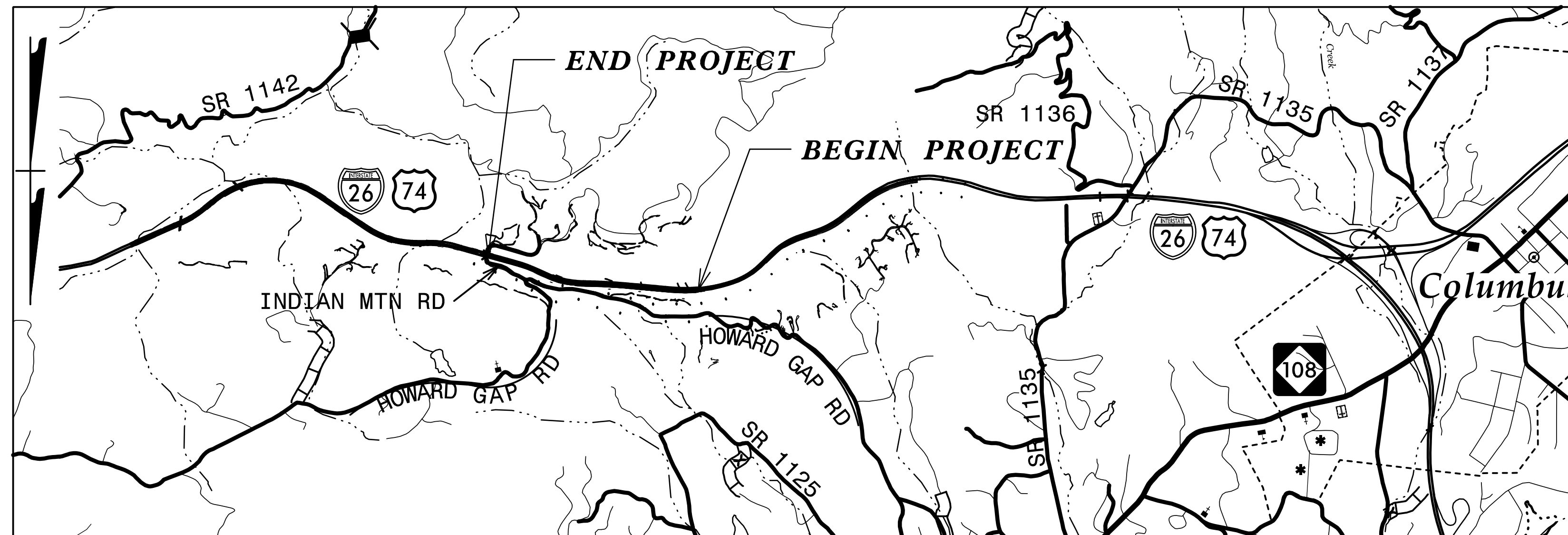
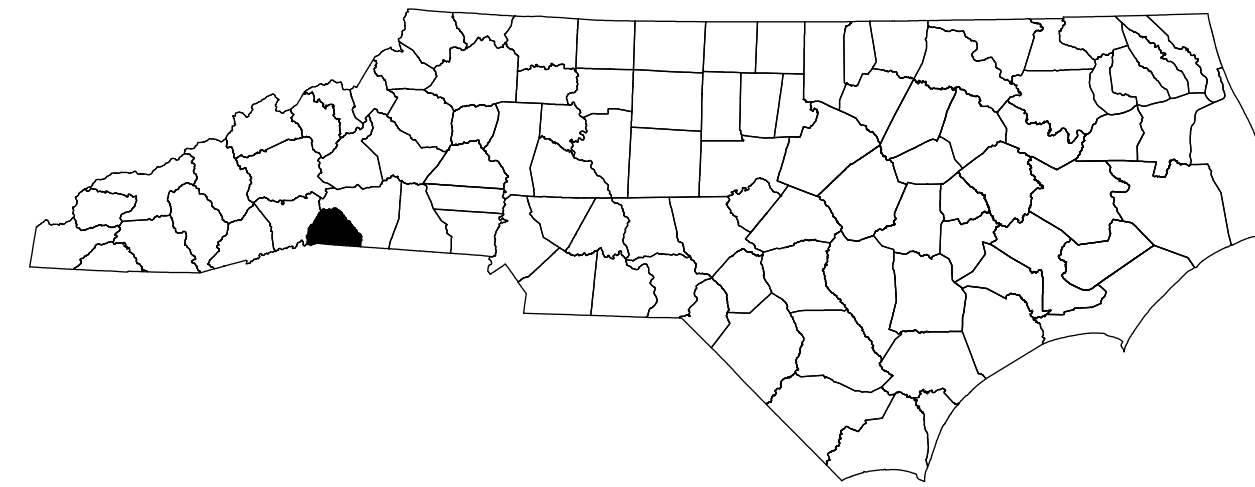


STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

TRANSPORTATION MANAGEMENT PLAN

POLK COUNTY

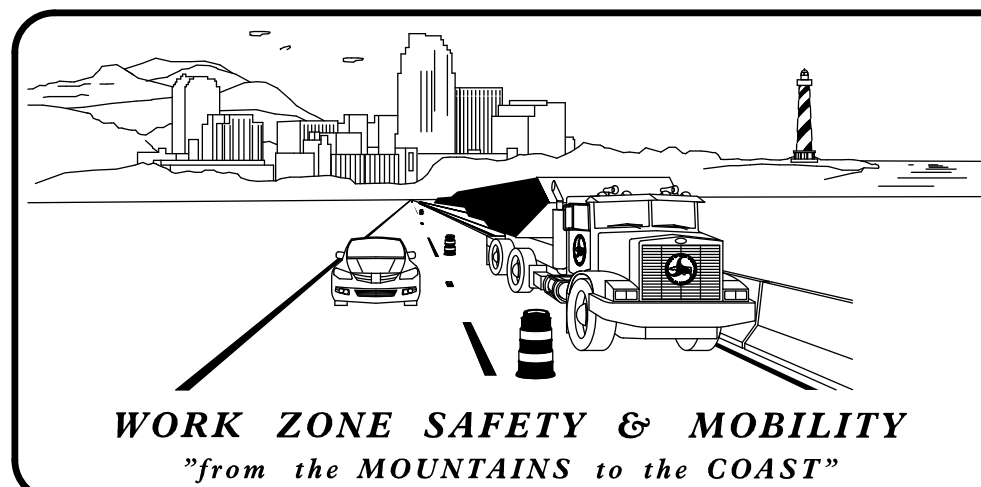


SHEET NO.	TITLE
TMP-1	TITLE SHEET, VICINITY MAP, AND INDEX OF SHEETS
TMP-1A	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, AND LEGEND
TMP-1B	TRANSPORTATION OPERATIONS PLAN: (MANAGEMENT STRATEGIES, GENERAL NOTES, AND LOCAL NOTES)
TMP-2	TRAFFIC CONTROL PHASING
TMP-2A	DYNAMIC ZIPPER MERGE SYSTEM LAYOUT
TMP-3 THRU TMP-4	TEMPORARY TRAFFIC CONTROL PHASE I DETAILS
TMP-5 THRU TMP-7	TEMPORARY TRAFFIC CONTROL PHASE II DETAILS
TMP-8	TEMPORARY TRAFFIC CONTROL PHASE III

SHEET NO.
TMP-1

PROJECT: 15614.1075010

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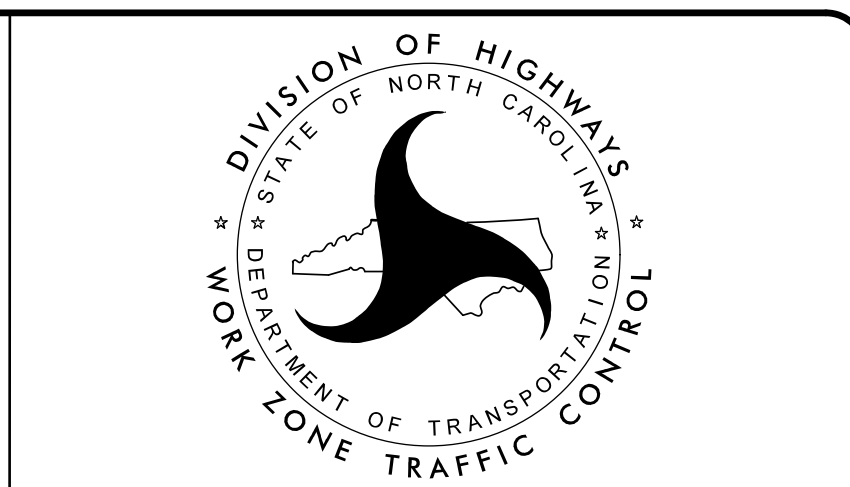
PLANS PREPARED BY:

JIMMY TERRY, PE
PROJECT ENGINEER

SANDRA MELVIN
DESIGN ENGINEER

NCDOT CONTACTS:

JEANETTE WHITE, PE
PROJECT ENGINEER



TGS ENGINEERS
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SHELBY, NC 28150
PH (704) 476-0003
CORP. LICENSE NO.: C-0275

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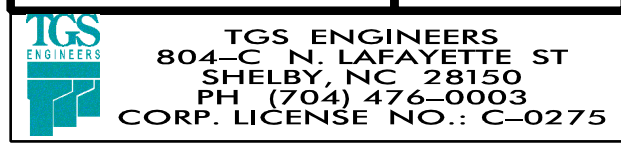
APPROVED: *Jimmy Terry*

DATE: 5/12/2021

SEAL

GENERAL NOTES

PROJ. REFERENCE NO.	SHEET NO.
15614.1075010	TMP-1B



CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS, AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS OR RESULT IN DUPLICATE OR UNDESIRABLE OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING, OR REMOVAL OF DEVICES AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT EXCEPT WHEN OTHERWISE NOTED IN THE PLAN OR DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
I-26	MON TO SUN 6:00 AM TO 9:00 PM

NOTE: THE ABOVE TIME RESTRICTIONS DO NOT APPLY DURING THE LONG-TERM CLOSURE PERMITTED AND DESCRIBED IN INTERMEDIATE CONTRACT TIME NUMBER 3, HOWEVER THE FOLLOWING HOLIDAY AND HOLIDAY WEEKEND RESTRICTIONS BELOW DO APPLY TO INTERMEDIATE CONTRACT TIME NUMBER 3.

B) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS:

ROAD NAME
I-26

HOLIDAY

- FOR ANY UNEXPECTED OCCURRENCE THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
- FOR NEW YEAR'S, BETWEEN THE HOURS OF 3:00 P.M. DECEMBER 31st TO 7:00 A.M. JANUARY 2ND. IF NEW YEAR'S DAY IS ON A FRIDAY, SATURDAY, SUNDAY, OR MONDAY THEN UNTIL 7:00 A.M. THE FOLLOWING TUESDAY.
- FOR EASTER, BETWEEN THE HOURS OF 3:00 P.M. THURSDAY AND 7:00 A.M. MONDAY.
- FOR MEMORIAL DAY, BETWEEN THE HOURS OF 3:00 P.M. FRIDAY TO 7:00 A.M. TUESDAY.
- FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 3:00 P.M. THE DAY BEFORE INDEPENDENCE DAY AND 7:00 A.M. THE DAY AFTER INDEPENDENCE DAY.

