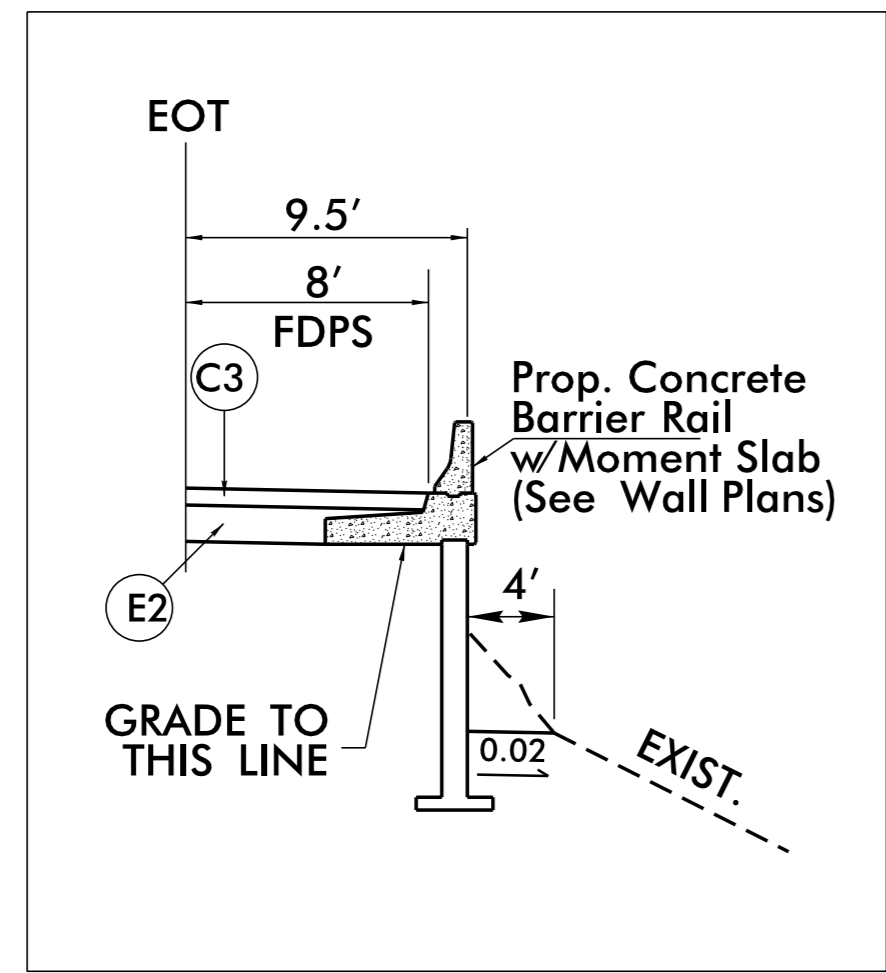


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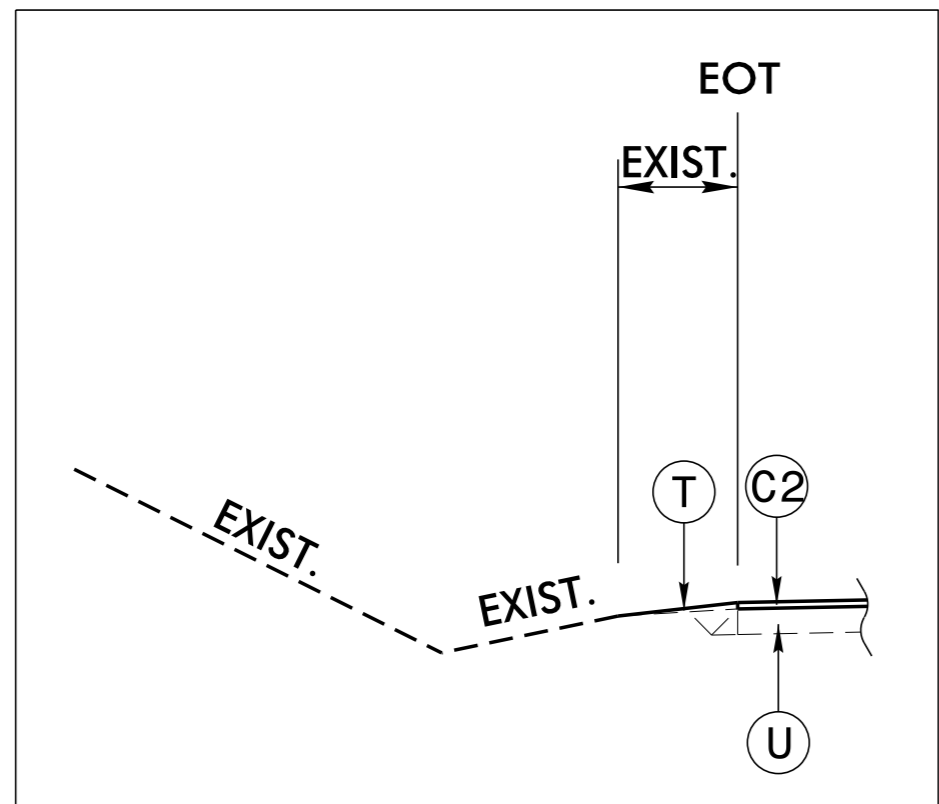
PAVEMENT SCHEDULE (FINAL PAVEMENT DESIGN)	
C1	PROP. VAR. DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 110 LBS. PER SQ. YD. PER 1" DEPTH. TO BE PLACED IN LAYERS NOT TO EXCEED 1.5" IN DEPTH.
C2	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
C3	PROP. APPROX. 3" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YARD. IN EACH OF TWO LAYERS.
E1	PROP. VAR. DEPTH ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 114 LBS. PER SQ. YD. PER 1" DEPTH. TO BE PLACED IN LAYERS NOT TO EXCEED 5.5" IN DEPTH.
E2	PROP. APPROX. 5.5" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 627 LBS. PER SQ. YD.
J	PROP. 8" AGGREGATE BASE COURSE
U	EXISTING PAVEMENT
R1	EXPRESSWAY GUTTER
T	EARTH MATERIAL
W	VARIABLE DEPTH ASPHALT PAVEMENT (SEE STANDARD WEDGING DETAIL)

