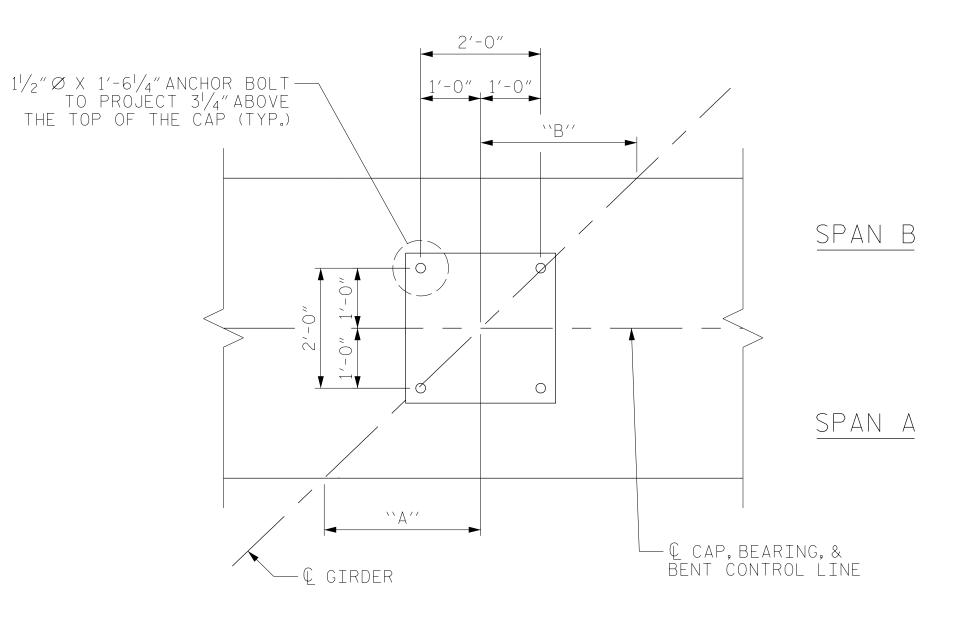


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DOCUMENT NOT CONSIDERED
FINAL UNLESS ALL
SIGNATURES COMPLETED
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NOTES: FOR SECTION A-A, PARTIAL SECTION B-B, VIEW C-C, AND VIEW D-D, SEE SHEET 2 OF 2. FOR REINFORCING STEEL BILL OF MATERIAL, SEE SHEET 2 OF 2. STIRRUPS AND #4 U4 BARS IN CAP MAY BE SHIFTED AS NECESSARY TO AVOID ANCHOR BOLTS. #4 U1 BARS IN ENDS OF CAP MAY BE SHIFTED AS NECESSARY TO AVOID #5 B3 BARS. HOOKS ON ``V'' BARS MAY BE TURNED AS NECESSARY FOR PLACING REINFORCING STEEL. THE CONTRACTOR'S ATTENTION IS CALLED TO THE FACT THAT A MINIMUM OF 1'-6"EARTH COVER OVER ALL FOOTINGS SHALL BE MAINTAINED AFTER CONSTRUCTION IS COMPLETE.

★ INVERT ALTERNATE #6 S1 STIRRUP PAIRS.



DETAIL ``A'' STEP NOT SHOWN FOR CLARITY

	``A''	``B''
GIRDER 1	2'-7 /2"	2'-7 ⁷ / ₁₆ "
GIRDER 2	2'-75/ ₁₆ "	2′-7 ¹ /4″
GIRDER 3	2'-7 /8"	2'-7 / ₁₆ "
GIRDER 4	2′-6 ¹⁵ / ₁₆ ″	2'-67/8"

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Dochelighter by ARO/ SCEPTRESSIGNATS SEAL 18442 HB MG INE FR. 10/14/2021	state of north carolina DEPARTMENT OF TRANSPORTATION RALEIGH SUBSTRUCTURE BENT 1						
RS&H Architects-Engineers-Planners, Inc.	REVISIONS					SHEET NO.	
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