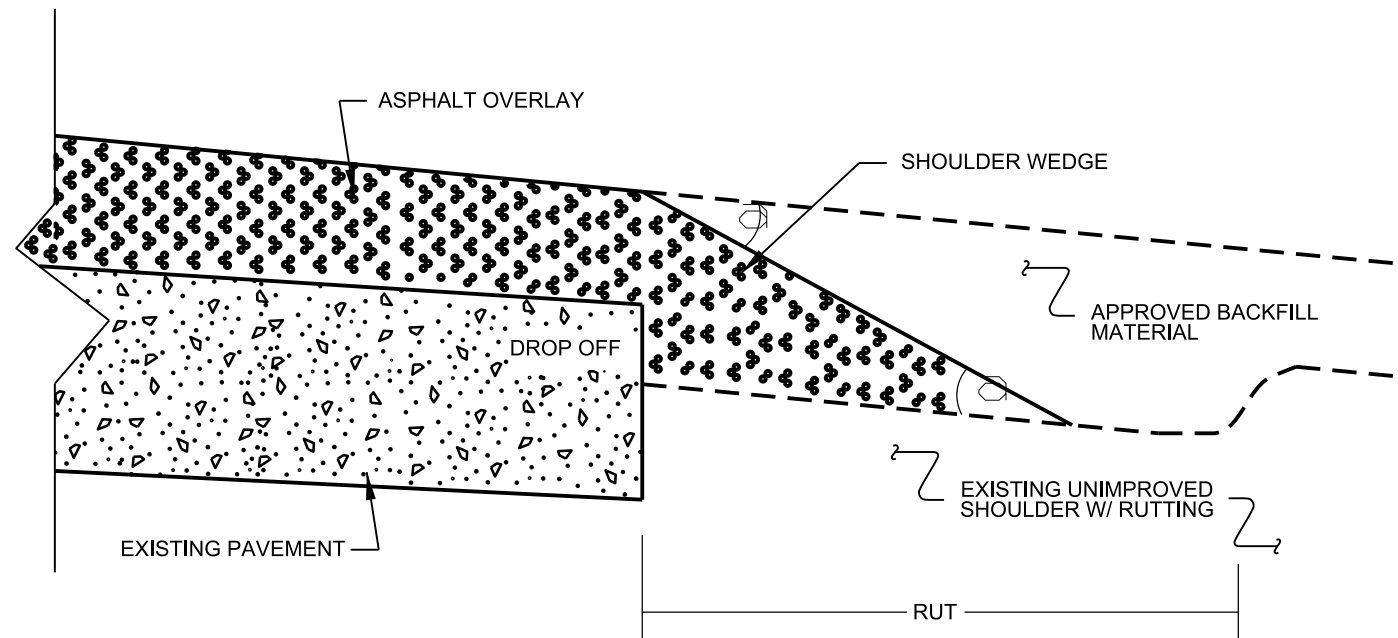
**SHOULDER WEDGE DETAIL**

(Resurfacing Projects w/ Widening or with Existing Paved Shoulder having no dropoffs)



**SHOULDER WEDGE DETAIL**

(Resurfacing Projects w/ NO Widening)



**SHOULDER WEDGE DETAIL**

(Resurfacing Adjacent to Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

<b>CONTRACT STANDARDS AND DEVELOPMENT UNIT</b>	
Office 919-707-6950	FAX 919-250-4119
<b>SHOULDER WEDGE DETAILS</b>	
ORIGINAL BY: T.SPELL	DATE: 7-19-11
MODIFIED BY:	DATE: 10/16/12
CHECKED BY:	DATE:
FILE SPEC: s:\usr\details\stand\shoulderwedge\detail.dgn	



## SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	010600000-E	122000000-E	124500000-E	129700000-E	133000000-E	149100000-E	151900000-E	152300000-E	157500000-E	170400000-E	177500000-E	183800000-E	283000000-N	284500000-N	608400000-E	611000000-E	744400000-E				
												BORROW EXCAVATION	INCIDENTAL STONE	SHOULDER RECONSTRUCTION	MILLING ASPHALT PAVEMENT, ***"DEPTH (1 1/2")	MILLING ASPHALT PAVEMENT, ***"DEPTH (1-1/4")	MILLING ASPHALT PAVEMENT, ***"DEPTH (2 1/4")	INCIDENTAL MILLING	ASPHALT CONC BASE COURSE, TYPE B2.5.0C	ASPHALT CONC SURFACE COURSE, TYPE 59.5B	ASPHALT CONC SURFACE COURSE, TYPE 59.5C	ASPHALT BINDER FOR PLANT MIX	PATCHING EXISTING PAVEMENT	ASPHALT SURFACE TREATMENT, MAT COAT, #78M STONE	ASPHALT SURFACE TREATMENT, MAT COAT, #67 STONE	EMULSION FOR ASPHALT SURFACE TREATMENT	ADJ. OF MANHOLES	ADJUSTMENT OF METER BOXES OR VALVE BOXES	SEEDING & MULCHING	RESIDENTIAL SEEDING	INDUCTIVE LOOP SAW CUT	
MI	FT	CY	TONS	SMI	SY	SY	SY	SY	TON	TONS	TONS	TON	TONS	TONS	TONS	TONS	TONS	TONS	TONS	TONS	TONS	TONS	TONS	TONS	TONS	TONS	TONS	TONS	TONS			
2022CPT.07.21.20411	Guilford	26	SR 2299 - FOGLEMAN WY	FROM SR 2129 - FOGLEMAN RD TO DEAD END	3	2	2WU	NO	NO	0.346	18-21							321	22	15	4,226											
<b>TOTAL FOR MAP NO. 26</b>										<b>0.346</b>								<b>321</b>	<b>22</b>	<b>15</b>	<b>4,226</b>											
2022CPT.07.21.20411	Guilford	27	SR 2129 - FOGLEMAN RD	FROM NC 68 TO DEAD END	3	2	2WU	NO	NO	0.524	18-20		25			175		455	31	70	6,214											
<b>TOTAL FOR MAP NO. 27</b>										<b>0.524</b>		<b>25</b>			<b>175</b>		<b>455</b>	<b>31</b>	<b>70</b>	<b>6,214</b>												
2022CPT.07.21.20411	Guilford	28	SR 5027 - IVES DR	FROM SR 2299 - FOGLEMAN WY TO DEAD END	3	2	2WU	NO	NO	0.139	21			3				133	9	5	1,746											
<b>TOTAL FOR MAP NO. 28</b>										<b>0.139</b>		<b>3</b>					<b>133</b>	<b>9</b>	<b>5</b>	<b>1,746</b>												
2022CPT.07.21.20411	Guilford	29	SR 4439 - LEES GLENN RD	FROM SR 2029 - W HARRELL RD TO SR 4440 - W GLENN RD	3	2	2WU	NO	NO	0.18	21-22		3			200		185	12	15	2,426											
<b>TOTAL FOR MAP NO. 29</b>										<b>0.18</b>		<b>3</b>			<b>200</b>		<b>185</b>	<b>12</b>	<b>15</b>	<b>2,426</b>												
2022CPT.07.21.20411	Guilford	30	SR 4441 - LEES RIDGE RD	FROM SR 4440 - W GLENN RD TO CUL DE SAC	3	2	2WU	NO	NO	0.379	21							392	26	50	5,154											
<b>TOTAL FOR MAP NO. 30</b>										<b>0.379</b>							<b>392</b>	<b>26</b>	<b>50</b>	<b>5,154</b>												
2022CPT.07.21.20411	Guilford	31	SR 4440 - W GLENN RD	FROM SR 4439 - LEES GLENN RD TO SR 4441 - LEES RIDGE RD	3	2	2WU	NO	NO	0.061	21							67	5	10	885											
<b>TOTAL FOR MAP NO. 31</b>										<b>0.061</b>							<b>67</b>	<b>5</b>	<b>10</b>	<b>885</b>												
2022CPT.07.21.20411	Guilford	32	SR 4482 - CHARLES PLACE DR	FROM SR 2028 - HAW RIVER RD TO CUL DE SAC	3	2	2WU	NO	NO	0.496	21					215		502	34	60	6,609											
<b>TOTAL FOR MAP NO. 32</b>										<b>0.496</b>					<b>215</b>		<b>502</b>	<b>34</b>	<b>60</b>	<b>6,609</b>												
2022CPT.07.21.20411	Guilford	33	SR 4642 - MISTY MEADOW DR	FROM SR 4482 - CHARLES PLACE DR TO CUL DE SAC	3	2	2WU	NO	NO	0.057	21							87	6		1,144											
<b>TOTAL FOR MAP NO. 33</b>										<b>0.057</b>							<b>87</b>	<b>6</b>		<b>1,144</b>												
2022CPT.07.21.20411	Guilford	34	SR 2054 - SHILLING ST / STOKESDALE ST	FROM SR 2053 - NEWBERRY ST TO SR 2056 - CLINTWOOD DR	3	2	2WU	NO	NO	0.532	18-19		53			173		470	31	40	6,179											
<b>TOTAL FOR MAP NO. 34</b>										<b>0.532</b>		<b>53</b>			<b>173</b>		<b>470</b>	<b>31</b>	<b>40</b>	<b>6,179</b>												
2022CPT.07.21.20411	Guilford	35	SR 2053 - NEWBERRY ST	FROM NC 65 TO US 158	3	2	2WU	NO	NO	0.32	19-22		15			383		315	21	7	4,148											
<b>TOTAL FOR MAP NO. 35</b>										<b>0.32</b>		<b>15</b>			<b>383</b>		<b>315</b>	<b>21</b>	<b>7</b>	<b>4,148</b>												
2022CPT.07.21.20411	Guilford	36	SR 2057 - IVAN ST	FROM US 158 TO SR 2046 - ELLISBORO RD	4	2	2WU	NO	NO	0.028	22		13					27	2													
<b>TOTAL FOR MAP NO. 36</b>										<b>0.028</b>		<b>13</b>					<b>27</b>	<b>2</b>														
2022CPT.07.21.20411	Guilford	37	SR 1476 - FARLIN AV	FROM SR 1475 - DEWBERRY DR TO SR 1382 - ROSE LAKE DR	3	2	2WU	NO	NO	0.275	19		13					238	16	59	3,129											
