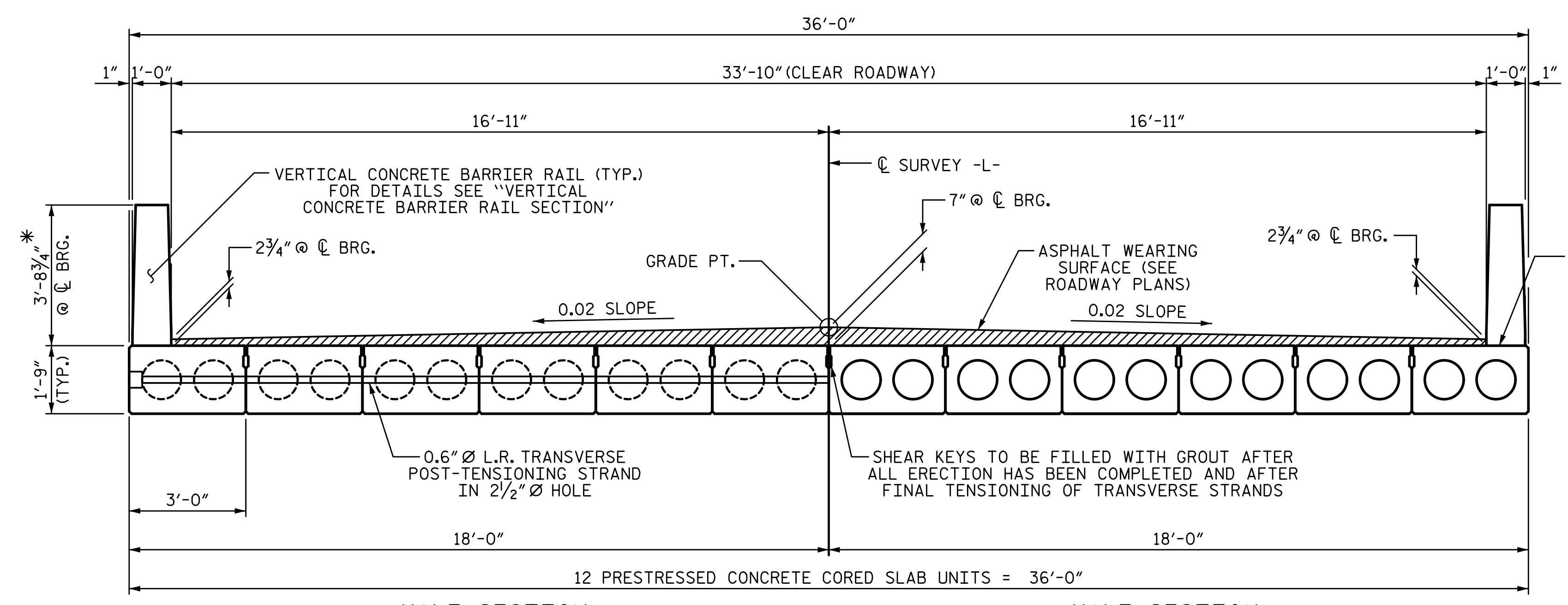


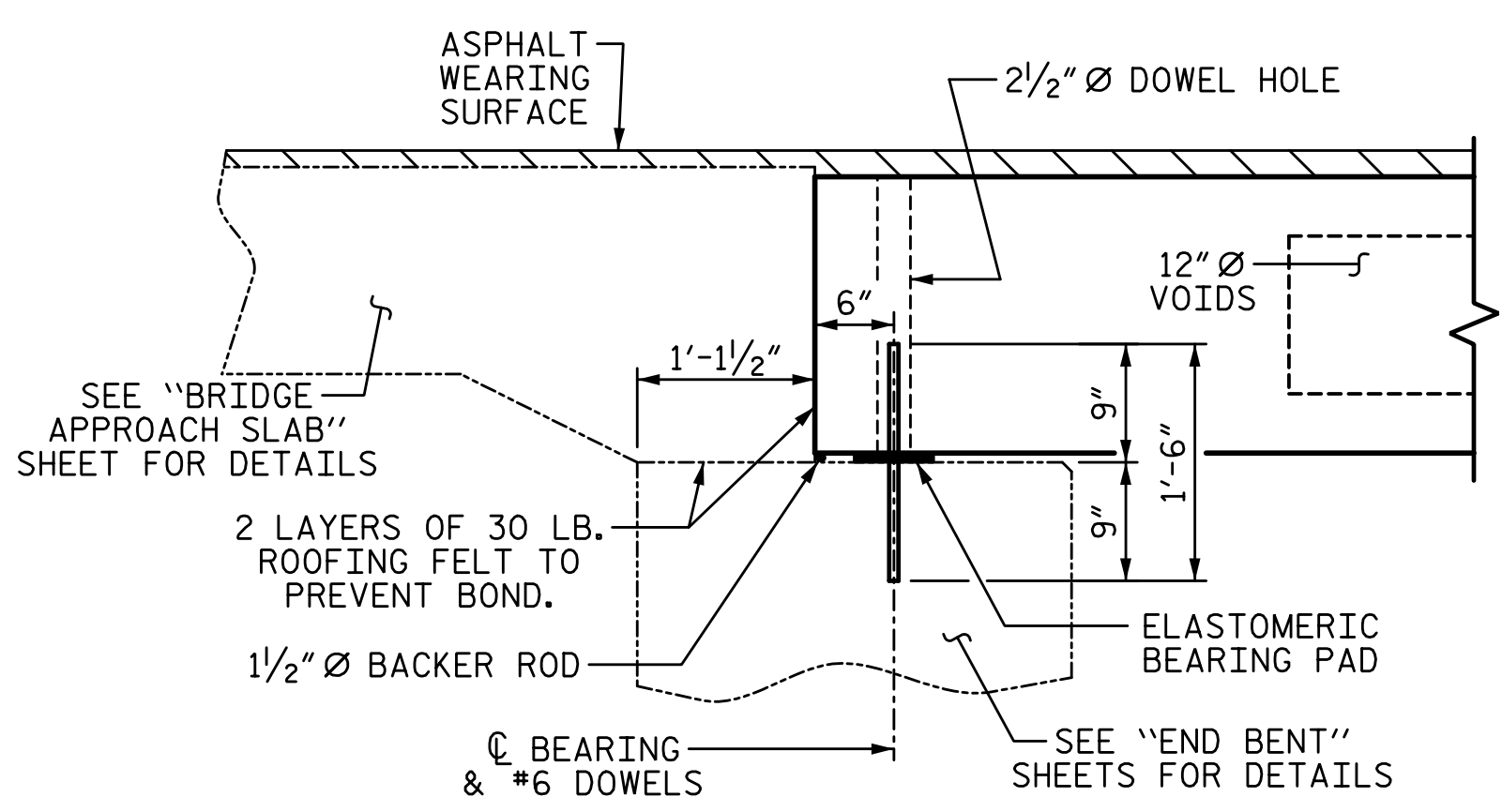
2/20/2020 2:19:47 PM G:\Projects\2018\Division 3 (SEA)\B-5626 (Onslow 31 NC50 over Sandy Run Swamp)\Structures\Drawings\Final\401_B5626_SMU_SUPER_660031.dgn



