4'-9"

---#4S3 @ 1′-0″ CTS.

— #5V1 @ 1'-0" CTS.

1'-0"

■ = SLOPE 2% BETWEEN

BRIDGE SEATS

1'-0"

#4M2 ¬

#5S1-

1'-8"

1'-4"

1'-10"

4'-9"

1'-4"

8-#4B3 @

1'-0"

2" CLR.

FILL FACE —

2" CLR.

(TYP.)

CONSTR. JT.-

#4B4 @ 4'-0" CTS. —

© HP 14×73 VERTICAL PILE-

CTS. ACE)

3′-9″

SECTION A-A DES BY: A. ZEITOUNI DWG BY: B. PETERSON DATE : 07/19 DES CHK: J. EARNEST _ DATE : 07/19 CHK BY: J. EARNEST

1'-7"

NOTES:

-DROP IN ELEVATION

BETWEEN BRIDGE SEAT

BUILDUPS IS SHOWN

_2" CLR. TO #5S1

AT THIS POINT

6-#9B1

2" CLR. (MIN.)

5-#9B1

└─3" HIGH B.B. @ 5'-0" CTS.

- #7U1 (BETWEEN

P 14×73 BRACE

PILE (BATTER 3H:12V)

PILES)

FOR PIPE INSERT DETAILS, SEE BEARING SHEETS.

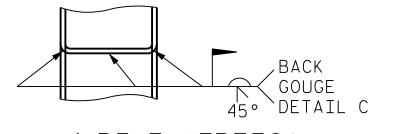
THE TOP SURFACE AREAS OF THE END BENT CAP SHALL BE CURED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS EXCEPT THAT THE MEMBRANE CURING COMPOUND METHOD SHALL NOT BE USED.

BACKWALL SHALL BE PLACED BEFORE APPLYING THE EPOXY PROTECTIVE COATING.

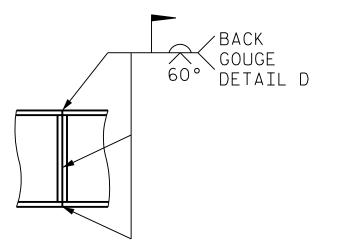
THE TOP SURFACE OF THE END BENT CAP, EXCEPT THE BRIDGE SEAT BUILDUPS, SHALL BE SLOPED TRANSVERSELY FROM THE FILL FACE TO THE BACK FACE AT THE RATE OF 2%.

STIRRUPS IN CAP MAY BE SHIFTED AS NECESSARY TO CLEAR ANCHOR BOLTS.

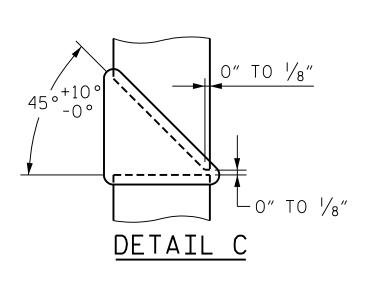
SEE GENERAL DRAWING "FOUNDATION LAYOUT" FOR ADDITIONAL NOTES FOR DRIVING PILES.

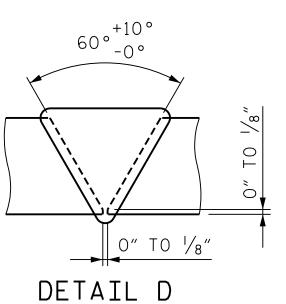


* PILE VERTICAL



* PILE HORIZONTAL OR VERTICAL





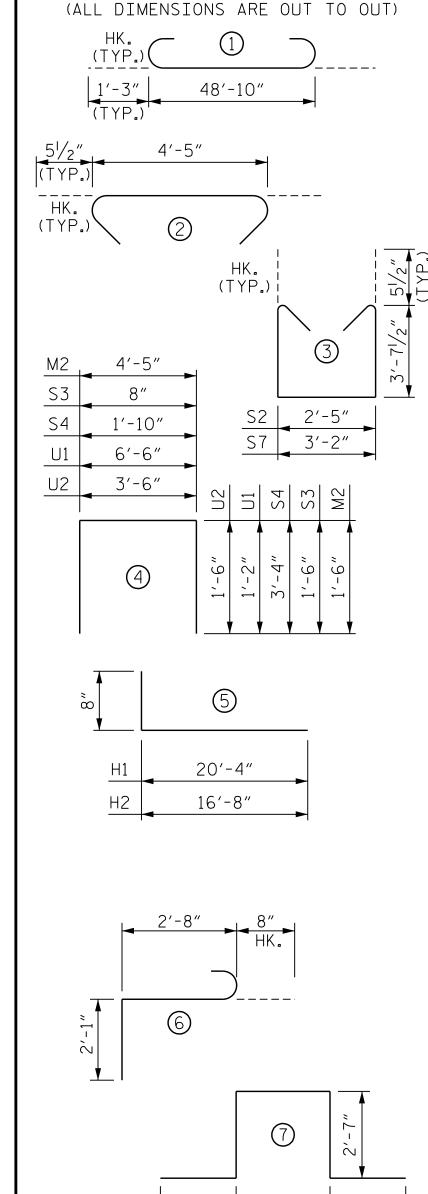
PILE SPLICE DETAILS

* = POSITION OF PILE DURING WELDING

-MINIMUM OF 3-ONE CUBIC FOOT BAGS OF #78M STONE. BAGS SHALL BE OF POROUS FABRIC, SECURELY TIED. GRADE TO DRAIN └6" (MIN.) PIPE FOR DRAINAGE