<b>TOTAL FOR MAP NO. 37</b>										<b>0.275</b>		<b>13</b>					<b>238</b>	<b>16</b>	<b>59</b>	<b>3,129</b>												
2022CPT.07.21.20411	Guilford	38	SR 1429 - ROSE DR	FROM SR 1475 - DEWBERRY DR TO SR 1431 - CRANE AV	3	2	2WU	NO	NO	0.213	14-20		15					157	11		2,063											
<b>TOTAL FOR MAP NO. 38</b>										<b>0.213</b>		<b>15</b>					<b>157</b>	<b>11</b>		<b>2,063</b>												
2022CPT.07.21.20411	Guilford	39	SR 1430 - CORAL AV	FROM SR 1429 - ROSE DR TO SR 1382 - ROSE LAKE DR	3	2	2WU	NO	NO	0.098	14-17		3					68	5	62	887											
<b>TOTAL FOR MAP NO. 39</b>										<b>0.098</b>		<b>3</b>					<b>68</b>	<b>5</b>	<b>62</b>	<b>887</b>												
2022CPT.07.21.20411	Guilford	40	SR 1475 - DEWBERRY DR	FROM SR 1431 - CRANE AV TO SR 1476 - FARLIN AVE	3	2	2WU	NO	NO	0.204	19-20		9					177	12	48	2,334											
<b>TOTAL FOR MAP NO. 40</b>										<b>0.204</b>		<b>9</b>					<b>177</b>	<b>12</b>	<b>48</b>	<b>2,334</b>												
2022CPT.07.21.20411	Guilford	41	SR 1431 - CRANE AV	FROM SR 1475 - DEWBERRY DR TO SR 1382 - ROSE LAKE DR	3	2	2WU	NO	NO	0.14	15-20		3			158		109	7	59	1,437											
<b>TOTAL FOR MAP NO. 41</b>										<b>0.14</b>		<b>3</b>			<b>158</b>		<b>109</b>	<b>7</b>	<b>59</b>	<b>1,437</b>												
2022CPT.07.21.20411	Guilford	42	SR 1382 - ROSE LAKE DR	FROM SR 1129 - GROOMETOWN RD TO SR 4221 - GREEN LAKE CT	3	2	2WU	NO	NO	0.698	20-37		10			483		646	43	121	8,490							650				
<b>TOTAL FOR MAP NO. 42</b>										<b>0.698</b>		<b>10</b>			<b>483</b>		<b>646</b>	<b>43</b>	<b>121</b>	<b>8,490</b>										<b>650</b>		
2022CPT.07.21.20411	Guilford	43	SR 1403 - S FREMONT DR	FROM DEAD END TO SR 1404 - ADAMSON RD	5	2	2WU	NO	NO	0.379	21-26							5,209	550	360	24											
<b>TOTAL FOR MAP NO. 43</b>										<b>0.379</b>							<b>5,209</b>	<b>550</b>	<b>360</b>	<b>24</b>												
2022CPT.07.21.20411	Guilford	44	SR 1404 - ADAMSON RD	FROM SR 1403 - N FREMONT DR TO DEAD END	5	2	2WU	NO	NO	0.208	21-28							2,821	333	203	14											
<b>TOTAL FOR MAP NO. 44</b>										<b>0.208</b>							<b>2,821</b>	<b>333</b>	<b>203</b>	<b>14</b>												
2022CPT.07.21.20411	Guilford	45	SR 1388 - SEDGFIELD PARK DR	FROM SR 1129 - GROOMETOWN RD TO SR 1403 - S FREMONT DR	5	2	2WU	NO	NO	0.081	21-35							75	5													
<b>TOTAL FOR MAP NO. 45</b>										<b>0.081</b>							<b>75</b>	<b>5</b>														
2022CPT.07.21.20411	Guilford	46	SR 1104 - OLD RANDLEMAN RD	FROM SR 1113 - KIVETT DR TO SR 1007 - RANDLEMAN RD	6,7,8	2	2WU	NO	NO	1.932	22-70	37	73	1.45	26,931		4,892	904		4,080	285	205	15,796				5,213		0.10	0.40	450	
<b>TOTAL FOR MAP NO. 46</b>										<b>1.932</b>		<b>37</b>	<b>73</b>	<b>1.45</b>	<b>26,931</b>		<b>4,892</b>	<b>904</b>		<b>4,080</b>	<b>285</b>	<b>205</b>	<b>15,796</b>				<b>5,213</b>		<b>0.10</b>	<b>0.40</b>	<b>450</b>	
2022CPT.07.21.20411	Guilford	47	SR 1554 - S CHIMNEY ROCK RD	FROM SR 1556 - GALLIMORE DAIRY RD TO END MAINT	6,7	2	2WU	NO	NO	1.848	26-51		15			5,142		3,494	210													
<b>TOTAL FOR MAP NO. 47</b>										<b>1.848</b>		<b>15</b>			<b>5,142</b>		<b>3,494</b>	<b>210</b>														
2022CPT.07.21.20411	Guilford	48	SR 1607 - BURNT POPLAR RD	FROM SR 1695 - S REGIONAL RD TO SR 1556 - GALLIMORE DAIRY RD	6,7	2	2WU	NO	NO	0.943	24-49		288					1,451	87													
<b>TOTAL FOR MAP NO. 48</b>										<b>0.943</b>		<b>288</b>					<b>1,451</b>	<b>87</b>														
2022CPT.07.21.20411	Guilford	49	SR 4307 - RADAR RD	FROM SR 2492 - N CHIMNEY ROCK RD TO BEG C & G	10	2	2WU	NO	NO	0.265	25-49							573	386	23	130	4,579										
<b>TOTAL FOR MAP NO. 49</b>										<b>0.265</b>							<b>573</b>	<b>386</b>	<b>23</b>	<b>130</b>	<b>4,579</b>											
2022CPT.07.21.20411	Guilford	50	SR 2137 - OLD OAK RIDGE RD	FROM BRIDGE APPROACH TO SR 2133 - PLEASANT RIDGE RD	9	2	2WU	NO	NO	1.321	24-85		40			31,605		7,075		2,946	197											
<b>TOTAL FOR MAP NO. 50</b>										<b>1.321</b>		<b>40</b>			<b>31,605</b>		<b>7,075</b>		<b>2,946</b>	<b>197</b>												
<b>TOTAL FOR PROJ NO. 2022CPT.07.21.20411</b>										<b>20.131</b>		<b>37</b>																				



