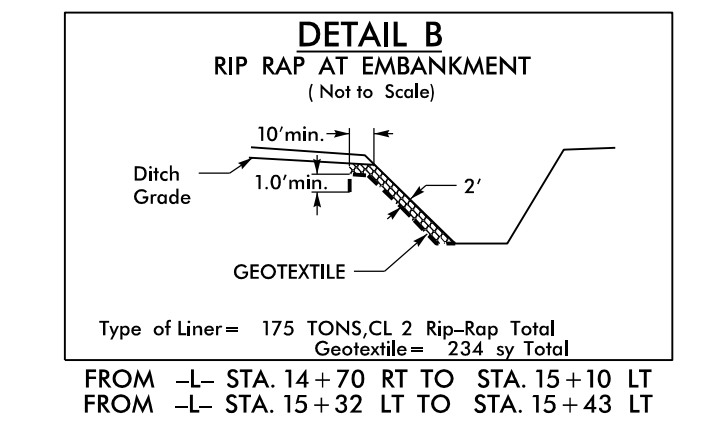
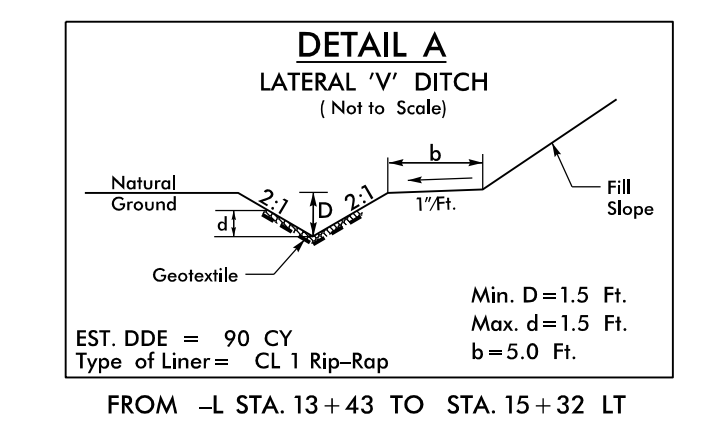
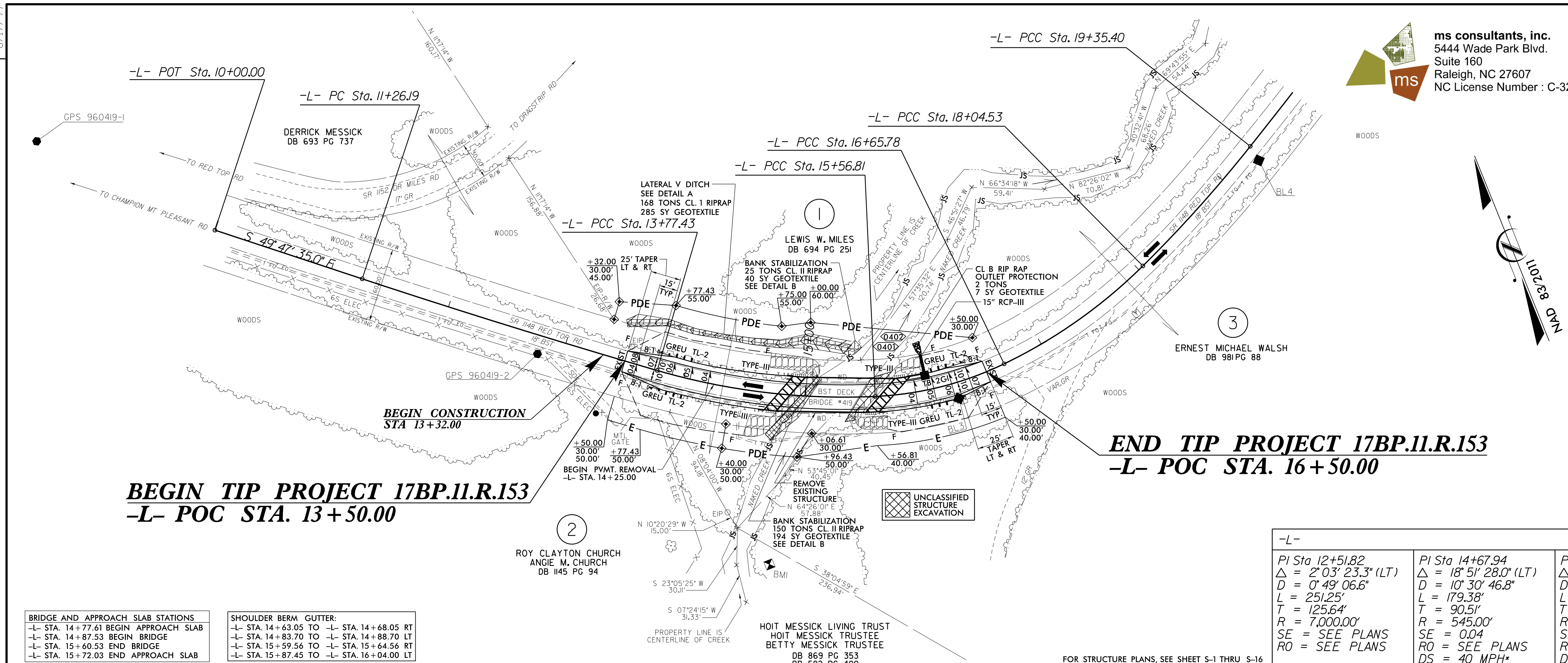


