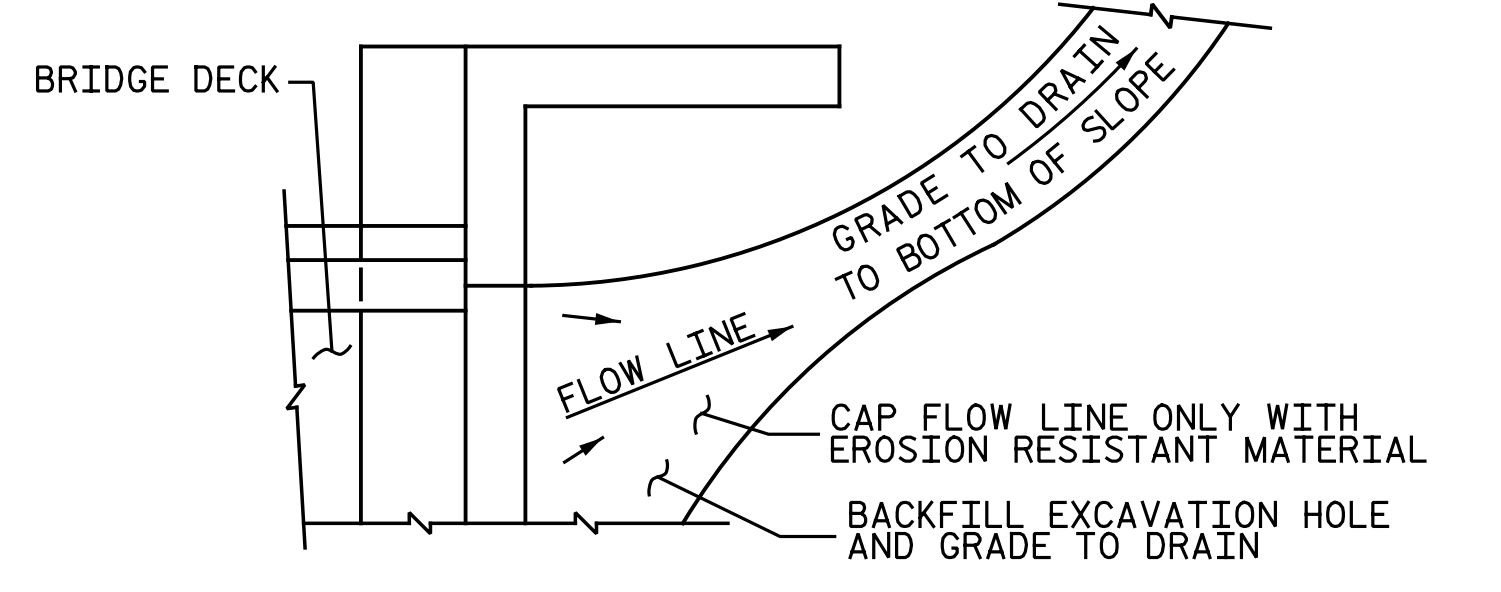