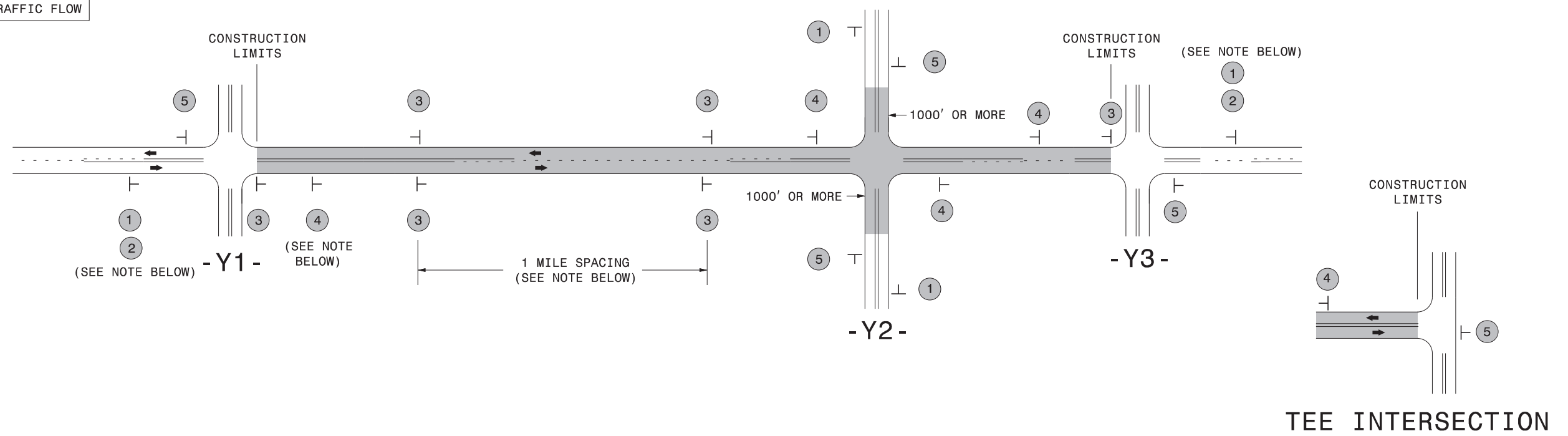


# SIGNING FOR RESURFACING PROJECTS

**LEGEND**

┆ STATIONARY SIGN

← DIRECTION OF TRAFFIC FLOW



## MAINLINE (-L-) SIGNING

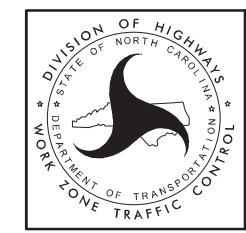
## -Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	1		PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> <li>LESS THAN 1000' OF RESURFACING ALONG -Y- LINE</li> <li>SUBDIVISION ROADS</li> <li>DEAD END ROADS</li> </ol> <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;">               PLACED 500' IN ADVANCE OF FLAGGER.         </div> <div style="text-align: center;">               PLACED 250' IN ADVANCE OF FLAGGER.         </div> </div>
	2		#2 SIGN ONLY USED WHEN CONSTRUCTION LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)	
	3		- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.	
	4		- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.	
	5		PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.	

THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.

### MAPS LESS THAN 2 MILES

FOR RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, NO STATIONARY SIGNS ARE REQUIRED. USE PORTABLE "ROAD UNDER CONSTRUCTION" OR "ROAD WORK AHEAD" SIGNS IN LIEU OF STATIONARY ADVANCE WARNINGS SIGNS.