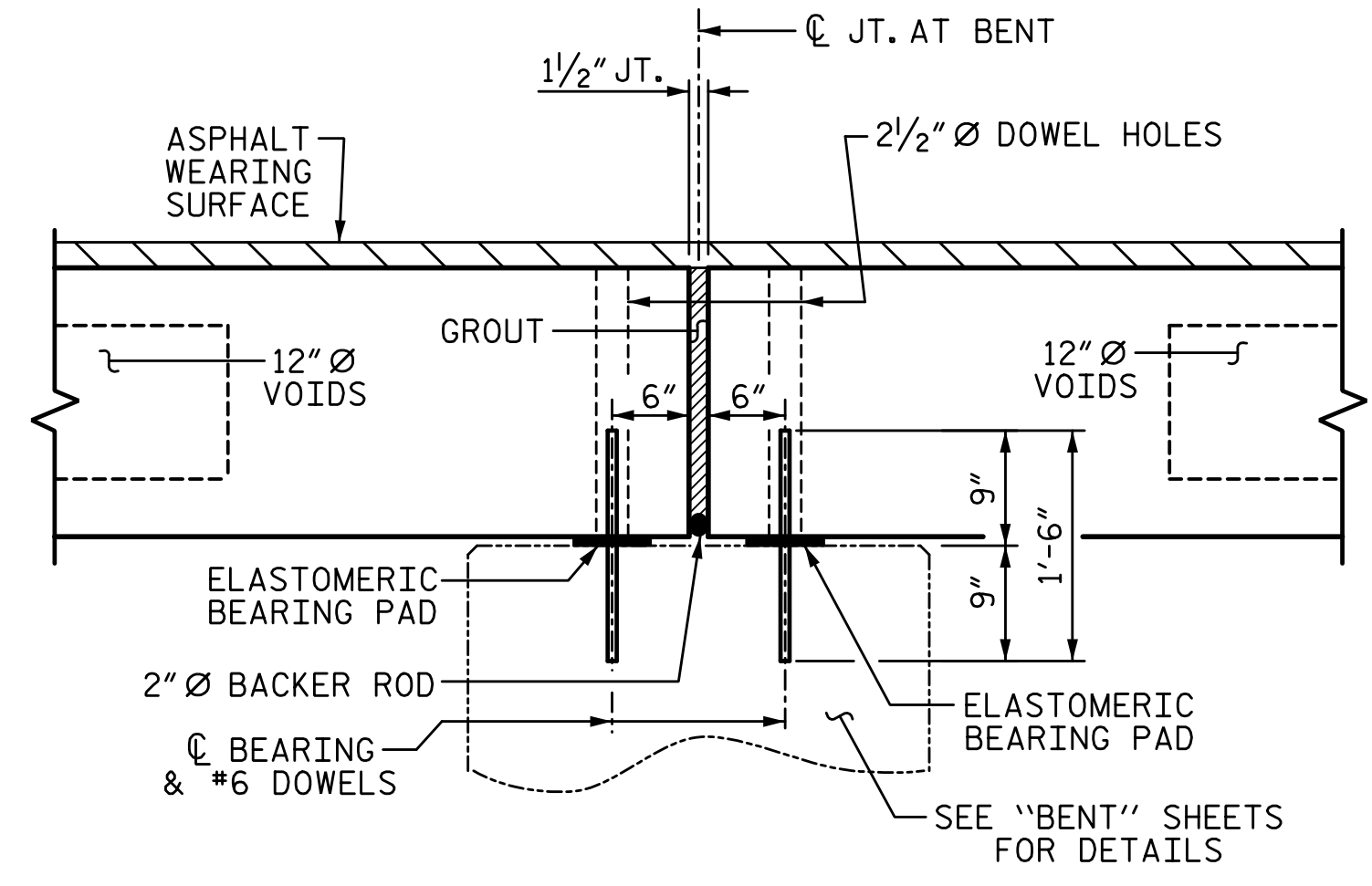
TYPICAL SECTION
 HALF SECTION AT INTERMEDIATE DIAPHRAGMS HALF SECTION THROUGH VOIDS

* - THE MAXIMUM BARRIER RAIL HEIGHT AND ASPHALT THICKNESS IS SHOWN. THE HEIGHT OF THE BARRIER RAIL AND ASPHALT THICKNESS VARIES WHILE THE TOP OF THE BARRIER RAIL FOLLOWS THE PROFILE OF THE GUTTERLINE. FOR RAIL HEIGHT DETAILS AND ASPHALT THICKNESS SEE THE "VERTICAL CONCRETE BARRIER RAIL SECTION" DETAIL.