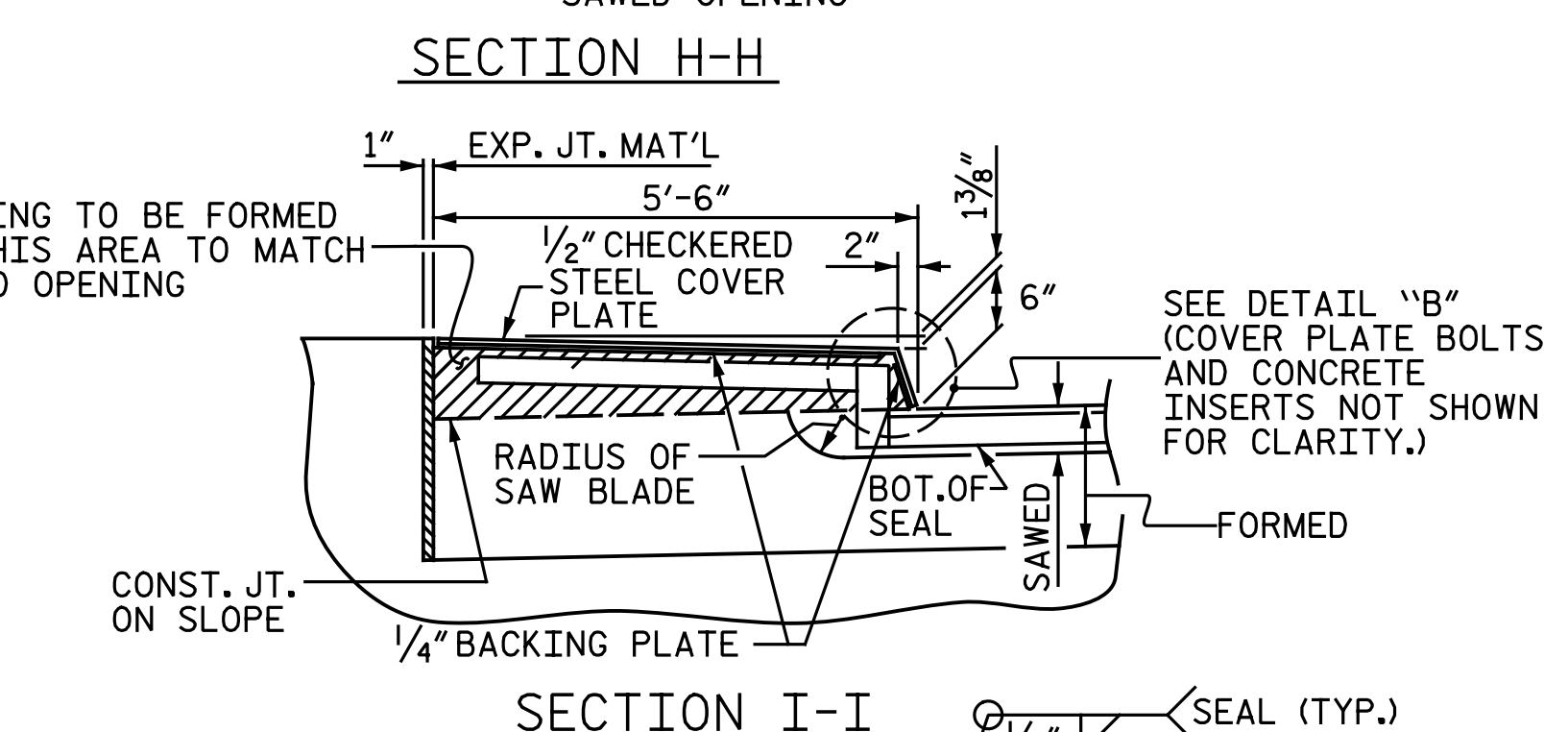
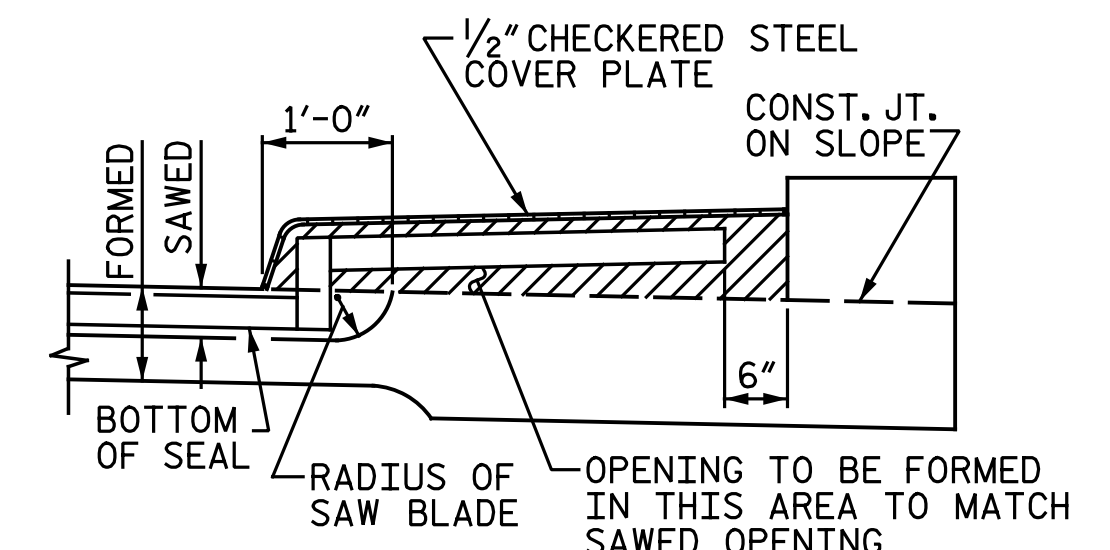