PROJECT REFERENCE NO. <b>17BP.11.R.153</b>	SHEET NO. <b>4</b>
RW SHEET NO.	
ROADWAY DESIGN ENGINEER <b>SEAL 041453</b> M. Travis Potts	HYDRAULICS ENGINEER <b>SEAL 12786</b> Steven Bondor
ms consultants, inc. 5444 Wade Park Blvd. Suite 160 Raleigh, NC 27607 NC License Number : C-3239	
DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED	

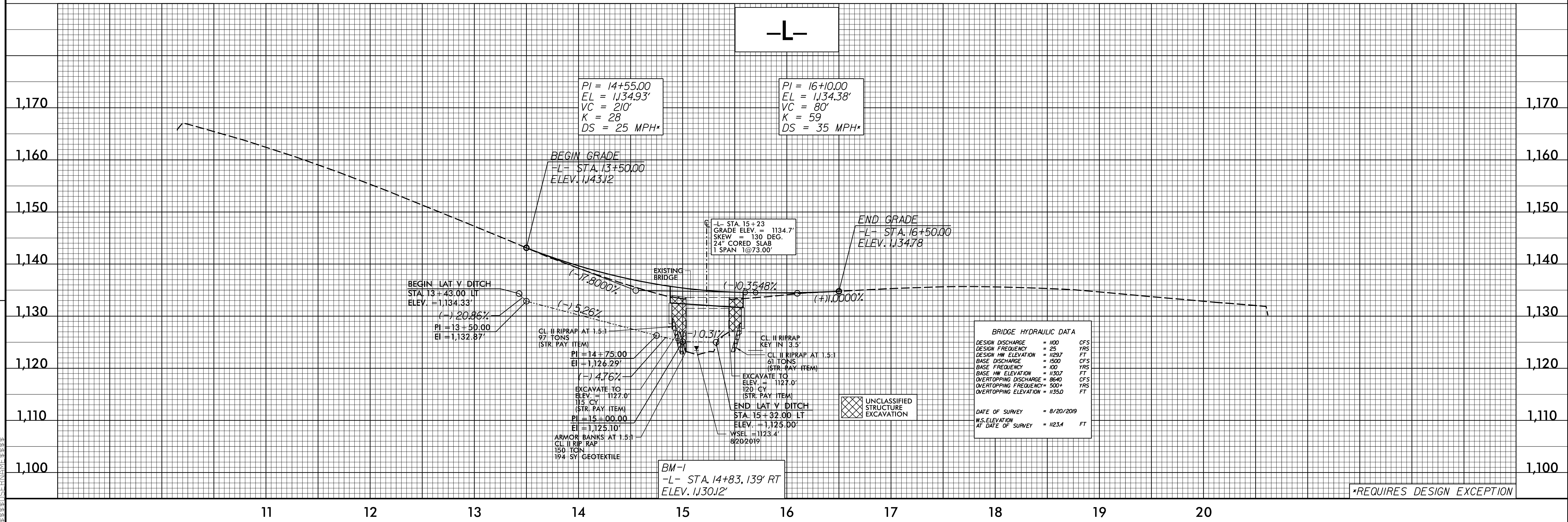


**END TIP PROJECT 17BP.11.R.153**  
**-L- POC STA. 16+50.00**

**BEGIN TIP PROJECT 17BP.11.R.153**  
**-L- POC STA. 13+50.00**

-L-	PI Sta 12+51.82	PI Sta 14+67.94	PI Sta 16+12.05	PI Sta 17+35.80
	$\Delta = 2^{\circ} 03' 23.3''$ (LT)	$\Delta = 18^{\circ} 51' 28.0''$ (LT)	$\Delta = 23^{\circ} 07' 26.6''$ (LT)	$\Delta = 18^{\circ} 55' 43.6''$ (LT)
	$D = 0^{\circ} 49' 06.6''$	$D = 10^{\circ} 30' 46.8''$	$D = 21^{\circ} 13' 14.4''$	$D = 13^{\circ} 38' 30.7''$
	$L = 251.25'$	$L = 179.38'$	$L = 108.97'$	$L = 138.76'$
	$T = 125.64'$	$T = 90.51'$	$T = 55.24'$	$T = 70.02'$
	$R = 7,000.00'$	$R = 545.00'$	$R = 270.00'$	$R = 420.00'$
	SE = SEE PLANS	SE = 0.04	SE = 0.04	SE = 0.04
	RO = SEE PLANS	RO = SEE PLANS	RO = SEE PLANS	RO = SEE PLANS
		DS = 40 MPH*	DS = 30 MPH*	DS = 30 MPH*

BRIDGE AND APPROACH SLAB STATIONS	SHOULDER BERM GUTTER:
-L- STA. 14+77.61 BEGIN APPROACH SLAB	-L- STA. 14+63.05 TO -L- STA. 14+68.05 RT
-L- STA. 14+87.53 BEGIN BRIDGE	-L- STA. 14+83.70 TO -L- STA. 14+88.70 LT
-L- STA. 15+60.53 END BRIDGE	-L- STA. 15+59.56 TO -L- STA. 15+64.56 RT
-L- STA. 15+72.03 END APPROACH SLAB	-L- STA. 15+87.45 TO -L- STA. 16+04.00 LT



\*REQUIRES DESIGN EXCEPTION

REVISIONS

8/17/09  
 06 MAY 2021 12:50  
 NS:\06\6\17BP.11.R.153\06 17BP.11.R.153\Prj\17BP.11.R.153\_r.dwg\_psh.dgn  
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