IF INDEPENDENCE DAY IS ON A FRIDAY, SATURDAY, SUNDAY OR MONDAY THEN BETWEEN THE HOURS OF 3:00 P.M. THE THURSDAY BEFORE INDEPENDENCE DAY AND 7:00 A.M. THE TUESDAY AFTER INDEPENDENCE DAY.

- FOR LABOR DAY, BETWEEN THE HOURS OF 3:00 P.M. FRIDAY AND 7:00 A.M. TUESDAY.
- FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 3:00 P.M. TUESDAY TO 7:00 A.M. MONDAY.
- FOR CHRISTMAS, BETWEEN THE HOURS OF 3:00 P.M. THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 7:00 A.M. THE FOLLOWING TUESDAY AFTER THE WEEK OF CHRISTMAS.
- FOR LEAF SEASON FROM THE THURSDAY CLOSEST TO OCTOBER 7th TO THE MONDAY CLOSEST TO NOVEMBER 4th, NO LANE CLOSURES ARE ALLOWED BETWEEN 6:00 AM TO 7:00 PM MONDAY THROUGH THURSDAY, NOR BETWEEN 6:00 AM TO 9:00 PM ON FRIDAY, NOR BETWEEN 9:00 AM TO 9:00 PM ON SATURDAY, NOR BETWEEN NOON AND 8:00PM ON SUNDAY.

C) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE HAULING OPERATION IS PROTECTED BY BARRIER OR GUARDRAIL OR AS DIRECTED BY THE ENGINEER.

LANE AND SHOULDER CLOSURE REQUIREMENTS

- D) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED OR AS DIRECTED BY THE ENGINEER.
- E) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 15 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- F) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

G) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS, OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.

H) DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY, RAMP, OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.

I) DO NOT INSTALL MORE THAN ONE LANE CLOSURE IN ANY ONE DIRECTION ON I-26.

J) PROVIDE TRAFFIC CONTROL FOR APPROPRIATE LANE CLOSURES FOR SURVEYING DONE BY THE DEPARTMENT.

PAVEMENT EDGE DROP OFF REQUIREMENTS

K) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS AN EDGE OF PAVEMENT DROP-OFF AS FOLLOWS:

BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.

BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

L) DO NOT EXCEED A DIFFERENCE OF 2 INCHES IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC FOR NOMINAL LIFTS OF 1.5 INCHES. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) 100 IN ADVANCE AND A MINIMUM OF EVERY HALF MILE THROUGHOUT THE UNEVEN AREA.

TRAFFIC PATTERN ALTERATIONS

M) NOTIFY THE ENGINEER THIRTY (30) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

N) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 40 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.

O) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

TRAFFIC BARRIER

P) INSTALL TEMPORARY BARRIER ACCORDING TO THE TRANSPORTATION MANAGEMENT PLANS A MAXIMUM OF TWO (2) WEEKS PRIOR TO BEGINNING WORK IN ANY LOCATION. ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION PROCEED IN A CONTINUOUS MANNER TO COMPLETE THE PROPOSED WORK IN THAT LOCATION UNLESS OTHERWISE STATED IN THE TRANSPORTATION MANAGEMENT PLANS OR AS DIRECTED BY THE ENGINEER.

DO NOT PLACE BARRIER DIRECTLY ON ANY SURFACE OTHER THAN ASPHALT OR CONCRETE.

ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION AND NO WORK IS PERFORMED BEHIND THE TEMPORARY BARRIER FOR A PERIOD LONGER THAN TWO (2) MONTHS, REMOVE / RESET TEMPORARY BARRIER AT NO COST TO THE DEPARTMENT UNLESS OTHERWISE STATED IN THE TRANSPORTATION MANAGEMENT PLANS, TEMPORARY BARRIER IS PROTECTING A HAZARD, OR AS DIRECTED BY THE ENGINEER.

INSTALL TEMPORARY BARRIER WITH THE TRAFFIC FLOW BEGINNING WITH THE UPSTREAM SIDE OF TRAFFIC. REMOVE TEMPORARY BARRIER AGAINST THE TRAFFIC FLOW BEGINNING WITH THE DOWNSTREAM SIDE OF TRAFFIC.

INSTALL AND SPACE DRUMS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH) TO CLOSE OR KEEP THE SECTION OF THE ROADWAY CLOSED UNTIL THE TEMPORARY BARRIER CAN BE PLACED OR AFTER THE TEMPORARY BARRIER IS REMOVED.

Q) PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER AT ALL TIMES DURING THE INSTALLATION AND REMOVAL OF THE BARRIER BY EITHER A TRUCK MOUNTED ATTENUATOR (MAXIMUM 72 HOURS) OR A TEMPORARY CRASH CUSHION.

PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER FROM ONCOMING TRAFFIC AT ALL TIMES BY A TEMPORARY CRASH CUSHION UNLESS THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER IS OFFSET FROM ONCOMING TRAFFIC AS FOLLOWS OR AS SHOWN IN THE PLANS: (SEE ALSO 1101.05)

POSTED SPEED LIMIT	MINIMUM OFFSET
40 OR LESS	15 FT
45 - 50	20 FT
55	25 FT
60 MPH or HIGHER	30 FT

TRAFFIC CONTROL DEVICES

R) WHEN LANE CLOSURES ARE NOT IN EFFECT SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER IN FEET THAN TWICE THE POSTED SPEED LIMIT (MPH) EXCEPT, 10 FT ON-CENTER IN RADII, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY. REFER TO STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES SECTIONS 1130 (DRUMS), 1135 (CONES) AND 1180 (SKINNY DRUMS) FOR ADDITIONAL REQUIREMENTS.

S) PLACE ADDITIONAL SETS OF THREE CHANNELIZING DEVICES PERPENDICULAR TO THE EDGE OF TRAVELWAY ON 500 FT CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC.

PAVEMENT MARKINGS AND MARKERS

- T) INSTALL PAVEMENT MARKINGS AND PAVEMENT MARKERS ON THE FINAL SURFACE.
- U) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.
- V) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS BY THE END OF EACH DAY'S OPERATION.

MISCELLANEOUS

W) LAW ENFORCEMENT SHALL BE USED TO MAINTAIN TRAFFIC THROUGH THE WORK AREA AND/OR INTERSECTIONS AS DIRECTED BY THE ENGINEER.

MANAGEMENT STRATEGIES

THE FOLLOWING LISTED WORK ZONE STRATEGIES ARE RECOMMENDED FOR INCLUSION WITHIN THIS TRANSPORTATION MANAGEMENT PLAN (TMP).

RECOMMENDED STRATEGIES:

TRAFFIC MANAGEMENT STRATEGIES:
LANE SHIFTS OR CLOSURES
SHOULDER CLOSURES
NIGHT WORK

WORK ZONE SAFETY & MOBILITY STRATEGIES:
SPEED LIMIT REDUCTION
SEQUENTIAL LIGHTING

TRAFFIC / INCIDENT MANAGEMENT & SPEED ENFORCEMENT STRATEGIES:
COORDINATION WITH MEDIA
DEDICATED (PAID) LAW ENFORCEMENT
INCREASED PENALTIES FOR WORK ZONE VIOLATIONS


CONTRACTING & INNOVATIVE CONSTRUCTION STRATEGIES:
INTERMEDIATE CONTRACT TIMES / LIQUIDATED DAMAGES

LOCAL NOTES

TEMPORARY SHORING WILL BE REQUIRED FOR INSTALLATION OF DRAINAGE STRUCTURES. POSITIVE PROTECTION IS REQUIRED IF TEMPORARY SHORING IS LOCATED IN THE CLEAR ZONE IN ACCORDANCE WITH THE AASHTO ROADSIDE DESIGN GUIDE. THE LOCATION AND DESIGN OF THE TEMPORARY SHORING WILL BE THE RESPONSIBILITY OF THE CONTRACTOR. ALL DESIGNS AND LOCATIONS ARE TO BE SUBMITTED TO THE DEPARTMENT FOR REVIEW AND APPROVAL. ONCE ACCEPTED, THE WORK IN THE APPROVED PHASE MAY PROCEED. FOR ADDITIONAL INFORMATION REGARDING THE REQUIREMENTS FOR THE TEMPORARY SHORING AND ANY POSITIVE PROTECTION THAT IS NEEDED IN CONJUNCTION WITH THE SHORING, REFER TO THE TEMPORARY SHORING AND TEMPORARY SOIL NAIL WALLS SPECIAL PROVISIONS PROVIDED IN THE CONTRACT.

UNLESS OTHERWISE STATED 2 LANES OF TRAFFIC MUST BE OPEN ON I-26 AT ALL TIMES.

APPROVED: DATE: 5/12/2021 SEAL			TRANSPORTATION OPERATIONS PLAN
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PROJ. REFERENCE NO.	SHEET NO.
15614.1075010	TMP-02
 TGS ENGINEERS 804-C N. LAFAYETTE ST SHELBY, NC 28150 PH (704) 476-0003 CORP. LICENSE NO.: C-0275	

PHASING

NOTE: THE CONTRACTOR MAY ELECT TO PERFORM SOME OF THE WORK AT NIGHT.

NOTE: UNLESS OTHERWISE NOTED I-26 WB TRAFFIC SHOULD BE MAINTAINED ON TWO LANES AT ALL TIMES.

BEFORE BEGIN OF CONSTRUCTION PLACE A CHANGEABLE MESSAGE SIGN ALONG I-26 WB ADVISING THE TRUCK TRAFFIC TO REMAIN ON THE MOST OUTSIDE LANE DURING ALL TIMES. LOCATION OF THE MESSAGE SIGN TO BE DETERMINED BY THE ENGINEER.

PHASE I

STEP 1 :

INSTALL ALL ADVANCED WARNING SIGNS ON I-26 ACCORDING TO NCDOT STD DRAWING 1101.01, SHEET 1 OF 3.

STEP 2 (TMP-3):

USING TEMPORARY LANE CLOSURES IN ACCORDANCE WITH NCDOT STD DRAWING 1101.02, SHEET 4 OF 14, PLACE TEMPORARY PORTABLE CONCRETE BARRIER, IMPACT ATTENUATOR, AND DRUMS ALONG I-26EB AS SHOWN ON PLANS.

BEGIN CONSTRUCTION OF DRAINAGE STRUCTURES AND PIPE INSTALLATION ALONG I-26 EASTBOUND LANES AND HOWARD GAP RD (SEE ROADWAY PLANS FOR DRAINAGE LAYOUT AND DETAILS).

INSTALL SHOULDER BERM GUTTER FROM I-26 STA 451+00 TO 476+00. CONSTRUCT TIE-IN TO EXISTING SHOULDER FROM I-26 STA 450+50 TO 451+00 AND 476+00 TO 477+31+/-.

