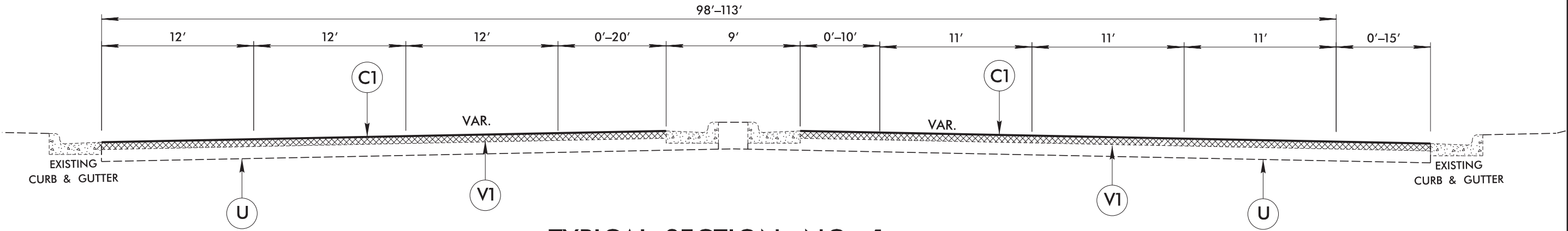


8/17/99

* "PATCHING EXISTING PAVEMENT" HAS BEEN INCLUDED AS A PAY ITEM ON MAPS
 PATCHING MAY OR MAY NOT BE NEEDED, DEPENDING ON THE CONDITION OF THE
 MAP SURFACE AT THE TIME PAVING SESSION BEGINS. AREAS TO BE DELINEATED BY THE ENGINEER.

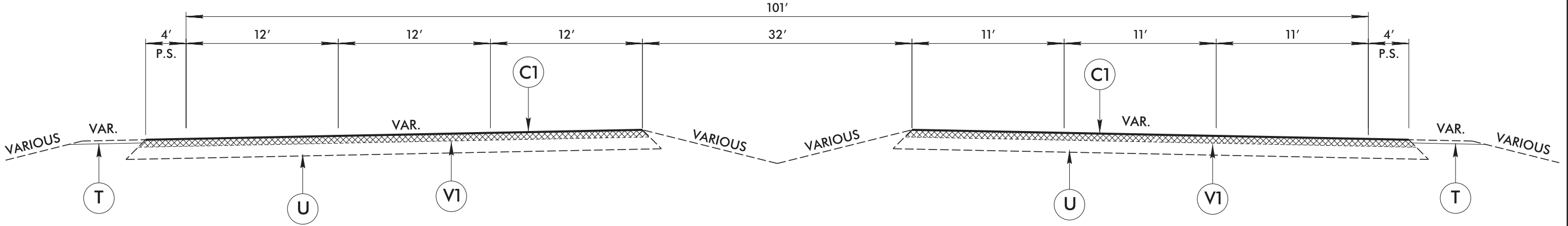
* MILL AND REPLACE
 * SIGNAL DEVICES TO BE INSTALLED PRIOR TO FINAL RESURFACING.



TYPICAL SECTION NO. 1

MAP 1 - US 70 FROM WOMAN'S CLUB DR. TO ROUNDABOUT ACCESS - 430'

* MILL AND REPLACE
 * SIGNAL DEVICES TO BE INSTALLED PRIOR TO FINAL RESURFACING.



TYPICAL SECTION NO. 2

MAP 1 - US 70 FROM ROUNDABOUT ACCESS TO
 I-440 EB OFF RAMP - 850'

EXISTING
 CURB & GUTTER
 400'

NOTES: SEE SHEET 17 FOR DETAILS OF:

- PATCHING EXISTING PAVEMENT
- UTILIZE INCIDENTAL MILLING TO MAKE PAVEMENT TIE-INS
- DETAIL OF PROJECT LIMITS AT SIGNALIZED Y LINES
- DETAIL OF PROJECT LIMITS AT UNSIGNALIZED Y LINES

PAVEMENT SCHEDULE			
C1	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.	U	EXISTING PAVEMENT
T	SHOULDER GRADING	V1	MILLING OF EXISTING ASPHALT PAVEMENT AT DEPTH OF 1½"