

605

606

**THE FINAL ESTIMATE
PROJECT 606
ANSON-STANLY COUNTIES**

112 606

DATE	
BY	
SUBMITTED	
PLOTTED	
TEMPERATURE	
AREAS CHECKED	
FINAL SURVEY NOTE BOOK NO.	

DATE	
BY	
SUBMITTED	
PLOTTED	
TEMPERATURE	
AREAS CHECKED	
ORIGINAL SURVEY NOTE BOOK NO.	

FINALESTIMATE ON NORTH CAROLINA STATE PROJECT N° 606 ✓
 FOR
ROCKY RIVER BRIDGE ✓
 ANSON-STANLY COUNTIES

CONTRACTOR- CONCRETE STEEL BRIDGE CO
 CLARKSBURG N VA,

CONTRACT LET - MARCH 18th, 1922
 WORK COMPLETED - JUNE 6th, 1923

J. B. PRIDGEN - DISTRICT ENGINEER
 C. D. LUCAS - RESIDENT ENGINEER
 J. B. BROACH - INSPECTOR

Note:- The Final Survey Note Books properly marked and described submitted herewith as a part of this final estimate are as follows:-

- ✓ 1 Transit Book
- ✓ 1 Level Book
- ✓ 1 Masonry Book, Showing Detailed Sketches, Actual Dimensions and Computations of Quantities, Alignment and Reference points
- 4 Time Books (Bridge Cost Record Books)
- ✓ 1 Diary Showing the Resident Engineer's Daily Record of Forces and Equipment, Working Days, Weather Conditions and All important Events connected with the work.

INDEX OF SHEETS

Sheet No. 1.	Title Page.
" " 2.	General Drawing.
" " 3.	Details of Piers Nos. 1 & 2.
" " 4.	Details of Piers Nos. 3, 4, 5, 6, 7, 8 and 9.
" " 5.	10-20 ft. KC Deck Girders.
" " 6.	Wooden Guard Rail.
" " 7.	Name Plate.

STATE OF NORTH CAROLINA
STATE HIGHWAY COMMISSION

PLAN AND PROFILE OF PROPOSED
STATE HIGHWAY

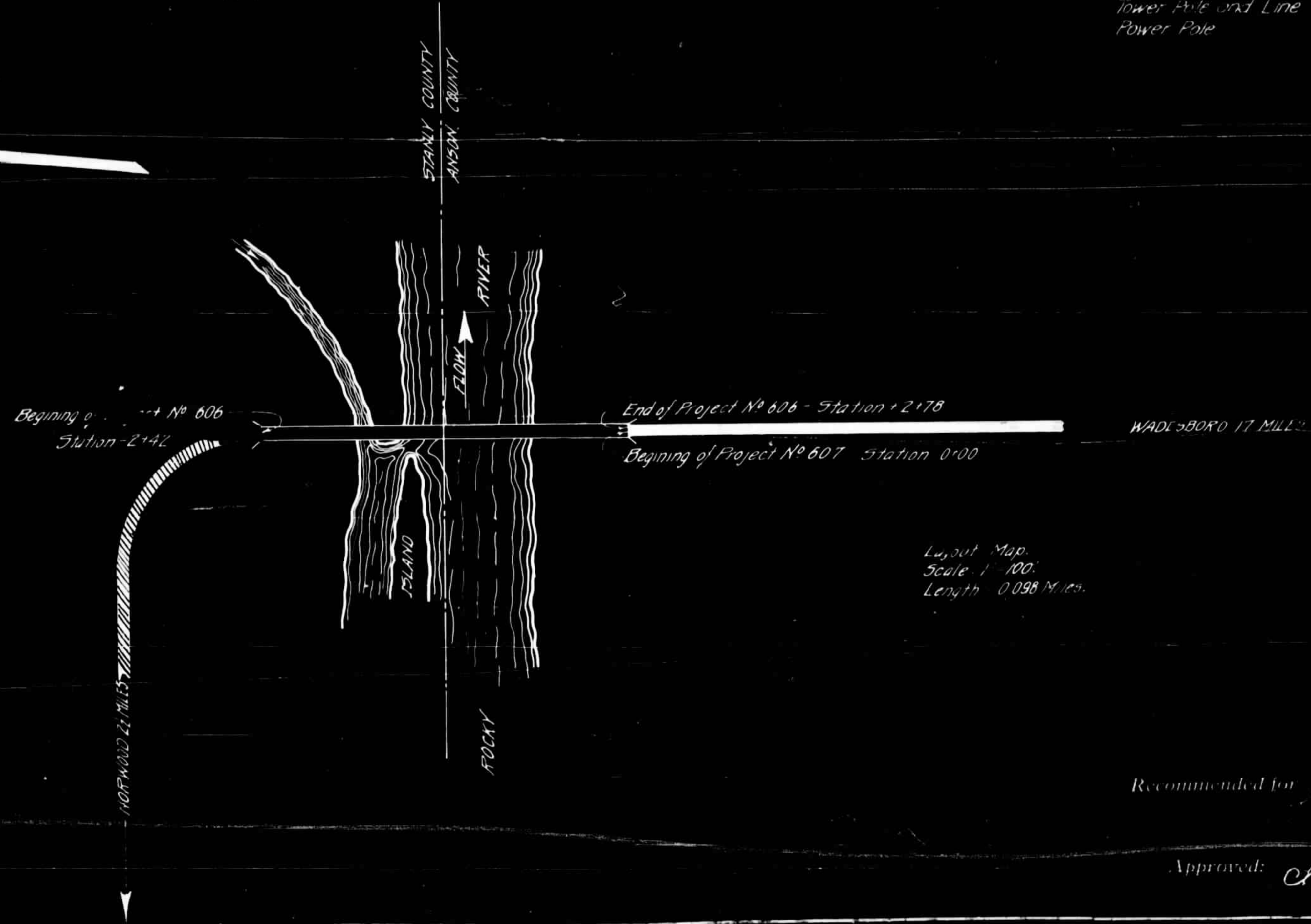
CONVENTIONAL SIGNS

- County Line
- Township Line
- City or Town Line
- Right-of-Way Line
- Survey Line
- Property Line
- Wire Fence
- Board Fence
- Present Traveled Road
- Railroad
- Grade Elevation
- Ground Elevation
- Pipe Culvert
- Box Culvert
- Woods
- Telephone or Telegraph Pole
- Tower Pole and Line
- Power Pole

ANSON COUNTY

STANLY COUNTY

BRIDGE OVER ROCKY RIVER
BETWEEN ANSON COUNTY AND STANLY COUNTY
ON THE
SALISBURY - ALBEMARLE - WADESBORO HIGHWAY



Prepared in Office of
State Highway Commission,
Raleigh, N. C.

Surveyed by R. T. James
Drawn by E. L. Erickson
Date: January 1922

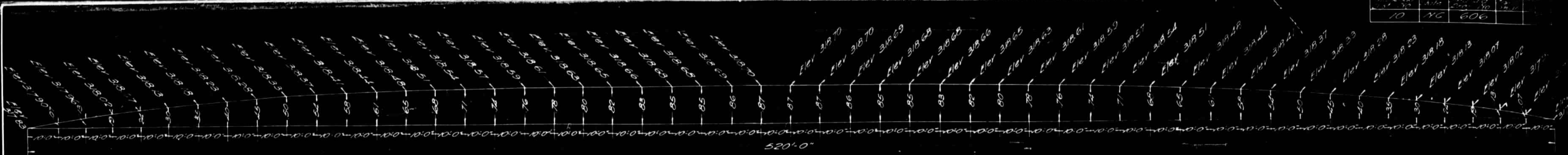
Recommended for Approval:

District Engineer

Approved:

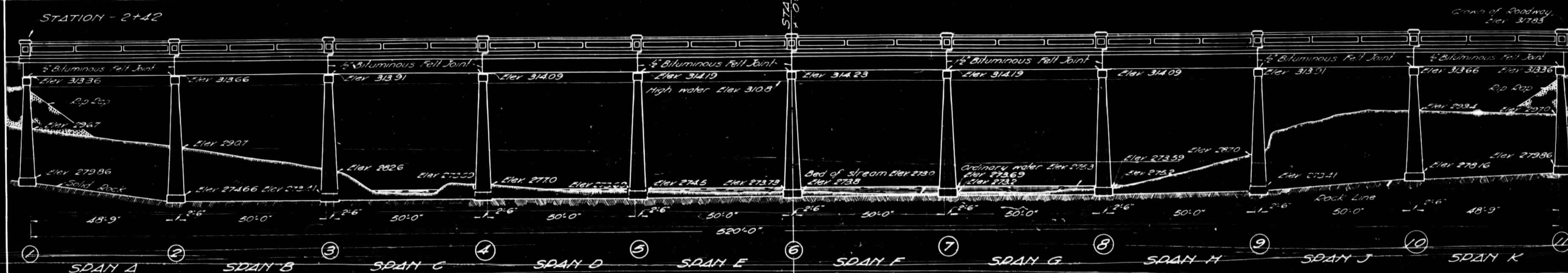
State Highway Engineer

PLAN IV
4 1 1
Jan 1
1922

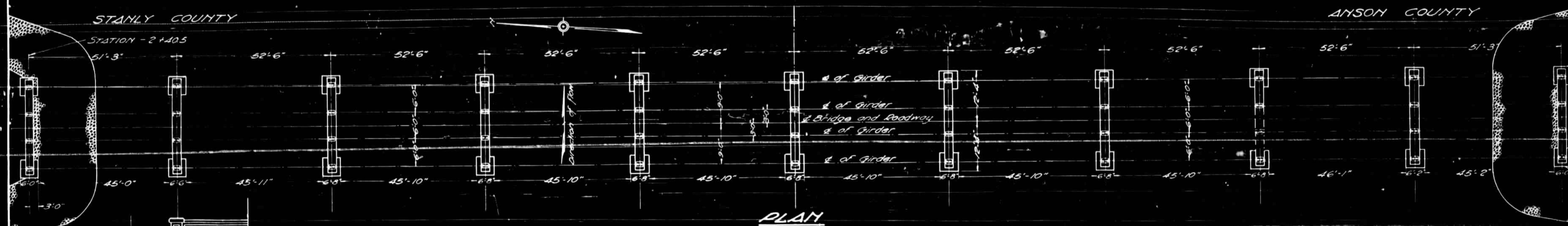


CAMBER DIAGRAM

NOTE
Camber diagram does not include allowance for settlement of individual spans which should be provided for in addition to elevations given after the falsework has been removed. The finished structure should have the camber as shown. The elevations are given for crown of finished roadway. The rails, curbs and coping follow the same line of camber.