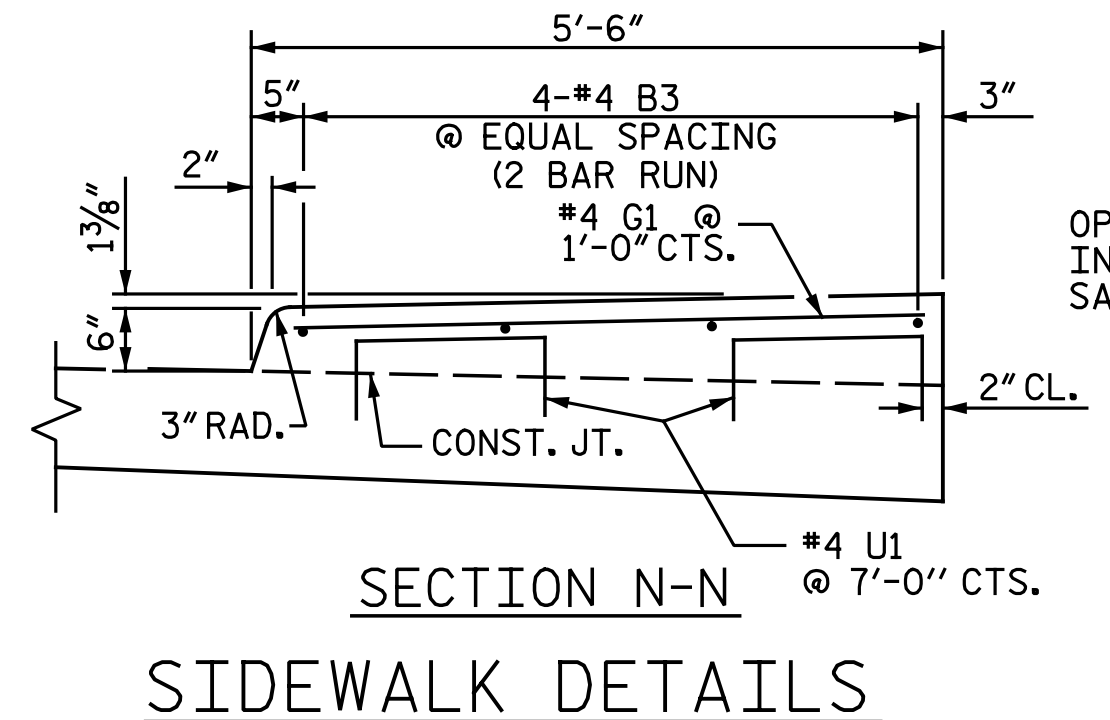