REMOVE EXISTING GUARDRAIL AND INSTALL NEW GUARDRAIL ALONG SHOULDER BERM GUTTER AS SHOWN ON PLANS.

STEP 3(TMP-4):

REDUCING TRAFFIC TO 1 LANE, CLOSE THE OUTSIDE LANE OF I-26EB FROM I-26 STA 450+00+/- TO 499+75+/-.

REMOVE THE TEMPORARY PORTABLE CONCRETE BARRIER AND IMPACT ATTENUATOR.

COMPLETE THE PAVED SHOULDER INCLUDING THE FINAL LAYER OF SURFACE COURSE FROM I-26 STA 450+50+/- TO 477+31+/-, LT.

INSTALL MILLED RUMBLE STRIPS FROM I-26 450+50+/- TO 477+31+/-, LT.

PHASE II

NOTE: THE CONTRACTOR MAY DECIDE TO WORK TO INSTALL THE DRAINAGE SYSTEM AT I-26 STA 416+14+/- CONCURRENTLY WITH WORK DESCRIBED IN PHASE 2, STEPS 2 AND 2A.

STEP 1 (SEE TMP-5):

USING TEMPORARY LANE CLOSURES IN ACCORDANCE WITH NCDOT STD DRAWING 1101.02, SHEET 4 OF 14 PLACE TEMPORARY PORTABLE CONCRETE BARRIER AND IMPACT ATTENUATOR ALONG I-26WB AS SHOWN ON PLANS.

CONSTRUCT THE DRAINAGE SYSTEM AT I-26 STA 416+14+/- (SEE ROADWAY PLANS FOR DRAINAGE LAYOUT.)

STEP 2 (SEE TMP-5):

USING TEMPORARY LANE CLOSURES IN ACCORDANCE WITH NCDOT STD DRAWING 1101.02, SHEET 4 OF 14, INSTALL TEMPORARY PORTABLE CONCRETE BARRIER (TPCB) AND TEMPORARY CRASH CUSHION ALONG THE I-26 WB LANES FROM I-26 STA 448+90+/- TO 483+65+/- . PLACE DRUMS FROM I26WB 483+65+/- TO 498+25+/-.

WIDEN THE EXISTING PAVED SHOULDER UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE FROM I-26 STA 450+40+/- TO 482+65+/-, RT.

CONSTRUCT THE ADJACENT CONCRETE DITCH.

CONSTRUCT THE PROPOSED ACCESS DRIVES FOR FOREBAYS #1 TO # 5.

CONSTRUCT FOREBAYS #1 TO #5

USING TEMPORARY LANE CLOSURES IN ACCORDANCE WITH NCDOT STD DRAWING 1101.02, SHEET 4 OF 14, AS NEEDED, INSTALL DRAINAGE STRUCTURES 0520 & 0605 AT THE EASTBOUND OUTSIDE SHOULDER, USING TRENCHLESS CONSTRUCTION INSTALL THE CROSS PIPES AT I-26 STA 451+17+/-, 452+88+/-, AND 463+18+/-.

CONSTRUCT THE DRAINAGE STRUCTURES ALONG THE WESTBOUND SHOULDER AS INSTRUCTED BY THE ENGINEER. (SEE ROADWAY PLANS FOR DRAINAGE LAYOUT AND DETAILS).

STEP 2A (SEE TMP 6):

WHILE WORKING ON PHASE 2, STEP 2 THE CONTRACTOR SHALL WORK IN A CONTINUOUS MATTER TO COMPLETE THE WORK DESCRIBED IN PHASE 2, STEP 2A IN FIVE CONSECUTIVE DAYS PER ICT #2.

USING DRUMS AND OTHER APPROPRIATE DEVICES, DEPLOY THE DYNAMIC ZIPPER MERGE SYSTEM AND CLOSE BOTH THE WESTBOUND AND EASTBOUND INTERIOR LANES TO TRAFFIC (SEE SPECIAL PROVISIONS AND TMP-2A FOR ADDITIONAL INFORMATION).
 CUT THE EXISTING CONCRETE MEDIAN CONCRETE BARRIER.
 INSTALL DRAINAGE STRUCTURE 0521
 RECONSTRUCT CONCRETE MEDIAN BARRIER.

REMOVE THE DYNAMIC ZIPPER MERGE SYSTEM AND OTHER APPROPRIATE DEVICES AND OPEN TRAFFIC BACK TO EXISTING PATTERN.

STEP 3 (SEE TMP-7):

KEEPING TRAFFIC ON 2 LANES, CLOSE THE OUTSIDE LANE OF I-26WB FROM ITS BEGIN AT APPROX MILEMARKER 65 TO I-26 STA 498+65+/-.

COVER THE "TRUCK LANE 500 FEET" SIGN.

REMOVE THE TEMPORARY PORTABLE CONCRETE BARRIER AND TEMPORARY CRASH CUSHION PLACED IN PHASE II, STEP 1 & 2.

COMPLETE THE PAVED SHOULDER BY PLACING THE FINAL LAYER OF SURFACE COURSE FROM I-26 STA 450+40+/- TO 482+65+/-, RT.

INSTALL MILLED RUMBLE STRIPS FROM I-26 450+40+/- TO 482+65+/-, RT.

STEP 4:

REMOVE DRUMS ALONG I-26 WB, UNCOVER "TRUCK LANE 500 FEET" SIGN, AND OPEN ALL WB-LANE TO TRAFFIC

PHASE III

STEP 1 (SEE TMP-8):

USING TEMPORARY LANE CLOSURES IN ACCORDANCE WITH NCDOT STD DRAWING 1101.02, SHEET 4 OF 14, PLACE TEMP. PORTABLE MEDIAN BARRIERS, IMPACT ATTENUATORS, AND DRUMS AS SHOWN ON PLANS.

PLACE CONCRETE MEDIAN DITCH FROM I-26 483+95 TO 497+50

INSTALL MEDIAN DRAINAGE SYSTEM. (SEE ROADWAY PLANS FOR DRAINAGE LAYOUT AND DETAILS)

REMOVE EXISTING GUARDRAIL AND INSTALL PROPOSED GUARDRAIL ALONG BOTH THE WESTBOUND AND EASTBOUND MEDIAN FROM I-26 483+19 TO 497+50

CONSTRUCT THE INSIDE SHOULDERS UP TO AND INCLUDING THE FINAL LAYER OF SURFACE FROM I-26 483+19 TO 497+50

INSTALL MILLED RUMBLE STRIPS ON BOTH MEDIAN SHOULDERS FROM I-26 STA 483+19 TO 497+50

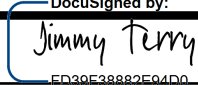
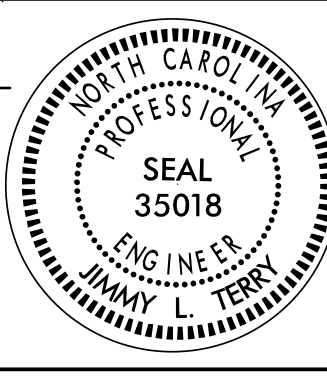

PHASE IV

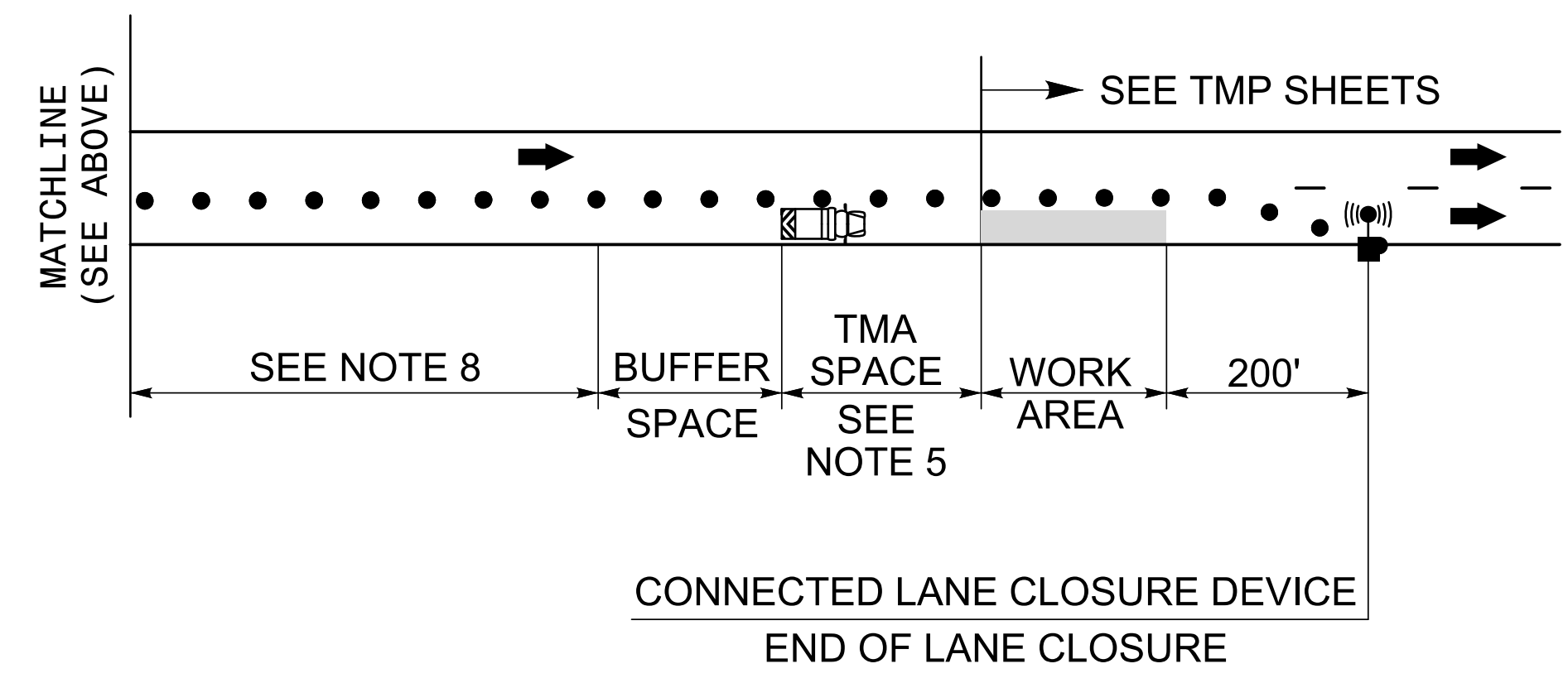
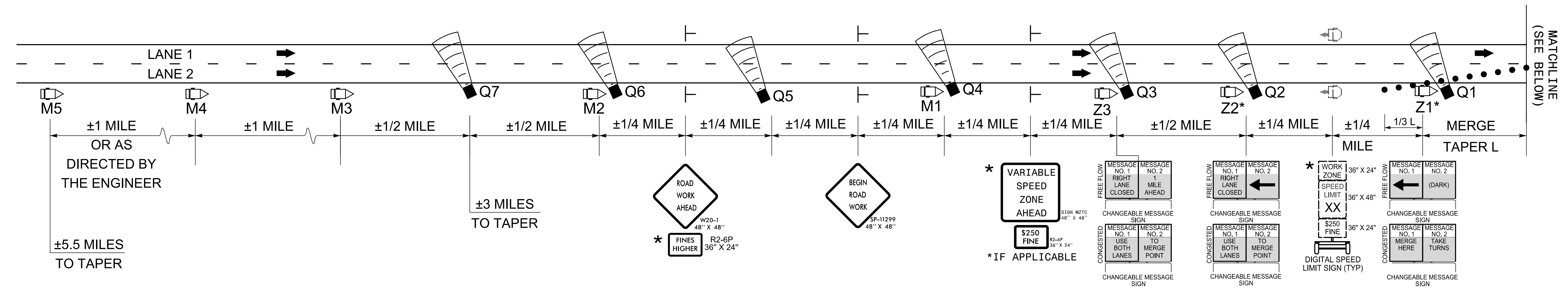
USING TEMPORARY LANE CLOSURES IN ACCORDANCE WITH NCDOT STD DRAWING 1101.02, SHEET 4 OF 14, PLACE FINAL PAVEMENT MARKING AS DIRECTED BY THE ENGINEER AND SHOWN ON FINAL PAVEMENT MARKING PLANS.