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.



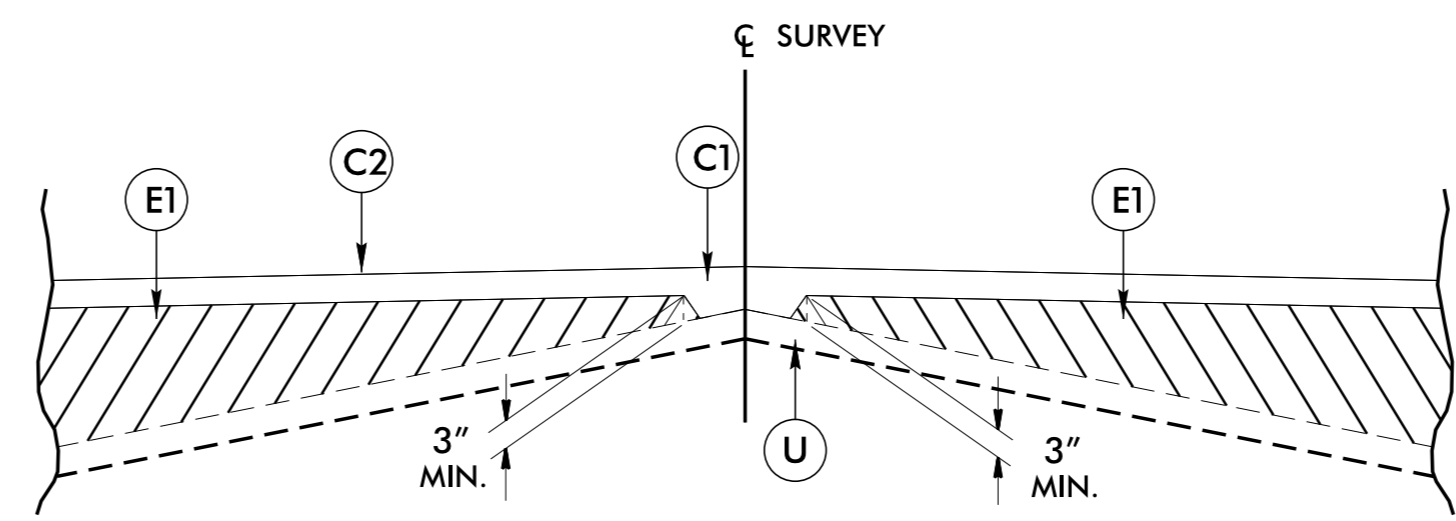
INSET A

TO BE USED WITH TYPICAL SECTION NO. 1 & 2
-L- STA. 15+00.00 TO STA. 20+00.00 RT.

