

NOTES:

FOR PILES, SEE SECTION 450 OF THE STANDARD SPECIFICATIONS.

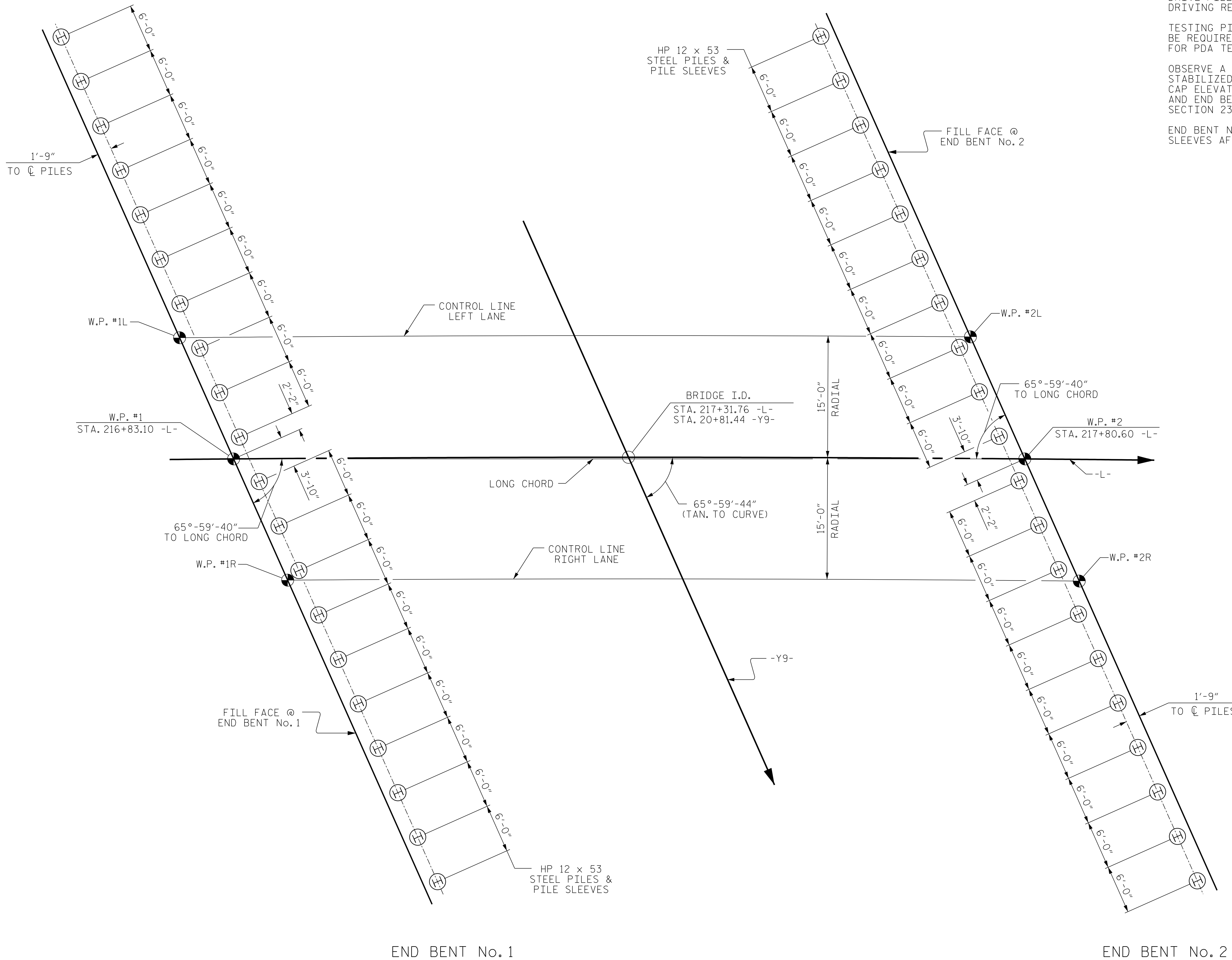
PILES AT END BENT No.1 AND END BENT No.2 ARE DESIGNED FOR A FACTORED RESISTANCE OF 85 TONS PER PILE.

DRIVE PILES AT END BENT No.1 AND END BENT No.2 TO A REQUIRED DRIVING RESISTANCE OF 145 TONS PER PILE.

TESTING PILES WITH THE PDA DURING DRIVING, RESTRIKING OR REDRIVING MAY BE REQUIRED. THE ENGINEER WILL DETERMINE THE NEED FOR PDA TESTING. FOR PDA TESTING, SEE SECTION 450 OF THE STANDARD SPECIFICATIONS.

OBSERVE A TWO MONTH WAITING PERIOD AFTER CONSTRUCTING THE MECHANICALLY STABILIZED EARTH (MSE) ABUTMENT WALL TO WITHIN 1 FT OF THE BOTTOM OF CAP ELEVATION BEFORE BEGINNING END BENT CONSTRUCTION AT END BENT No.1 AND END BENT No.2. FOR BRIDGE WAITING PERIODS, SEE ROADWAY PLANS AND SECTION 235 OF THE STANDARD SPECIFICATIONS.

END BENT No.1 AND END BENT No.2 PILES WILL BE DRIVEN THROUGH PILE SLEEVES AFTER THE WAITING PERIOD.



END BENT No. 1

END BENT No. 2

FOUNDATION LAYOUT

PROJECT NO. W-5600

JOHNSTON COUNTY

STATION: 217+31.76 -L-

SHEET 2 OF 4



STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH
GENERAL DRAWING
FOR BRIDGE
ON US 70 OVER WILSON'S
MILL RD. (SR 1913) BETWEEN
SR 1501 AND SR 1915

| REVISIONS | | | | | | SHEET NO. |
|-----------|-----|-------|-----|-----|-------|--------------|
| NO. | BY: | DATE: | NO. | BY: | DATE: | S1-2 |
| 1 | | | 3 | | | TOTAL SHEETS |
| 2 | | | 4 | | | 27 |

DOCUMENT NOT CONSIDERED FINAL
UNLESS ALL SIGNATURES COMPLETED

DRAWN BY: D. HODGE DATE: 2/18
CHECKED BY: B.C. HUNT DATE: 6/18

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