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# PAVEMENT SCHEDULE

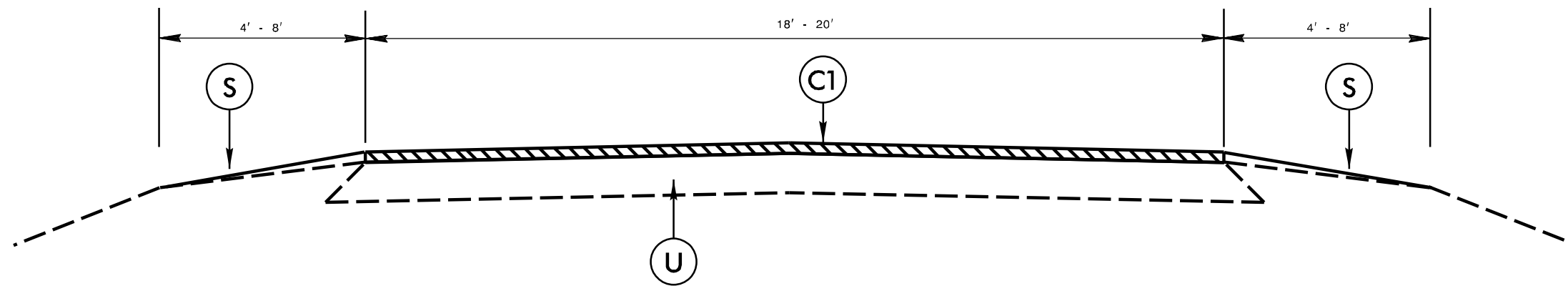
PROJECT REFERENCE NO.

SHEET NO.

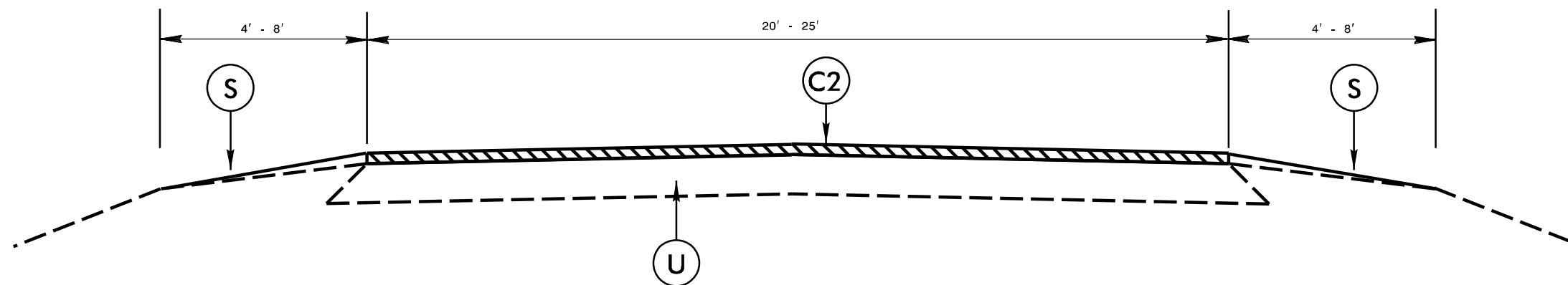
2022CPT.05.05.20351.1, etc.

4

C1	1¼" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 137.5 LBS. PER SQ. YD.
C2	1½" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
S	SHOULDER GRADING ASB REQUIRED (EXCEPT AT RESIDENTIAL AREAS)
U	EXISTING PAVEMENT



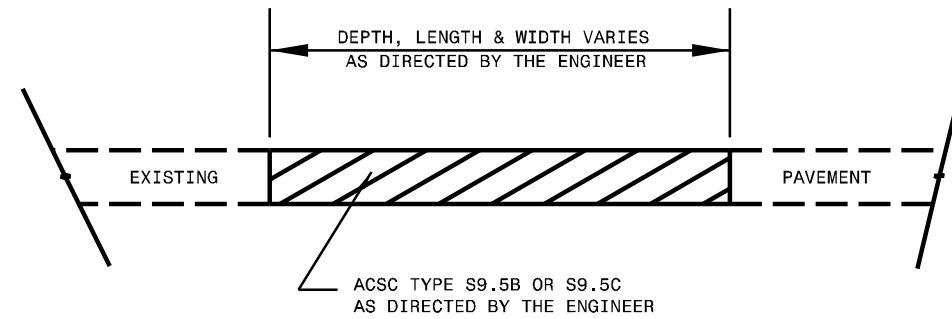
TYPICAL SECTION NO. 1



TYPICAL SECTION NO. 2

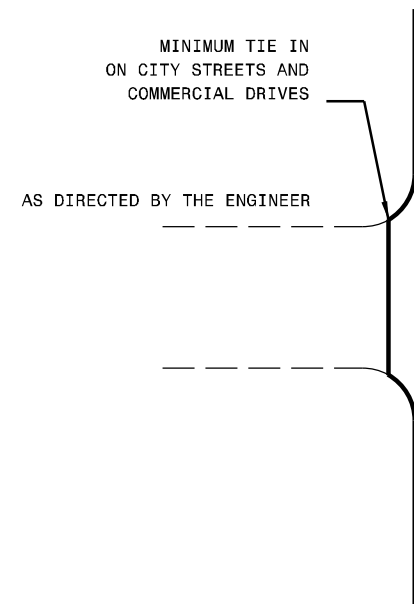
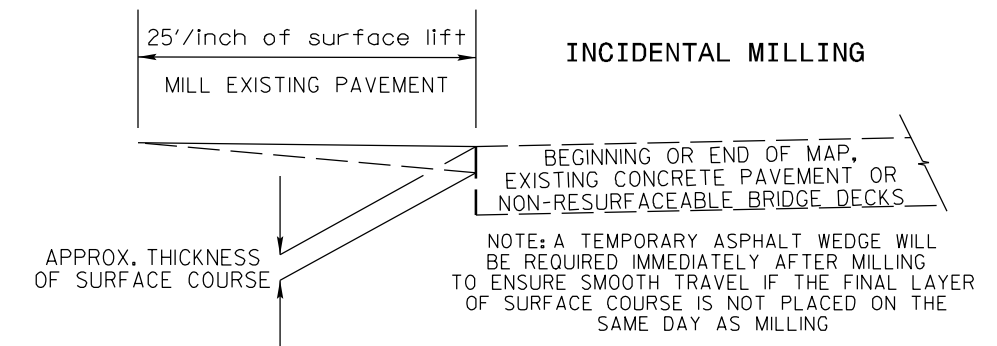
NOTES