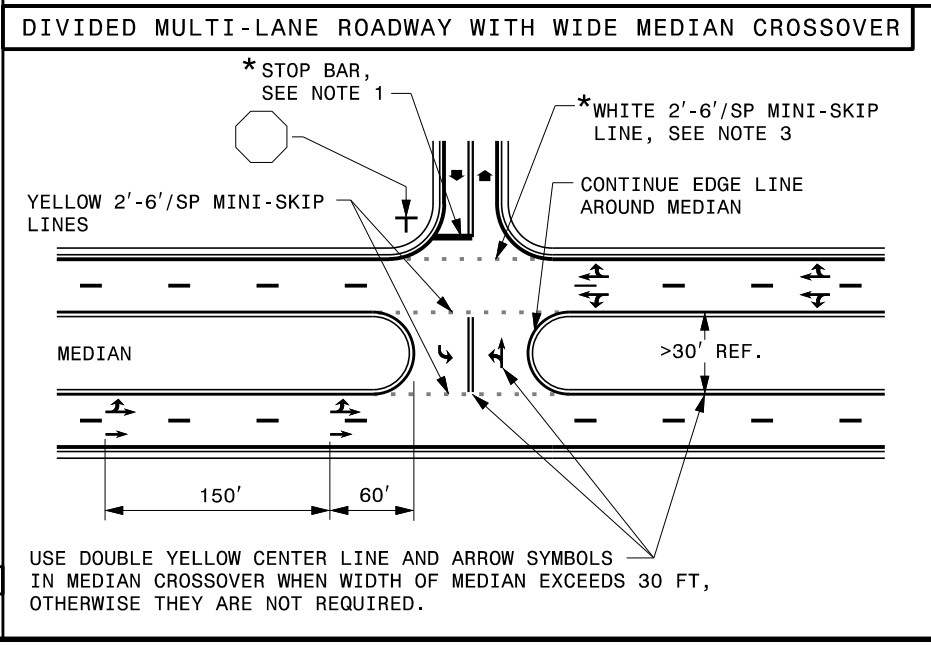
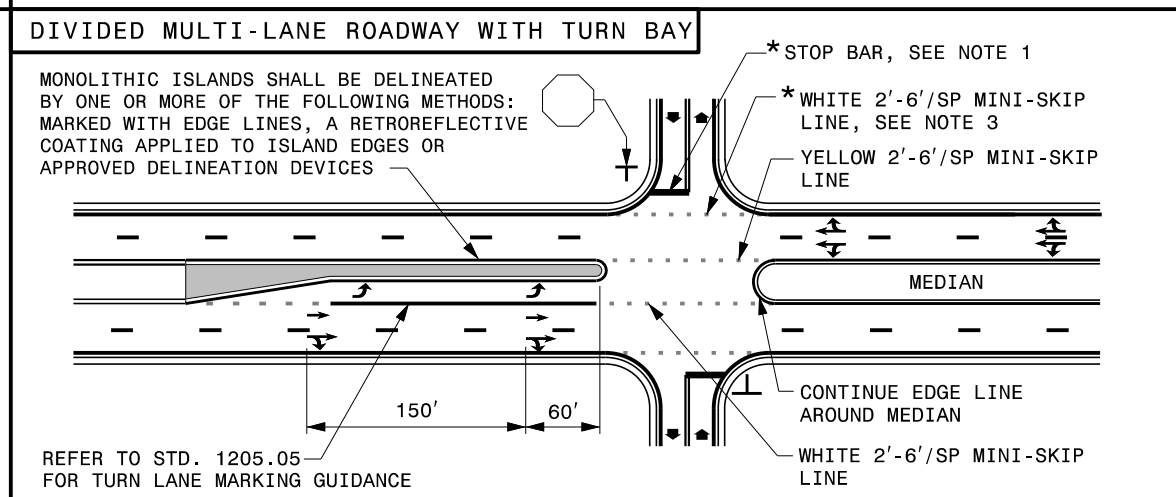
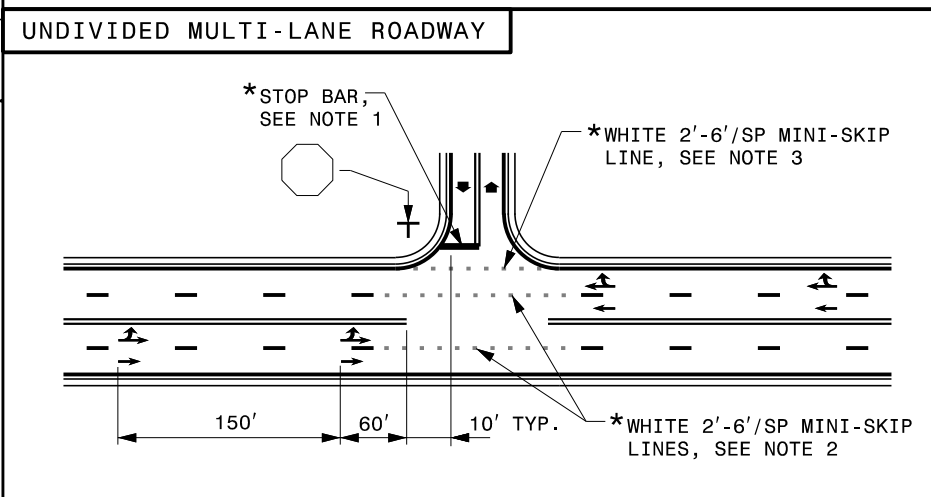
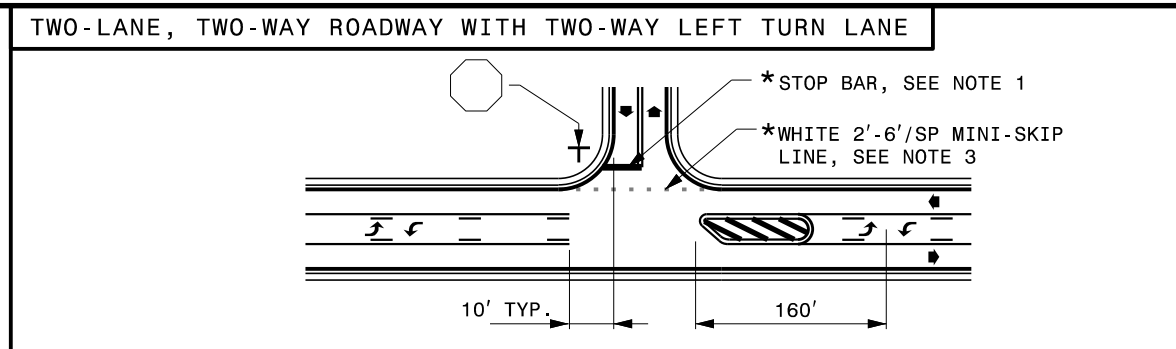
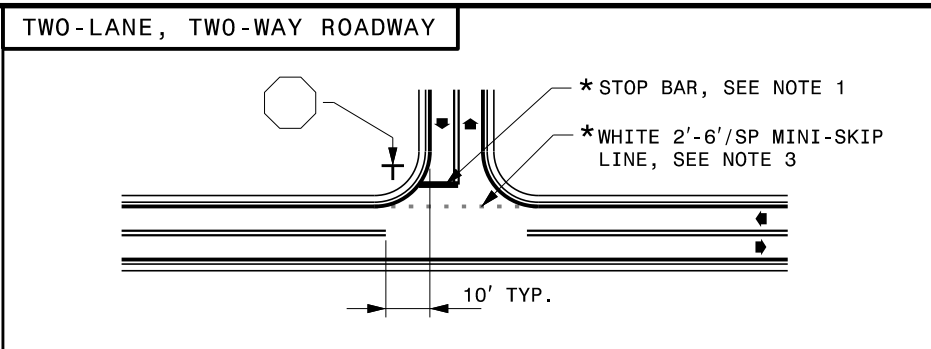