ELEVATION



PLAN



ELEVATION OF END PIERS SHOWING RID RAD

GENERAL NOTE
Class 41 concrete to be used throughout. Proportions 1:2:4. Maximum size of aggregate 1/2". Reinforcing steel shall be deformed bars. Square twisted bars are not to be considered deformed bars. All dimensions relative to reinforcement are to centers of bars. No splices of bars will be permitted other than those shown on plans. All material and workmanship as per specifications of the N.C. State Highway Commission.
B.M. 36" Gum tree 15 feet right of station 2+00 (Anson Co.)

TOTAL BILL OF MATERIAL									
SPAN	CONCRETE CLASS 41 CU YDS	REIN STEEL LBS	PLATES 1/4" THK SQ FT	PLATES 1/2" THK SQ FT	PIERS	CONCRETE CLASS 41 CU YDS	REIN STEEL LBS	PLATES 1/4" THK SQ FT	PLATES 1/2" THK SQ FT
SPAN A	853	16881	278	8125	PIER 3	853	16881	278	8125
SPAN B	850	16835	184	8165	PIER 4	850	16835	184	8165
SPAN C	850	16835	184	8165	PIER 5	850	16835	184	8165
SPAN D	850	16835	184	8165	PIER 6	850	16835	184	8165
SPAN E	850	16835	184	8165	PIER 7	850	16835	184	8165
SPAN F	850	16835	184	8165	PIER 8	850	16835	184	8165
SPAN G	850	16835	184	8165	PIER 9	850	16835	184	8165
SPAN H	850	16835	184	8165	PIER 10	850	16835	184	8165
SPAN I	850	16835	184	8165	PIER 11	850	16835	184	8165
SPAN J	850	16835	184	8165	TOTAL	14517	274	8074	8167
SPAN K	848	16834	278	8125	PIER 12	848	16834	278	8125
PIER 1	605	4901	186	—	ROAD RAD	50 YDS	—	—	—
PIER 2	630	5343	186	—	WOODEN GUARD RAIL	50 FT	—	—	—

STATE OF NORTH CAROLINA
STATE HIGHWAY COMMISSION
RALEIGH, N.C.
GENERAL DRAWING
ROCKY RIVER BRIDGE
10-50 FT. RC DECK GIDDERS
CLEAR ROADWAY 18'-0"
PROJ. NO. 606 STA. 0+18
ANSON-STANLY COUNTIES
JANUARY 1922

Submitted by W. A. ...
Approved by ...

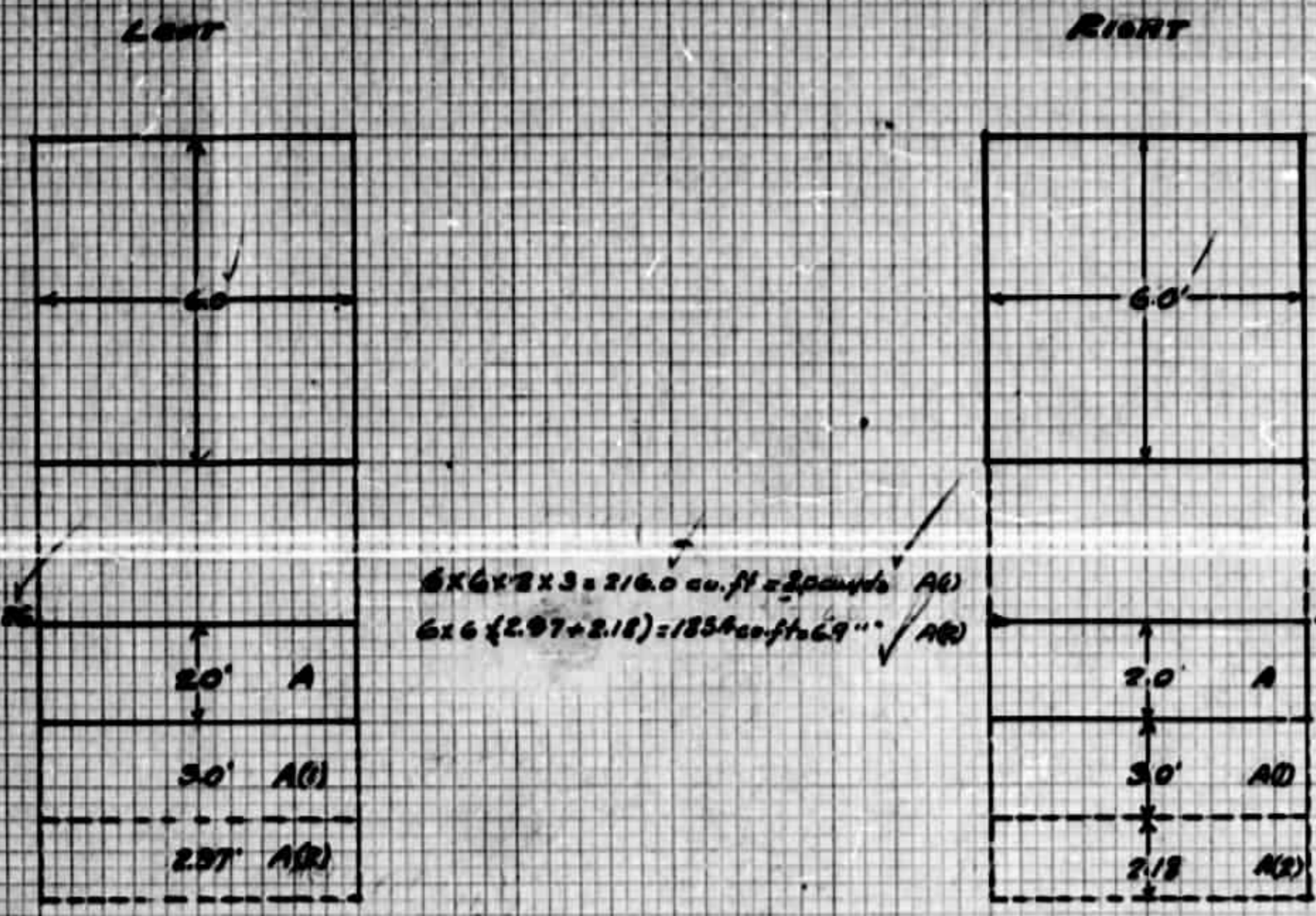
Designed by C. A. Taylor Date Jan 5, 22
Drawn by ... Date Jan 9, 22
Checked by ... Date Jan 13, 22

Revised weight of steel July 5, 1922 146.

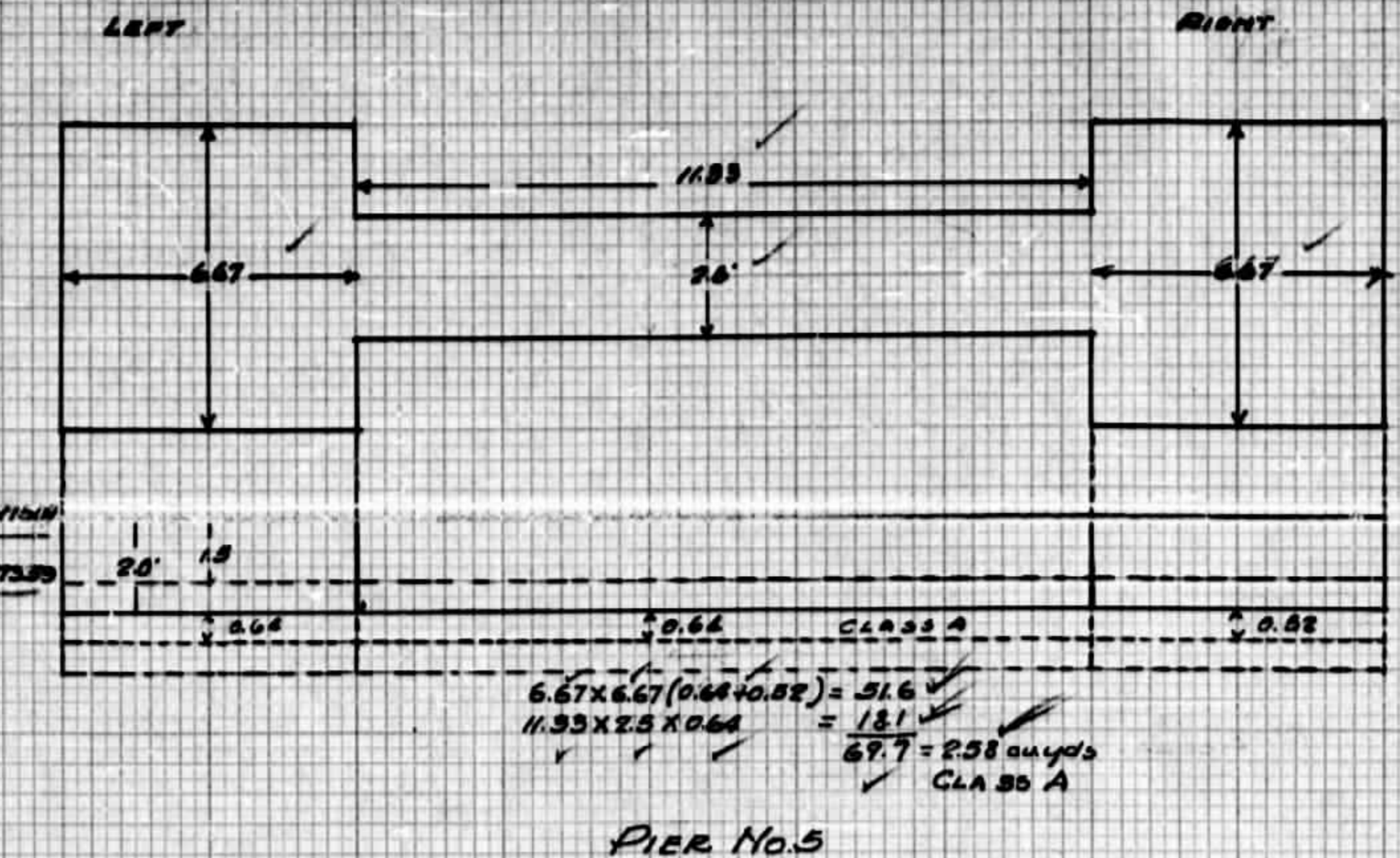
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SURVEY
TEMPLATE
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CHECKED