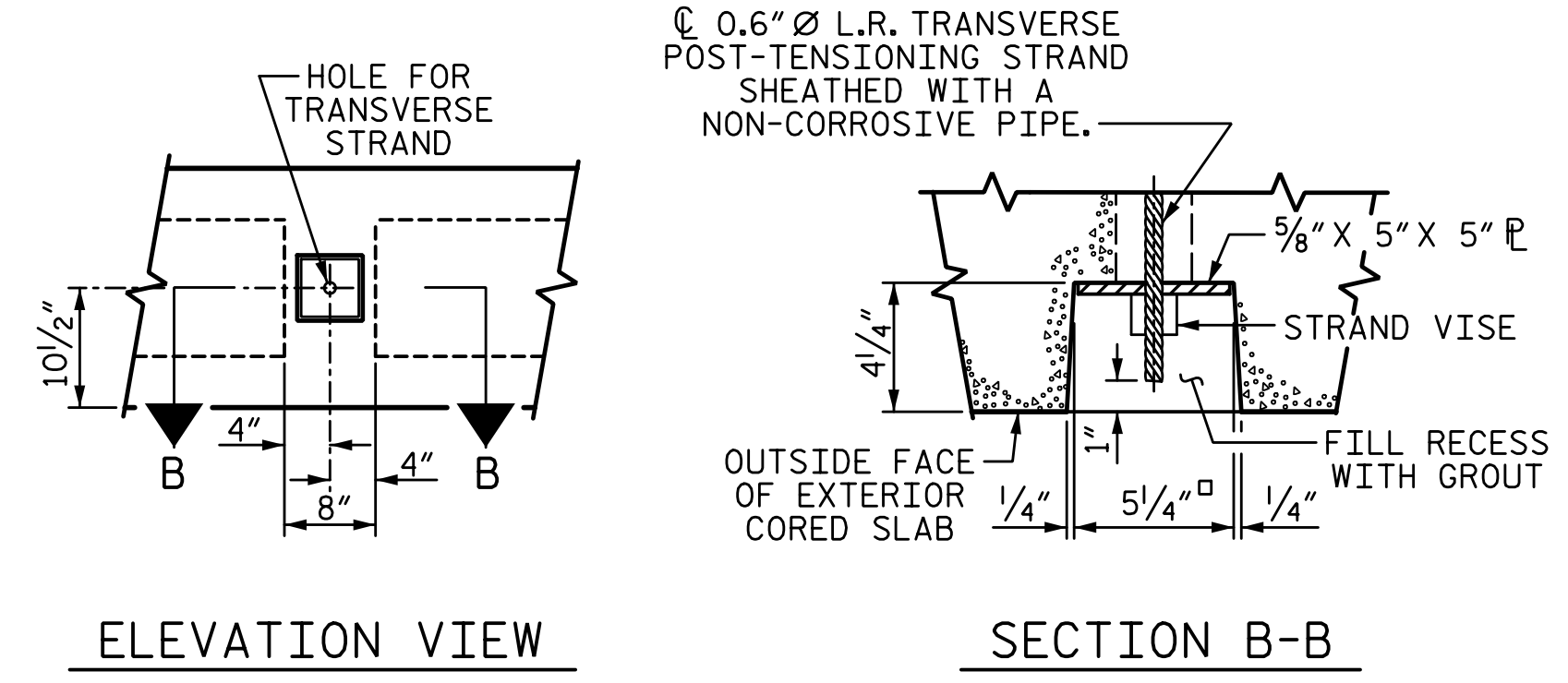
FIXED END **FIXED END** **FIXED END**



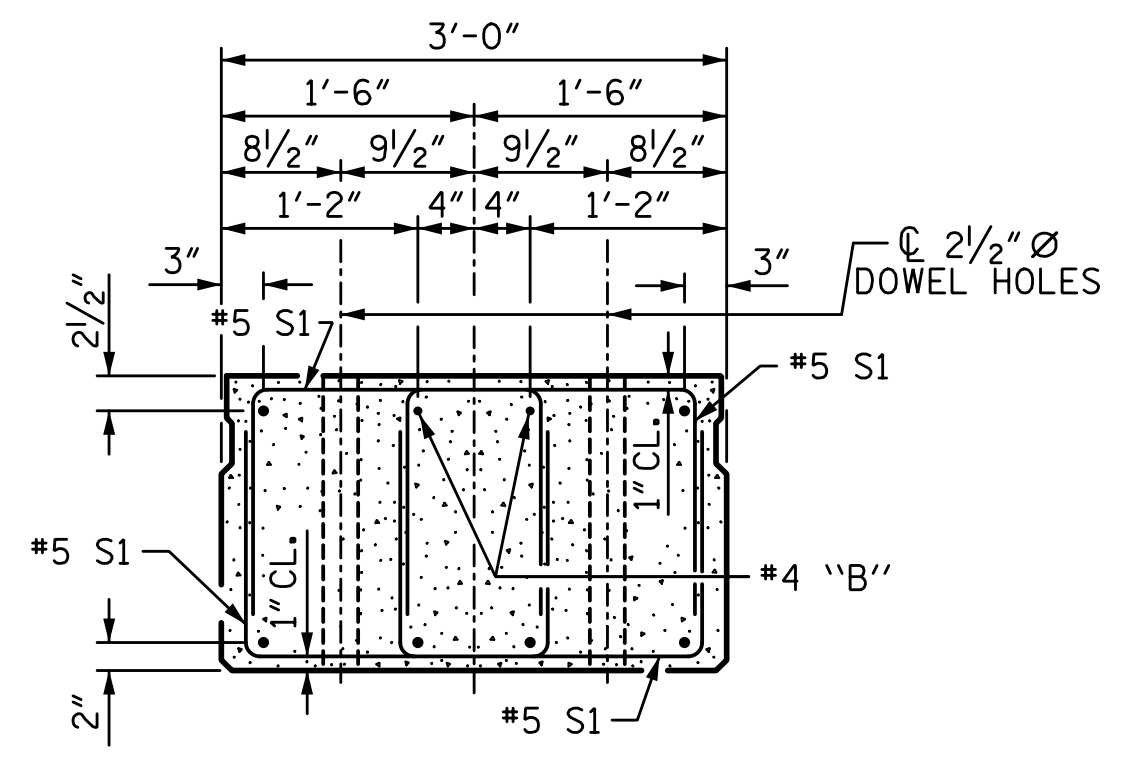
SECTION AT END BENT



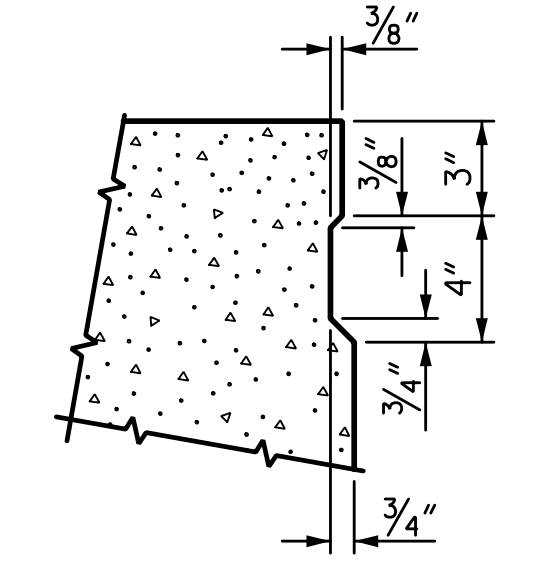