ADVANCE WARNING SIGNS FOR RURAL AND SUBURBAN 2-LANE ROADWAY RESURFACING

5/15/2017 S:\TMU\WZTC\Resurfacing\2L2W & AST Resurfacing Details\Resurfacing\_AdvWarn\_2Ln.dgn User:kedais

TIP NO.	SHEET NO.
DocuSigned by: <i>Matthew V. Springer</i>	
APPROVED:	8/13/2019
DATE:	6609-523B584403...
SEAL NORTH CAROLINA PROFESSIONAL ENGINEER SEAL 042546 MATTHEW V. SPRINGER	

STATE OF NORTH CAROLINA  
 DEPT. OF TRANSPORTATION  
 DIVISION OF HIGHWAYS  
 RALEIGH, N.C.  
 8-19  
 ENGLISH DETAIL DRAWING FOR  
**PAVEMENT MARKINGS**  
 INTERSECTIONS  
 SHEET 2 OF 2  
**1205D04**



GENERAL NOTES:

- 1- PLACEMENT OF STOP BARS AT NON-SIGNALIZED INTERSECTIONS IS OPTIONAL AND USED WHERE IT IS IMPORTANT TO INDICATE THE POINT WHICH VEHICLES ARE REQUIRED TO STOP. PLACE STOP BARS NO LESS THAN 4 FEET AND NO MORE THAN 30 FEET FROM THE NEAREST EDGE OF THE INTERSECTING ROADWAY. USE 10 FEET AS THE TYPICAL SETBACK DISTANCE OR AS DIRECTED BY THE ENGINEER.
- 2- MINI-SKIP LANE LINE EXTENSIONS SHOULD BE USED AT INTERSECTIONS THAT HAVE REDUCED VISIBILITY CONDITIONS SUCH AS OFFSET, SKEWED, OR CURVED ROADWAYS.
- 3- MINI-SKIP EDGE LINE EXTENSIONS MAY BE PLACED THROUGH INTERSECTIONS AND MAJOR DRIVEWAYS.
- 4- REFER TO ROADWAY STANDARD DRAWINGS 1205.01, 1205.02, 1205.05, 1205.08 AND 1205.09 FOR ADDITIONAL PAVEMENT MARKING GUIDANCE.

