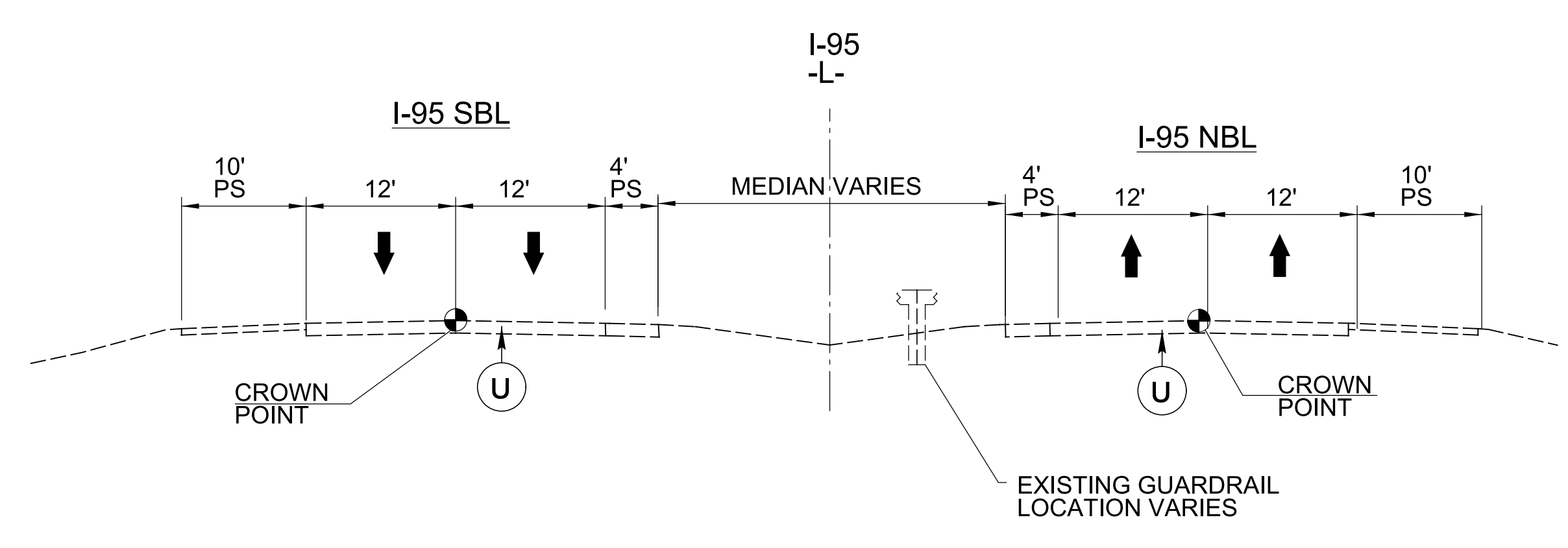


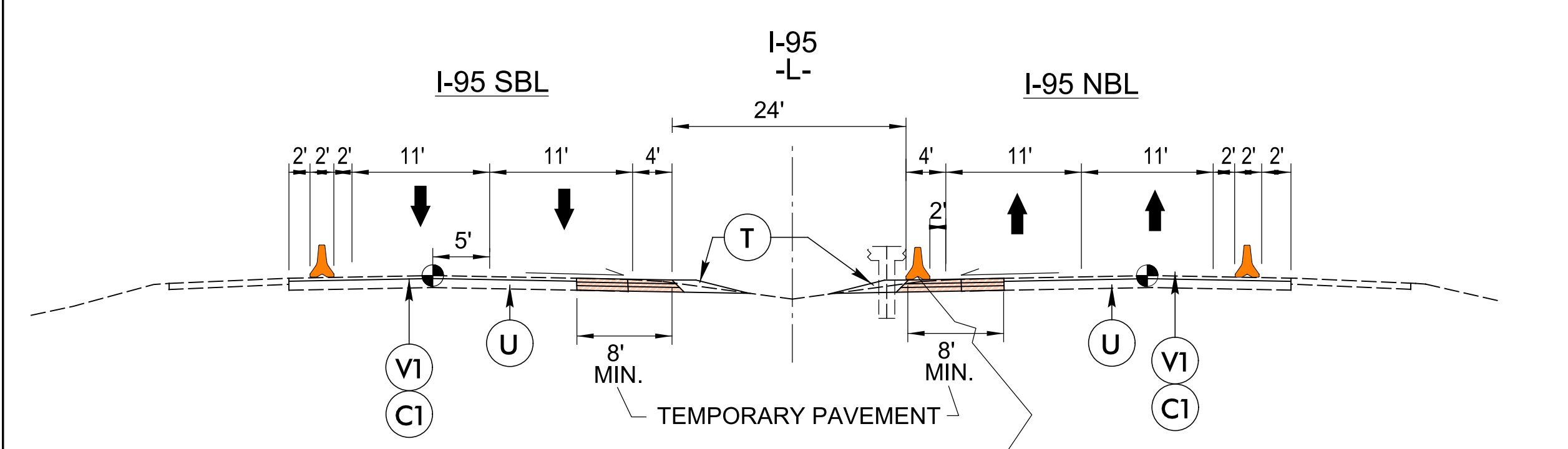
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EXISTING I-95 TYPICAL SECTION