SECTION AT BENT



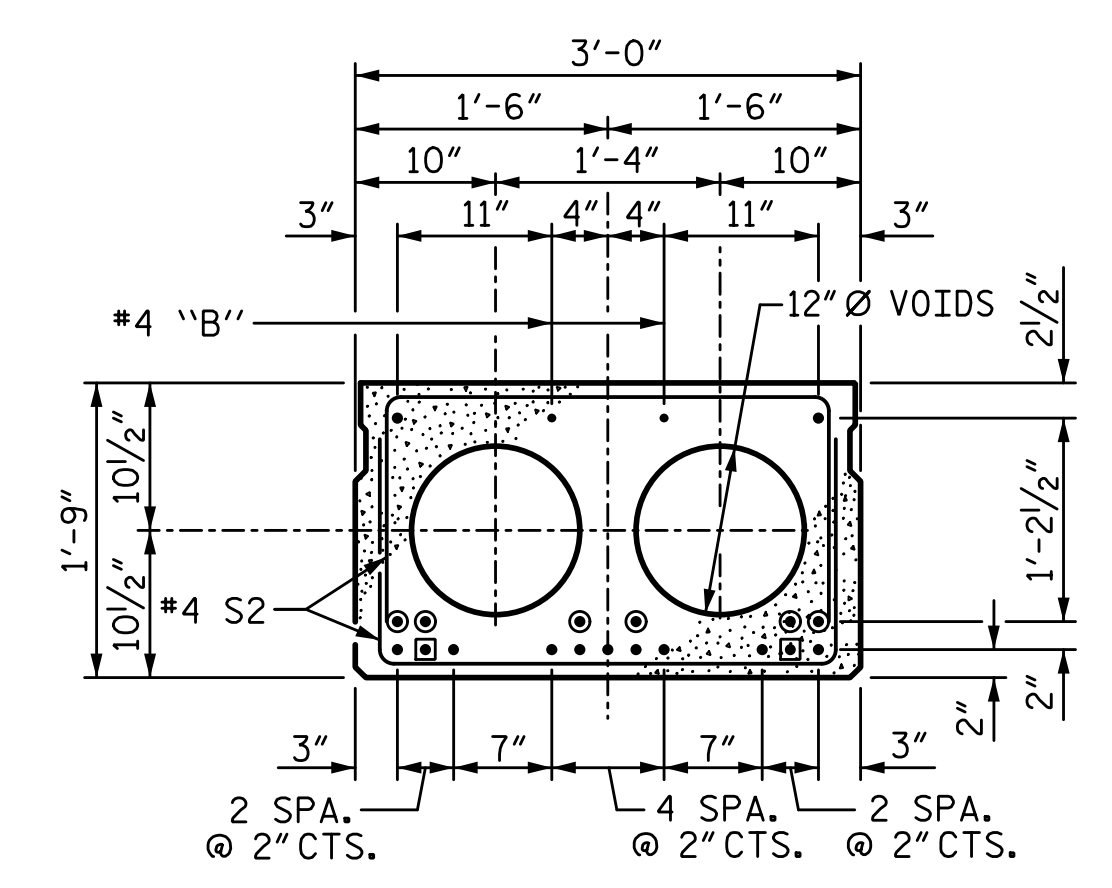
ELEVATION VIEW **SECTION B-B**
GROUTED RECESS AT END OF POST-TENSIONED STRAND FOR CORED SLABS



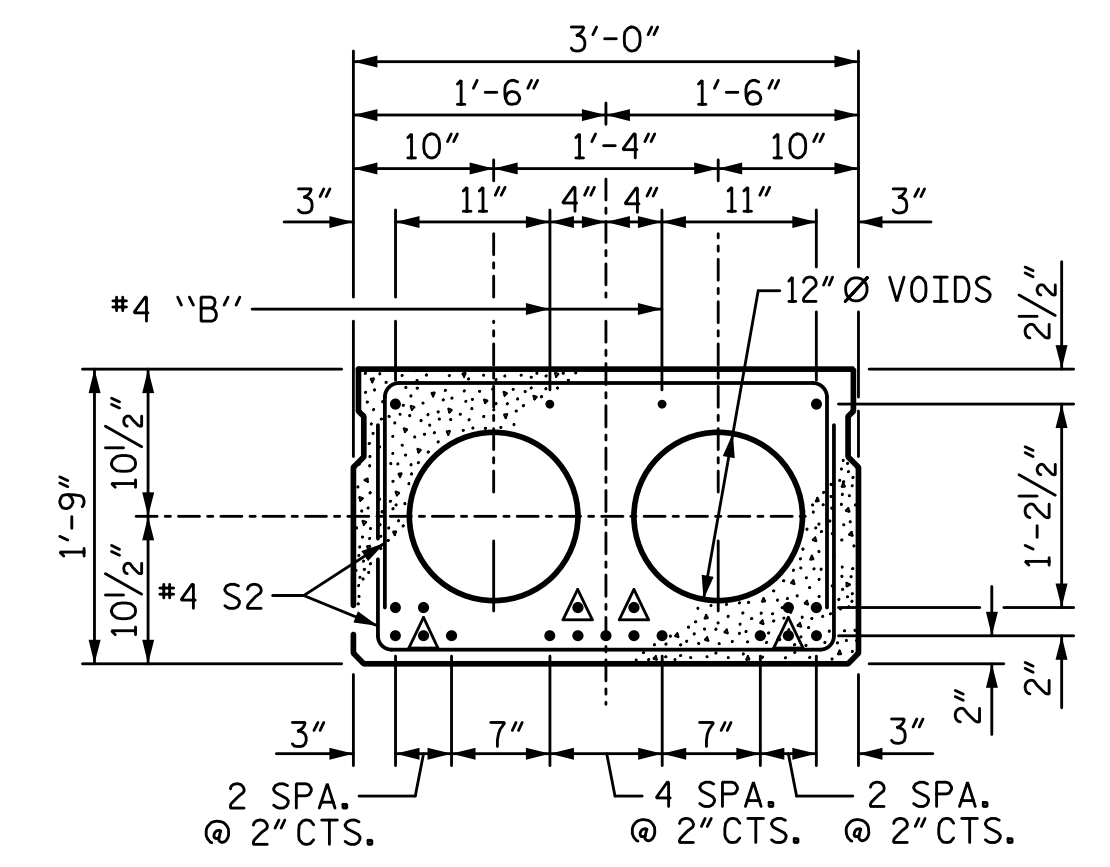
END ELEVATION
 SHOWING PLACEMENT OF DOUBLE STIRRUPS AND LOCATION OF DOWEL HOLES. (STRAND LAYOUT NOT SHOWN). INTERIOR SLAB UNIT SHOWN-EXTERIOR SLAB UNIT SIMILAR EXCEPT SHEAR KEY LOCATION.