LEGEND	
	STOP SIGN
	STATIONARY SIGN
	DIRECTION OF TRAFFIC FLOW
	PAVEMENT MARKING SYMBOLS
*	OPTIONAL

SHEET 2 OF 2  
**1205D04**

STATE OF NORTH CAROLINA  
 DEPT. OF TRANSPORTATION  
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 RALEIGH, N.C.  
 8-19  
 ENGLISH DETAIL DRAWING FOR  
**PAVEMENT MARKINGS**  
 INTERSECTIONS  
 SHEET 2 OF 2  
**1205D04**

**REVISED PAVEMENT MARKING ROADWAY STANDARD DRAWING**



STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

ENGLISH STANDARD DRAWING FOR  
**DEEP-CUT INDUCTIVE DETECTION LOOPS**  
(FOR INSTALLATION PRIOR TO MILLING)

SHEET 1 OF 1

**NOTES**

- OVERLAP SAW CUTS AT CORNERS AND INTERSECTION POINTS TO ENSURE UNIFORM SAW SLOT DEPTH.
- MAINTAIN 12" SPACING BETWEEN LOOP WIRE TAIL SECTIONS.
- WIRE LOOPS CONNECTED TO THE SAME DETECTOR IN SERIES.
- LOCATE LOOPS IN CENTER OF LANES UNLESS OTHERWISE SHOWN ON PLANS.
- USE A SERIES OF ONE INCH PIECES OF BACKER ROD SPACED ONE FOOT APART ALONG THE ENTIRE LENGTH OF THE FEEDER SLOT AND LOOP SAW SLOT.
- CONSULT LOOP SEALANT MANUFACTURER TO DETERMINE CURING TIME REQUIRED PRIOR TO MILLING.
- REFER TO STANDARD DRAWING 1725.01 SHEETS 2 AND 3 FOR ADDITIONAL REQUIREMENTS.

**SAW SLOT DEPTH CHART**  
ASSUMING 2" MILLING DEPTH

DEPTH (IN)	MAX NO. OF WIRE LAYERS				
	2	3	4	5	6
SAW SLOT DEPTH	4.0	4.5	5.0	5.0	5.0
MINIMUM TOTAL ASPHALT DEPTH REQUIRED	5.0	5.5	6.0	6.0	6.0