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ORIGINAL SURVEY
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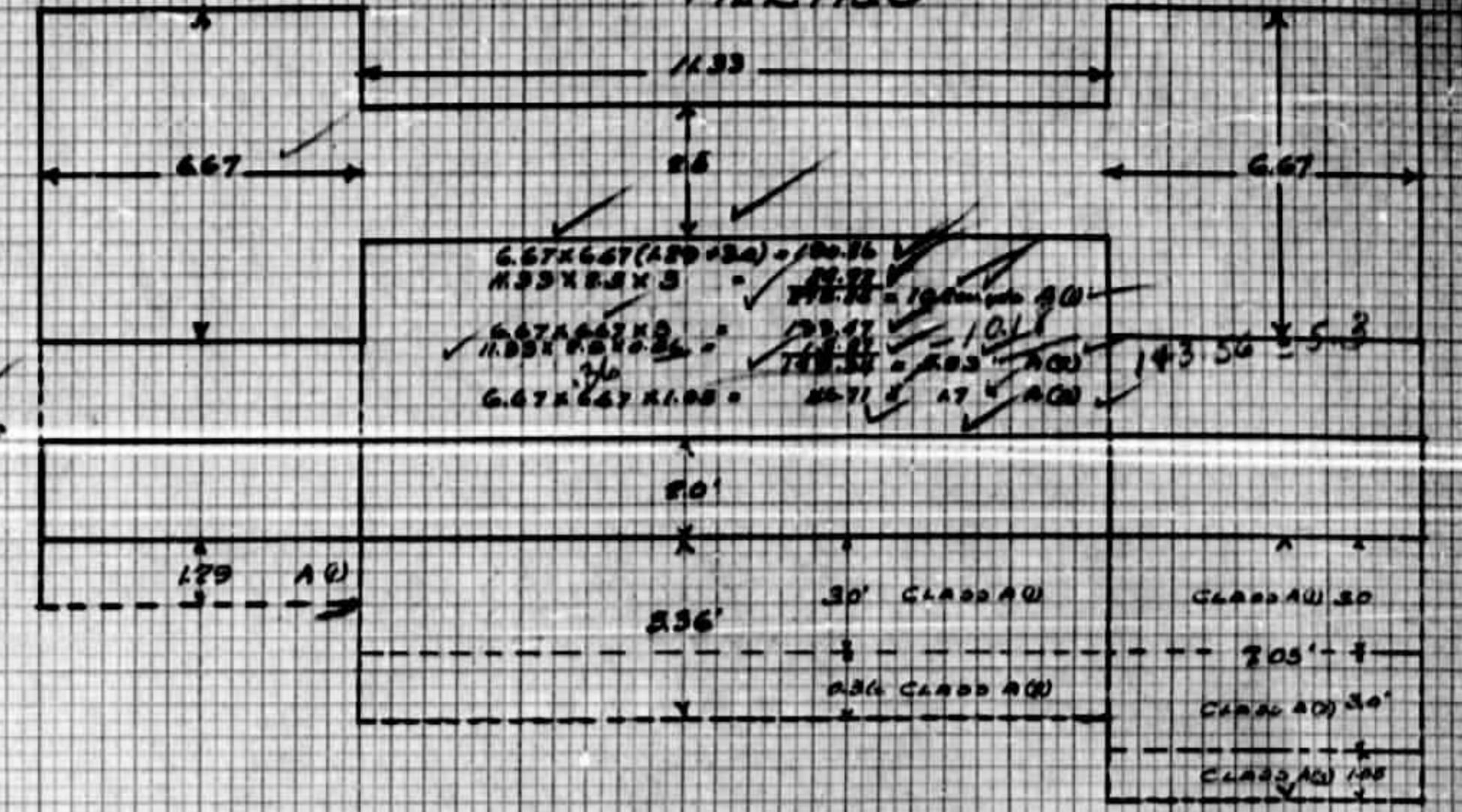
PIER No. 1