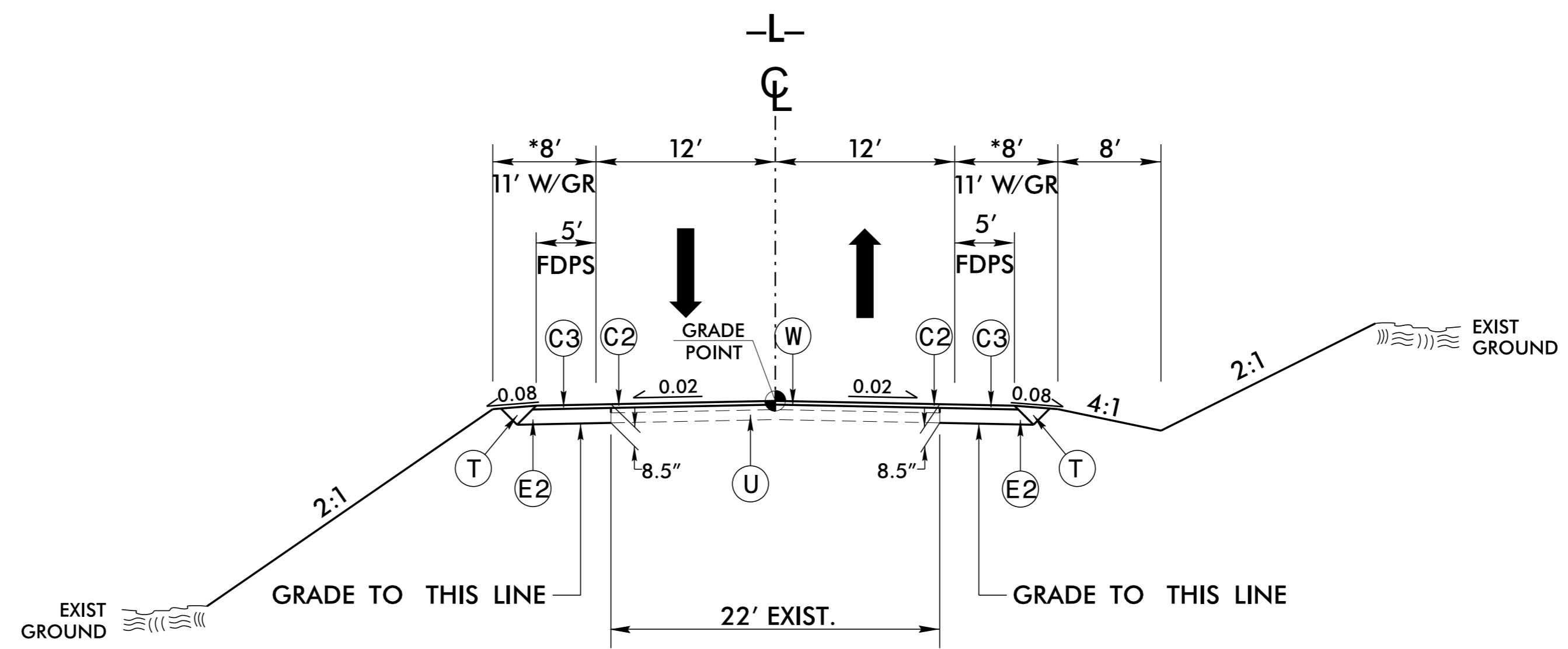


INSET B

TO BE USED WITH TYPICAL SECTION NO. 1
-L- STA. 30+10.00 TO STA. 32+25.00 LT.