ALL UNPAVED S.R. ROADS TO BE RESURFACED 50' FROM EDGE OF PAVEMENT OF MAIN PROJECT  
ALL PAVED S.R. ROADS TO BE RESURFACED TO THE ENDS OF THE RADIUS, OR AS DIRECTED BY THE ENGINEER.  
EDGES, PAVEMENT WIDENING, INTERSECTIONS AND BRIDGE FLARES ARE INCLUDED IN THE TABLE OF QUANTITIES.  
BRIDGES TO BE RESURFACED AT LOCATIONS AND TO DEPTH AS DIRECTED BY THE ENGINEER.

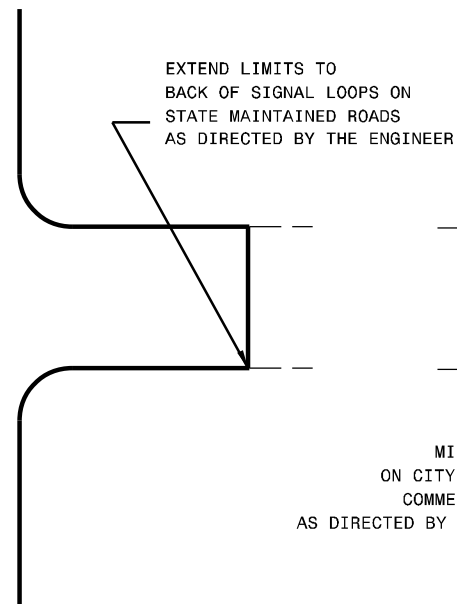


PATCHING EXISTING PAVEMENT

MILLING TO BE PERFORMED PRIOR TO PATCHING

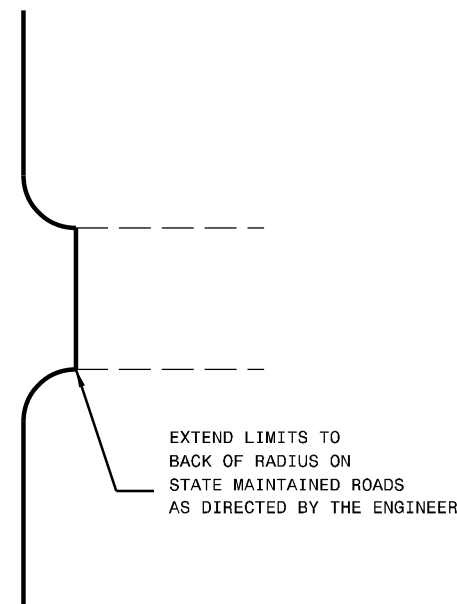


DETAIL OF PROJECT LIMITS AT SIGNALIZED Y LINES



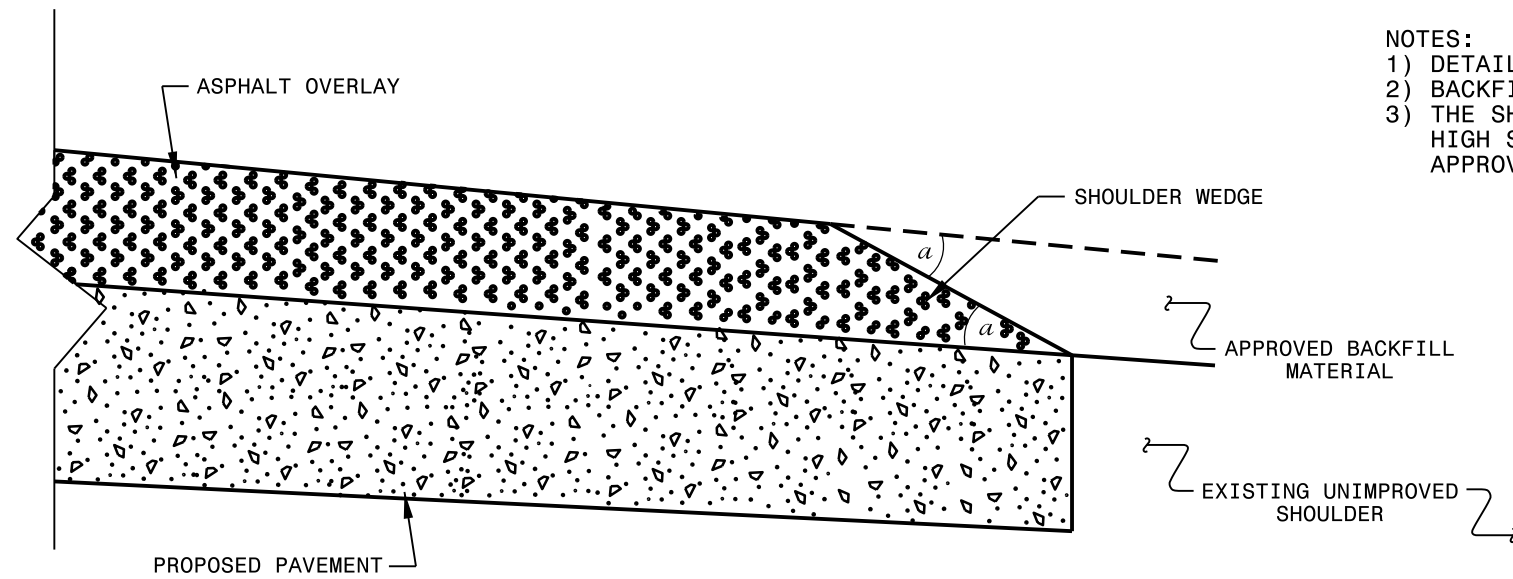
MINIMUM TIE IN ON CITY STREETS AND COMMERCIAL DRIVES AS DIRECTED BY THE ENGINEER