**LOOP WIRE TWISTING METHOD**

INCORRECT WAY TO TWIST WIRE

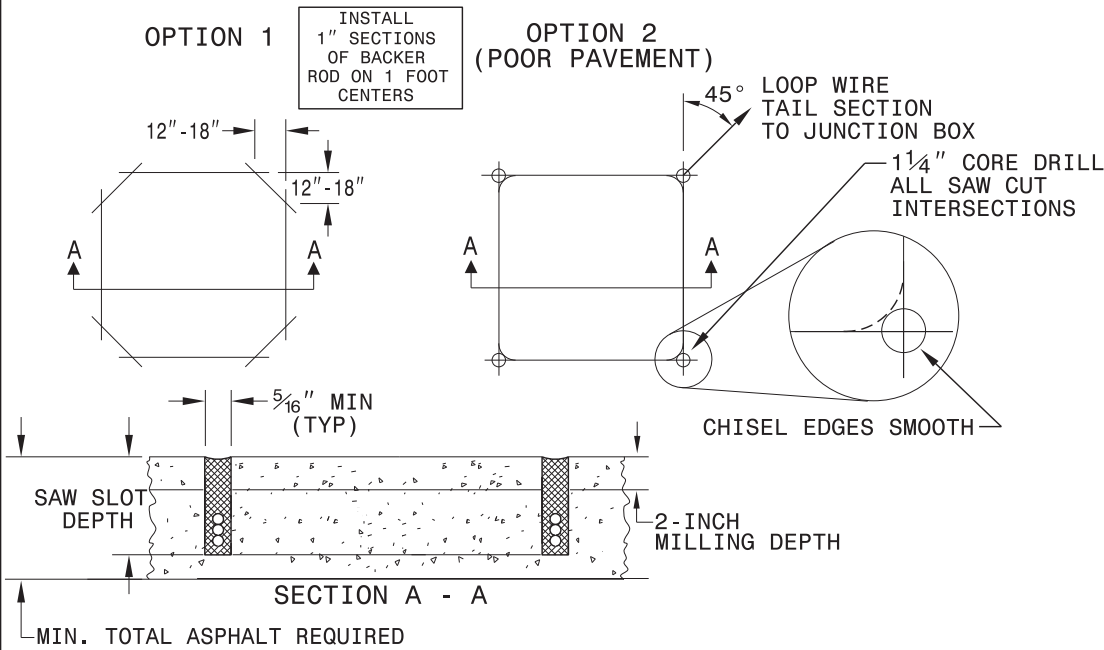


CORRECT WAY TO TWIST WIRE

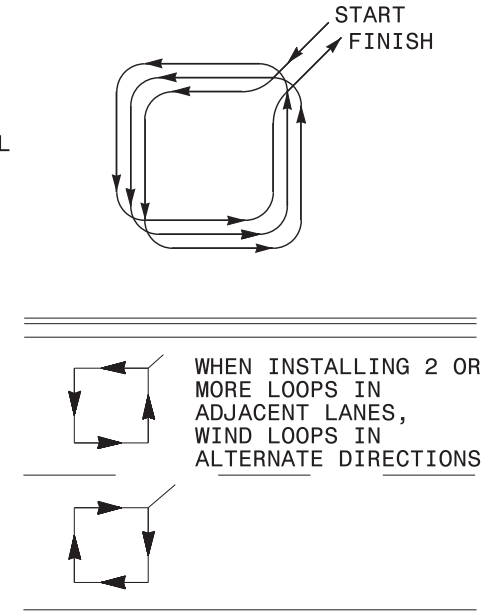


**CONVENTIONAL 4-SIDED LOOP**

**SAW CUT OPTIONS**

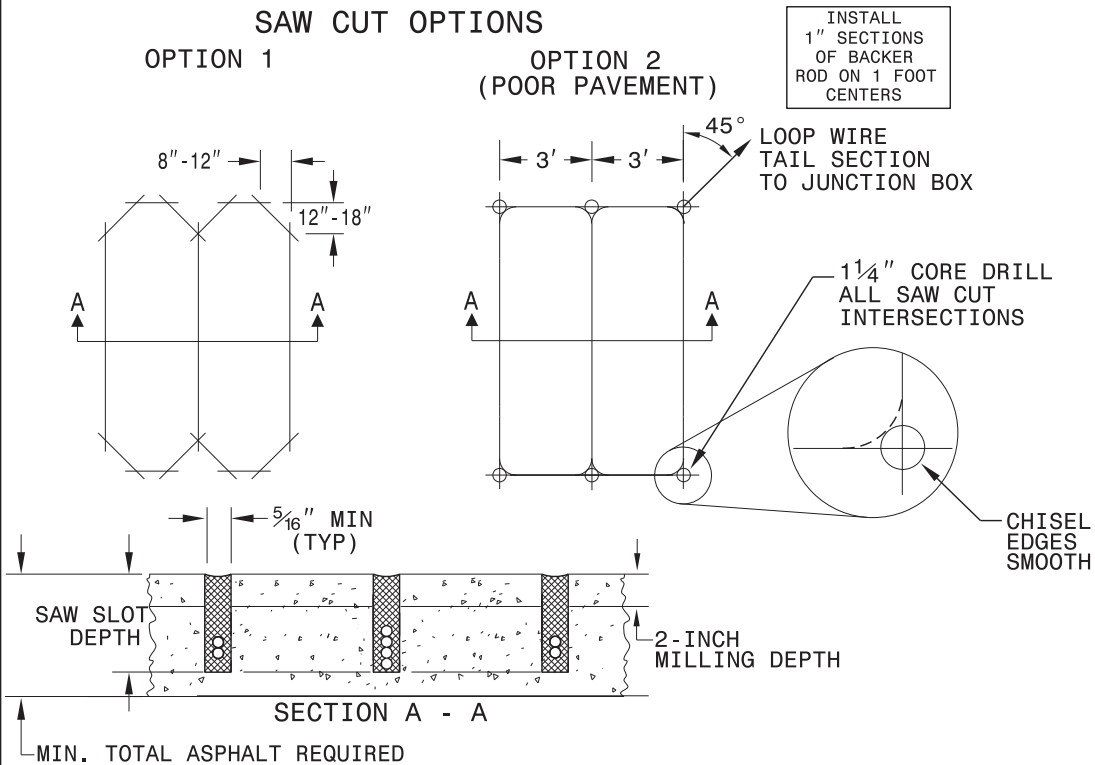


**LOOP WINDING METHOD**

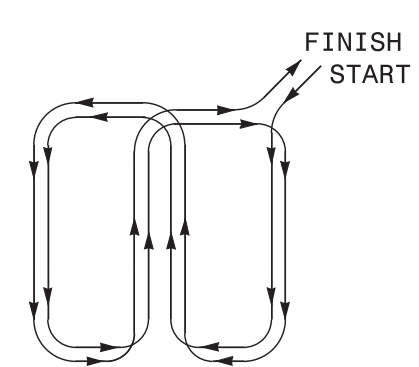


**QUADRUPOLE LOOP**

**SAW CUT OPTIONS**

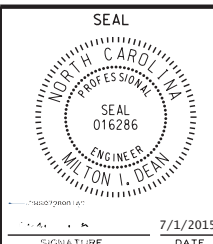
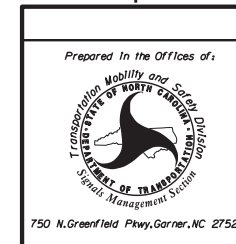


**LOOP WINDING METHOD**



**REVISIONS**

1. REMOVED TWISTING NOTES FROM TAIL SECT. TO JUNCTION BOX. 2/26/08 MWH
2. REVISED SECTION A - A DETAILS. 6/29/15 JTP



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ENGLISH STANDARD DRAWING FOR  
**DEEP-CUT INDUCTIVE DETECTION LOOPS**  
(FOR INSTALLATION PRIOR TO MILLING)

SHEET OF

7/1/2015