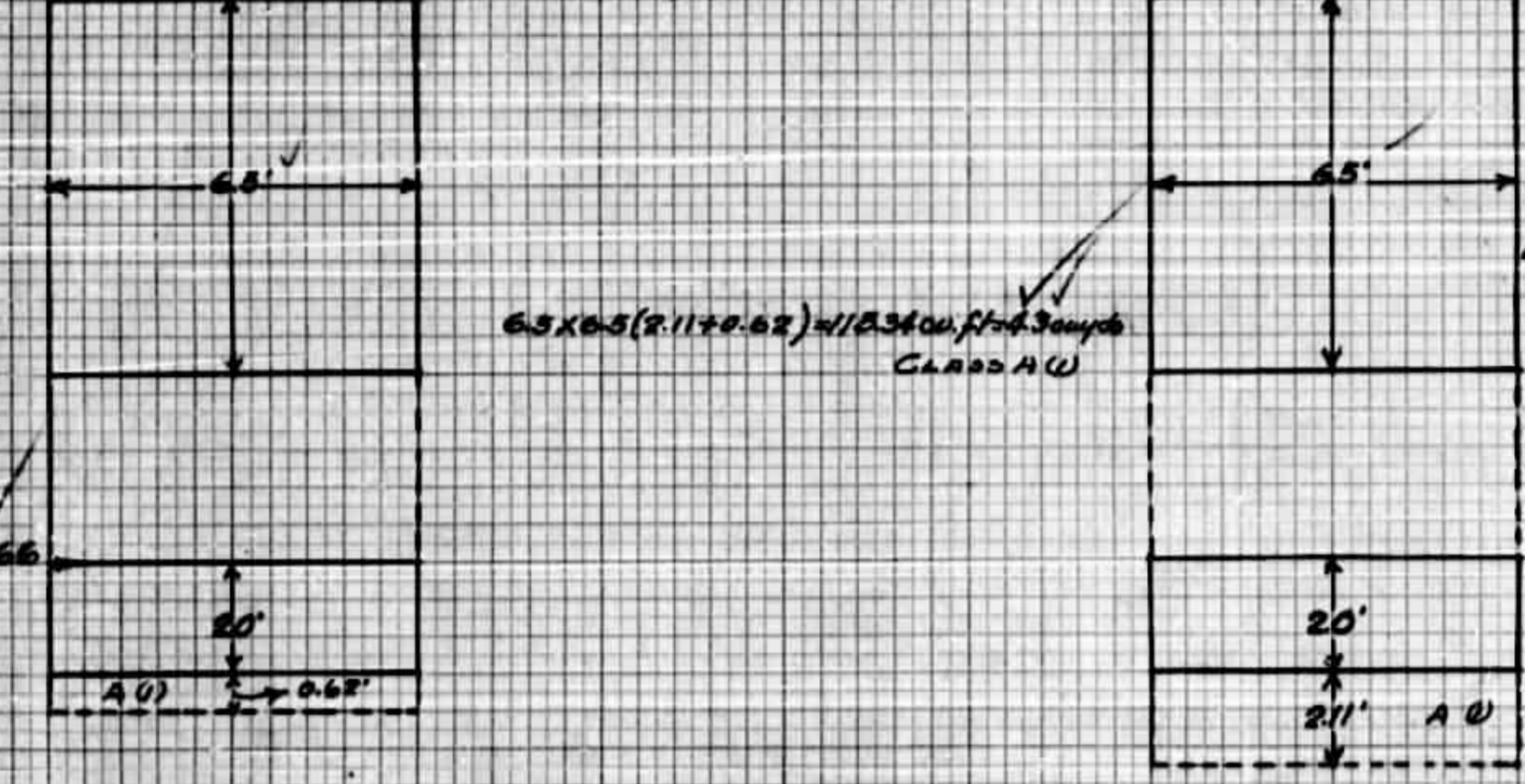
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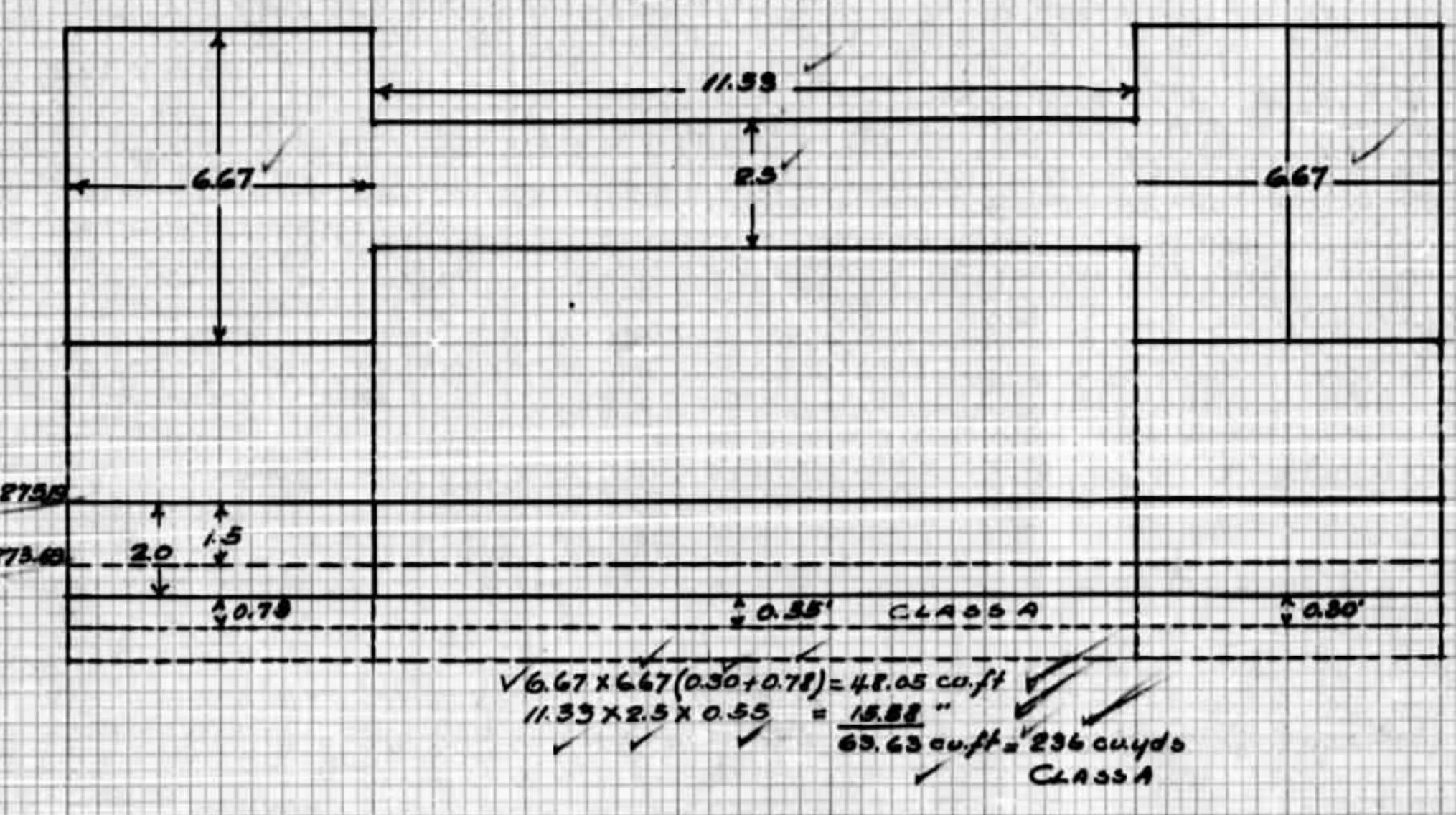
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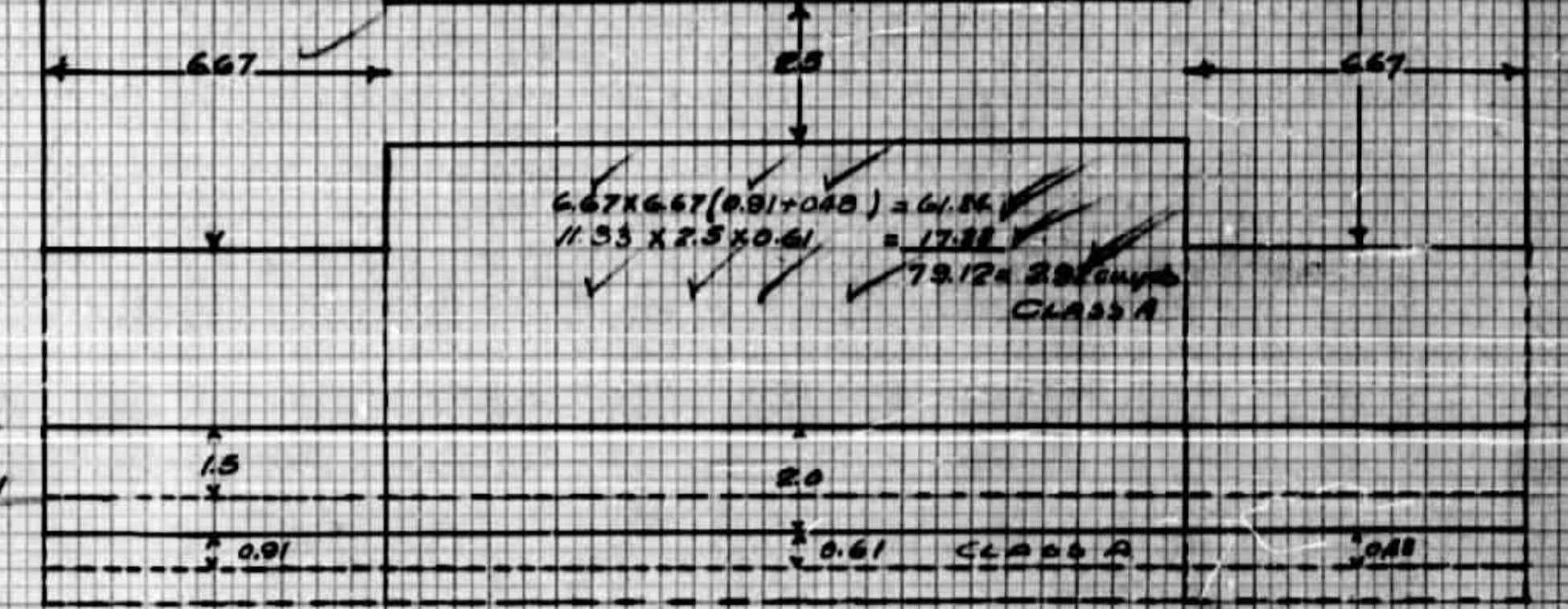
PIER No. 2



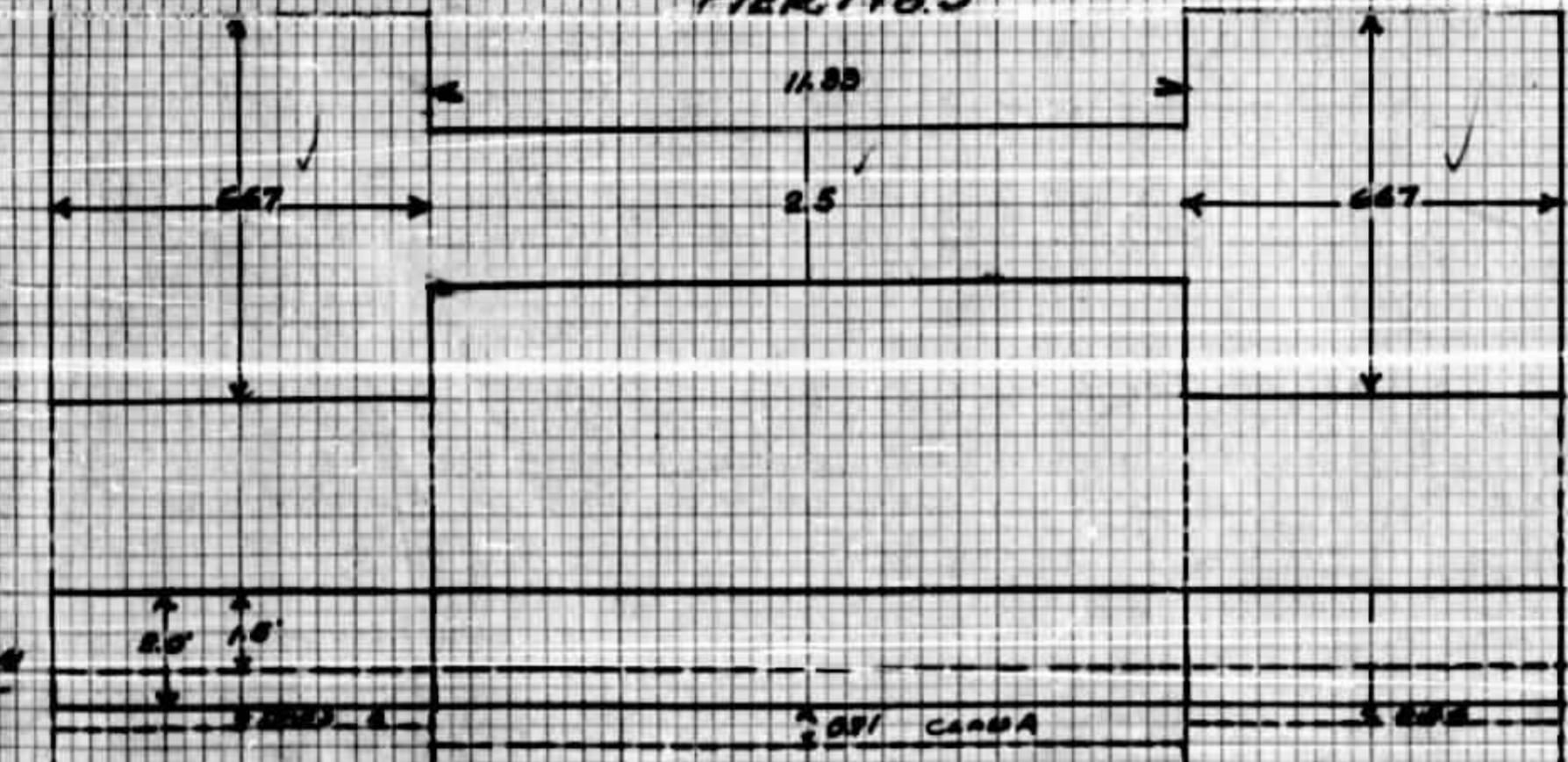
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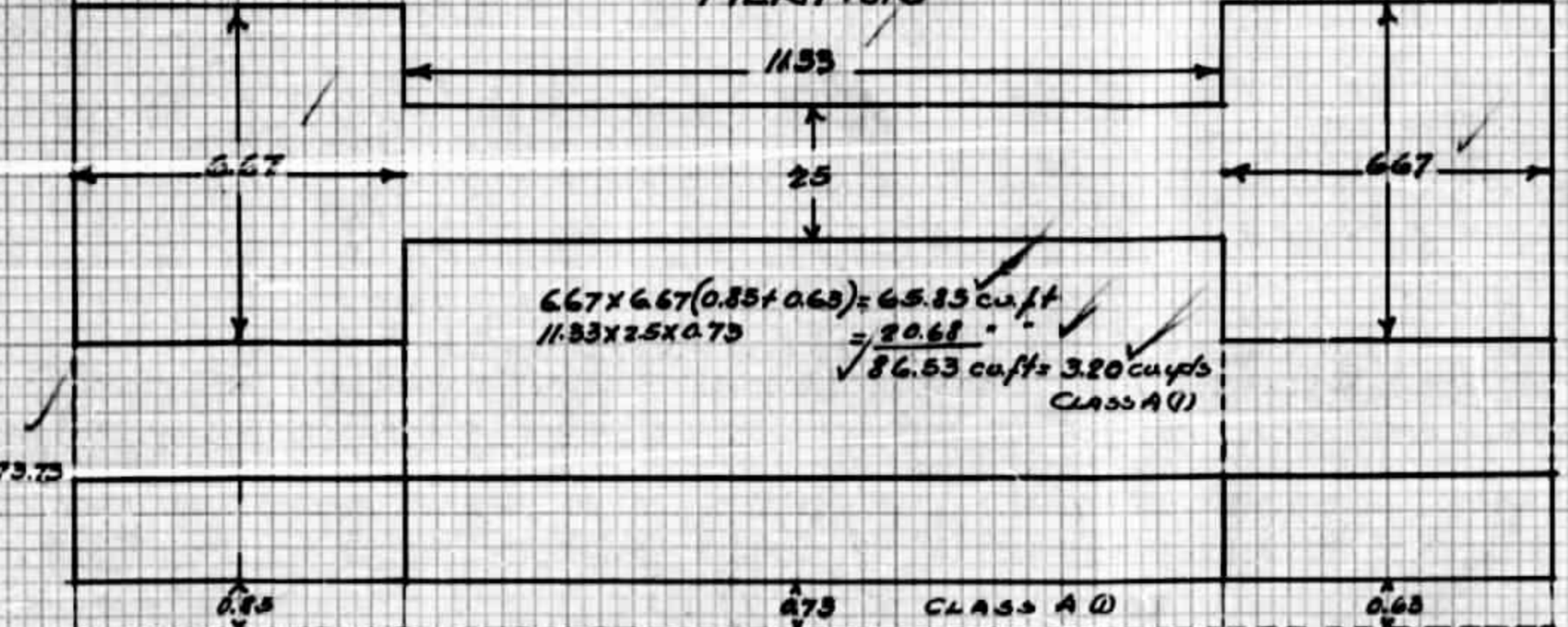
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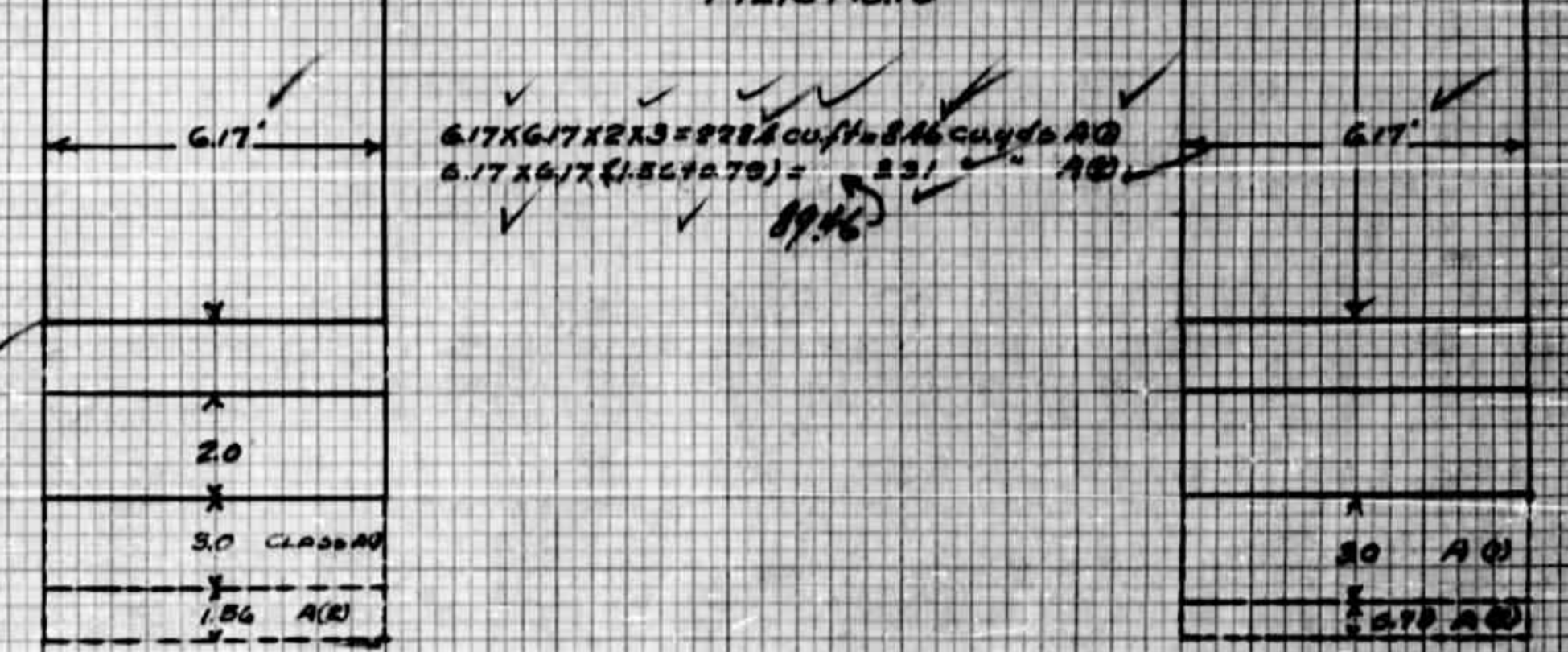
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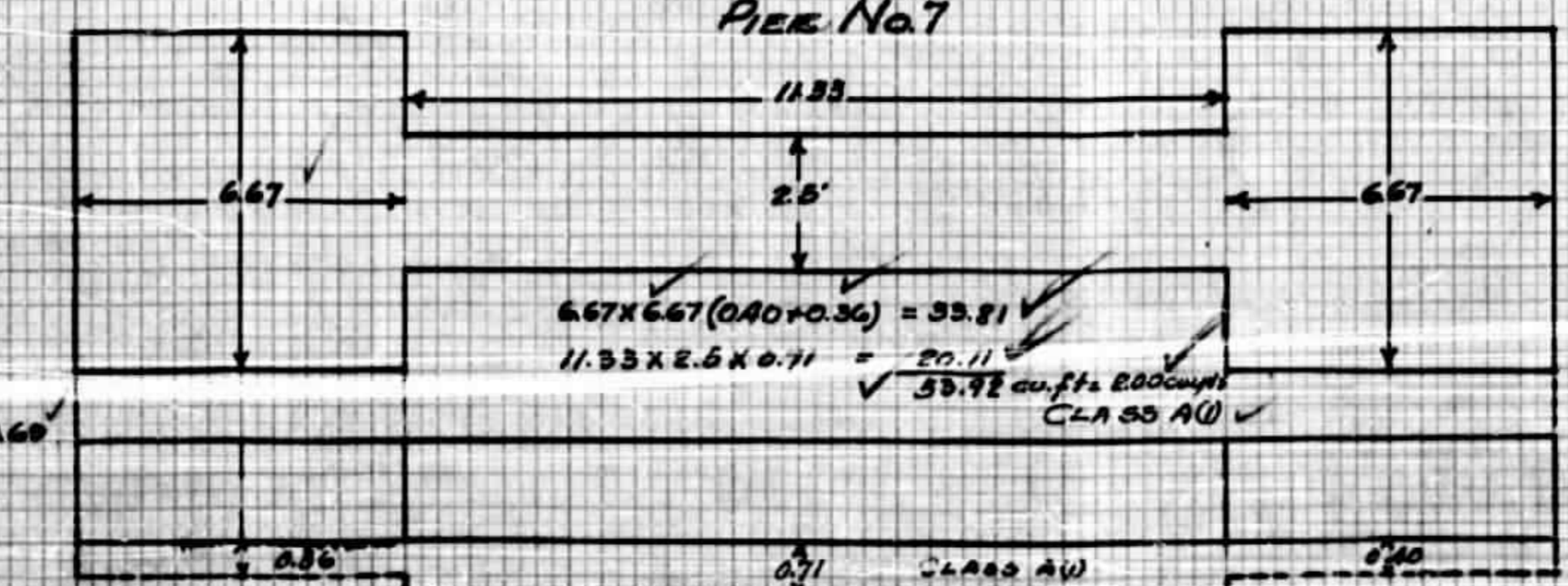
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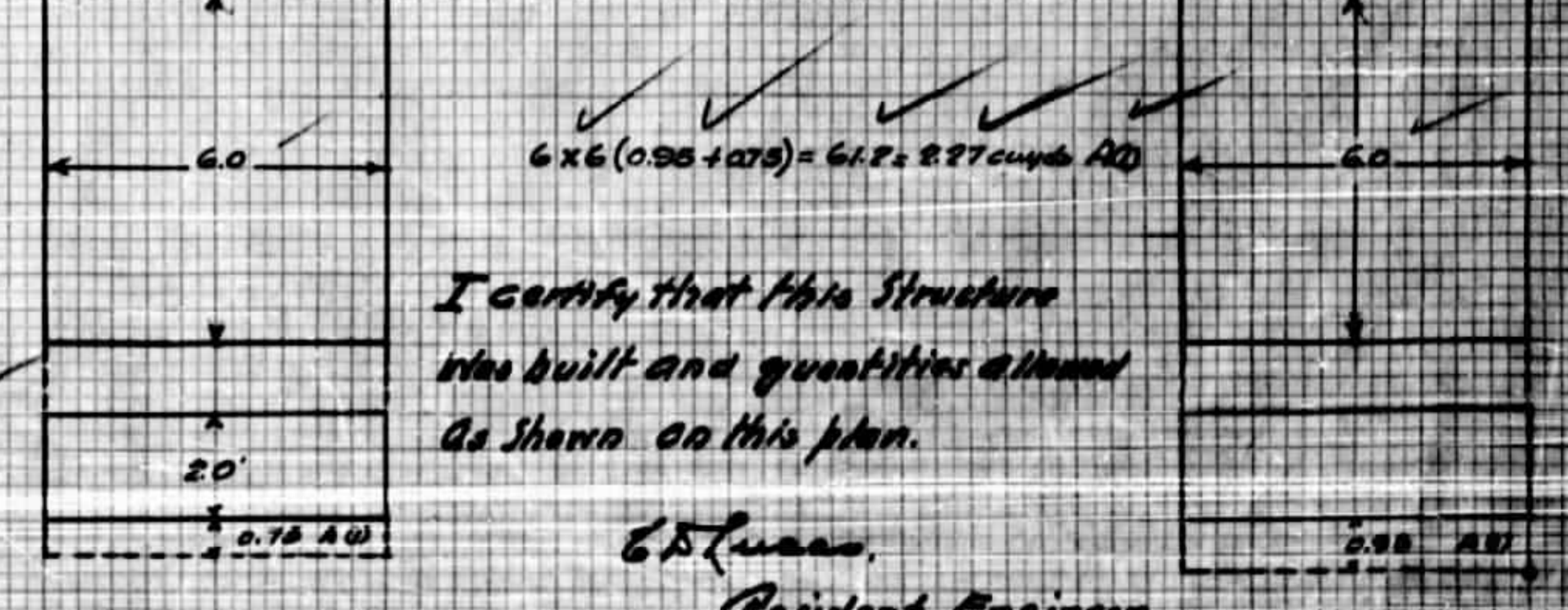
PIER No. 10



PIER No. 7



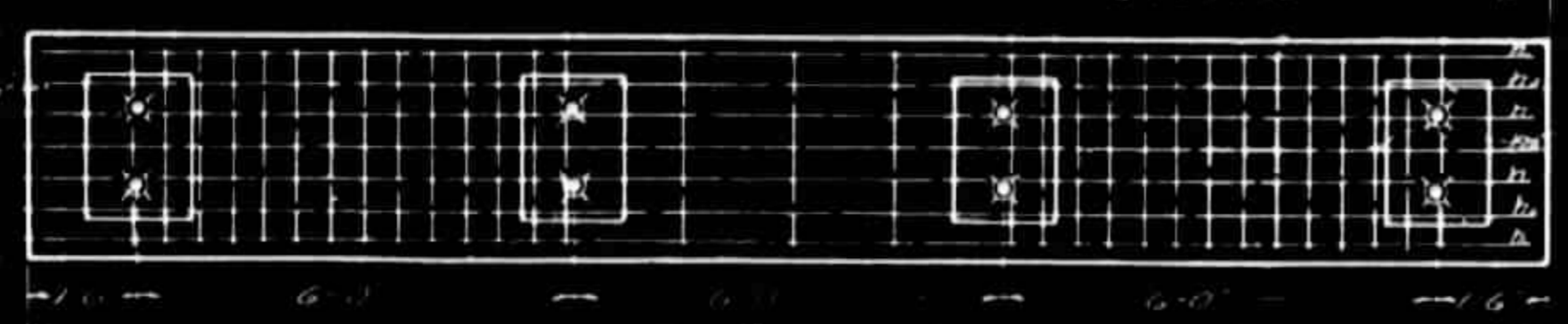
PIER No. 11



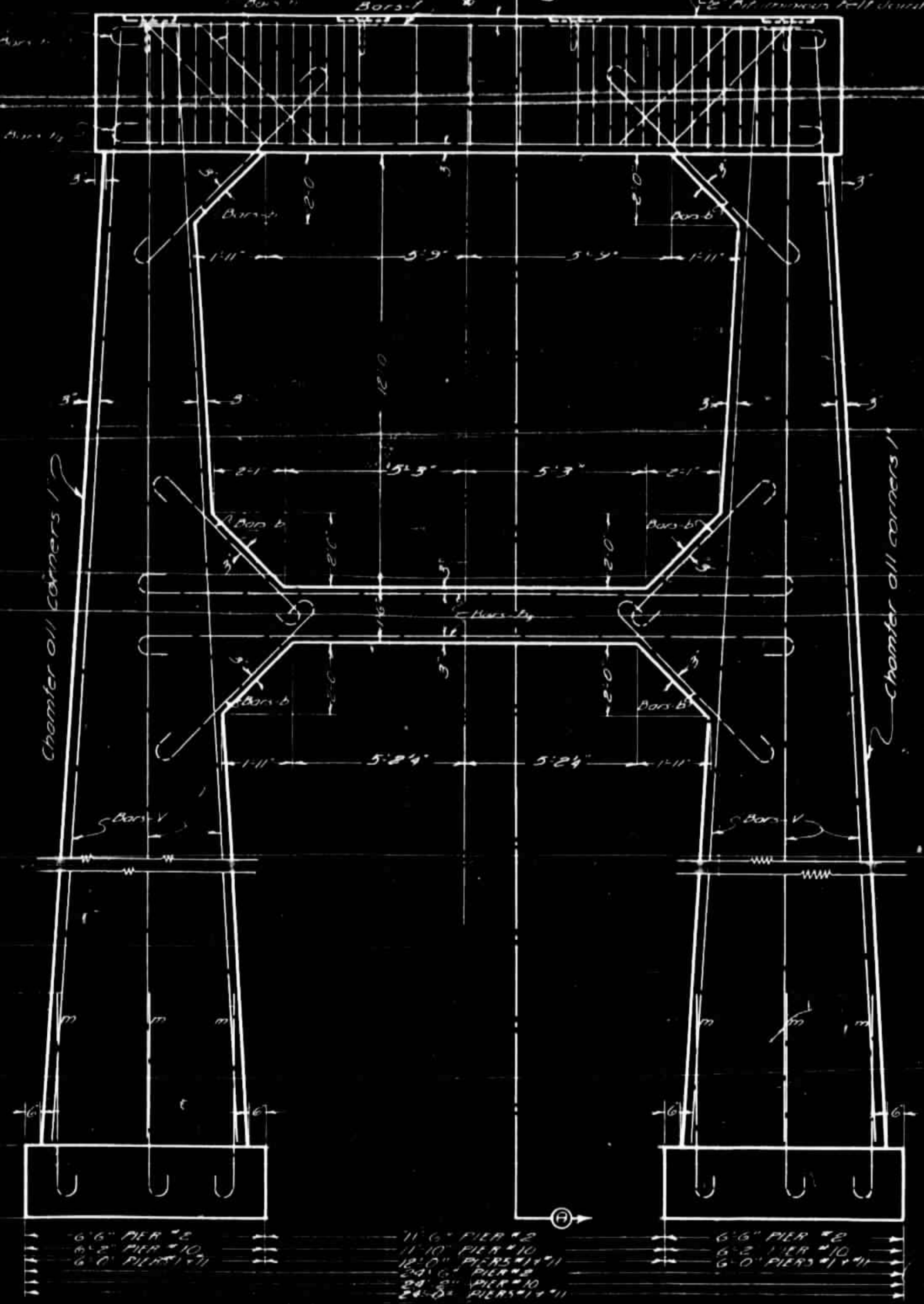
PIER NO.	CLASS	QTY	AD	AD	AD
PIER No. 1	CLASS A	2.18	AD	AD	AD
No. 2	CLASS A	1.18	AD	AD	AD
No. 3	CLASS A	0.01	AD	AD	AD
No. 4	CLASS A	0.47	AD	AD	AD
No. 5	CLASS A	0.86	AD	AD	AD
No. 6	CLASS A	0.86	AD	AD	AD
No. 7	CLASS A	0.86	AD	AD	AD
No. 8	CLASS A	0.47	AD	AD	AD
No. 9	CLASS A	0.47	AD	AD	AD
No. 10	CLASS A	0.47	AD	AD	AD
No. 11	CLASS A	0.47	AD	AD	AD
TOTAL		11.53	AD	AD	AD

I certify that this structure was built and quantities allowed as shown on this plan.

G. S. Lucas
Resident Engineer



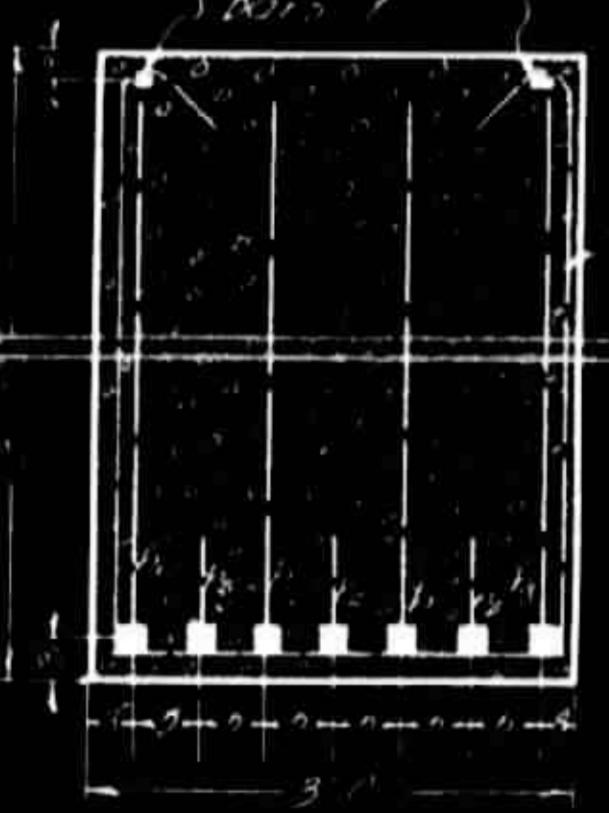
PLAN OF COPING



ELEVATION OF PIERS 1, 2, 10 or 11



SECTION A-A



DETAIL OF COPING

DETAIL OF BARS-m



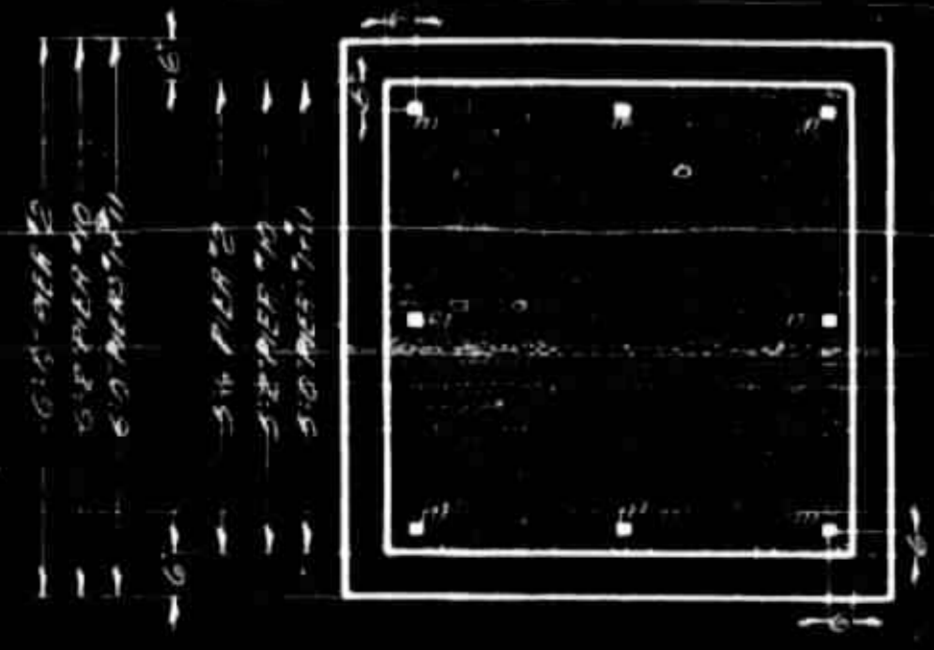
DETAIL OF BARS h-h



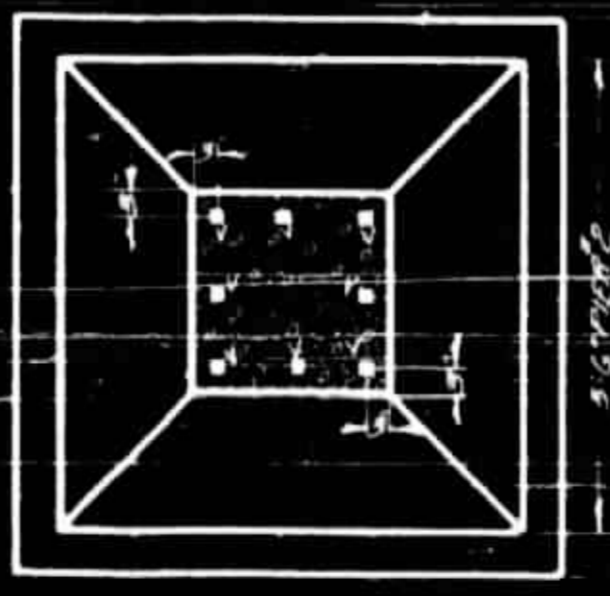
DETAIL OF BARS -S

GENERAL NOTE

Class A concrete to be used throughout. Proportions 1:2:4
 Maximum size aggregate 1 1/2"
 All reinforcing steel shall be deformed bars.
 Square twisted bars are not to be used unless detailed bars.
 All dimensions relative to main reinforcement unless otherwise
 of bars.
 Foundations to be correct as shown on plan and section with
 withstanding in thickness of footing.
 No rebar to be used unless approved by the Engineer.
 A.C.I. Code of Practice 318-29.



SHOWING FOOTING



SHOWING POST

PLAN

REINFORCEMENT

PIER #1 or #11

Bar No.	Size	Length	No.
B	2"	10'	131
V	1/2"	51'3"	2628
H	2"	25'6"	271
H	2"	25'6"	271
H	3"	22'6"	359
H	4"	20'6"	206
H	1/2"	6'6"	553
S	1/2"	17'6"	340
F	2"	19'0"	32

Reinforcing Steel 100# 8901
 Concrete Class A 12.50
 Piers 11 106
 Size per ft depth of concrete 11.13

PIER #2

Bar No.	Size	Length	No.
B	2"	10'	131
V	1/2"	57'0"	3145
H	2"	25'6"	271
H	2"	25'6"	271
H	3"	22'6"	359
H	4"	20'6"	206
H	1/2"	6'6"	553
S	1/2"	17'6"	340
F	2"	19'0"	32

Reinforcing Steel 100# 5840
 Concrete Class A 12.50
 Piers 2 106
 Size per ft depth of concrete 11.13

PIER #10

Bar No.	Size	Length	No.
B	2"	10'	131
V	1/2"	43'6"	2366
H	2"	25'6"	271
H	2"	25'6"	271
H	3"	22'6"	359
H	4"	20'6"	206
H	1/2"	6'6"	553
S	1/2"	17'6"	340
F	2"	19'0"	32