DETAIL OF PROJECT LIMITS AT UNSIGNALIZED Y LINES



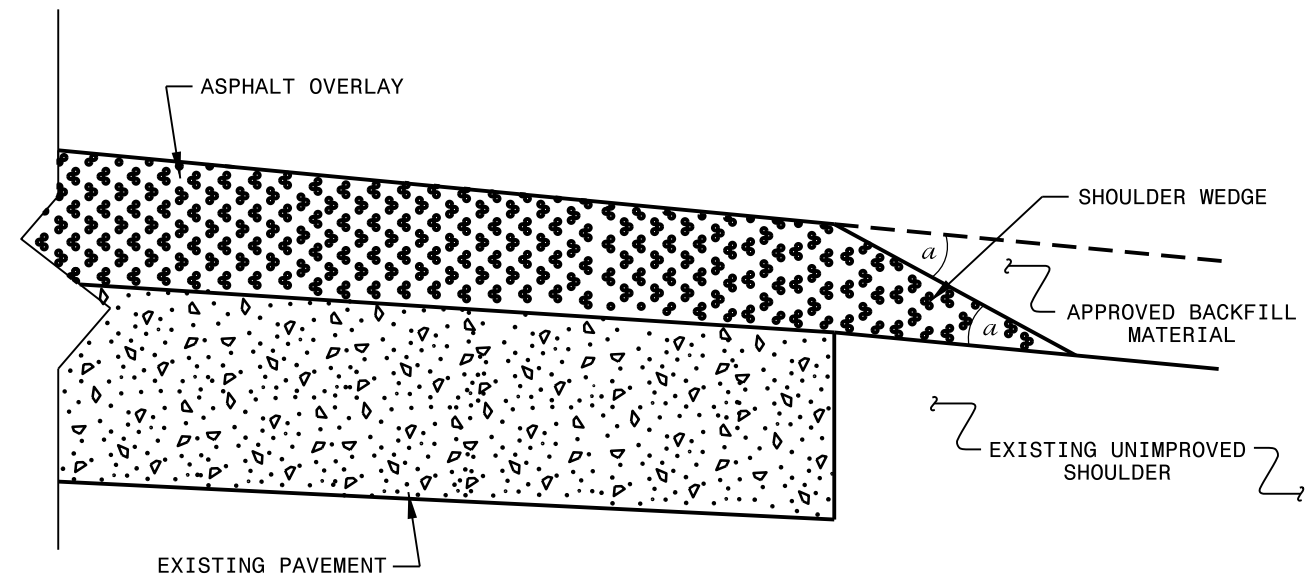
**NOTES:**

- 1) DETAIL DOES NOT APPLY TO OGAFB AND ULTRA-THIN BONDED WEARING COURSE.
- 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
- 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS, SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS APPROVED BY THE ENGINEER.



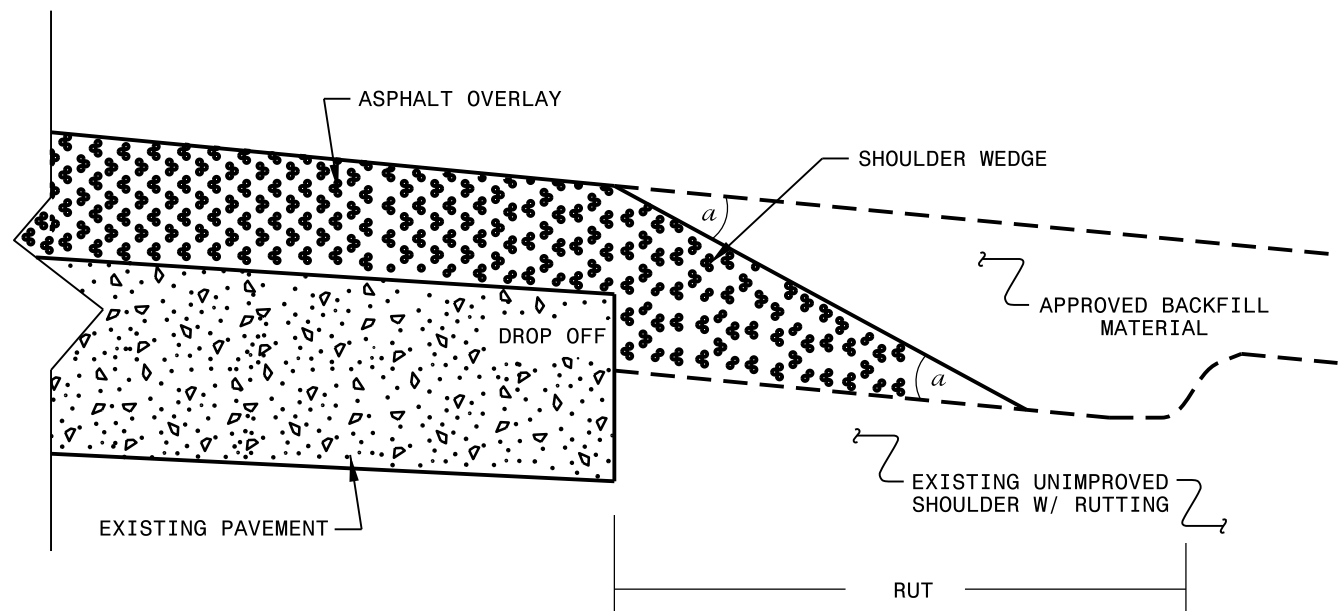
**SHOULDER WEDGE DETAIL**

(Resurfacing Projects w/ Widening or with Existing Paved Shoulder having no dropoffs)



**SHOULDER WEDGE DETAIL**

(Resurfacing Projects w/ NO Widening)



**SHOULDER WEDGE DETAIL**

(Resurfacing Adjacent to Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

<b>CONTRACT STANDARDS AND DEVELOPMENT UNIT</b>		
Office 919-707-6950 FAX 919-250-4119		
<b>SHOULDER WEDGE DETAILS</b>		
ORIGINAL BY: T.SPELL	DATE: 7-19-11	
MODIFIED BY:	DATE: 2/2/16	
CHECKED BY:	DATE:	
FILE SPEC.: szusr/details/stand/shoulderwedgedetail.dgn		

## SUMMARY OF QUANTITIES

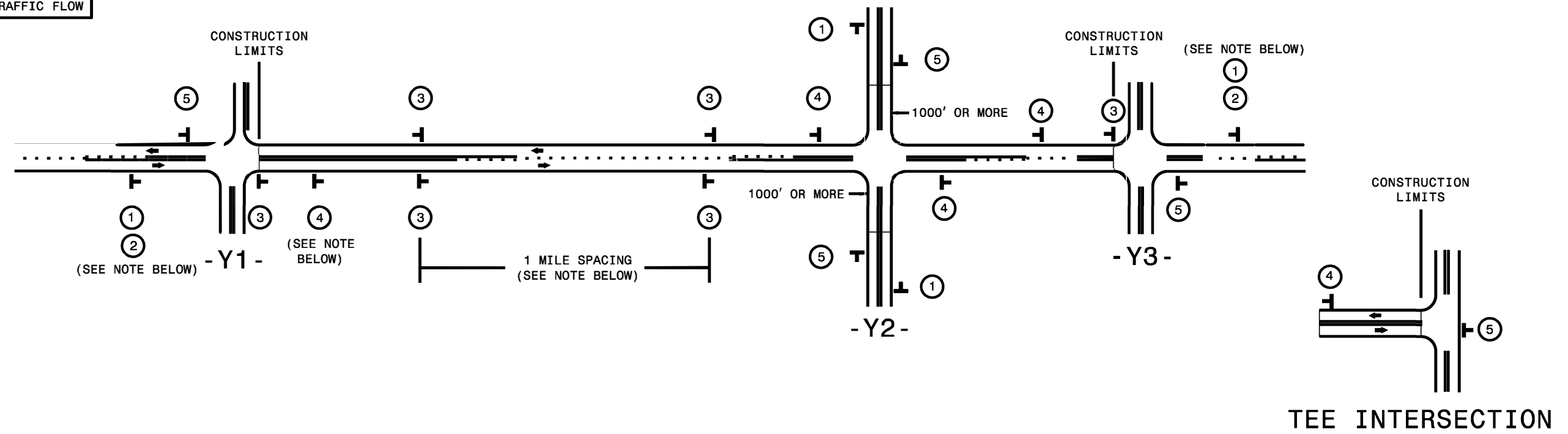
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	0106000000-E	0264000000-E	1220000000-E	1260000000-E	1330000000-E	1519000000-E	1575000000-E	1704000000-E	2845000000-N	6000000000-E	6071010000-E	6084000000-E	6117000000-N		
											BORROW	SHOULDER GRADING	INCIDENTAL STONE BASE	AGGREGATE SHOULDER BORROW	INCIDENTAL MILLING	SURFACE COURSE, S9.5B	ASPHALT BINDER FOR PLANT MIX	PATCHING EXISTING PAVEMENT	ADJUST METER OR VALVE BOX	TEMPORARY SILT FENCE	WATTLE	SEED & MULCHING	RESPONSE FOR EROSION CONTROL		
											MI	FT	CY	SMI	TONS	TON	SY	TONS	TON	TONS	EA	LF	LF	AC	EA
2022CPT.05.05.20351.1	Franklin	1	SR1930-LONGWOOD DR	GRANVILLE CO TO SR 1139 - SID MITCHELL RD	1	2	NO	NO	0.530	20	53	1.06	27		70	451	30	120		77	200	0.77	1		
		2	SR1931-CHARLESTON CT	SR 1930 - LONGWOOD DR TO CUL-DE-SAC	1	2	NO	NO	0.080	20	8		0.16	4		88	6	2		12	30	0.12			
		3	SR1932-WOODCROFT DR	GRANVILLE CO TO CUL-DE-SAC	1	2	NO	NO	0.390	20	39		0.78	20		353	24	335		57	150	0.57			
		4	SR1933-MARLOWE DR	GRANVILLE CO TO END MAINT	1	2	NO	NO	0.040	20	4		0.08	2		70	34	2	2		6	20	0.06		
		5	SR1935-LONGWOOD DR	SR 1139 - SID MITCHELL RD TO CUL-DE-SAC	1	2	NO	NO	0.290	20	29		0.58	15		70	271	18	100		42	110	0.42		
<b>TOTAL FOR PROJ NO. 2022CPT.05.05.20351.1</b>									<b>1.330</b>		<b>133</b>	<b>2.66</b>	<b>68</b>		<b>210</b>	<b>1,197</b>	<b>80</b>	<b>559</b>		<b>194</b>	<b>510</b>	<b>1.94</b>	<b>1</b>		
2022CPT.05.05.20391.1	Granville	6	SR1714-WOODLAND CHURCH RD	SR 1711 - GARNER RD TO SR 1716 - GRAHAM SHERRON RD	1	2	NO	NO	2.342	20	187	4.68	117	87	520	2,478	166	490		273	690	2.73	1		
		7	SR1715-HUGH DAVIS RD	SR 1714 - WOODLAND CHURCH RD TO FRANKLIN CO	2	2	NO	NO	1.186	21	95		2.37	59	44	382	1,299	87	60		138	350	1.38	1	
		8	SR1734-JONAH DAVIS RD	WAKE CO TO FRANKLIN CO	2	2	NO	NO	0.615	25	62		1.23	30		1,155	870	58	320		89	230	0.89		
		9	SR1738-OXFORD RIDGE RD	SR 1716 - GRAHAM SHERRON RD TO CUL-DE-SAC	1	2	NO	NO	0.300	18	30		0.60	15		60	250	17	185		44	110	0.44		
		10	SR1739-MEDLIN AVE	SR 1738 - OXFORD RIDGE RD TO CUL-DE-SAC	1	2	NO	NO	0.078	18	8		0.16	4			81	5	50		12	30	0.11		
		11	SR1745-FERN HOLLOW DR	NC 96 TO CUL-DE-SAC	2	2	NO	NO	0.509	20	51		1.02	25		117	548	37	440		74	190	0.74	1	
		12	SR1747-LONGVIEW CT	SR 1734 - JONAH DAVIS RD TO CUL-DE-SAC	1	2	NO	NO	0.087	20	9		0.17	4			95	6	15		13	40	0.13		
		13	SR1748-FAIRFIELD CT	SR 1734 - JONAH DAVIS RD TO CUL-DE-SAC	1	2	NO	NO	0.096	20	10		0.19	5			101	7	8		14	40	0.14		
		14	SR1750-NORTHBEND DR	SR 1734 - JONAH DAVIS RD TO CUL-DE-SAC	1	2	NO	NO	0.223	20	22		0.45	11			210	14	60		32	90	0.32		
		15	SR1751-LANDMARK DR	NC 96 TO CUL-DE-SAC	2	2	NO	NO	0.603	20	60		1.21	30		138	641	43	445		88	220	0.88	1	
		16	SR1752-PRATT CIR	SR 1751 - LANDMARK DR TO CUL-DE-SAC	2	2	NO	NO	0.180	20	18		0.36	9			214	14	30		26	70	0.26		
		17	SR1753-WOODCREST DR	SR 1734 - JONAH DAVIS RD TO CUL-DE-SAC	1	2	NO	NO	0.443	20	44		0.89	22			401	27	30		64	170	0.64		
		18	SR1754-GREENWOOD CT	SR 1753 - WOODCREST DR TO CUL-DE-SAC	1	2	NO	NO	0.198	20	20		0.40	10			188	13	10		29	80	0.29		
		19	SR1755-WOODLAND CHURCH RD	SR 1711 - BRUCE GARNER RD TO CUL-DE-SAC	1	2	NO	NO	0.750	20	75		1.50	37		150	659	44	240		109	280	1.09	1	
		20	SR1756-MAYAPPLE CT	SR 1755 - WOODLAND CHURCH RD TO CUL-DE-SAC	1	2	NO	NO	0.092	20	9		0.18	5			99	7	135		13	40	0.13		
		21	SR1758-APPALOOSA TRL	SR 1711 - WAYSIDE FARM RD TO CUL-DE-SAC	1	2	NO	NO	0.558	20	55		1.12	28		100	495	33	610		81	210	0.81		
		22	SR1759-CLYDESDALE CT	SR 1758 - APPALOOSA TRAIL TO CUL-DE-SAC	1	2	NO	NO	0.109	20	11		0.22	5			113	8	90		16	40	0.16		
		23	SR1760-WILD ORCHID CT	SR 1755 - WOODLAND CHURCH RD TO CUL-DE-SAC	1	2	NO	NO	0.068	20	7		0.14	3			78	5	16		10	30	0.10		
		24	SR1763-LONGWOOD DR	GRANVILLE CO TO CUL-DE-SAC	1	2	NO	NO	0.218	20	22		0.44	11			204	14	60		32	80	0.32		
		25	SR1764-WOODCROFT DR	SR 1765 - MARLOWE DR TO FRANKLIN CO	1	2	NO	NO	0.403	20	40		0.81	20			343	23	370		59	150	0.59		
		26	SR1765-MARLOWE DR	FRANKLIN CO TO CUL-DE-SAC	1	2	NO	NO	0.384	20	38		0.77	19			345	23	260		56	140	0.56		
		27	SR1766-HIGHVIEW DR	CUL-DE-SAC TO CUL-DE-SAC	1	2	NO	NO	0.725	20	73		1.45	36		100	663	44	485		105	270	1.05		
		28	SR1767-SUMMIT RIDGE CT	SR 1766 - HIGHVIEW DR TO CUL-DE-SAC	1	2	NO	NO	0.203	20	20		0.41	10			197	13	115		30	80	0.30		
		29	SR1768-CROSSCREEK DR	SR 1764 - WOODCROFT DR TO SR 1769 - HIDDEN COVE CT	1	2	NO	NO	0.130	20	13		0.26	7			111	7	115		19	50	0.19		
		30	SR1769-HIDDEN COVE CT	CUL-DE-SAC TO CUL-DE-SAC	1	2	NO	NO	0.118	20	12		0.24	6			146	10	3		17	50	0.17		
		31	SR1771-WHITWINDS WAY	NC 96 TO SR 1745 - FERN HOLLOW DR	2	2	NO	NO	0.454	20	45		0.91	23		138	464	31	115		66	170	0.66		
		32	SR1772-WHISPERS PINES LN	SR 1771 - WHITWINDSWAY TO CUL-DE-SAC	2	2	NO	NO	0.165	20	17		0.33	8			192	13	35		24	60	0.24		
		33	SR1773-BEECH CREEK COVE	SR 1771 - WHITWINDSWAY TO CUL-DE-SAC	2	2	NO	NO	0.048	20	5		0.10	3			75	5	6		7	20	0.07		
		34	SR1774-THOROUGHbred LN	SR 1758 - APPALOOSA TRAIL TO CUL-DE-SAC	1	2	NO	NO	0.254	20	25		0.51	13			236	16	70		37	100	0.37		
		35	SR1775-BEN-MILL CT	SR 1751 - LANDMARK DR TO CUL-DE-SAC	2	2	NO	NO	0.654	20	66		1.31	33			695	47	185		95	240	0.95	1	
		36	SR1782-CRAVEN CT	SR 1765 - MARLOWE DR TO CUL-DE-SAC	1	2	NO	NO	0.074	20	7		0.15	3			63	4	2		11	30	0.11		
		37	SR1787-SILVERLEAF DR	CUL-DE-SAC TO CUL-DE-SAC	1	2	NO	NO	1.025	20	103		2.05	51		77	918	62	555		149	380	1.49	1	
		38	SR1788-WINDCHIME LN	SR 1787 - SILVERLEAF DR TO CUL-DE-SAC	1	2	NO	NO	0.084	20	8		0.17	4			91	6	10		12	40	0.12		
		39	SR1789-FERNBANK WAY	SR 1787 - SILVERLEAF DR TO CUL-DE-SAC	1	2	NO	NO	0.166	20	16		0.33	8			162	11	40		24	70	0.24		
		40	SR1790-BIRDSONG CT	SR 1789 - FERNBANK WAY TO CUL-DE-SAC	1	2	NO	NO	0.073	20	7		0.15	4			62	4	25		11	30	0.11		
		41	SR1794-SUNFLOWER MEADOWS LN	SR 1734 - JONAH DAVIS RD TO SR 4597-SUNFLOWER MEADOWS LN	1	2	NO	NO	0.201	20	20		0.40	10		70	171	11	20	1	29	80	0.29		
		42	SR1795-WHITE PINE WAY	SR 1766 - HIGHVIEW DR TO SR 1796 - EMERALD CREST CT	1	2	NO	NO	0.117	20	12		0.23	6			100	7	25		17	50	0.17		
		43	SR1796-EMERALD CREST CT	CUL-DE-SAC TO CUL-DE-SAC	1	2	NO	NO	0.256	20	26		0.51	13			260	17	145		37	100	0.37		
		44	SR1806-EVENSONG CT	SR 1787 - SILVERLEAF DR TO CUL-DE-SAC	1	2	NO	NO	0.225	20	23		0.45	11			211	14	50		33	90	0.33		
		45	SR1807-WISTERIA CT	SR 1755 - WOODLAND CHURCH RD TO CUL-DE-SAC	1	2	NO	NO	0.110	20	11		0.22	6			115	8	40		16	40	0.16		
		46	SR1808-JASMINE CT	SR 1755 - WOODLAND CHURCH RD TO CUL-DE-SAC	1	2	NO	NO	0.130	20	13		0.26	6			130	9	40		19	50	0.19		
		47	SR1858-CASHMERE LN	SR 1714 - WOODLAND CHURCH TO CUL-DE-SAC	1	2	NO	NO	0.538	20	54		1.08	27			478	32	5		78	200	0.78		
		48	SR1859-CANTERBURY CT	CASHMERE LN TO CUL-DE-SAC	1	2	NO	NO	0.070	20	7		0.14	3			84	6	3		10	30	0.10		
		49	SR1860-CHATHAM CT	CASHMERE LN TO CUL-DE-SAC	1	2	NO	NO	0.090	20	9		0.18	4			103	7	3		13	40	0.13		
		50	SR1861-BROWNING PL	CASHMERE LN TO CUL-DE-SAC	1	2	NO	NO	0.116	20	12		0.23	6			120	8	3		17	50	0.17		
		51	SR4597-SUNFLOWER MEADOWS LN	SR 1794 - SUNFLOWER MEADOWS LN TO SR 4581 BAYNAM POND DR	1	2	NO	NO	0.060	20	6		0.12	3		70	51	3	5		9	30	0.09		
		<b>TOTAL FOR PROJ NO. 2022CPT.05.05.20391.1</b>									<b>15.528</b>		<b>1,483</b>	<b>31.09</b>	<b>773</b>	<b>131</b>	<b>3,077</b>	<b>15,610</b>	<b>1,046</b>	<b>6,024</b>	<b>1</b>	<b>2,157</b>	<b>5,630</b>	<b>21.56</b>	<b>7</b>
		<b>GRAND TOTAL</b>									<b>16.858</b>		<b>1,616</b>	<b>33.75</b>	<b>841</b>	<b>131</b>	<b>3,287</b>	<b>16,807</b>	<b>1,126</b>	<b>6,583</b>	<b>1</b>	<b>2,351</b>	<b>6,140</b>	<b>23.50</b>	<b>8</b>











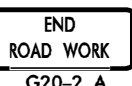
# SIGNING FOR RESURFACING PROJECTS

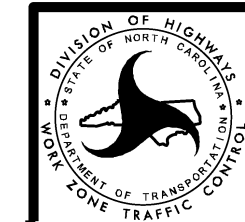
**LEGEND**  
 STATIONARY SIGN  
 DIRECTION OF TRAFFIC FLOW



## MAINLINE (-L-) SIGNING

## -Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	 	<p>PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</p> <p>#2 SIGN ONLY USED WHEN CONSTRUCTION LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)</p>	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> <li>LESS THAN 1000' OF RESURFACING ALONG -Y- LINE</li> <li>SUBDIVISION ROADS</li> <li>DEAD END ROADS</li> </ol> <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;">               W20-1 48" X 48"              PLACED 500' IN ADVANCE OF FLAGGER.         </div> <div style="text-align: center;">               W20-7 A 48" X 48"              PLACED 250' IN ADVANCE OF FLAGGER.         </div> </div>
		<p>- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER.</p> <p>- AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.</p>	
		<p>- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS.</p> <p>- DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS.</p> <p>- INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE.</p> <p>- FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH.</p> <p>- A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</p> <p>- FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.</p>	
		<p>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.</p>	
	<p>THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.</p>		
<p><b>MAPS LESS THAN 2 MILES</b></p>	<p>FOR RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, NO STATIONARY SIGNS ARE REQUIRED. USE PORTABLE "ROAD UNDER CONSTRUCTION" OR "ROAD WORK AHEAD" SIGNS IN LIEU OF STATIONARY ADVANCE WARNINGS SIGNS.</p>		



ADVANCE WARNING SIGNS FOR RURAL AND SUBURBAN 2-LANE ROADWAY RESURFACING

\$\$\$\$SYTIME\$\$\$\$  
 \$\$\$DCN\$\$\$\$  
 \$\$\$USERNAME\$\$\$\$

**DIVISION OF HIGHWAYS  
STATE OF NORTH CAROLINA**

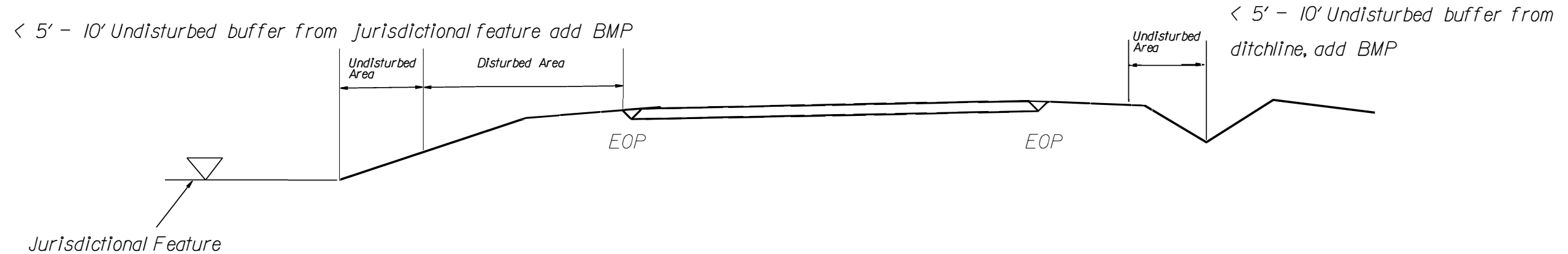
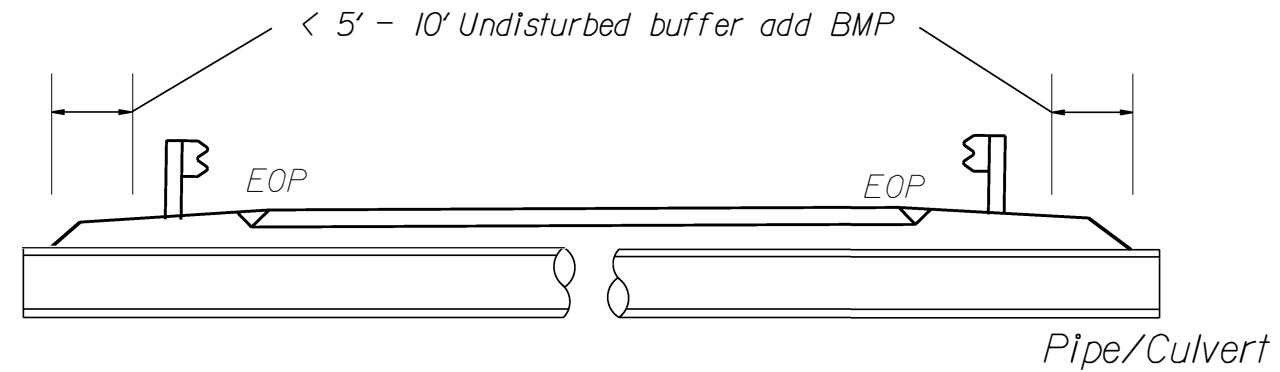
***SOIL STABILIZATION TIMEFRAMES***

<i>SITE DESCRIPTION</i>	<i>STABILIZATION TIME</i>	<i>TIMEFRAME EXCEPTIONS</i>
<b>PERIMETER DIKES, SWALES, DITCHES AND SLOPES</b>	<b>7 DAYS</b>	<b>NONE</b>
<b>HIGH QUALITY WATER (HOW) ZONES</b>	<b>7 DAYS</b>	<b>NONE</b>
<b>SLOPES STEEPER THAN 3:1</b>	<b>7 DAYS</b>	<b>IF SLOPES ARE 10' OR LESS IN LENGTH AND ARE NOT STEEPER THAN 2:1, 14 DAYS ARE ALLOWED.</b>
<b>SLOPES 3:1 OR FLATTER</b>	<b>14 DAYS</b>	<b>7 DAYS FOR SLOPES GREATER THAN 50' IN LENGTH.</b>
<b>ALL OTHER AREAS WITH SLOPES FLATTER THAN 4:1</b>	<b>14 DAYS</b>	<b>NONE, EXCEPT FOR PERIMETERS AND HOW ZONES.</b>

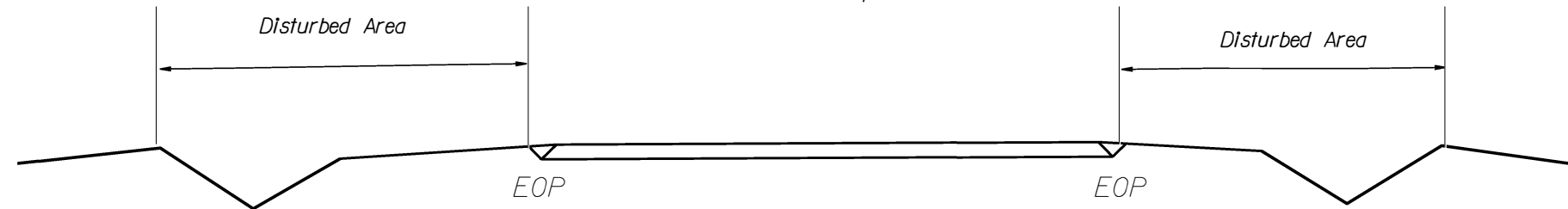
NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

BMP Options: Wattle, Silt Fence or Hardened Aggregate.

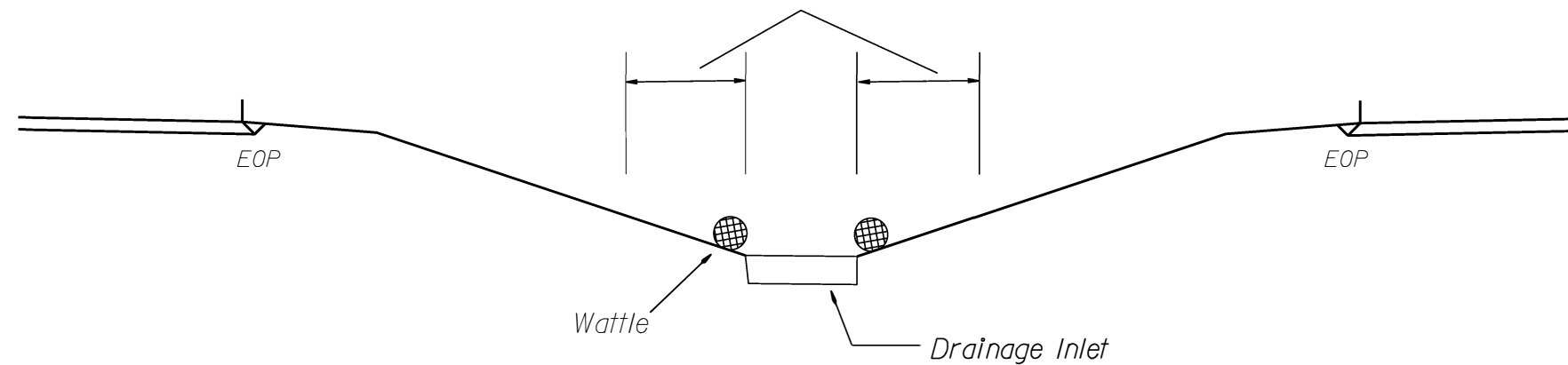
# EROSION CONTROL DETAIL



Use BMP's if shoulders and/or frontslopes and/or ditchline and/or backslopes are disturbed

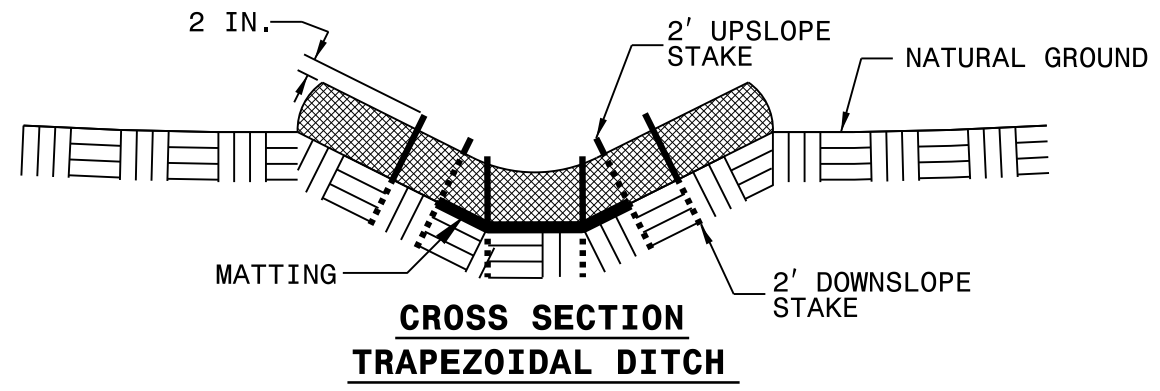
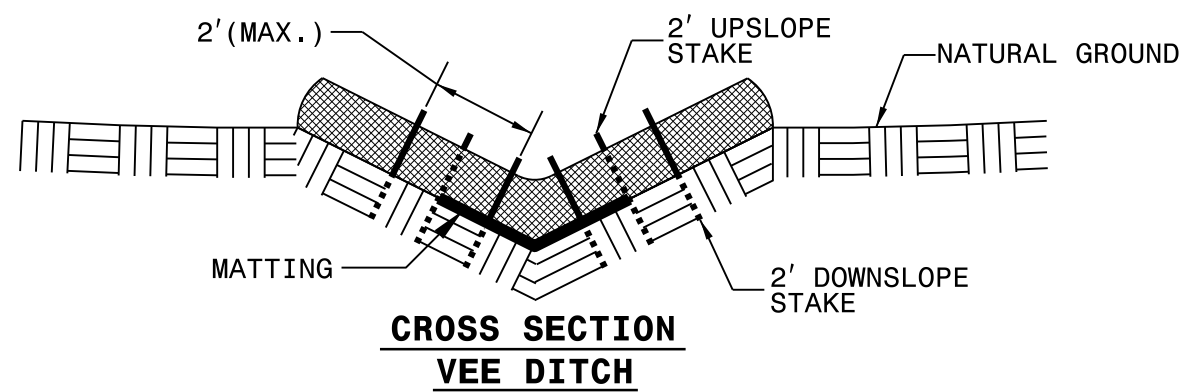
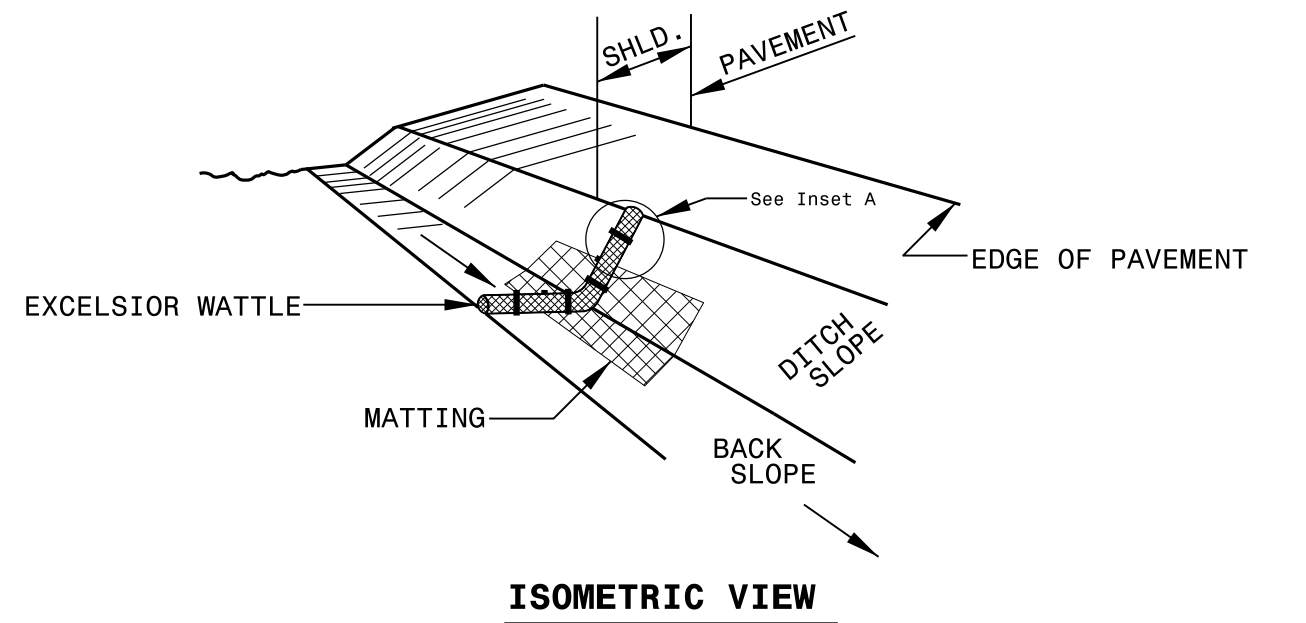


< 5' - 10' Undisturbed buffer from inlet, add wattle



NOT TO SCALE

# WATTLE DETAIL



- NOTES:**
- USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.
  - USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.
  - ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.
  - INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.
  - PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.
  - INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.
  - INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.