SHEAR KEY DETAIL
 NOTE: OMIT SHEAR KEY ON OUTSIDE FACE OF EXTERIOR CORED SLABS.



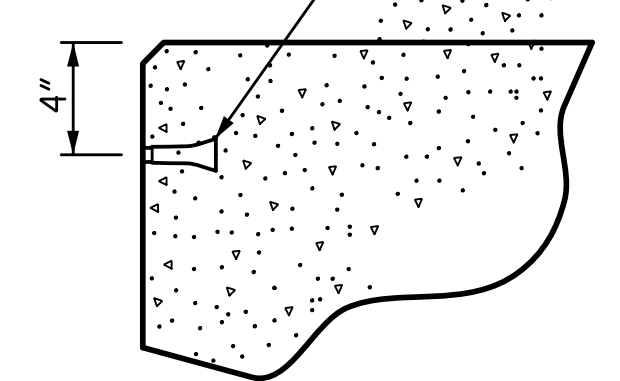
INTERIOR SLAB SECTION (40'-0" & 45'-0" UNIT)
 (13 STRANDS REQUIRED)



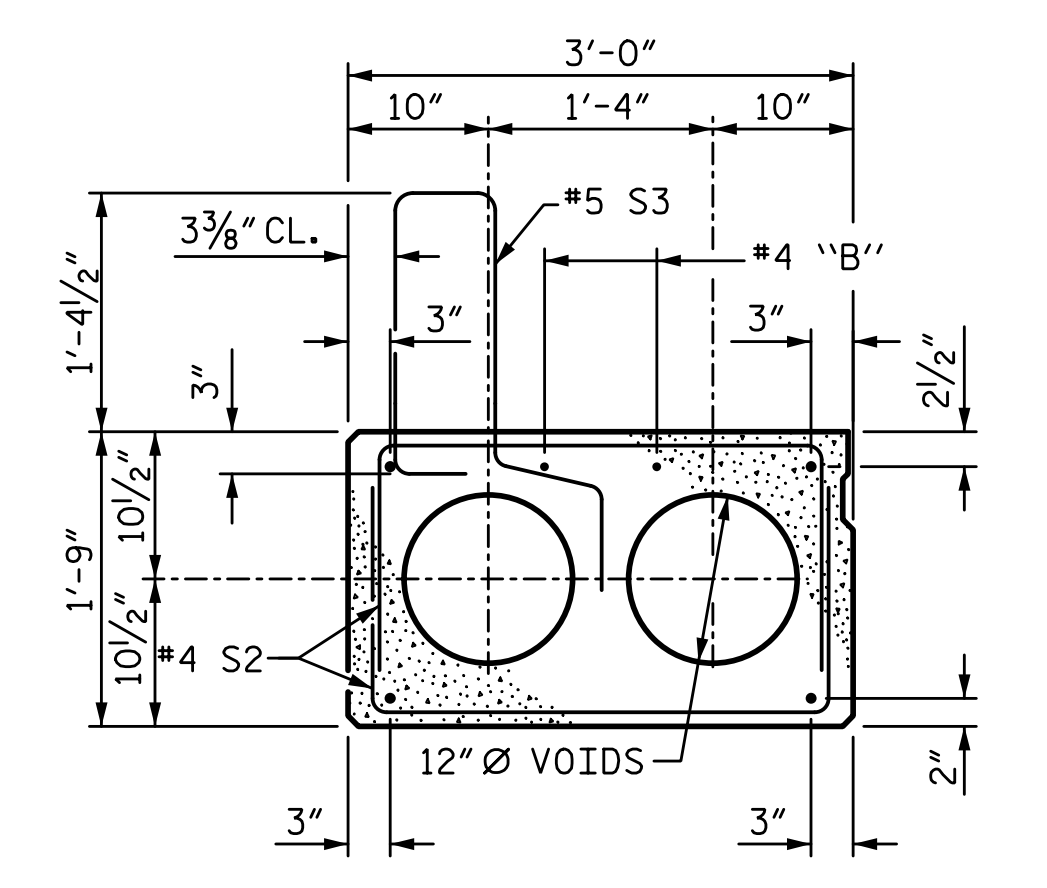
INTERIOR SLAB SECTION (50'-0" UNIT)
 (19 STRANDS REQUIRED)

