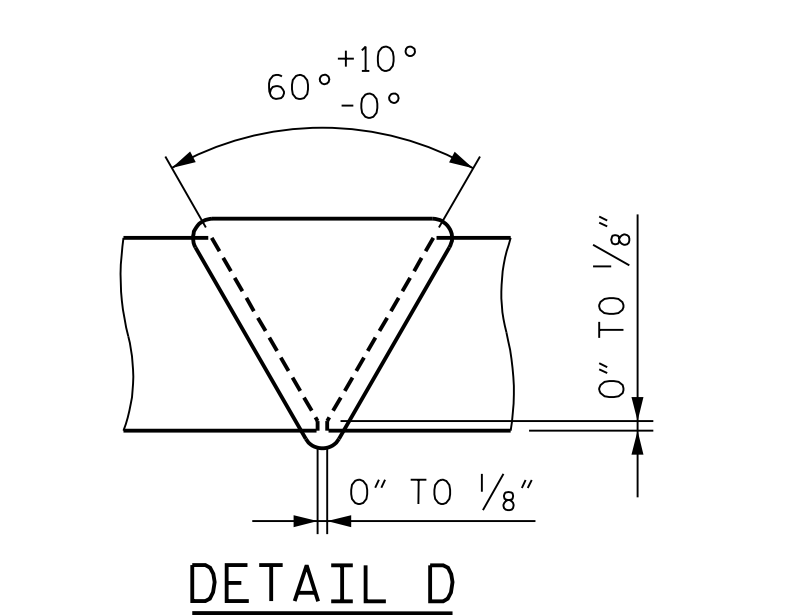
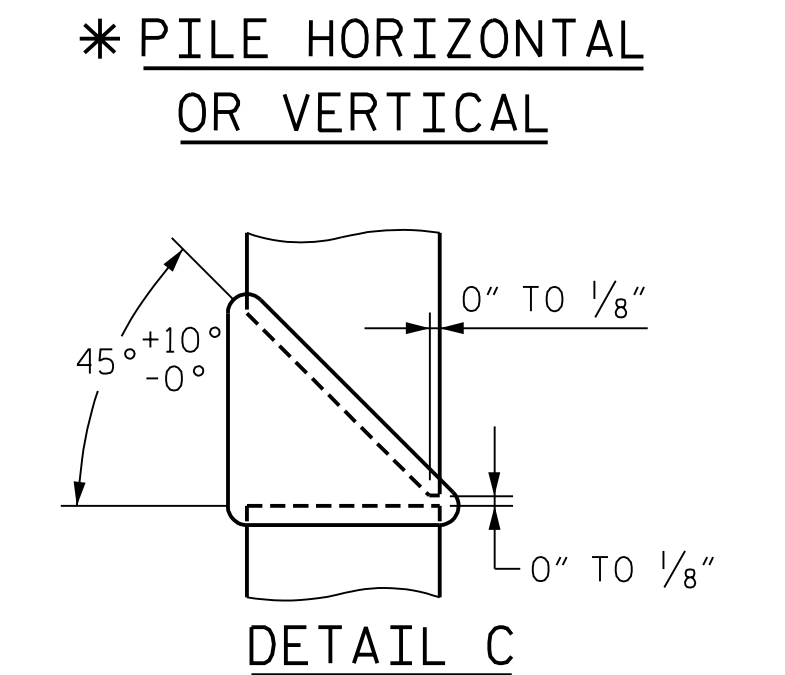
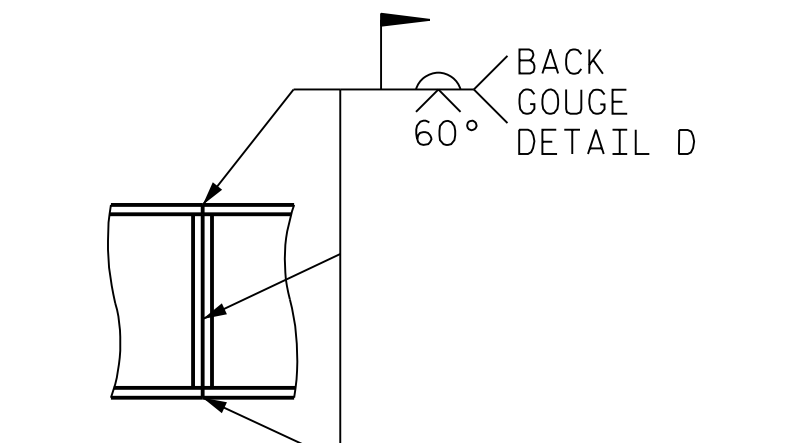