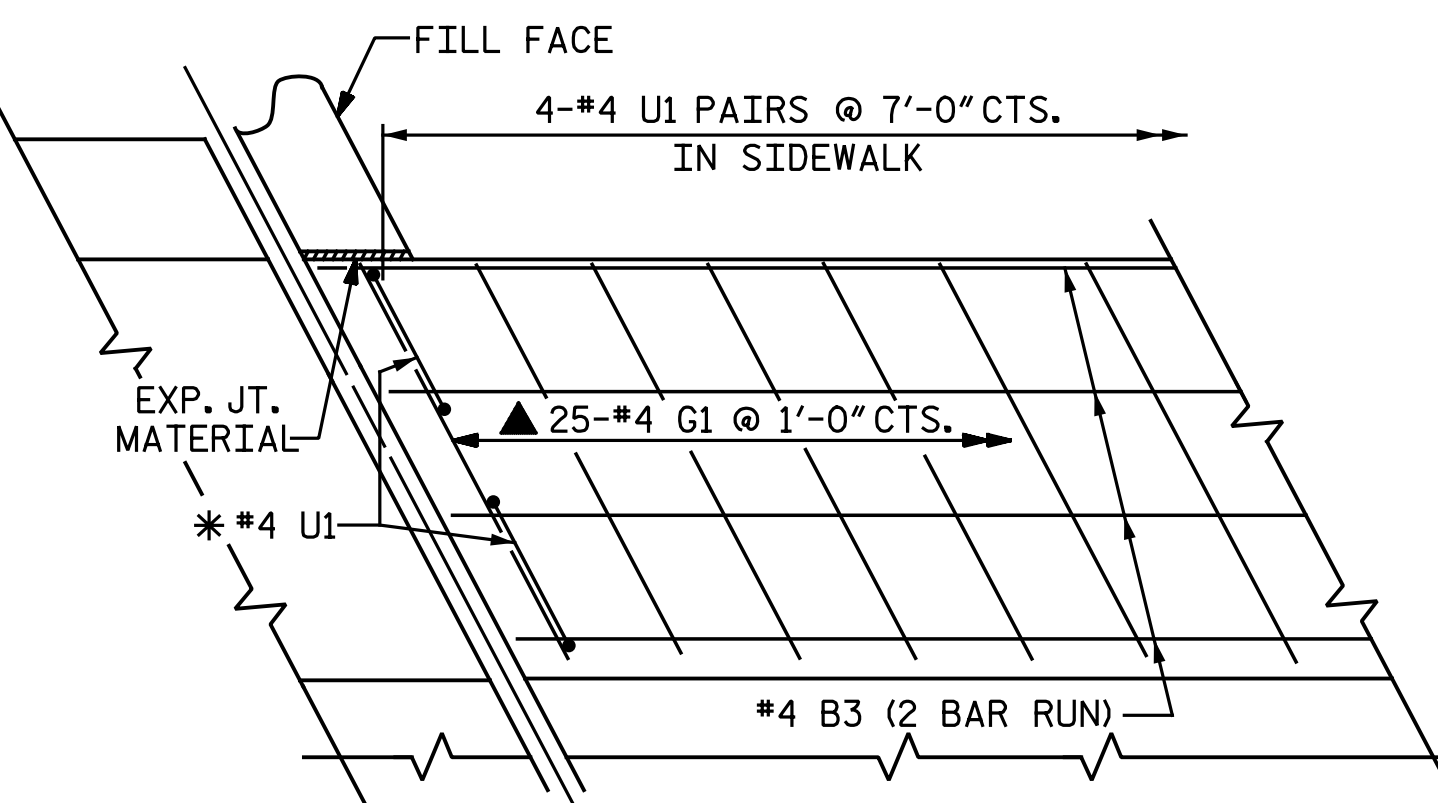


ELASTOMERIC CONCRETE	
END BENT NO.	ELASTOMERIC CONCRETE * (CU. FT.)
1	7.4
2	7.4
TOTAL	14.8

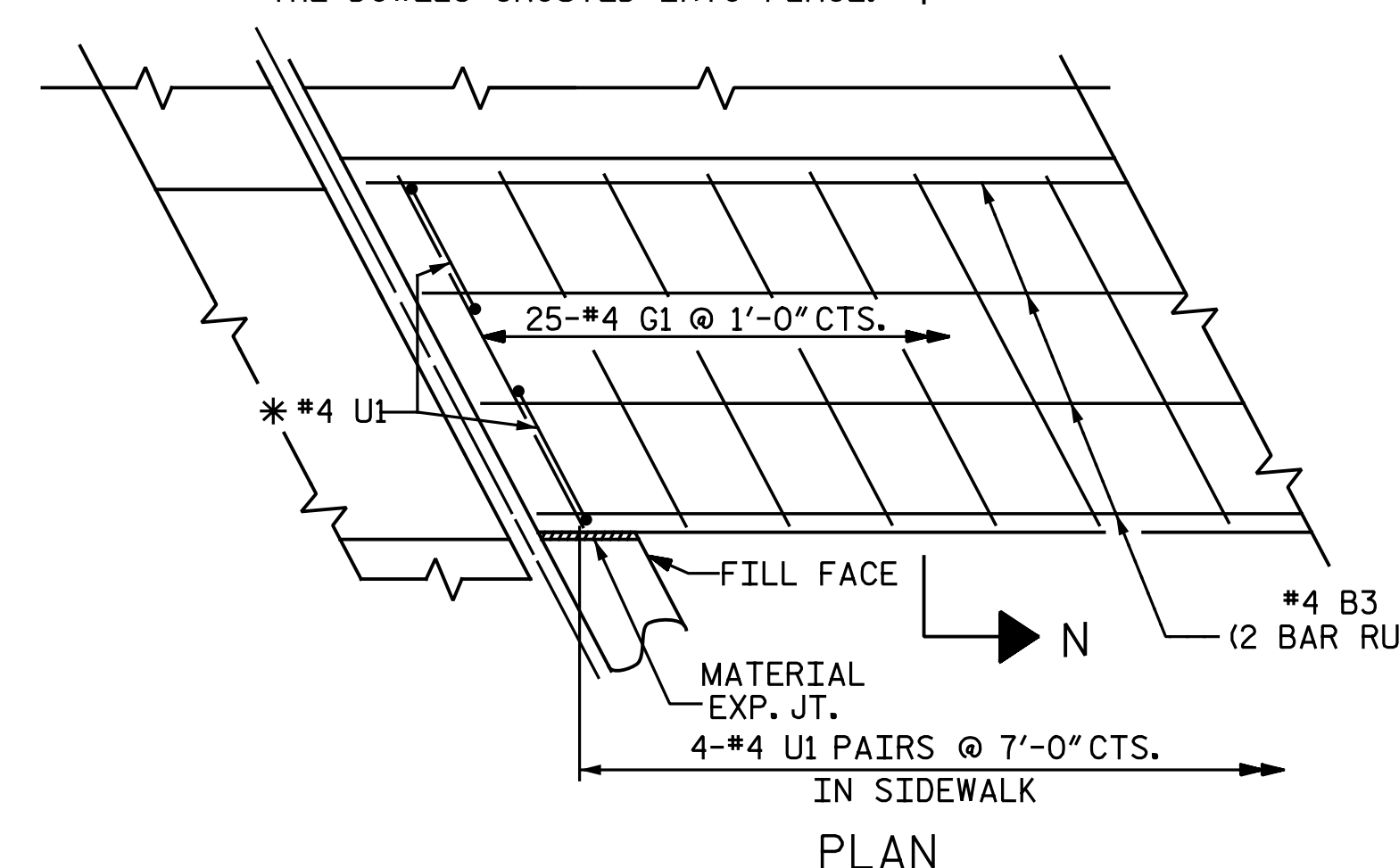
* BASED ON THE MINIMUM BLOCKOUT SHOWN.



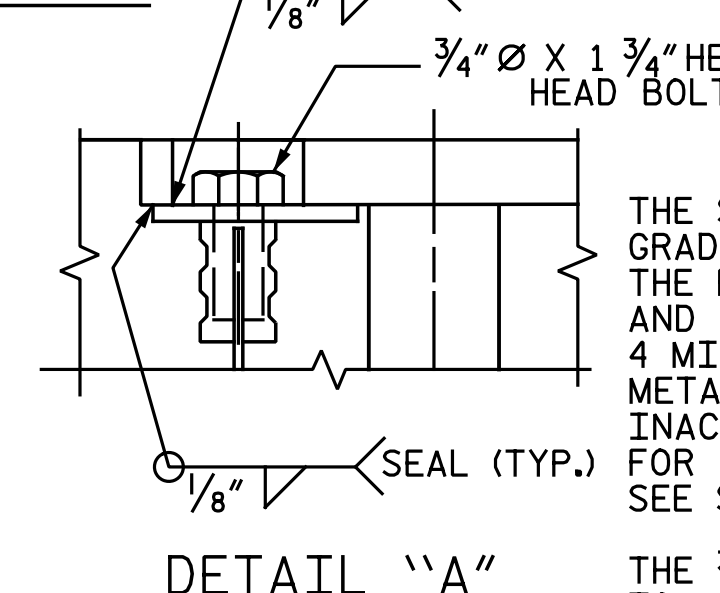
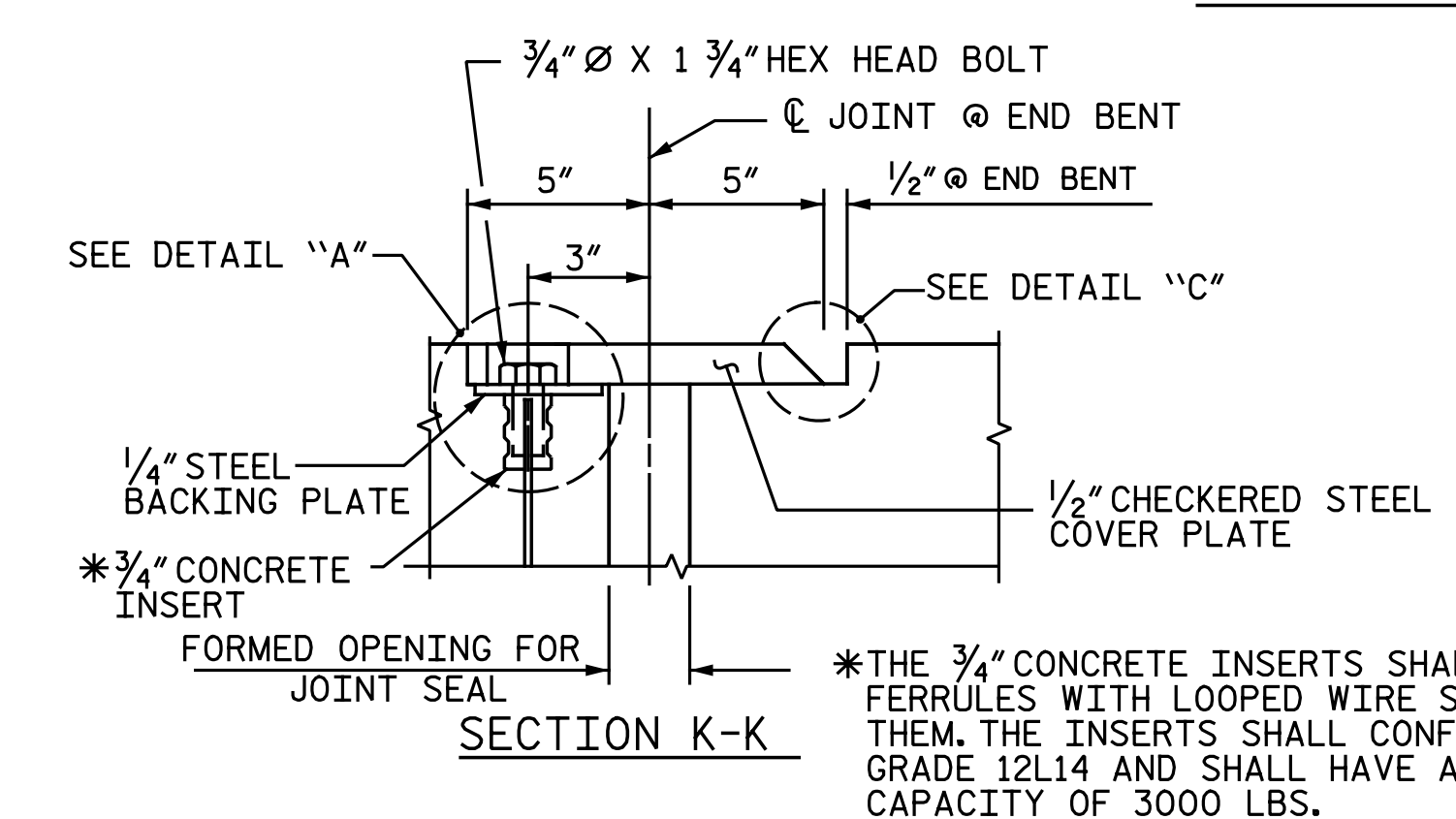
NOTE: IF THE APPROACH SLAB IS NOT CONSTRUCTED IMMEDIATELY AFTER THE BACKFILLING OF THE END BENT EXCAVATION, GRADE TO DRAIN TO THE BOTTOM OF THE SLOPE AND PROVIDE EROSION RESISTANT MATERIAL, SUCH AS FIBERGLASS ROVING OR AS DIRECTED BY THE ENGINEER TO PREVENT SOIL EROSION AND TO PROTECT THE AREA ADJACENT TO THE STRUCTURE. THE CONTRACTOR WILL BE REQUIRED TO REMOVE THESE MATERIALS PRIOR TO CONSTRUCTION OF THE APPROACH SLAB.



* THESE BARS ARE TO BE PLACED AFTER THE SAWING OF THE JOINT. THE HOLES SHALL BE DRILLED AND THE DOWELS GROUTED INTO PLACE.



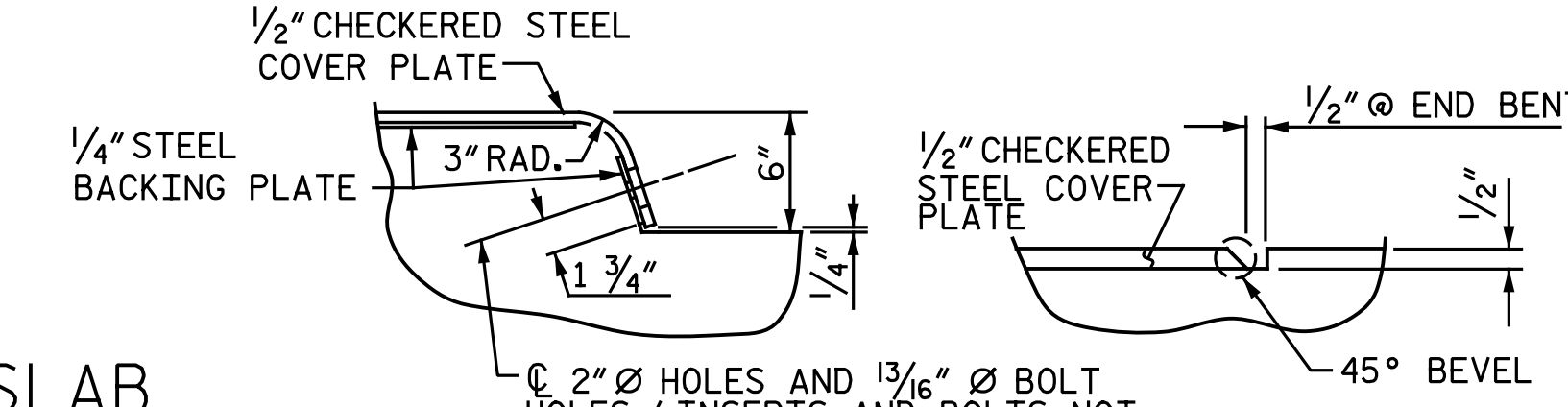
DETAILS OF SIDEWALK ON APPROACH SLAB



THE STEEL PLATES SHALL CONFORM TO AASHTO M270 GRADE 36 OR APPROVED EQUAL. AFTER FABRICATION, THE PLATES SHALL BE COMMERCIALY BLAST CLEANED AND EITHER COATED WITH A MINIMUM THICKNESS OF 4 MILS (DRY) OF ZINC-RICH PAINT, GALVANIZED OR METALLIZED TO A MINIMUM THICKNESS OF 6 MILS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS. FOR THERMAL SPRAYED COATINGS (METALLIZATION), SEE SPECIAL PROVISIONS.

THE 3/4" DIAMETER HEX HEAD BOLTS SHALL CONFORM TO ASTM F593 ALLOY 304 STAINLESS STEEL.

NO SEPARATE PAYMENT WILL BE MADE FOR FURNISHING AND INSTALLING THE COVER PLATE. THE ENTIRE COST OF THIS WORK SHALL BE INCLUDED IN THE LUMP SUM PRICE FOR "FOAM JOINT SEALS".



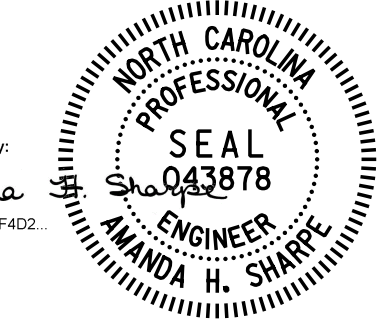
DETAIL "B" JOINT SEAL DETAILS @ END BENT

ASSEMBLED BY : N. B. SPEAKS
 CHECKED BY : A. H. SHARPE
 DATE : 7-15-19
 DATE : 9-9-19
 DRAWN BY : FCJ 11/88
 CHECKED BY : ARB 11/88
 REV. 6/13
 REV. 12/17
 REV. 5/18
 MAA/GM
 MAA/THC
 MAA/THC

▲ FOR APPROACH SLAB @ END BENT 2. SEE DETAIL "D" FOR "G" BAR LAYOUT IN THE LEFT SIDEWALK ON APPROACH SLAB @ END BENT 1.

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

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 Michael Baker Engineering
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 Cary, North Carolina 27518
 NC License No.: F-1084



PROJECT NO. I-5878
 HARNETT COUNTY
 STATION: 26+05.28 -Y14-

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH
 STANDARD
 BRIDGE APPROACH SLAB
 DETAILS

REVISIONS						SHEET NO. SI-35
NO.	BY:	DATE:	NO.	BY:	DATE:	
1			3			TOTAL SHEETS
2			4			35