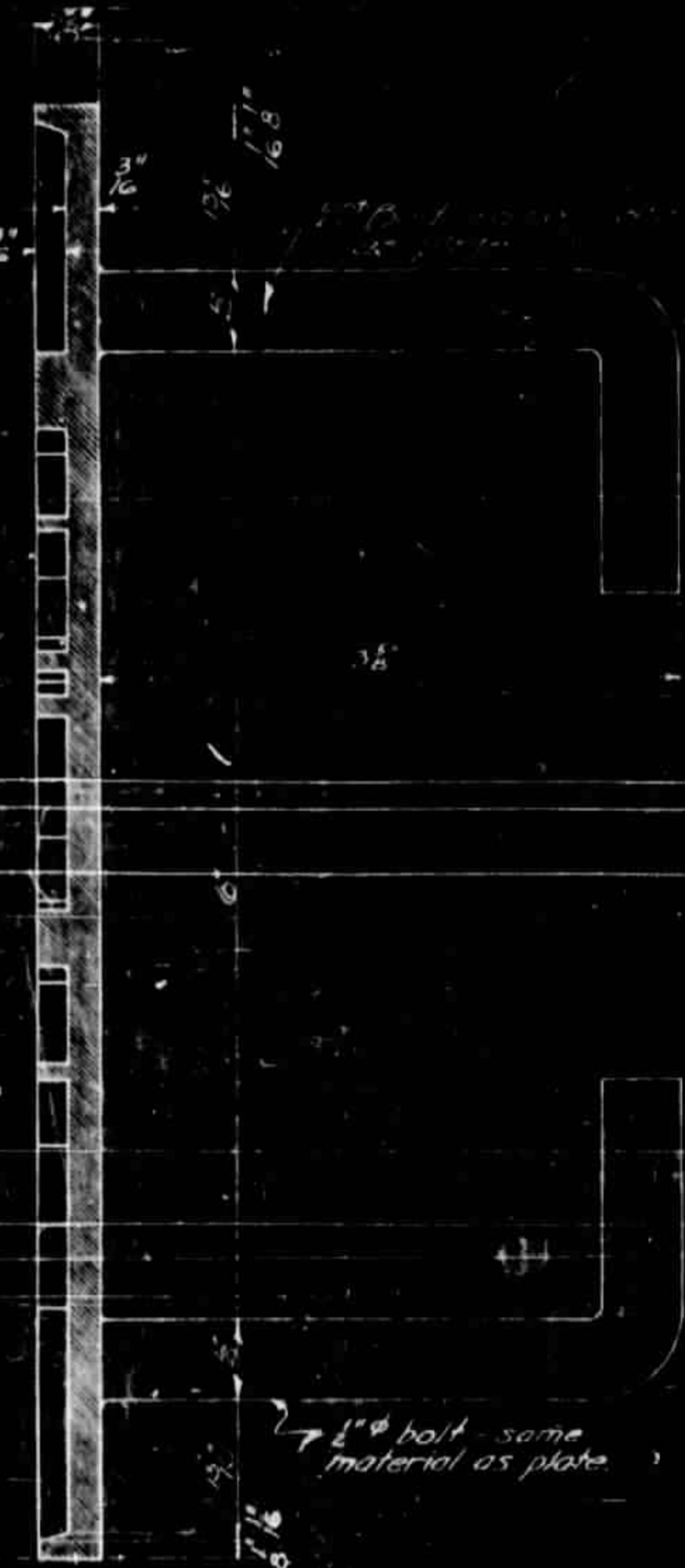
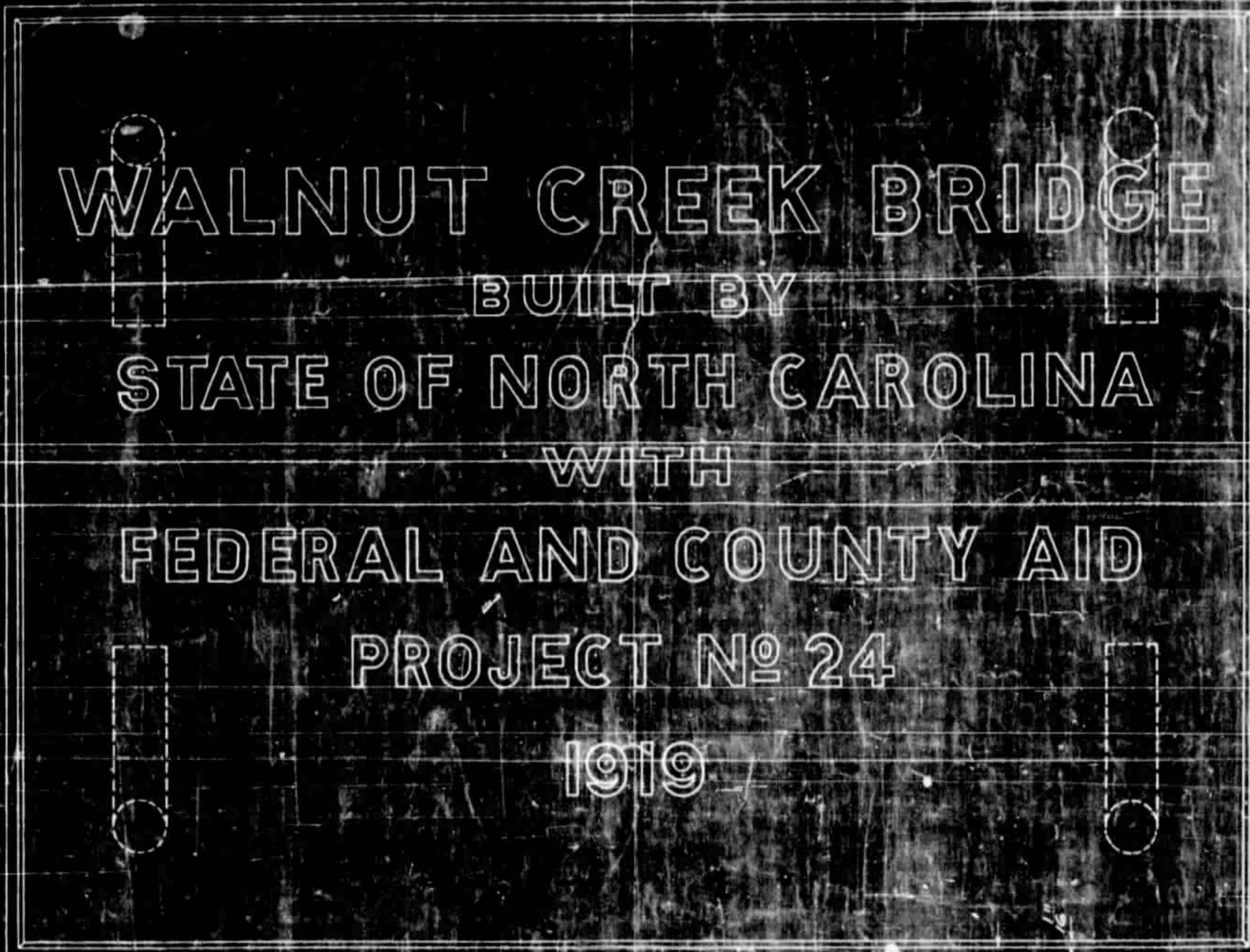
Reinforcing Steel 100# 5623
 Concrete Class A 12.50
 Piers 10 106
 Size per ft depth of concrete 11.13

STATE OF NORTH CAROLINA
 STATE HIGHWAY COMMISSION
 PIER DETAIL
 ROCKY RIVER BRIDGE
 ANSONY-STANLY COUNTIES
 PIERS 1, 2, 10 & 11
 PROJECT NO. 606
 JAN. 1922 SCALE 3/4"

SPECIAL
 APPROVED BY: [Signature]
 DRAWN BY: [Signature]
 CHECKED BY: [Signature]
 DATE: [Date]

GENERAL NOTE

Two name plates are required for each bridge. One name plate shall appear at each end of the bridge. Plates are to be made of copper or bronze and securely fastened to the ends of through girders or fastened to the hinged posts of steel or T-beam bridges on roadway side. The height of letters shall be 3" and 4" and letters shall be raised as indicated. The year 1919 shall be replaced by the date of the year in which the bridge is begun, and the name of the bridge to appear on the plate shall be determined by the name of the bridge to be constructed. The project number to appear on the plate shall be determined by the number of the project on which the bridge is to be constructed. The border and all lettering shall be raised 1/8" above the face of the plate, and the top surface of the raised border and lettering shall be smooth. All lettering shall be square cut and not tapered.



SECTION ON AA

ROCKY RIVER BRIDGE
ANDOVER STANLY COUNTIES
STATE PROJECT NO. 606
BUILT BY
NORTH CAROLINA
STATE HIGHWAY COMMISSION
1922

DETAIL SHOWING CORRECT LETTERING

Two name plates shall be provided for each bridge at station

STATE OF NORTH CAROLINA
STATE HIGHWAY COMMISSION
PROJECT NO. 606
STANDARD NAME PLATE
FOR CONCRETE BRIDGES
MARCH 1920
W. H. HARRIS

THE FINAL ESTIMATE
N.C. PROJECT No 606

Sheet No	Note Book Page	Amount	Units	Item	Contract Prices	Total Amt
2		16342	Cu. Yds	Class 'A' Concrete	20.25	33106.75
2a		102	" "	" A "	2.85	206.55
2a		38.5	" "	" A(1) "	30.00	1155.00
2a	15.5	187	" "	" A(2) "	35.00	549.50
2a		17	" "	" A(3) "	40.00	68.00
2		232239	Pounds	Reinforcing Steel	0.05 1/2	12773.15
2		4074	"	Plates & Bolts	0.20	814.80
2		8153	Sq. Yds	Asphalt Wear Surf	1.50	1223.55
2 & 36		458.9	" "	Rip Rap	2.50	1147.25
2	1005	1005	Lin. ft.	Wooden Guard Rail	1.50	1507.50
7		2	Nº	Name Plates	1500	30.00
					Total Contract	\$52570.03

Extra Work—
 For Demurrage on 5 Cars of Sand Which were held up by the Engineering Department and which afterwards passed the tests of the Laboratory at Raleigh. As per Extra Work Order No 6-34 ✓ 220.00 ✓

To Reimburse the Contractor (Concrete Steel Bridge Co) for the extra expense in importing Coarse Aggregate for Concrete due to the fact that the Contractor was informed in writing of the unquestionable approval of certain Quarries and later it being found necessary to import stone.

As per Extra Work Order No 6-47 ✓
 Total - 3000.00 ✓
~~55802.03~~
~~52790.03~~

Deduct from Final Est. Reduction in Freight on car of Asphalt from Louisville Ky. to Harwood, N.C.
 Freight 350.03 ÷ 90 = 388.98 ✓
 1000 350.03 ✓
 58.90 ✓

less 38.90 ✓ less 38.90 ✓ 55763.13

Previous Payments 52659.76 ✓
 Amount Due ~~52001.37~~
 \$3103.37 ✓

I hereby certify that I have checked this estimate and that it is true and correct according to my best knowledge and belief.
 Signed *C. D. Evans, Jr.*
 District Office Engineer.

I hereby Certify that I have Checked this estimate and that it is true and correct according to my best knowledge and belief.
 Signed: *G. D. Lucas*
 Resident Engineer

I hereby certify that I have examined this estimate and that it is true and correct according to my best knowledge and belief.
 Signed *J. D. Miller*
 District Construction Engineer.

I hereby Certify that I have Examined this estimate and that it is true and correct according to my best knowledge and belief.
 Signed: *J. D. Miller*
 District Engineer

DATE _____ BY _____
 SURVEYED SURVEY PLOTTED
 TEMPLATE AREA
 NOTE BOOK AREA CHECKED

DATE _____ BY _____
 SURVEYED SURVEY PLOTTED
 TEMPLATE AREA
 NOTE BOOK AREA CHECKED