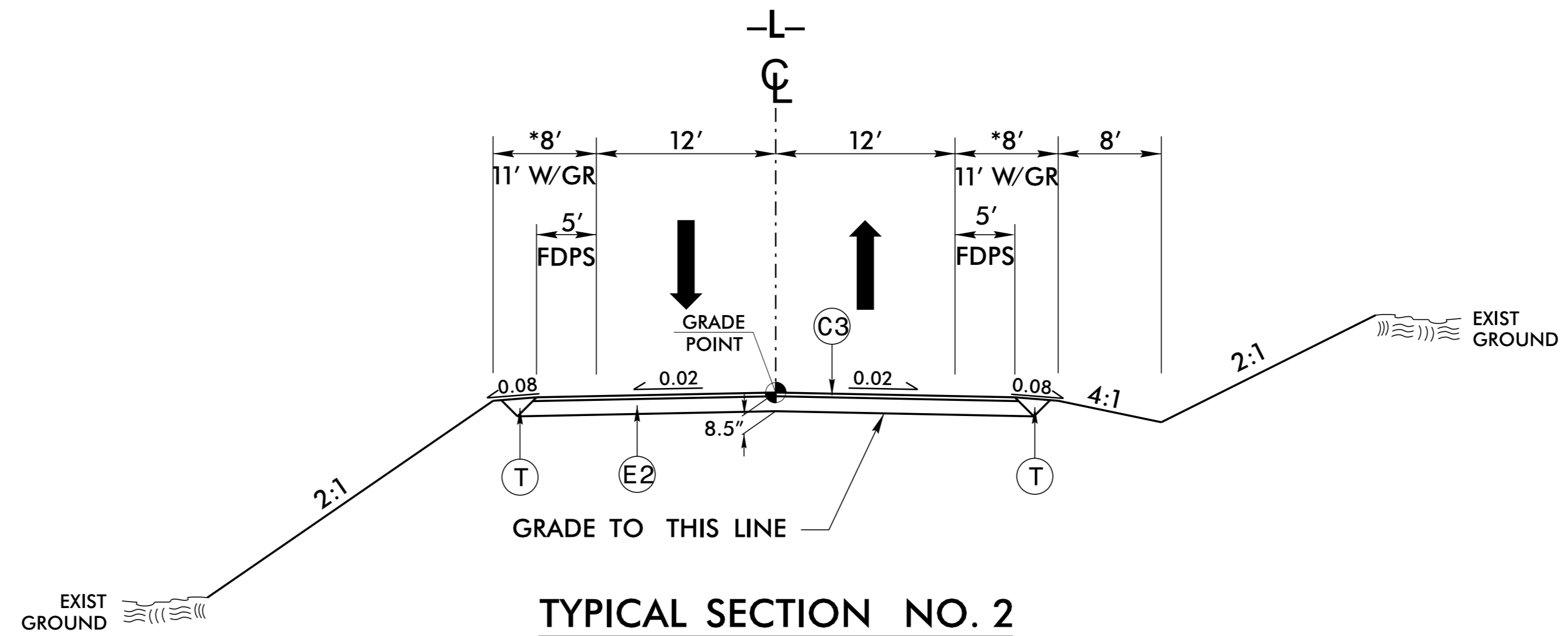
Detail Showing Method of Wedging



TYPICAL SECTION NO. 1

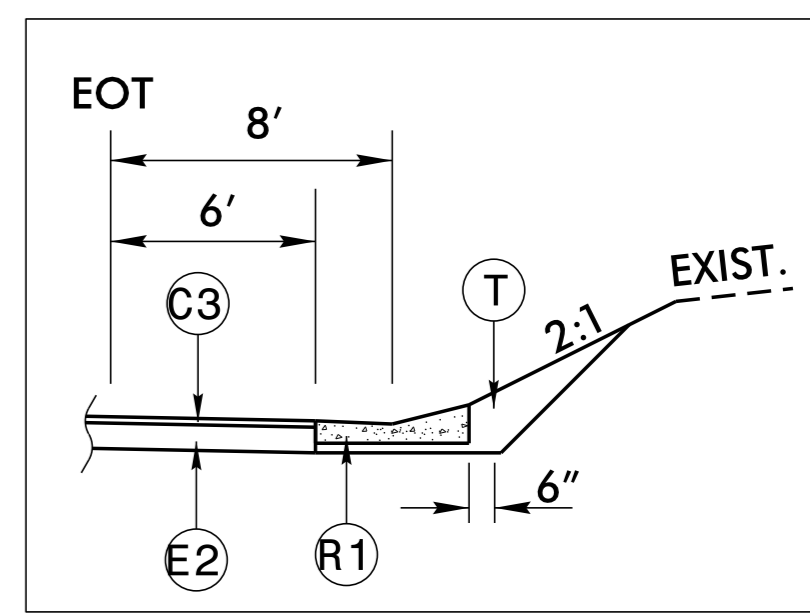
USE TYPICAL SECTION NO. 1
TRANSITION FROM EXISTING AT -L- STA. 15+25.00 TO TYPICAL NO. 1 AT -L- STA. 15+75.00
-L- STA. 15+75.00 TO STA. 17+50.00

-L- STA. 29+50.00 TO STA. 31+75.00
TRANSITION FROM TYPICAL NO. 1 AT -L- STA. 31+75.00 TO EXISTING AT -L- STA. 32+25.00
* PAVE TO THE FACE OF GUARDRAIL



TYPICAL SECTION NO. 2

USE TYPICAL SECTION NO. 2
-L- STA. 17+50.00 TO STA. 22+40.00 (BEGIN BRIDGE)
-L- STA. 25+20.00 (END BRIDGE) TO STA. 29+50.00
* PAVE TO THE FACE OF GUARDRAIL



INSET C

TO BE USED WITH TYPICAL SECTION NO. 2
-L- STA. 28+70.00 TO STA. 32+00.00 RT.

PROJECT REFERENCE NO. BR-0002	SHEET NO. 2A-1
ROADWAY DESIGN ENGINEER TATIA L. WHITE 7/28/2021 SEAL 024641	PAVEMENT DESIGN ENGINEER CLARK S. MORRISON 7/18/2021 SEAL 022896
<p>DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED</p>	

16 JUL 2021 06:54 \\BR0002.Rdy...tup.dgn