0.6" Ø LOW RELAXATION STRAND LAYOUT

PERMITTED THREADED INSERT CAST IN OUTSIDE FACE OF EXTERIOR UNIT AND RECESSED 3/4" SIZE TO BE DETERMINED BY CONTRACTOR.



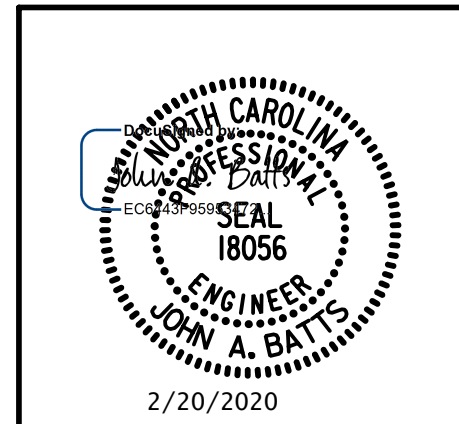
THREADED INSERT DETAIL



EXTERIOR SLAB SECTION
 (FOR PRESTRESSED STRAND LAYOUT, SEE INTERIOR SLAB SECTION.)

- DEBONDING LEGEND**
- ▲ BOND SHALL BE BROKEN ON THESE STRANDS FOR A DISTANCE OF 6'-0" FROM END OF CORED SLAB UNIT. SEE STANDARD SPECIFICATIONS, ARTICLE 1078-7.
 - BOND SHALL BE BROKEN ON THESE STRANDS FOR A DISTANCE OF 2'-0" FROM END OF CORED SLAB UNIT. SEE STANDARD SPECIFICATIONS, ARTICLE 1078-7.
 - OPTIONAL FULL LENGTH DEBONDED STRANDS. THESE STRANDS ARE NOT REQUIRED. IF THE FABRICATOR CHOOSES TO INCLUDE THESE STRANDS IN THE CORED SLAB UNIT, THE STRANDS SHALL BE DEBONDED FOR THE FULL LENGTH OF THE UNIT AT NO ADDITIONAL COST. SEE STANDARD SPECIFICATIONS, ARTICLE 1078-7.

PLANS PREPARED BY:
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 www.simpsonengr.com
 LICENSURE NO. C-2521



PROJECT NO. B-5626
 ONSLOW COUNTY
 STATION: 23+71.50 -L-

SHEET 1 OF 6
 STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH
 SUPERSTRUCTURE
 3'-0" X 1'-9"
 PRESTRESSED CONCRETE
 CORED SLAB UNIT
 90° SKEW

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S-8
1			3			TOTAL SHEETS
2			4			26

DRAWN BY: S.D. COOPER DATE: 2-20
 CHECKED BY: J.A. BATTS DATE: 2-20
 DESIGN ENGINEER OF RECORD: J.A. BATTS DATE: 2-20

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED