

PHASING

PROJ. REFERENCE NO.	SHEET NO.
R-5737	TMP-3

NOTES:
COMPLETE ANY PROPOSED OR TEMPORARY WIDENING IN SUCH A MANNER THAT PONDING OF WATER WILL NOT OCCUR IN THE TRAVEL LANE.
UNLESS OTHERWISE STATED ONLY CONSTRUCT UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE.

PHASE I

OBJECTIVE: CONSTRUCT ON-SITE DETOURS AND MEDIAN CROSSOVERS FOR PHASE II AND PHASE III TRAFFIC PATTERN. SEE SHEETS TMP-4 THROUGH TMP-11A

STEP 1: USING RSD 1101.01, SHEETS 2 AND 3 OF 3, INSTALL ADVANCE WARNING SIGNS ON -L- AND ALL -Y- LINES. SEE ALSO THE WORK ZONE SPEED LIMITS SIGNS ON SHEETS TMP-4 AND TMP-8.

STEP 2: AWAY FROM TRAFFIC AND USING RSD 1101.04 SHEETS 1 OF 1 WHEN NEEDED, BEGIN CONSTRUCTION OF THE FOLLOWING:

- CLEARING AND GRADING ALONG EAST SIDE OF -L- (US 29-70).
- BEGIN CONSTRUCTION OF -Y1- & -Y4-, C1 AND C2 (ROUNDBABOUTS) ALONG EAST SIDE OF -L- UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE. (SEE TMP-6&7)
- BEGIN CUT EXCAVATION FOR PROPOSED -Y1- ALONG WEST SIDE OF -L-. (SEE TMP-7)

WHILE MAINTAINING TRAFFIC ON EXISTING ROADWAYS AND USING RSD 1101.02 SHEETS 1, 2 AND 3 OF 14, PERFORM THE FOLLOWING:

- ASPHALT OVERLAY OF -Y6- /-Y3- (CALDCLEUGH RD) AND PLACE TEMPORARY PAVEMENT MARKINGS. SEE TMP-4 THRU 7
- CONSTRUCT TEMPORARY DRAINAGE AND TEMPORARY PAVEMENT TO COMPLETE -DETA-, -DETB-, -DETC-, -DETD AND -DETE- FOR LATER USE BY MEDIAN CROSSOVERS. SEE TMP-5, 6 & 8
- CONSTRUCT -DET3- UP TO THE CONTIGUOUS EXISTING PAVEMENT OF -L- (US 29-70) AND -Y6- (CALDCLEUGH RD), INCLUDING MODIFICATIONS TO THE EXISTING GUARDRAIL ON THE RIGHT SIDE OF SB -L- (US-29-70). (SEE TMP-5)
- REINFORCE EXISTING SHOULDER AS NEEDED FOR USE IN PHASE II AND III. SEE TMP-5 & 6
- CONSTRUCT -DET- FROM -DET- STA. 10+00 +/- TO -DET- STA. 18+19 +/- UP TO THE EDGE AND ELEVATION OF THE CONTIGUOUS EXISTING PAVEMENT OF -Y1- (OLD GREENSBORO RD) AND INCLUDING TIE-IN TO -Y3- (CALDCLEUGH RD). (SEE TMP-7)
- CONSTRUCT -DET2- UP TO THE EDGE AND ELEVATION OF THE CONTIGUOUS EXISTING PAVEMENT ON -L- (US 29-70) AND ACROSS -Y1- (OLD GREENSBORO RD) AND TO -DET2- STA 43+39 +/- . (SEE TMP-6 & 7)

THE CONTRACTOR SHALL NOTIFY THE ENGINEER THIRTY (30) CALENDAR DAYS PRIOR TO ANY CLOSURES.

THE CONTRACTOR SHALL COMPLETE PHASE I, STEPS 3 THRU 5 WITHIN TWENTY-ONE (21) CALENDAR DAYS. [SEE INTERMEDIATED CONTRACT TIME - LIQUIDATED DAMAGES]

STEP 3: CLOSE CALDCLEUGH RD TO ALL TRAFFIC BETWEEN -Y2- (DCCC RD) AND -L- (US 29-70) AS SHOWN ON TMP-9 AND TMP-10. DETOUR OLD GREENSBORO RD, CALDCLEUGH RD, AND DCCC RD TRAFFIC APPROACHING FROM THE NORTH ON SOUTHBOUND -L- VIA EVANS ROAD AS SHOWN ON SHEET TMP-21. DETOUR TRAFFIC DESTINED FOR NORTHBOUND -L- FROM CALDCLEUGH RD, OLD GREENSBORO RD, AND DCCC RD TO NORTHBOUND -L- VIA EVANS ROAD AS SHOWN ON TMP-22. MAINTAIN ACCESS TO -Y2- (DCCC RD) FROM OLD GREENSBORO RD VIA CALDCLEUGH RD AS SHOWN ON SHEET TMP-9.

STEP 4: USING RSD 1101.02 SHEET 1 AND 3 OF 14, PERFORM THE FOLLOWING:

- CONSTRUCT -DET2- AND -DET4- UP TO THE EDGE AND ELEVATION OF THE CONTIGUOUS EXISTING PAVEMENT OF -L- AND INCLUDING TIE-IN TO -Y2-. (SEE TMP-9 & 10)
- PLACE TEMPORARY MARKINGS ON -DET4-. (SEE TMP-11)

STEP 5: UNCOVER GUIDE SIGN ON SOUTHBOUND -L- AND REOPEN EXIT FROM SOUTHBOUND -L- TO DAVIDSON-DAVIE COMMUNITY COLLEGE (DDCC) VIA -DET4-. (SEE SHEETS TMP-11 AND TMP-11A)

THE CONTRACTOR MAY PERFORM WORK IN PHASE I, STEP 6 CONCURRENT WITH PHASE I, STEPS 3 THRU 5 AS APPROVED BY THE ENGINEER.

STEP 6: PERFORM THE FOLLOWING WORK TO PREPARE FOR SHIFT TO PHASE II TEMPORARY TRAFFIC PATTERN SHOWN ON SHEETS TMP-12 THRU TMP-18:

- USING RSD 1101.02, SHEETS 3 AND 4 OF 14 AND RSD 1101.04, INSTALL AND TEMPORARILY COVER OFF-SITE OVERSIZE VEHICLE ADVANCE WARNING SIGNS AS SHOWN ON SHEET TMP-48.
- INSTALL PHASE II SIGNS AS MUCH AS POSSIBLE AND TEMPORARILY COVER.
- PLACE PHASE II TEMPORARY PAVEMENT MARKINGS ON -DET2- AS MUCH AS POSSIBLE AWAY FROM TRAFFIC.
- INSTALL P.C.B. AS MUCH AS POSSIBLE ON -DET2- BETWEEN OLD GREENSBORO RD AND -DET4-.

PHASE II

OBJECTIVE: SHIFT US 29-70 INTO TEMPORARY TWO-LANE, TWO-WAY PATTERN ON ON-SITE DETOUR ON WEST SIDE OF EXISTING US 29-70 TO CONSTRUCT NEW BRIDGES, NEW OVAL ROUNDBABOUT AS MUCH AS POSSIBLE AND NEW ROADWAYS ON EAST SIDE OF US 29-70. SEE SHEETS TMP-12 THROUGH TMP-20

THE ENGINEER WILL CONTACT THE OVERSIZE / OVERWEIGHT UNIT (SEE SHEET TMP-2B, GENERAL NOTE EE)

STEP 1: UNCOVER OVERSIZED VEHICLE ADVANCE WARNING SIGNING SHOWN ON SHEET TMP-48. USING RSD 1101.02, SHEET 3 OF 14, CLOSE THE LEFT NORTHBOUND LANE OF -L- (US 29-70) BETWEEN THE -Y5- (OLD US 29) / -Y6- (CALDCLEUGH RD) INTERSECTION TO -L- 78+80 +/- . ONCE THIS LANE IS CLOSED, PERFORM THE FOLLOWING WORK:

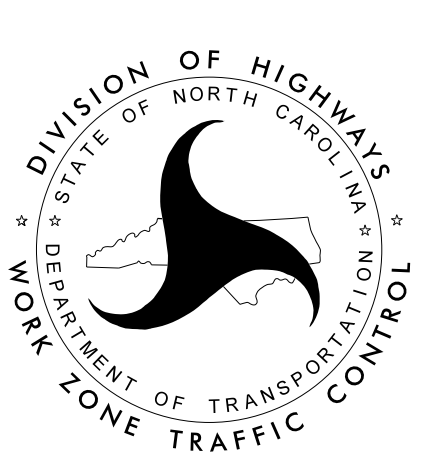
- REVISE THE NORTHBOUND PAVEMENT MARKINGS AS SHOWN ON TMP-12 AND TMP-13.
- PLACE DRUMS, P.C.B., AND MARKINGS FOR -DETA- ON TMP-13 & 14 AND FOR -DETB- ON TMP-16 AS MUCH AS POSSIBLE BEHIND LANE CLOSURE.

NOTE: MAINTAIN THIS CLOSURE OF THE LEFT NORTBOUND LANE OF -L- (US 29-70) FOR THE DURATION OF PHASE II.

STEP 2: USING RSD 1101.02, SHEET 3 OF 14, CLOSE THE RIGHT SOUTHBOUND LANE OF -L- (US 29-70) FROM -L- STA 78+00 +/- TO THE -Y5- (OLD US 29) / -Y6- (CALDCLEUGH RD) INTERSECTION AND PERFORM THE FOLLOWING WORK:

- INSTALL P.C.B. AND TEMPORARY MARKINGS ALONG PHASE II SOUTHBOUND LANE AS MUCH AS POSSIBLE BEHIND RIGHT LANE CLOSURE FROM -L- STA 41+93 +/- TO -L- STA 71+80 +/- . (SEE TMP-14 THRU TMP-17)
- PROVIDE A GAP IN THE PHASE II P.C.B. TO TEMPORARILY MAINTAIN EXISTING LIMITED MOVEMENT ACCESS AT THE EXISTING OLD GREENSBORO ROAD INTERSECTION.
- FLARE TEMPORARY APPROACH ENDS OF INCOMPLETE P.C.B. TO 30 FT OFFSET FROM EDGE OF TRAVEL LANE OR PROTECT APPROACH END WITH TMA OR CRASH CUSHION IMMEDIATELY FOLLOWING P.C.B. INSTALLATION.

PHASE II CONTINUED ON SHEET TMP-3A

APPROVED: _____ DATE: _____ SEAL			PHASING
DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED			



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