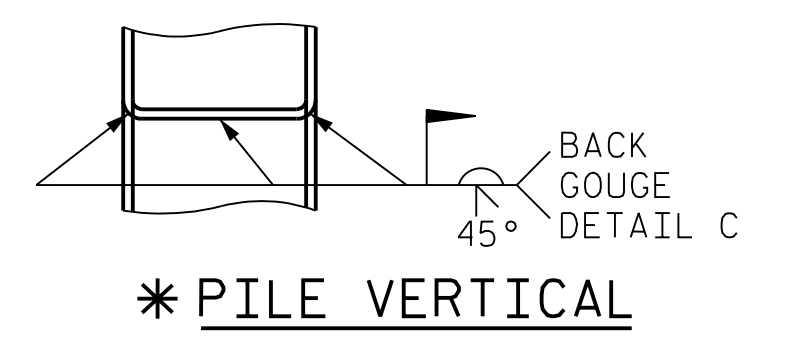
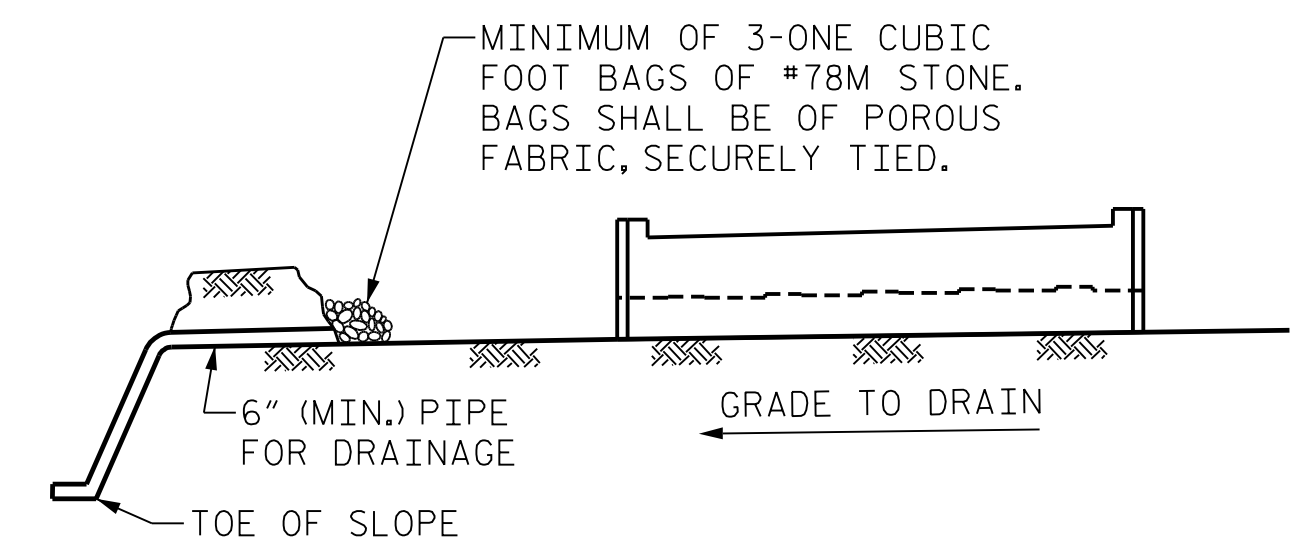