NOTE: TO ENHANCE SAFETY THE CONTRACTOR SHALL RESTRIP ALL MARKINGS , EDGE LINE AND LANE LINES, FROM I-26 STA 450+40+/- TO 497+50+/- TO ASSURE MATCHING RETRO-REFLECTIVITY OF ALL PAVEMENT MARKING.

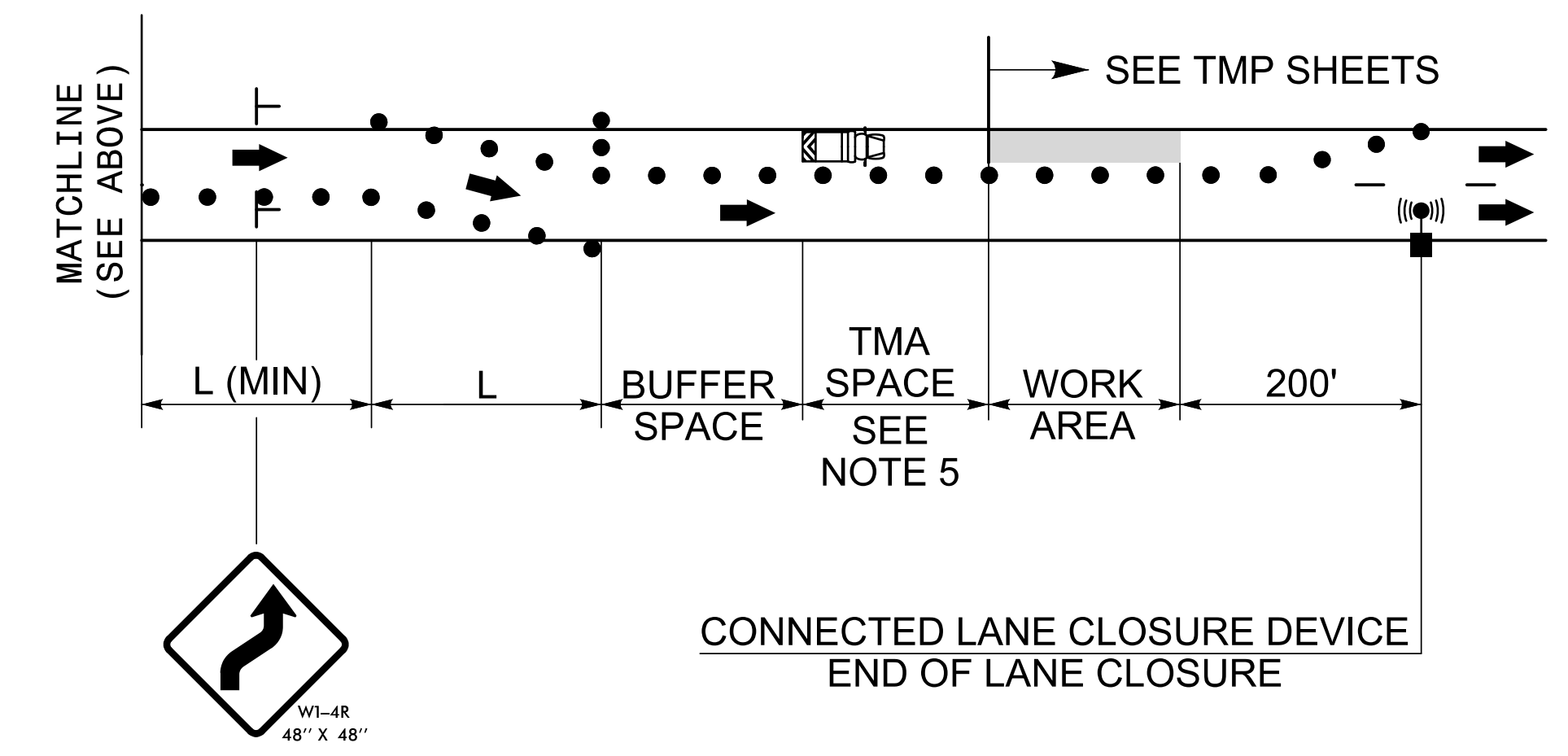
REMOVE ALL TRAFFIC CONTROL DEVICES AND OPEN ALL LANES TO TRAFFIC.

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APPROVED:  DATE: 5/12/2021 SEAL			TRAFFIC CONTROL PHASING
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RIGHT LANE WORK AREA



LEFT LANE WORK AREA (IF APPLICABLE)

GENERAL NOTES

- PLACE DRUMS IN TAPERS AT THE MAXIMUM SPACING EQUAL IN FEET TO THE POSTED SPEED LIMIT. PLACE DRUMS ALONG THE WORK AREA AT THE MAXIMUM SPACING EQUAL TO 2 TIMES THE POSTED SPEED LIMIT.
- REFER TO RSD. 1101.11, SHEETS 1 & 2, FOR "L" DISTANCE AND BUFFER SPACE.
- REFER TO RSD. 1101.02, SHEETS 9 & 10, FOR TREATMENT OF LANE CLOSURES THRU INTERCHANGES.
- INSTALL LANE CLOSURES WITH THE FLOW OF TRAFFIC, BEGINNING WITH DEVICES ON THE UPSTREAM SIDE OF TRAFFIC. REMOVE LANE CLOSURES AGAINST THE FLOW OF TRAFFIC, BEGINNING WITH DEVICES ON THE DOWNSTREAM SIDE OF TRAFFIC.
- POSITION THE TMAS TO MAINTAIN A ROLL-AHEAD DISTANCE AS RECOMMENDED BY THE MANUFACTURER AND CONTINUOUSLY ADVANCE TMAS AS WORK PROGRESSES. USE TMAS AS REQUIRED TO SHIELD WORKERS FROM ERRANT VEHICLES AND TRAFFIC FROM POTENTIAL HAZARDS IN THE WORK AREA.
- REMOVE OR COVER EXISTING ADVANCED WARNING SIGNS PREVIOUSLY INSTALLED USING RSD 1101.01, SHEET 2 OF 3.
- IN THE EVENT OF A SYSTEM MALFUNCTION, IMMEDIATELY INSTALL WORK ZONE SIGNS ACCORDING TO RSD 1101.02, SHEET 4. SIGNAGE SHOULD REMAIN IN PLACE UNTIL SYSTEM OPERATION IS FULLY RESTORED.
- IF A LEFT LANE WORK AREA WILL BE NEEDED AT ANY TIME DURING THE PROJECT, USE 2L. IF NO LEFT LANE WORK AREA IS NEEDED, OMIT THIS SPACE.
- REMOVE ANY CONFLICTING PAVEMENT MARKINGS & MARKERS.

LEGEND

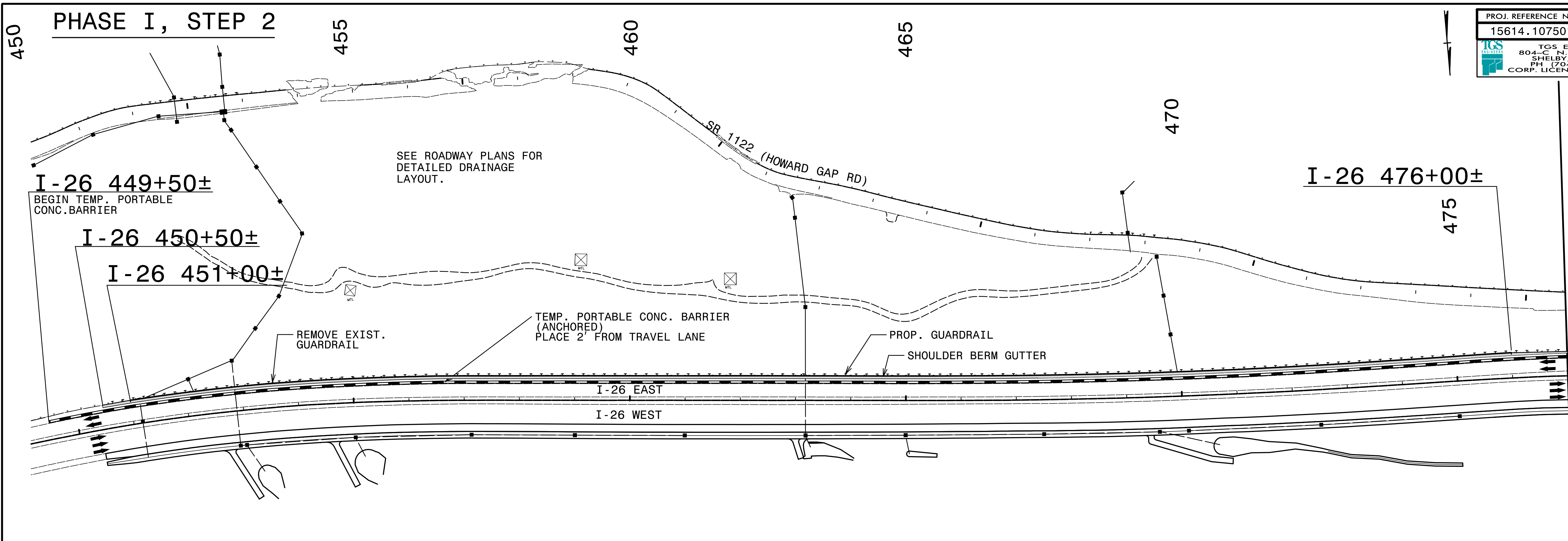
- SPEED SENSOR Q# (1-7)
- PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS)
M#(1-5) = MAINLINE
Z#(1-3) = ZIPPER MERGE
*Z1 & Z2 SHALL BE FULL MATRIX
- CONNECTED LANE CLOSURE DEVICE
- DRUM
- TRUCK MOUNTED ATTENUATOR (TMA)
- STATIONARY SIGN

APPROVED: *Jimmy Terry*
DATE: 5/12/2021

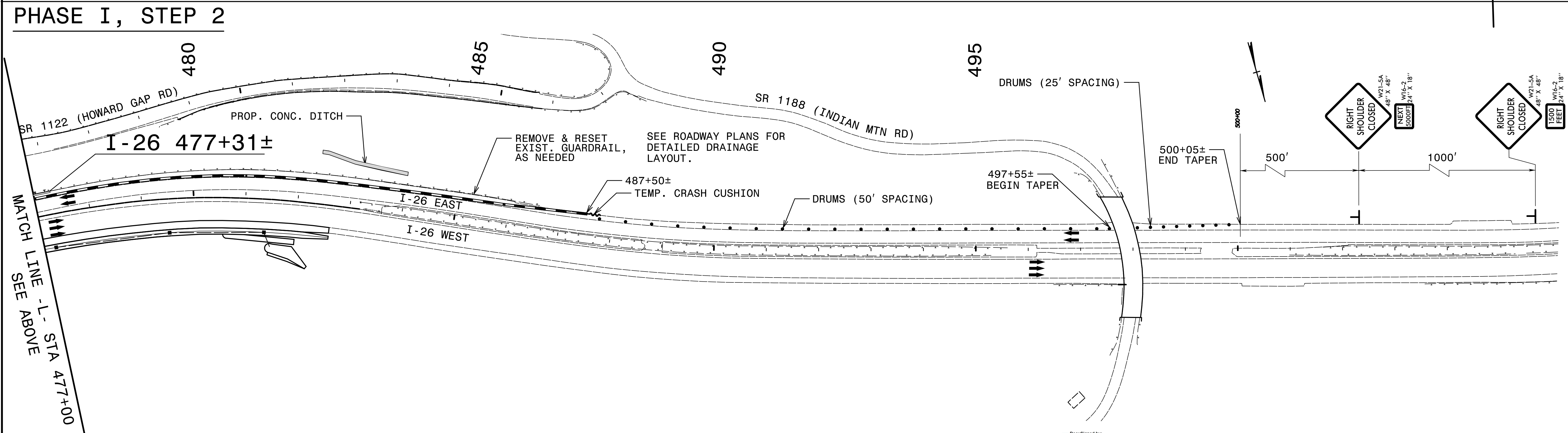
SEAL

DYNAMIC ZIPPER MERGE SYSTEM LAYOUT

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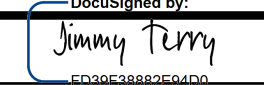
MATCH LINE - L - STA 477+00
SEE BELOW



MATCH LINE - L - STA 477+00
SEE ABOVE

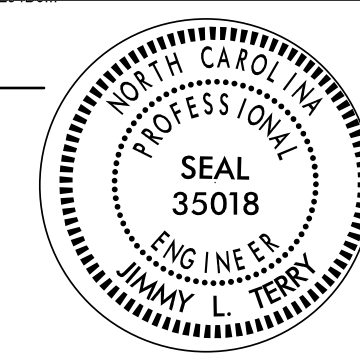


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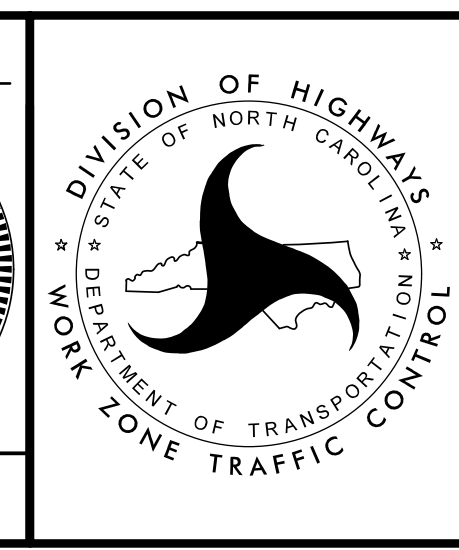
APPROVED: 

DATE: 5/11/2021


SEAL

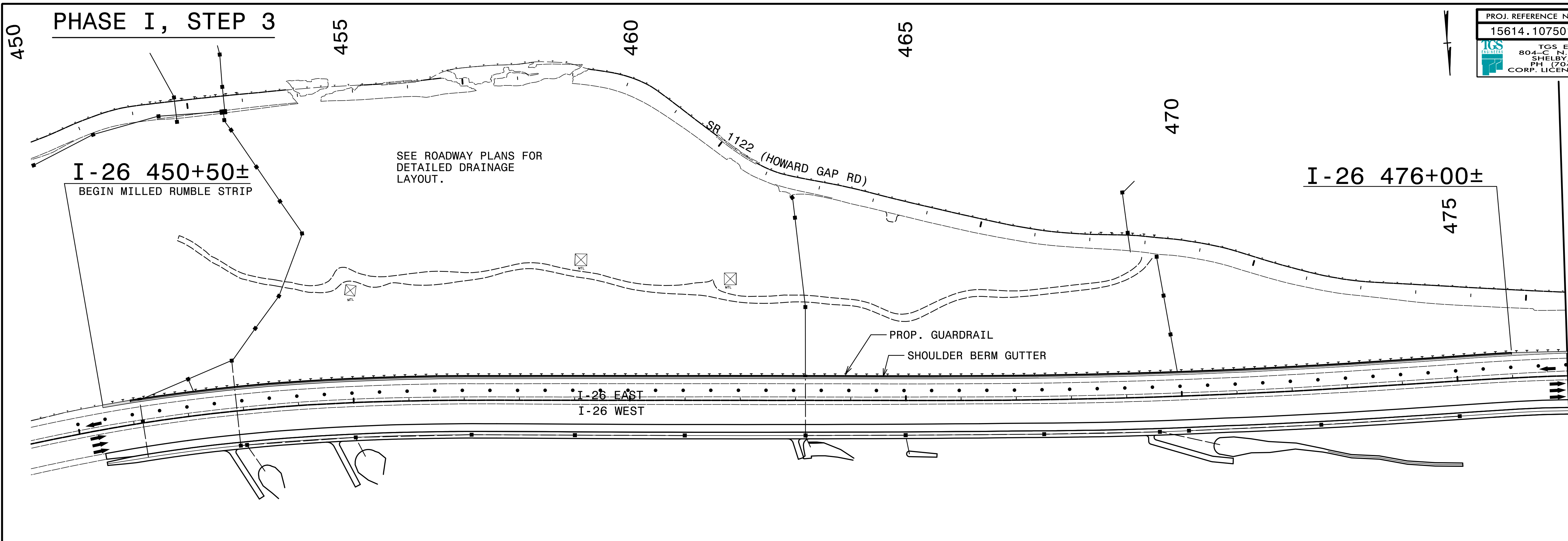


**DOCUMENT NOT CONSIDERED FINAL
UNLESS ALL SIGNATURES COMPLETED**

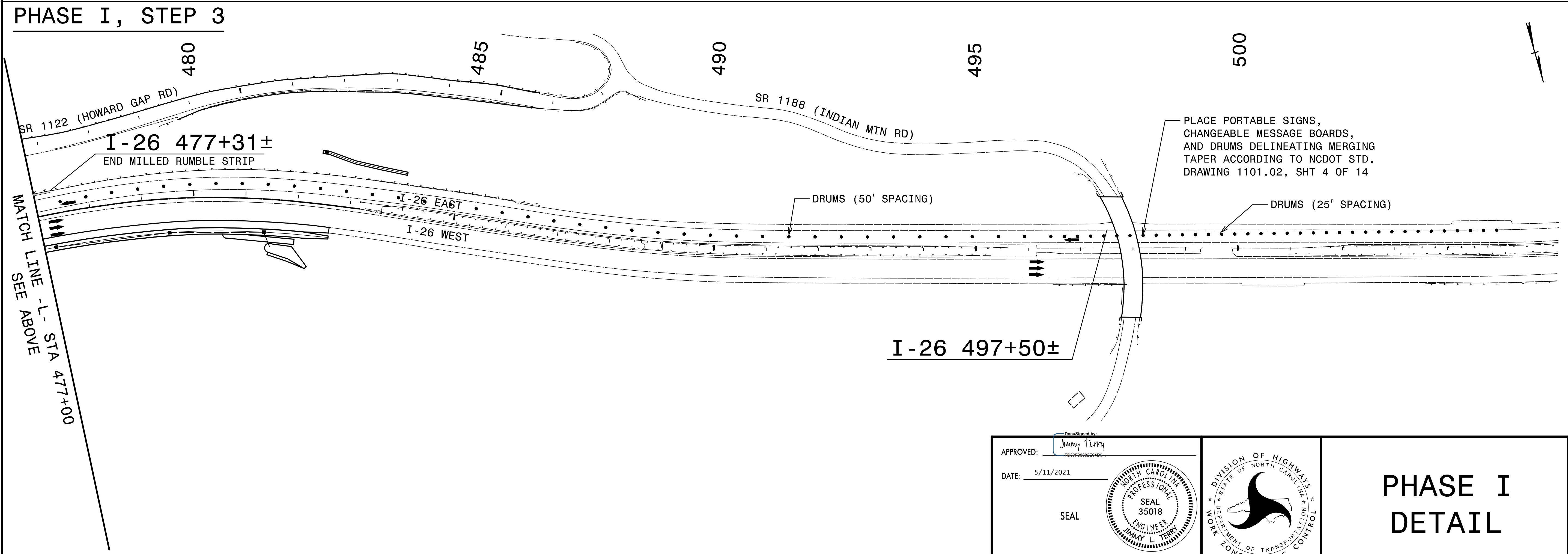


PHASE I DETAIL

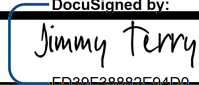
PROJ. REFERENCE NO.	SHEET NO.
15614.1075010	TMP-4
 TGS ENGINEERS 804-C N. LAFAYETTE ST SHELBY, NC 28150 PH (704) 476-0003 CORP. LICENSE NO.: C-0275	



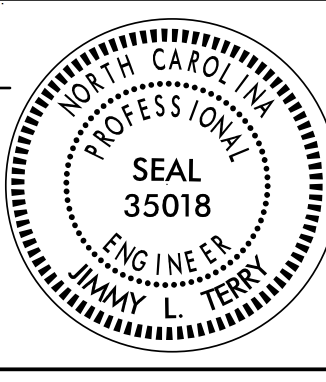
MATCH LINE - L - STA 477+00
SEE BELOW



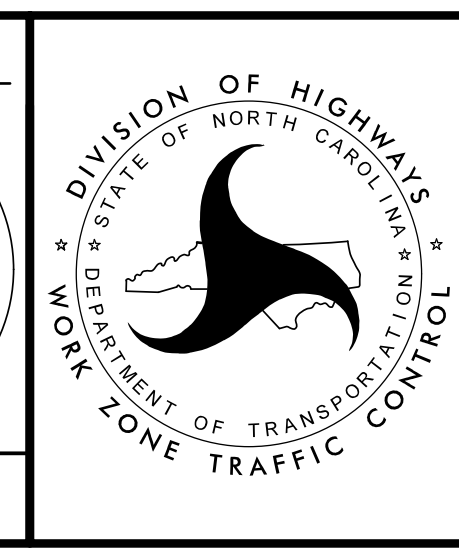
MATCH LINE - L - STA 477+00
SEE ABOVE

APPROVED: 
 DATE: 5/11/2021

SEAL




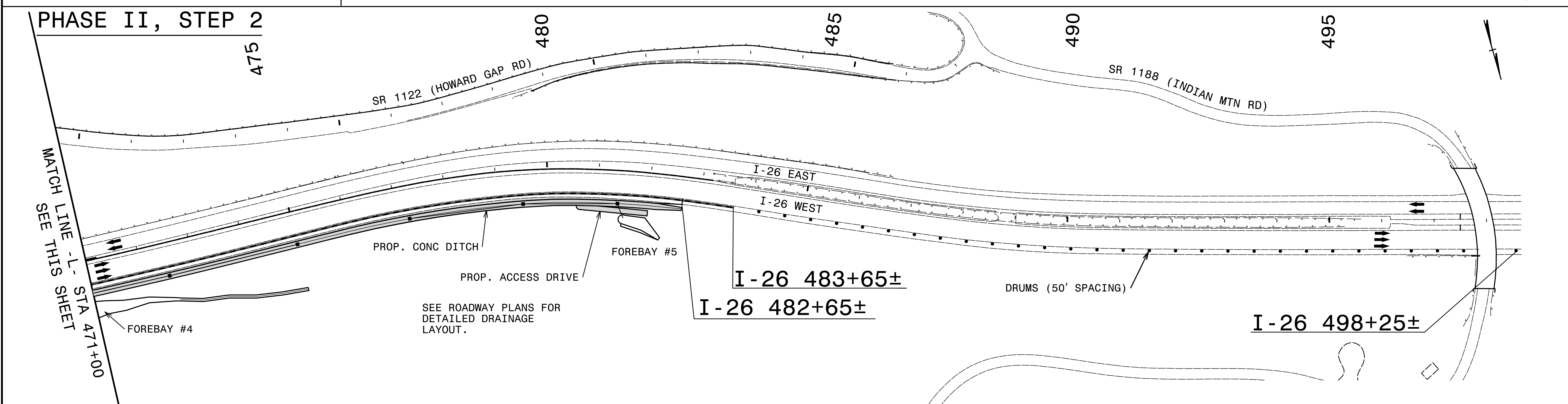
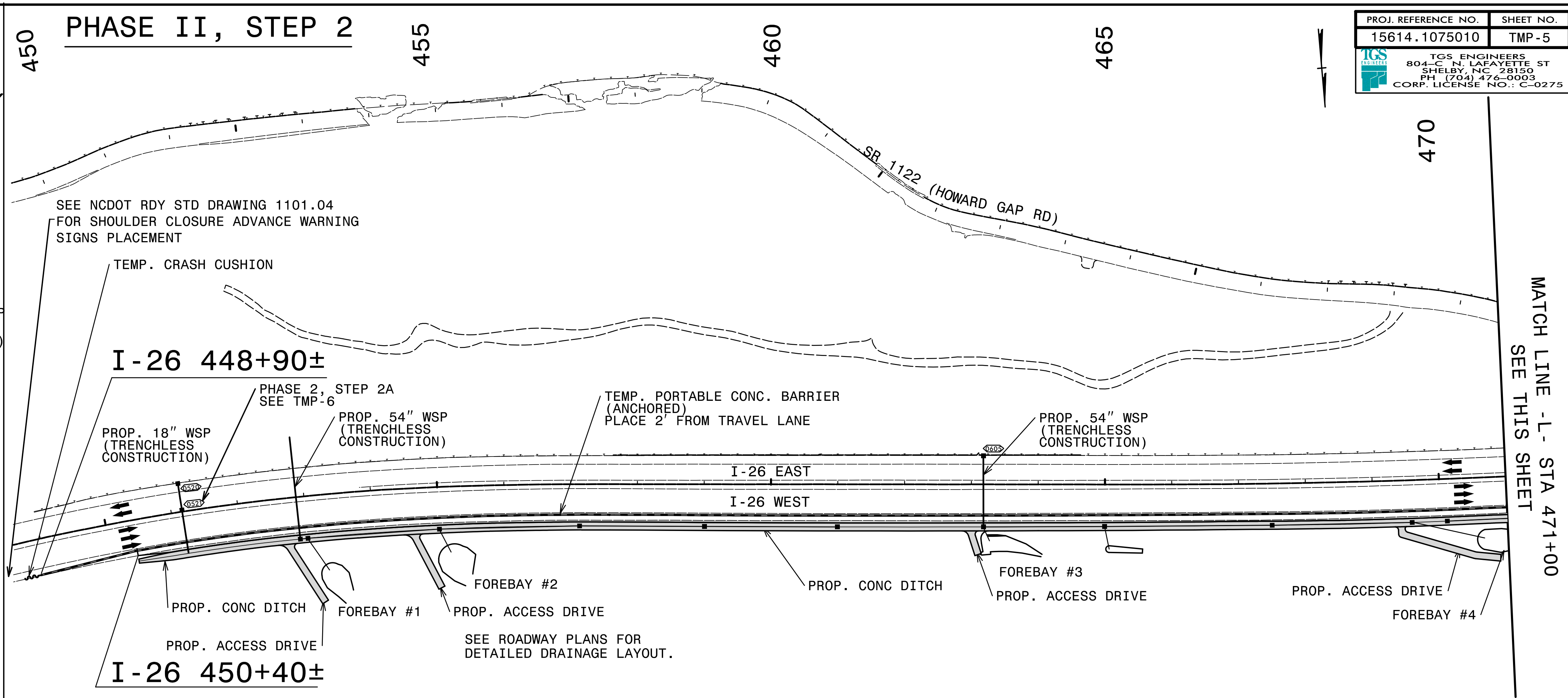
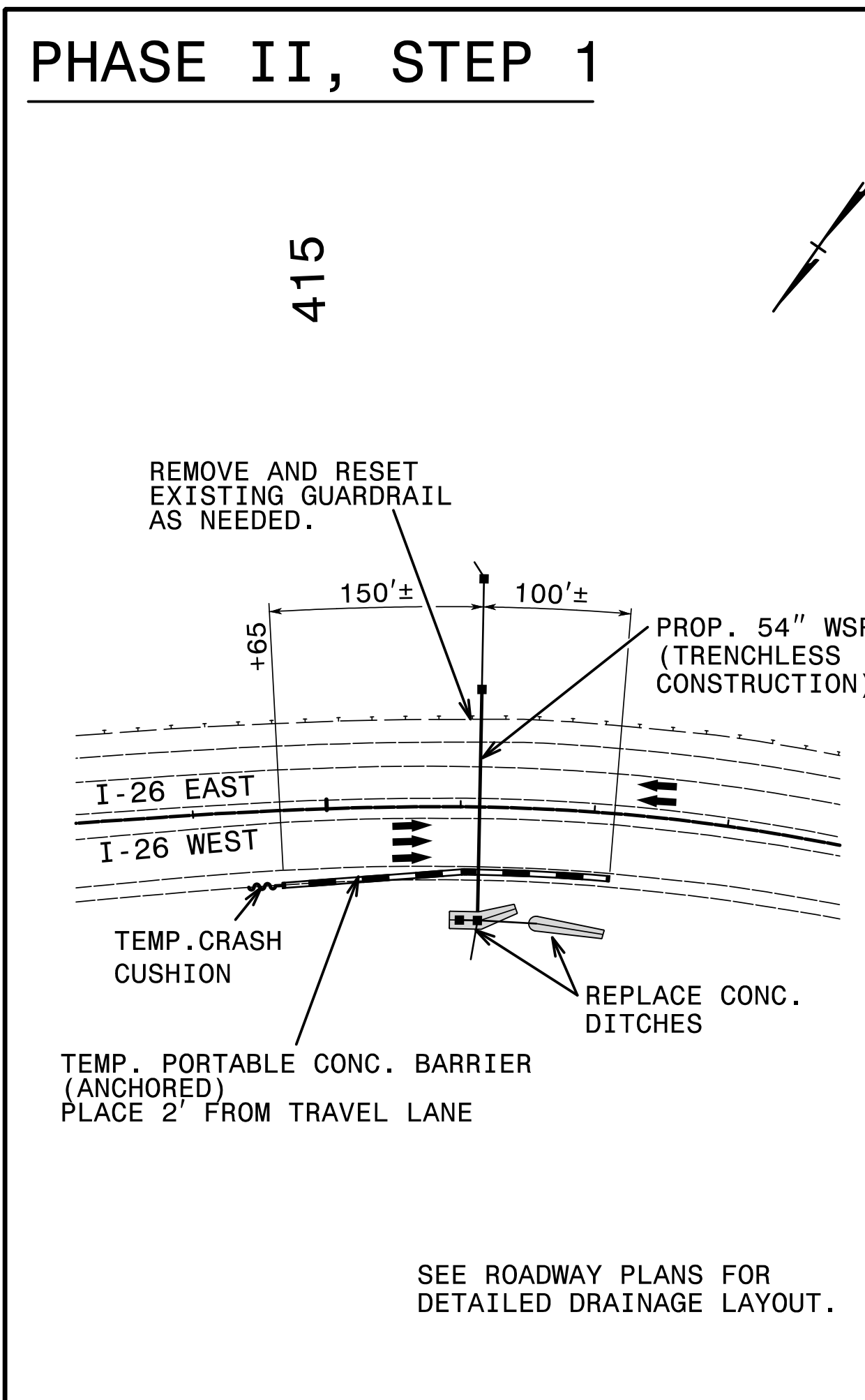
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UNLESS ALL SIGNATURES COMPLETED**



**PHASE I
DETAIL**

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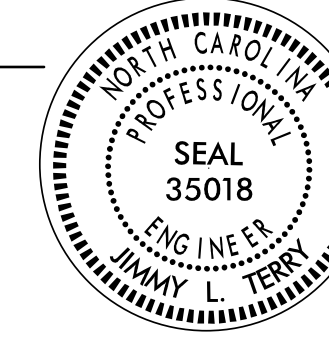
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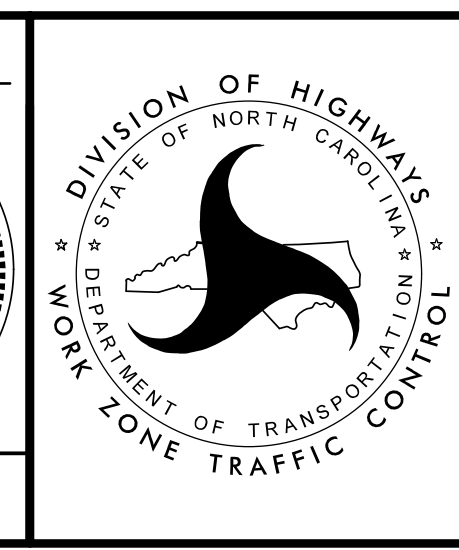
APPROVED: Jimmy Terry
FD039F3882E64DD0

DATE: 5/11/2021

SEAL




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UNLESS ALL SIGNATURES COMPLETED

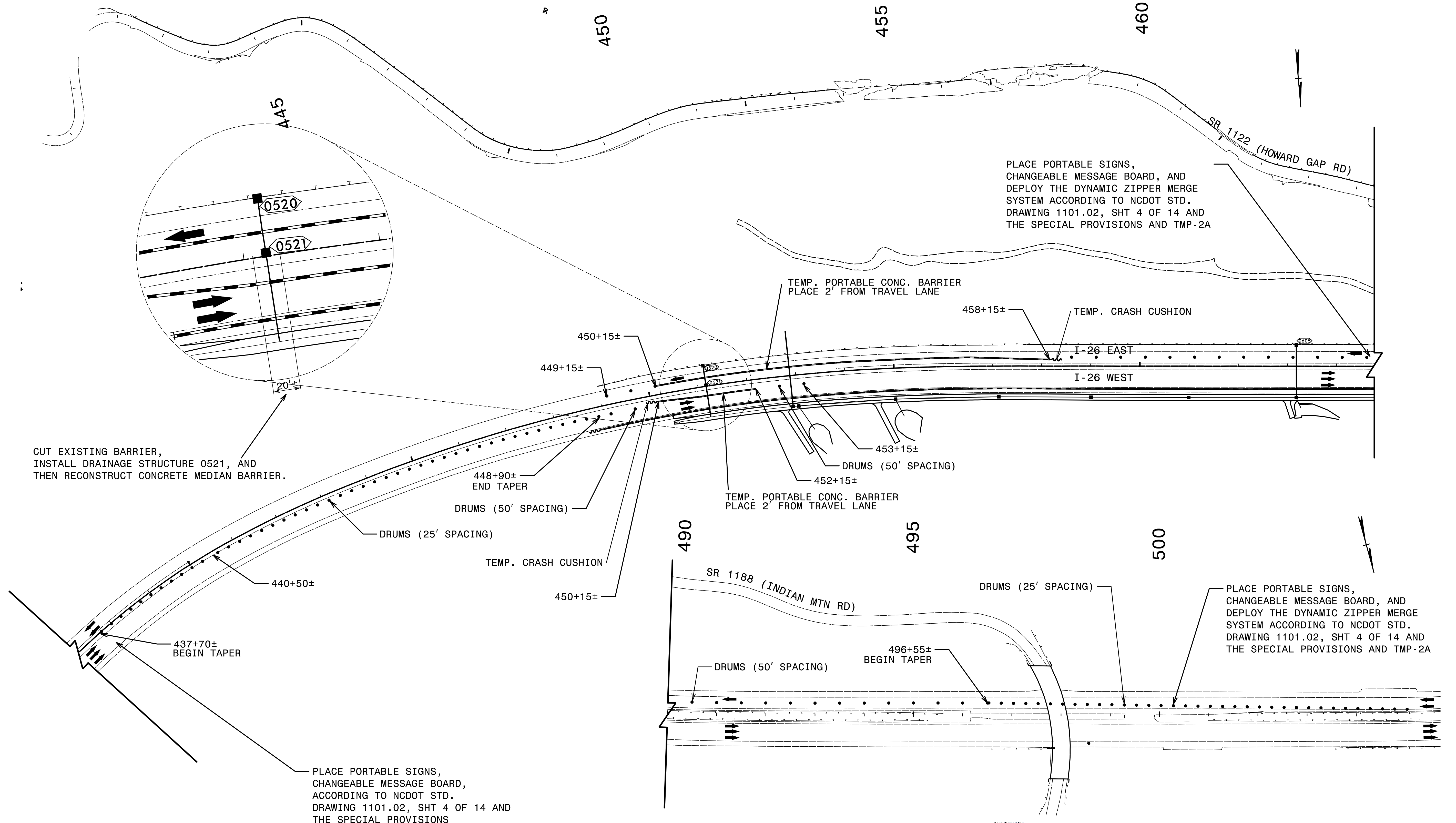


PHASE II DETAILS

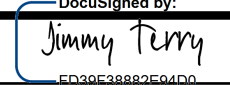
3/26/2021
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 User:jmelvin

PHASE II, STEP 2A

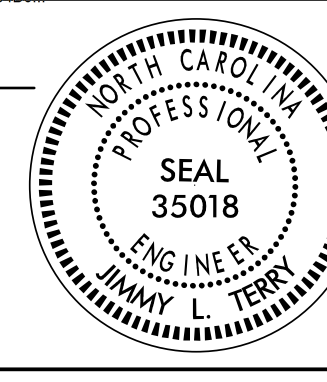
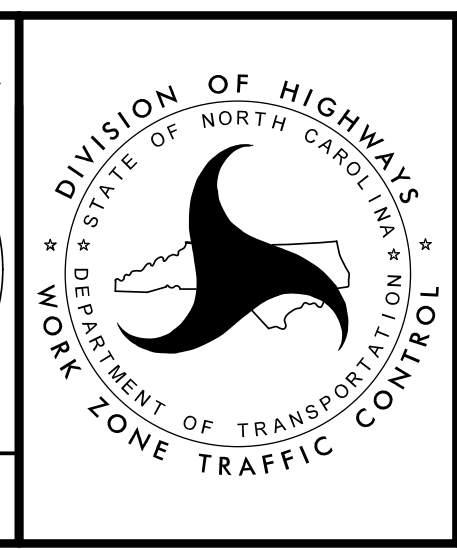
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 TGS ENGINEERS 804-C N. LAFAYETTE ST SHELBY, NC 28150 PH (704) 476-0003 CORP. LICENSE NO. C-0275	



5/12/2021 X:\NCDOT\I-26 Howard Gap Rd Rehab\TrafficControl\I-26-Howard Gap_Tc_TMP_06(PH2 Details).dgn User:tpurlett


APPROVED: 
 DATE: 5/12/2021

SEAL

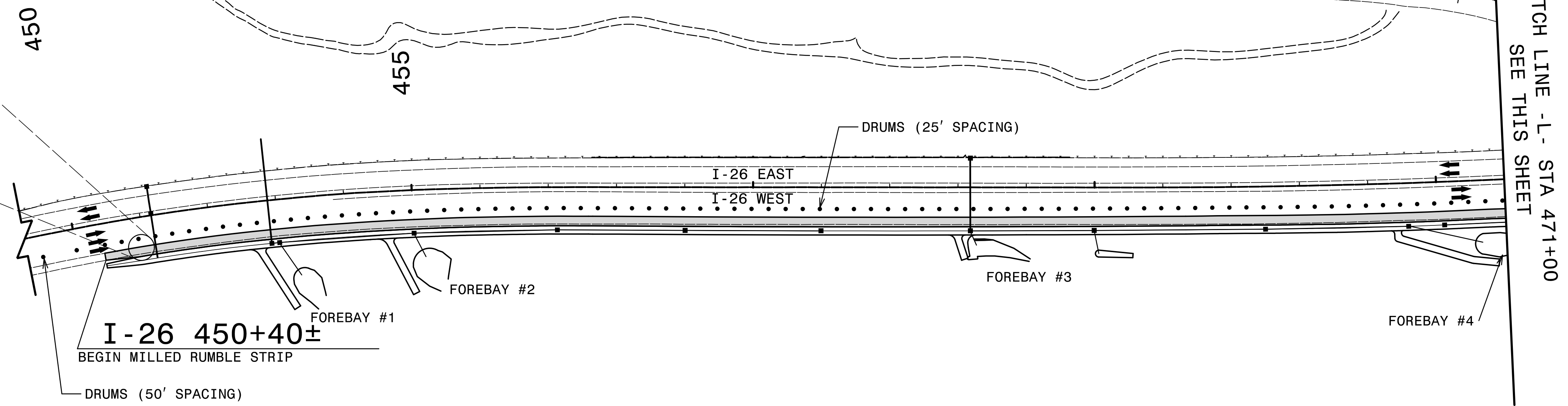
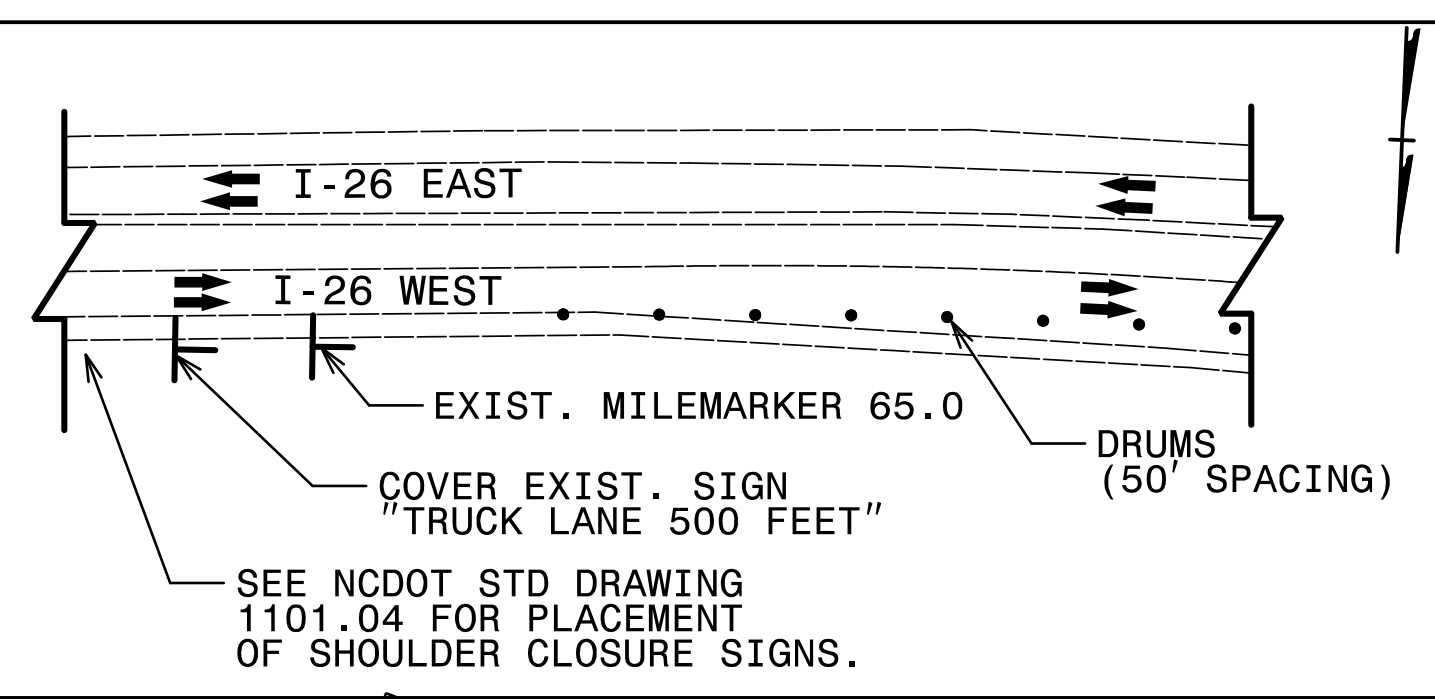
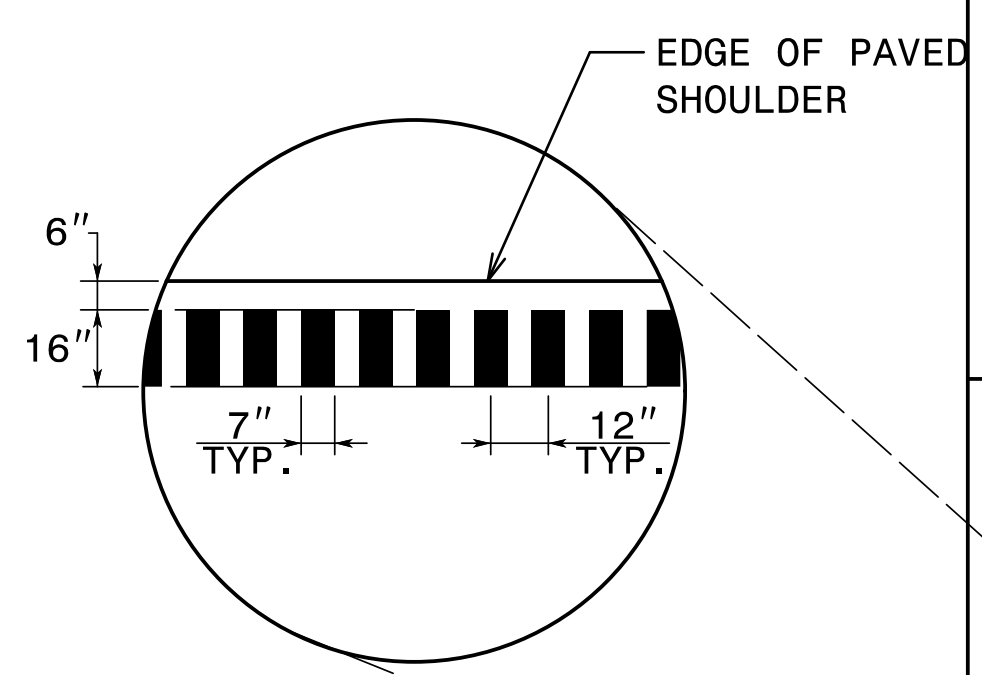
PHASE II DETAILS