TYPICAL SECTION NO. 1

I-95 TEMPORARY MEDIAN PAVEMENT



- TEMPORARY MEDIAN PAVEMENT USED TO SHIFT TRAFFIC TOWARDS MEDIAN FOR CONSTRUCTION OF OUTSIDE WIDENING AND AT CULVERT LOCATIONS.
- 8' MIN. TEMPORARY PAVEMENT TO REPLACE EXISTING MEDIAN SHOULDER.
- AT CULVERT LOCATIONS ADDITIONAL TEMPORARY PAVEMENT IS REQUIRED (SEE PLANS)
- TEMPORARY BARRIER INSTALLED WHERE EXISTING GUARDRAIL HEIGHT CANNOT BE MAINTAINED DUE TO TEMPORARY SHOULDER CONSTRUCTED OFF EXISTING THRU LANE CROSS-SLOPE. REFER TO PLANS FOR PLACEMENT FOR I-95 NB & SB.

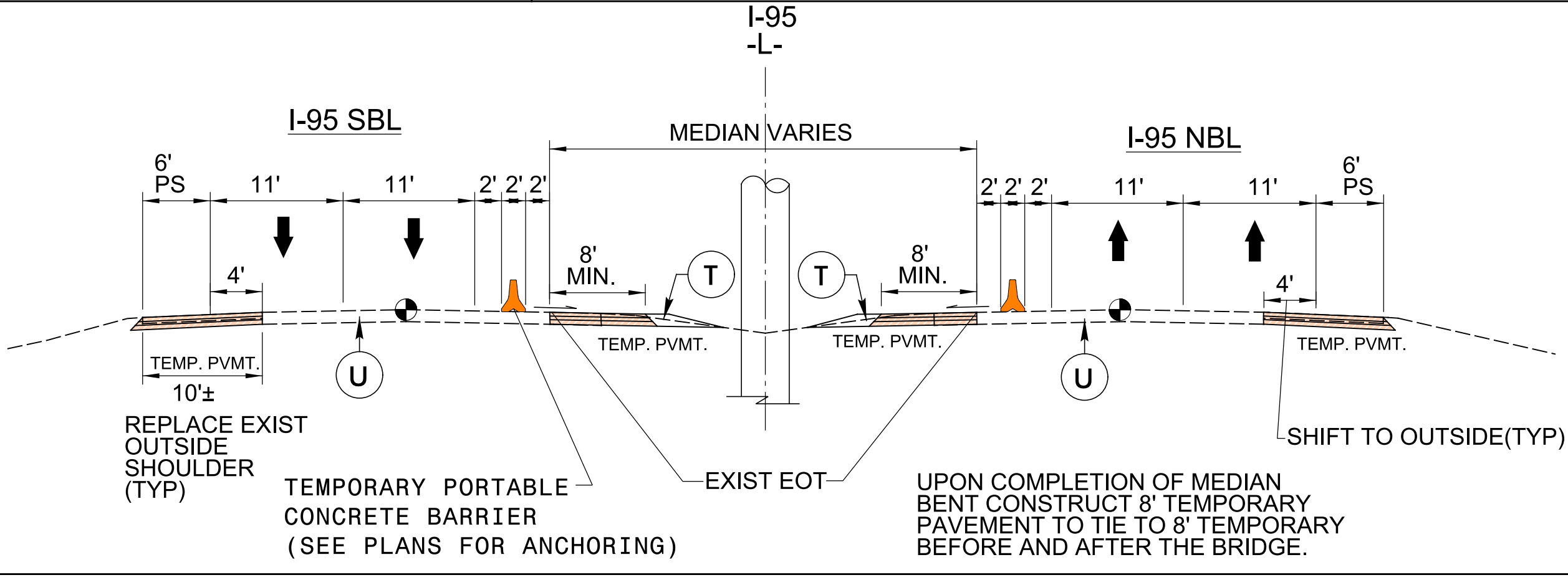
PROJECT REFERENCE NO. I-5878 / I-5883 / I-5986B SHEET NO. TMP-2C



DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED
Michael Baker INTERNATIONAL

TYPICAL SECTION NO. 2

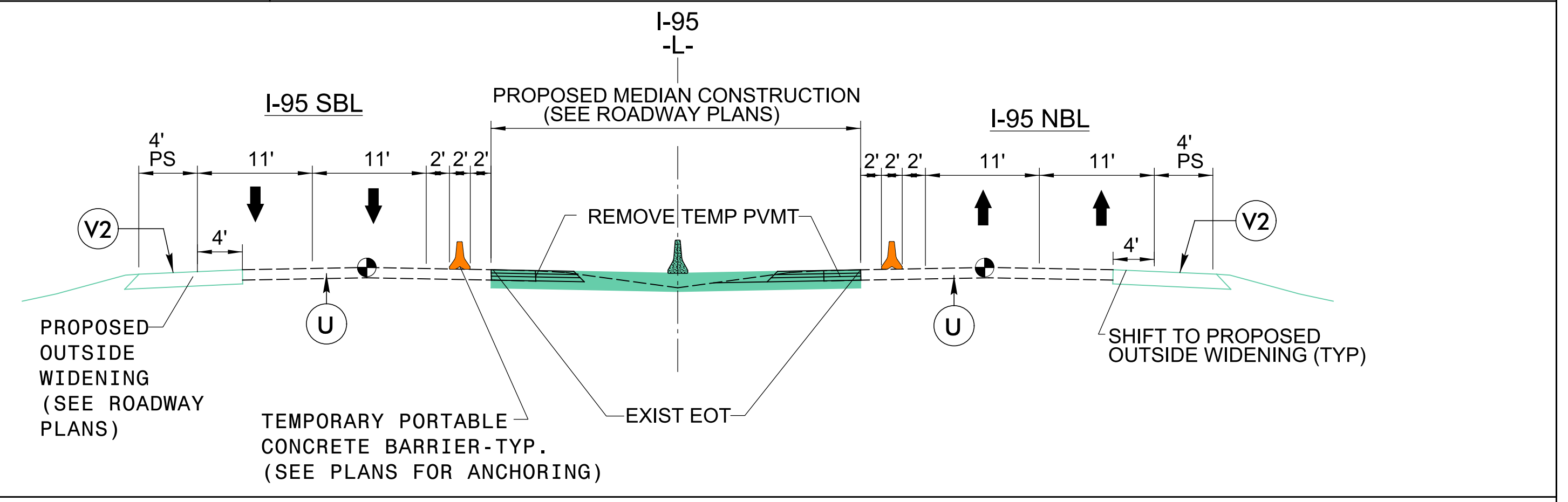
TYPICAL MEDIAN BENT CONSTRUCTION



- REPLACE EXIST OUTSIDE SHOULDER (TYP)
- TEMPORARY PORTABLE CONCRETE BARRIER (SEE PLANS FOR ANCHORING)
- UPON COMPLETION OF MEDIAN BENT CONSTRUCT 8' TEMPORARY PAVEMENT TO TIE TO 8' TEMPORARY BEFORE AND AFTER THE BRIDGE.
- SHIFT TO OUTSIDE(TYP)

TYPICAL SECTION NO. 3

TYPICAL PROPOSED MEDIAN CONSTRUCTION



- PROPOSED OUTSIDE WIDENING (SEE ROADWAY PLANS)
- TEMPORARY PORTABLE CONCRETE BARRIER-TYP. (SEE PLANS FOR ANCHORING)
- SHIFT TO PROPOSED OUTSIDE WIDENING (TYP)