SECTION A-A

NOTES:
 FOR PIPE INSERT DETAILS, SEE BEARING SHEETS.
 THE TOP SURFACE AREAS OF THE END BENT CAP SHALL BE CURED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS EXCEPT THAT THE MEMBRANE CURING COMPOUND METHOD SHALL NOT BE USED.
 BACKWALL SHALL BE PLACED BEFORE APPLYING THE EPOXY PROTECTIVE COATING.
 THE TOP SURFACE OF THE END BENT CAP, EXCEPT THE BRIDGE SEAT BUILDUPS, SHALL BE SLOPED TRANSVERSELY FROM THE FILL FACE TO THE BACK FACE AT THE RATE OF 2%.
 STIRRUPS IN CAP MAY BE SHIFTED AS NECESSARY TO CLEAR ANCHOR BOLTS.
 SEE GENERAL DRAWING "FOUNDATION LAYOUT" FOR ADDITIONAL NOTES FOR DRIVING PILES.



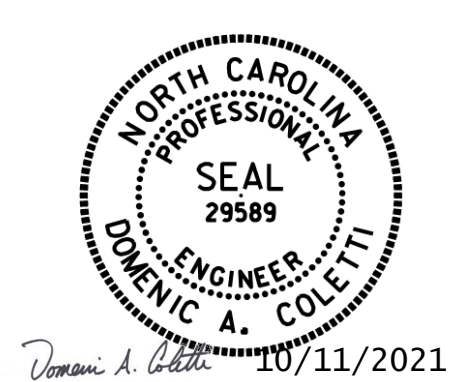
PILE SPLICE DETAILS
 * = POSITION OF PILE DURING WELDING



NOTES:
 BAGGED STONE AND PIPE SHALL BE PLACED IMMEDIATELY AFTER COMPLETION OF END BENT EXCAVATION. PIPE MAY BE EITHER CONCRETE, CORRUGATED STEEL, CORRUGATED ALUMINUM ALLOY OR CORRUGATED PLASTIC. PERFORATED PIPE WILL NOT BE ALLOWED.
 BAGGED STONE SHALL REMAIN IN PLACE UNTIL THE ENGINEER DIRECTS THAT IT BE REMOVED. THE CONTRACTOR SHALL REMOVE AND DISPOSE OF SILT ACCUMULATIONS AT BAGGED STONE WHEN SO DIRECTED BY THE ENGINEER. BAGS SHALL BE REMOVED AND REPLACED WHENEVER THE ENGINEER DETERMINES THAT THEY HAVE DETERIORATED AND LOST THEIR EFFECTIVENESS.
 NO SEPARATE PAYMENT WILL BE MADE FOR THIS WORK AND THE ENTIRE COST OF THIS WORK SHALL BE INCLUDED IN THE UNIT CONTRACT PRICE FOR THE SEVERAL PAY ITEMS.

TEMPORARY DRAINAGE AT END BENT

BAR TYPES		BILL OF MATERIAL				
BAR NO.	SIZE	TYPE	LENGTH	WEIGHT		
B1	#9	1	51'-4"	1920		
B2	#6	STR	49'-0"	736		
B3	#4	STR	25'-9"	276		
B4	#4	STR	4'-5"	39		
H1	#4	5	21'-0"	281		
H2	#4	5	17'-4"	417		
K1	#4	STR	25'-9"	482		
K2	#4	STR	2'-7"	14		
M1	#4	STR	2'-8"	43		
M2	#4	4	7'-5"	60		
S1	#5	2	5'-4"	295		
S2	#5	3	10'-7"	586		
S3	#4	4	3'-8"	108		
S4	#4	4	8'-6"	409		
S5	#6	7	9'-1"	82		
S6	#6	6	5'-5"	49		
S7	#5	3	11'-4"	627		
U1	#7	4	8'-10"	181		
U2	#4	4	6'-6"	35		
V1	#5	STR	9'-8"	888		
V2	#5	STR	11'-0"	92		
V3	#5	STR	11'-2"	94		
V4	#5	STR	11'-4"	95		
V5	#5	STR	11'-6"	96		
V6	#5	STR	11'-8"	98		
V7	#5	STR	10'-5"	87		
V8	#5	STR	10'-7"	89		
V9	#5	STR	10'-9"	90		
V10	#5	STR	10'-11"	92		
V11	#5	STR	11'-1"	93		
REINFORCING STEEL				LBS.	8454	
CLASS "A" CONCRETE						
POUR 1: COLLAR, CAP, LOWER PART OF WINGS			CU. YDS.	45.5		
POUR 2: BACKWALL & UPPER PART OF WINGS			CU. YDS.	20.9		
TOTAL CONCRETE			CU. YDS.	66.4		
HP 14x73 STEEL PILES						
			NO.	14		
			LF	1260		
PILE DRIVING EQUIPMENT SETUP FOR HP 14x73 STEEL PILES			EA.	14		



PROJECT NO. U-2579AB
 FORSYTH COUNTY
 STATION: 47+63.62 -Y15FLYBD-
 SHEET 3 OF 3

REVISIONS						SHEET NO.	
NO.	BY:	DATE:	NO.	BY:	DATE:	S05-074	
1	--	--	3	--	--	TOTAL SHEETS 116	
2	--	--	4	--	--		

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DES BY: A. ZEITOUNI DATE: 07/19
 DES CHK: J. EARNEST DATE: 07/19
 DWG BY: B. PETERSON DATE: 07/19
 CHK BY: J. EARNEST DATE: 08/19



DOCUMENT NOT CONSIDERED FINAL
 UNLESS ALL SIGNATURES COMPLETED