BAGGED STONE AND PIPE SHALL PLACED IMMEDIATELY AFTER COMPLETION OF END BENT EXCAVATION. PIPE MAY BE EITHER CONCRETE, CORRUGATED STEEL, CORRUGATED ALUMINUM ALLOY OR CORRUGATED PLASTIC. PERFORATED

BAGGED STONE SHALL REMAIN IN PLACE UNTIL THE ENGINEER DIRECTS THAT IT BE REMOVED. THE CONTRACTOR SHALL REMOVE AND DISPOSE OF SILT ACCUMULATIONS AT BAGGED STONE WHEN SO DIRECTED BY THE ENGINEER. BAGS SHALL BE REMOVED AND REPLACED WHENEVER THE ENGINEER DETERMINES THAT THEY HAVE DETERIORATED AND LOST THEIR EFFECTIVENESS.



1'-0" 1'-11" 1'-0"

29589

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

HDR Engineering, Inc. of the Carolinas 555 Fayetteville St., Suite 900 Raleigh, N.C. 27601 N.C.B.E.L.S. License Number: F-0116

BAR TYPES

<i>'</i>	DAN	INO.	3175		LENGIN	WEIGHT	
	B1	11	#9	1	51'-4"	1920	
	В2	10	#6	STR	49′-0″	736	
	В3	16	#4	STR	25′-9″	276	
	B4	13	#4	STR	4′-5″	39	
			· · · · · · · · · · · · · · · · · · ·	• • • • • • • • • • • • • • • • • • • •			
	H1	20	#4	5	21'-0"	281	
	H2	36	#4	5	17'-4"	417	
	112	30	'	J	111	111	
	K1	20	#4	CTD	25/ 0"	102	
		28	#4	STR	25'-9" 2'-7"	482	
	K2	8	^{#4}	STR	2 - 1	14	
(TYP.)	N 4 1	2.4	# 4	CTD	0/ 0//	47	
	M1	24	#4	STR	2′-8″	43	
. 2	M2	12	#4	4	7′-5″	60	
5:-('/2''						225	
∵	S1	53	#5	2	5'-4"	295	
	S2	53	#5	3	10-7"	586	
	S3	44	#4	4	3′-8″	108	
	S4	72	#4	4	8′-6″	409	
	S5	6	#6	7	9'-1"	82	
	S6	6	#6	6	5′-5″	49	
	S7	53	#5	3	11'-4"	627	
	U1	10	#7	4	8'-10"	181	
	U2	8	#4	4	6′-6″	35	
	V1	88	#5	STR	9′-8″	888	
	V2	8	#5	STR	11'-0"	92	
	V3	8	#5	STR	11'-2"	94	
	V4	8	#5	STR	11'-4"	95	
	V5	8	#5	STR	11'-6"	96	
	٧6	8	#5	STR	11'-8"	98	
	V7	8	#5	STR	10'-5"	87	
	V8	8	#5	STR	10'-7"	89	
	V9	8	#5	STR	10'-9"	90	
	V10	8	#5	STR	10'-11"	92	
	V10	8	#5	STR	11'-1"	93	
	VII	0		3111	11 1		
	RETN	JE OR C	L TNG STEI	<u> </u> =	LBS.	8454	
	REINFORCING STEEL LBS. 8454						
		CLASS "A" CONCRETE					
	POUR 1: COLLAR , CAP, CU. YDS. 45.5						
	POUR 2: BACKWALL &				CU. YDS.	20.0	
	UPPE	er par	RT OF W	INGS	CU. IDS.	20.9	
	TOTAL CONCRETE				CU. YDS.	66.4	
	HP 14×73 STEEL PILES						
			14				
_	LF 1260						
	PILE DRIVING EQUIPMENT SETUP FOR HP 14X73						
		EL PIL		EA.	1	4	
				_ · •			

BILL OF MATERIAL BAR NO. SIZE TYPE LENGTH WEIGH

PROJECT NO. U-2579AB FORSYTH COUNTY

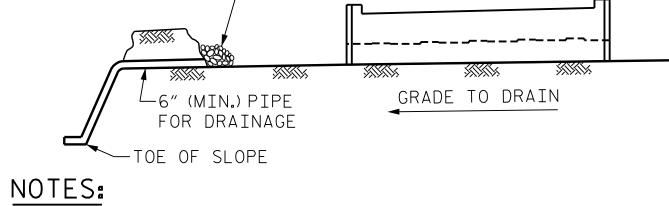
STATION: 47+63.62 -Y15FLYBD-

SHEET 3 OF 3

STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION

SUBSTRUCTURE END BENT 1 SECTION & DETAILS

REVISIONS NO. BY: DATE: BY: DATE: S05-074 TOTAL SHEETS 116



PIPE WILL NOT BE ALLOWED.

NO SEPARATE PAYMENT WILL BE MADE FOR THIS WORK AND THE ENTIRE COST OF THIS WORK SHALL BE INCLUDED IN THE UNIT CONTRACT PRICE FOR THE SEVERAL PAY ITEMS.

TEMPORARY DRAINAGE AT END BENT