TEMPORARY PAVEMENT DESIGN OPTIONS

TEMPORARY PAVEMENT SCHEDULE	
C1	S9.5C (VAR.)
T	EARTH MATERIAL
U	EXIST. PAVEMENT
V1	MILLING (VAR.)
V2	TEMPORARY RUMBLE STRIPS, TO BE COVERED BY FINAL LAYER

I-95 TEMPORARY PAVEMENT DESIGNS					
Design Life	1 year	1.5 years	2 years	2.5 years	3 years
Material	Thickness (in)				
Asphalt Surface Course	2.0" S9.5C	2.0" S9.5C	2.0" S9.5C	3.0" S9.5C	3.0" S9.5C
Asphalt Intermediate Course	3.5" I19.0C	3.5" I19.0C	4.0" I19.0C	3.5" I19.0C	3.5" I19.0C
Asphalt Base Course	4.5" B25.0C	5.5" B25.0C	5.5" B25.0C	5.0" B25.0C	5.5" B25.0C
Total Thickness	10.0"	11.0"	11.5"	11.5"	12.0"
Required SN	3.75	4.00	4.20	4.35	4.45
Design SN	3.77	4.07	4.29	4.36	4.51

MIX TYPE	RATE (LBS/SY/IN)	MINIMUM LIFT (INCHES)	MAXIMUM LIFT (INCHES)
S9.5C	112	1.5	2.0
I19.0C	114	2.5	4.0
B25.0C	114	4.0	5.5

NOTES

- 1) DUE TO THE TEMPORARY PAVEMENT DESIGNS BEING TIME SENSITIVE, IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO SELECT WHICH PAVEMENT DESIGN IS SUITABLE DEPENDING ON THE CONTRACTORS' SCHEDULE OF OPERATIONS.
- 2) TEMPORARY PAVEMENT DESIGN OPTIONS WERE PROVIDED BY S&ME ENGINEERING GEOTECHNICAL/PAVEMENT DESIGN ENGINEERS.
- 3) DURING THE TRAFFIC SHIFT TOWARDS THE MEDIAN MILL THE EXISTING PAVEMENT AND RESURFACE WITH S9.5C. THE DEPTH OF MILLING/RESURFACING WILL DEPEND ON THE CHOSEN TEMPORARY PAVEMENT DESIGN. FOLLOW PAVING OPERATIONS BEHIND MILLING, SUCH THAT WHEN LANES ARE OPEN, TRAFFIC DOES NOT RIDE ON A MILLED SURFACE. APPLY WORK ZONE PERFORMANCE PAVEMENT MARKINGS FOR THE SHIFTED LANE PATTERN.
- 4) ALTERNATE METHODS OF SHIFTING TRAFFIC LANES INCLUDE USING PATTERN MASKING MATERIAL, AS DIRECTED BY THE ENGINEER. APPLY WORK ZONE PERFORMANCE PAVEMENT MARKINGS. PAVEMENT MARKING REMOVAL WILL NOT BE ALLOWED.
- 5) DURING THE TRAFFIC SHIFTS TO THE OUTSIDE DURING CONSTRUCTION OF BRIDGE MEDIAN BENTS USE PATTERN MASKING AND APPLY WORK ZONE PERFORMANCE PAVEMENT MARKINGS. PAVEMENT MARKING REMOVAL WILL NOT BE ALLOWED.
- 6) REFER TO TEMPORARY PAVEMENT DESIGN TABLE FOR SELECTION OF APPROPRIATE TEMPORARY PAVEMENT DESIGN.
- 7) REFER TO PHASING NARRATIVE AND DETAIL SHEETS FOR TEMPORARY PAVEMENT LOCATIONS.
- 8) TEMPORARY ASPHALT RUMBLE STRIPS TO BE PAID FOR UNDER MILLED RUMBLE STRIPS (ASPHALT CONCRETE) PER LF. SEE SECTION 665 STD. SPECS. AND RSD 665.01.
- 9) TYPICAL SECTIONS SHOW 2 THROUGH LANES PER DIRECTION OF I-95. RAMP LANES AND AUXILIARY LANES ARE NOT SHOWN.

TEMPORARY PAVEMENT DESIGN TYPICAL SECTIONS