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PROJ. REFERENCE NO.	SHEET NO.
15614.1075010	TMP-7
 TGS ENGINEERS 804-C N. LAFAYETTE ST SHELBY, NC 28150 PH: (704) 476-0003 CORP. LICENSE NO.: C-0275	

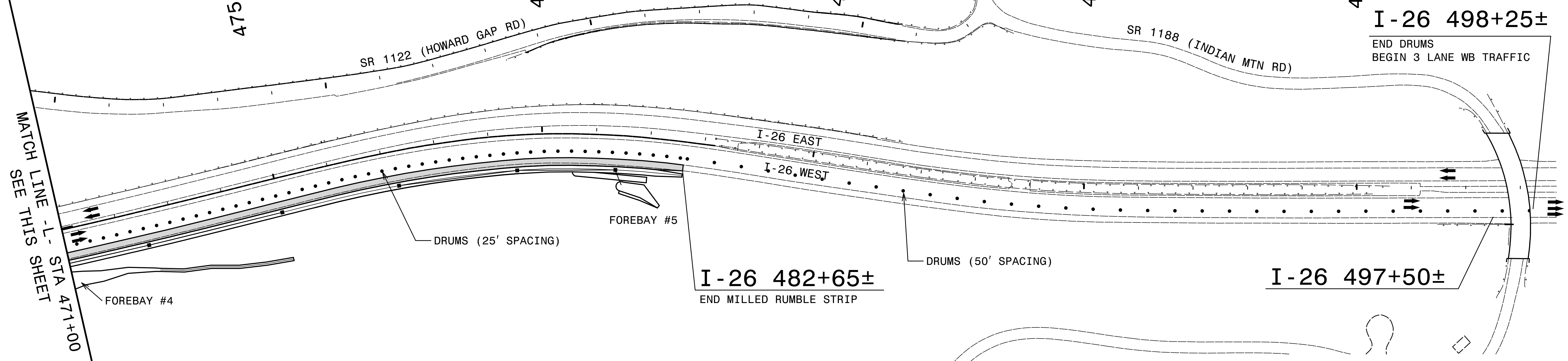
PHASE II, STEP 3

MILLED RUMBLE STRIP LAYOUT

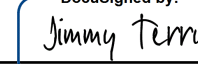


MATCH LINE -L- STA 471+00
SEE THIS SHEET

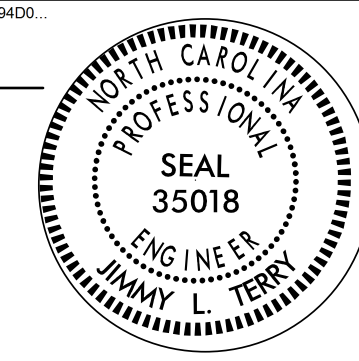
PHASE II, STEP 3



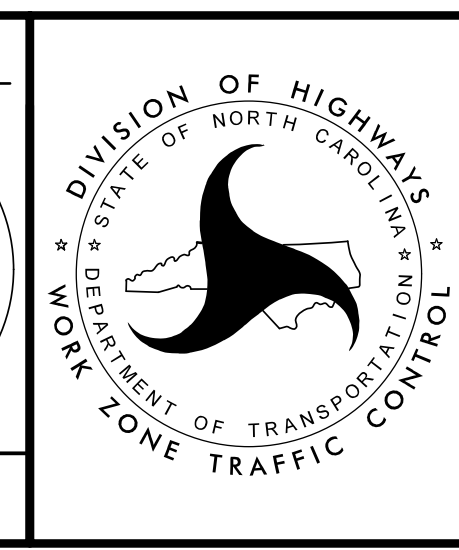
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APPROVED: 
 DATE: 5/11/2021

SEAL

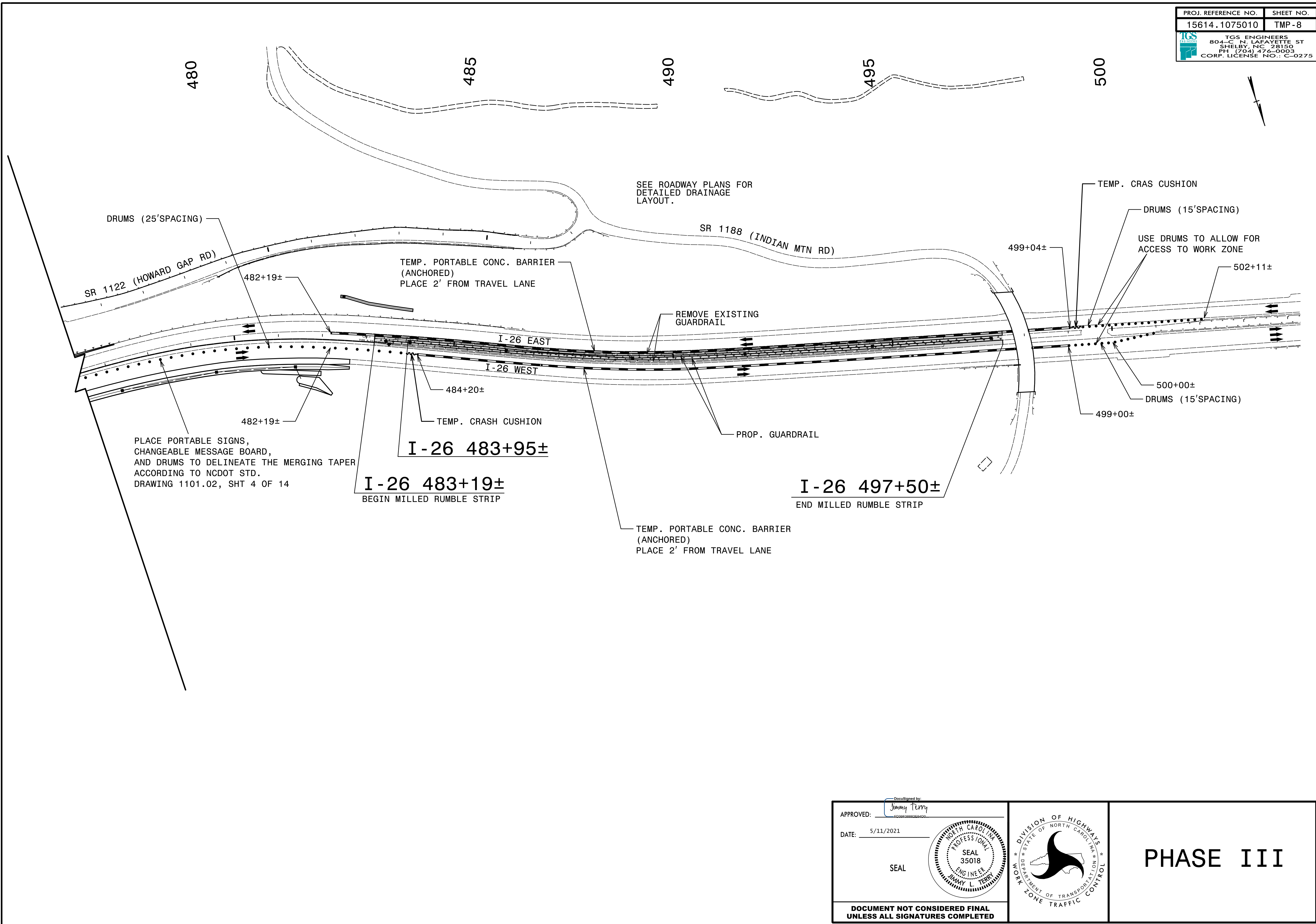


**DOCUMENT NOT CONSIDERED FINAL
UNLESS ALL SIGNATURES COMPLETED**

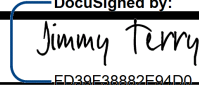
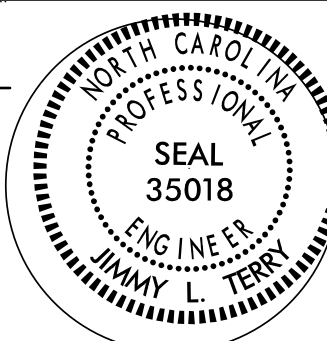



PHASE II DETAILS

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3/26/2021 1:26 Howard Gap Rd Rehab Traffic Control\TCP\I-26-Howard Gap_TC_TMP_08(PH3).dgn
 User: jsmelvin

APPROVED:  DATE: 5/11/2021 SEAL 		<h1>PHASE III</h1>
DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED		